

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	1	423



STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

**PROJECT PLANS FOR CONSTRUCTION ON
STATE HIGHWAY
IN STANISLAUS COUNTY
AT SALIDA
ON ROUTE 99
FROM 0.6 MILE SOUTH TO 0.4 MILE NORTH OF ROUTE 219
AND ON ROUTE 219 (KIERNAN AVENUE)
FROM 0.1 MILE WEST TO 0.5 MILE EAST OF ROUTE 99**

TO BE SUPPLEMENTED BY STANDARD PLANS DATED 2010

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APPROVED AS TO IMPACT ON STATE FACILITIES AND CONFORMANCE WITH APPLICABLE STATE STANDARDS AND PRACTICES AND THAT TECHNICAL OVERSIGHT WAS PERFORMED.

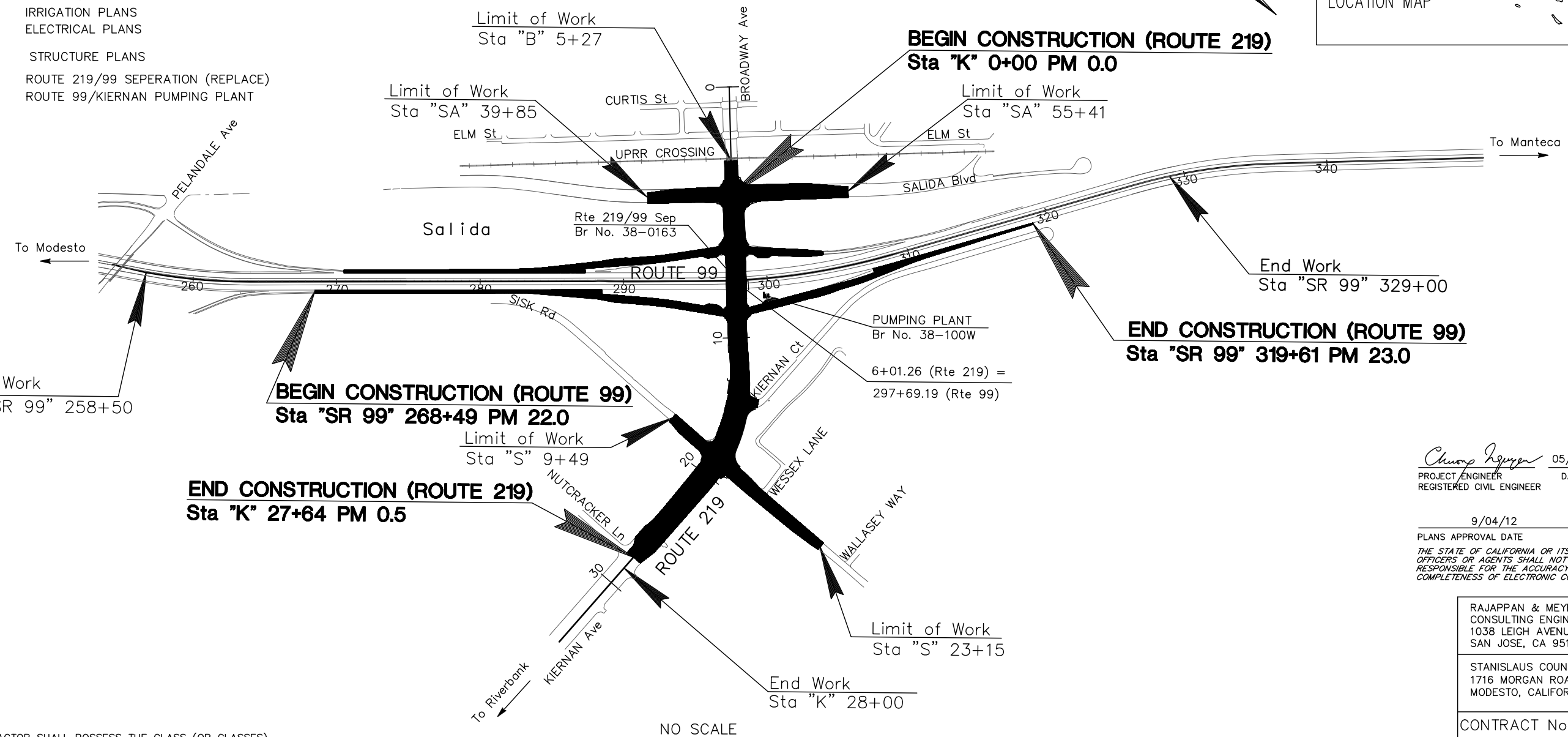
DATE SIGNED 9/13/12

LICENSE Exp DATE 12/31/12

REGISTRATION No. 52350

CALTRANS DESIGN OVERSIGHT APPROVAL JOSE HUERTA

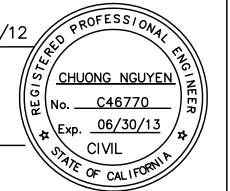
CONSULTANT DESIGN ENGINEER CHUONG NGUYEN



Chuong Nguyen 05/06/12
PROJECT ENGINEER DATE
REGISTERED CIVIL ENGINEER

9/04/12
PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.



RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CA 95126	
STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358	
CONTRACT No.	EA 10-0L3304
PROJECT ID	10 0000 0100
PROJECT NUMBER & PHASE 1000000100	

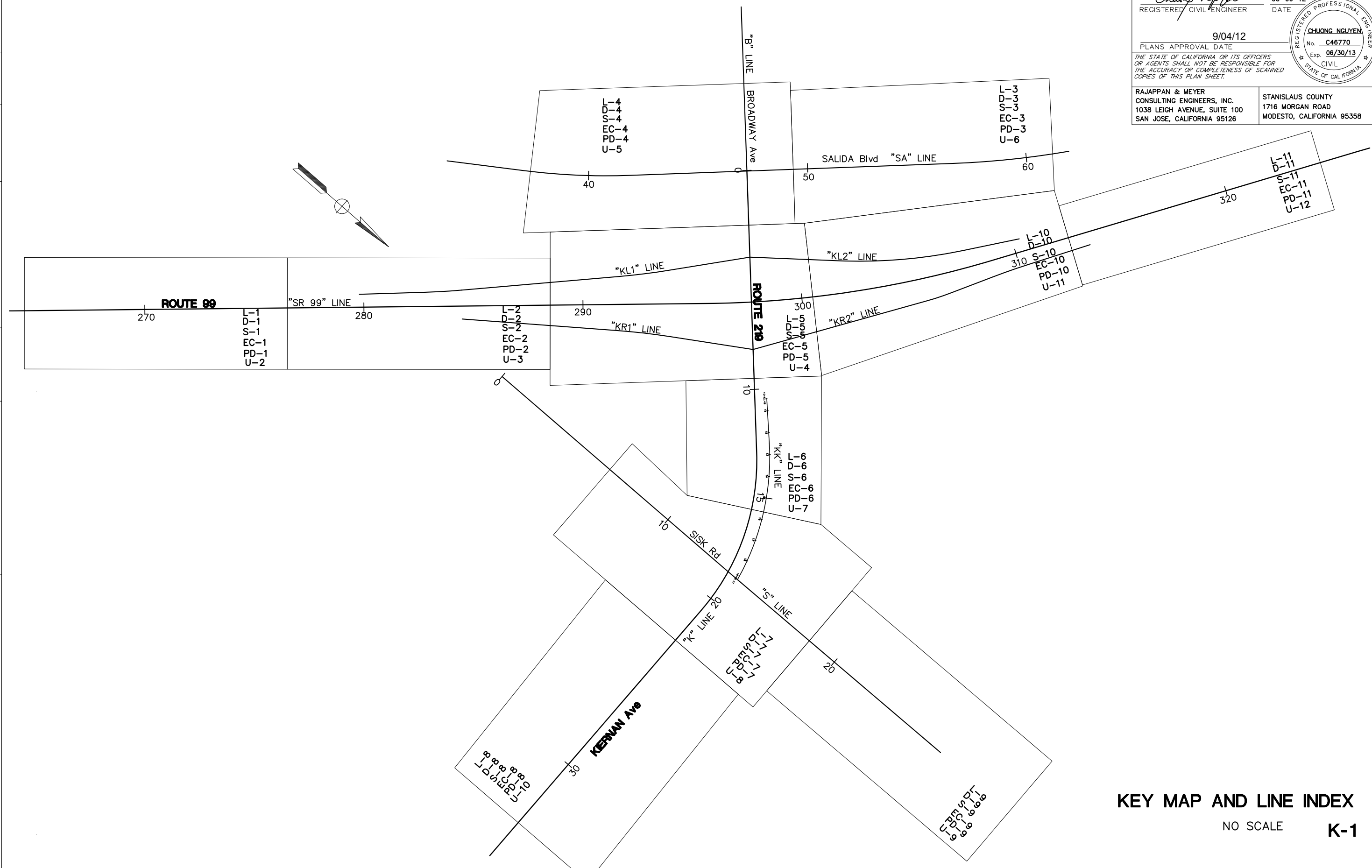
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: SRIKASHMI SAKHAMURI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: (blank)
 DATE REVISED: (blank)

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	2	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: JEROME R JONES
 REVISIONS: REVISOR: JEROME R JONES, DATE: 9/04/12
 CALCULATED/DESIGNED BY: JEROME R JONES, CHECKED BY: JEROME R JONES

NOTES:

- FOR COMPLETE PROJECT CONTROL DATA, SEE THE SURVEY RECORDS ON FILE IN THE SURVEYORS DEPARTMENT AT THE DISTRICT OFFICE.
- BASIS OF BEARINGS AND COORDINATES:
 THIS PROJECT IS BASED ON THE CALIFORNIA COORDINATE SYSTEM 1983, HPGN EPOCH ADJUSTMENT [CCS 83 (2007.00)], ZONE 3, U.S. SURVEY FOOT. 2ND ORDER HORIZONTAL CONTROL STATIONS WERE ESTABLISHED IN OCTOBER 2010 ACCORDING TO CRITERIA SET FORTH IN CHAPTER 6 OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION SURVEY MANUAL.

 THIS GPS STATIC SURVEY WAS CONSTRAINED TO "B" ORDER STATIONS: U 1414 (PID HS4500) AND HPGN D CA 10 GK (PID AA4251) AND "C" ORDER STATION MATTHEW (PID HS0118) PER NGS REFERENCE SYSTEM AND RESULTED IN RESIDUAL ERRORS RANGING FROM 0.01' TO 0.05' HORIZONTALLY AT THE 95% CONFIDENCE LEVEL. THE VALUES REPRESENTED IN THIS SURVEY MEET THE ABOVE-REFERENCED STANDARDS FOR 2ND ORDER HORIZONTAL REQUIREMENTS.

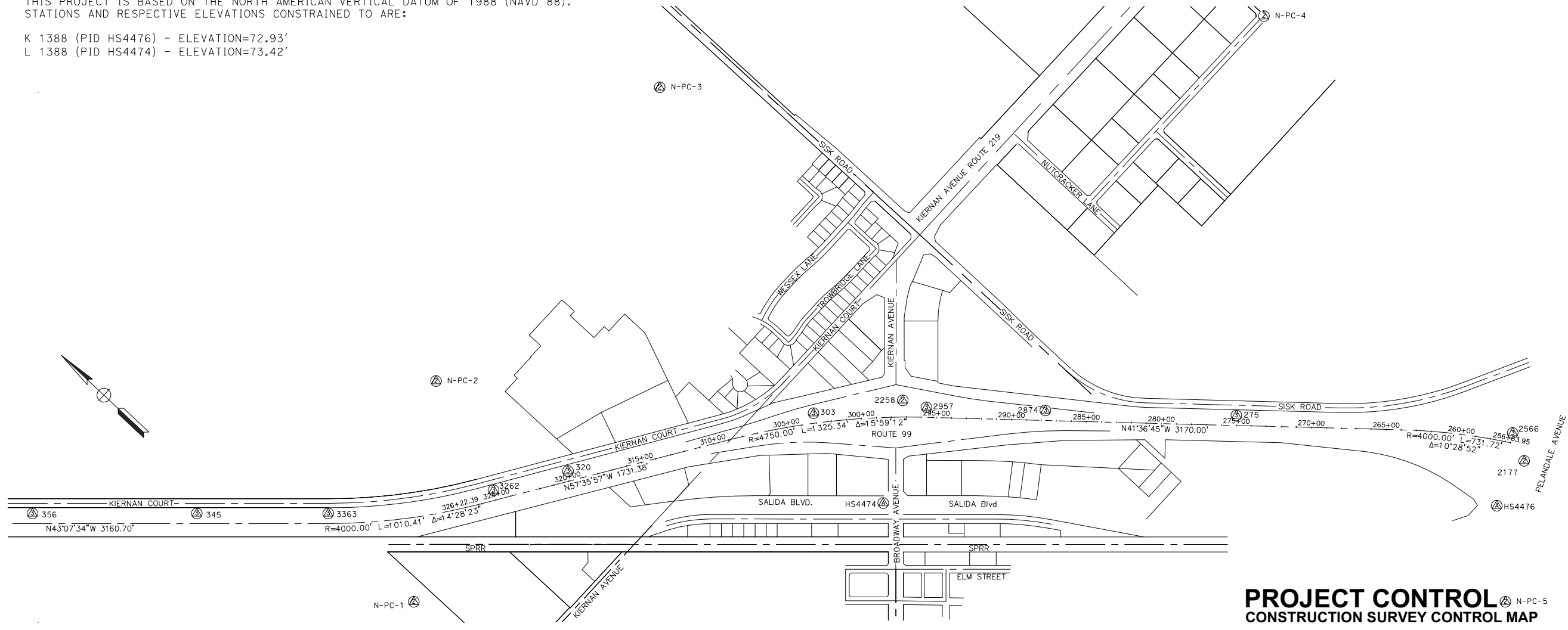
 SUBSEQUENT GPS AND CONVENTIONAL SURVEYS ESTABLISHED THE LISTED 2ND ORDER STATIONS.
- BASIS OF ELEVATIONS:
 THIS PROJECT IS BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88). STATIONS AND RESPECTIVE ELEVATIONS CONSTRAINED TO ARE:
 K 1388 (PID HS4476) - ELEVATION=72.93'
 L 1388 (PID HS4474) - ELEVATION=73.42'

CONTROL FOR DESIGN AND CONSTRUCTION

POINT #	ORDER	NORTHING	EASTING	Elev	DESCRIPTION
HS4500	B	2043646.05	6424136.00		STAINLESS STEEL ROD ENCASED IN PVC PIPE WITH ACCESS COVER
AA4251	B	2055146.12	6471864.90		SURVEY DISK ENCASED IN PVC PIPE WITH ACCESS COVER
HS0118	C	2144758.08	633794.75		BRONZE DISK STAMPED MATTHEW 1959, IN 10" SQUARE CONCRETE MONUMENT
HS4476	A			72.93	STAINLESS STEEL ROD ENCASED IN PVC PIPE WITH ACCESS COVER
HS4474	A			73.47	STAINLESS STEEL ROD ENCASED IN PVC PIPE WITH ACCESS COVER
275	3	2079491.7650	6395161.6820		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 275R
303	3	2081559.5810	6393245.1550		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 303R
320	3	2082490.8840	6391844.3130		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 320R
345	3	2084103.8450	6389946.2550		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 345R
356	3	2084903.4500	6389196.9790		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 356R
2177	2			93.95	BRASS DISK SET IN TOP OF CURB ON BRIDGE STAMPED CALIF DEPT OF TRANSPORTATION
2258	2			75.96	BRASS DISK SET IN TOP OF CURB ON BRIDGE STAMPED CALIF DEPT OF TRANSPORTATION
2566	3	2078067.5030	6396335.9070		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 256.6R
2874	3	2080444.1470	6394315.3900		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 287.4R
2957	3	2081037.1850	6393788.5200		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 295.7R
3262	3	2082765.6120	6391411.4770		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 326.2R
3363	3	2083464.0230	6390545.3140		2" BRASS DISK SET IN CONCRETE STAMPED CALIF DIV. OF HWY. 336.3R
N-PC-1	2	2082644.8160	6390505.1310	63.15	NOLTE PRIMARY CONTROL POINT FOUND IRON PIPE IN MONUMENT WELL
N-PC-2	2	2083543.2720	6391681.3530	66.97	NOLTE PRIMARY CONTROL POINT FOUND IRON PIPE IN MONUMENT WELL
N-PC-3	2	2083794.2280	6394130.7540	69.41	NOLTE PRIMARY CONTROL POINT FOUND IRON PIPE IN MONUMENT WELL
N-PC-4	2	2081178.9180	6397231.5530	73.76	NOLTE PRIMARY CONTROL POINT FOUND IRON PIPE IN MONUMENT WELL
N-PC-5	2	2077333.1360	6395480.8070	70.58	NOLTE PRIMARY CONTROL POINT FOUND IRON PIPE IN MONUMENT WELL

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10	Sta	99, 219	22.0/23.0, 0.0/0.5	3	423

LICENSED LAND SURVEYOR: *[Signature]*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 NO. L.S. 4922
 Exp. 12/31/12
 STATE OF CALIFORNIA
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



PROJECT CONTROL
CONSTRUCTION SURVEY CONTROL MAP
 NO SCALE
PC-1

APPROVED FOR PROJECT CONTROL INFORMATION ONLY

LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	4	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER DATE 05-18-12		
9/04/12 PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

DESIGN DESIGNATION

ROUTE 99

ADT (2007)	116,000	D	57%
ADT (2035)	165,360	T	13.5
DHV	18,210	V	70MPH
ESAL	106,591,056	TI	15.5
			20

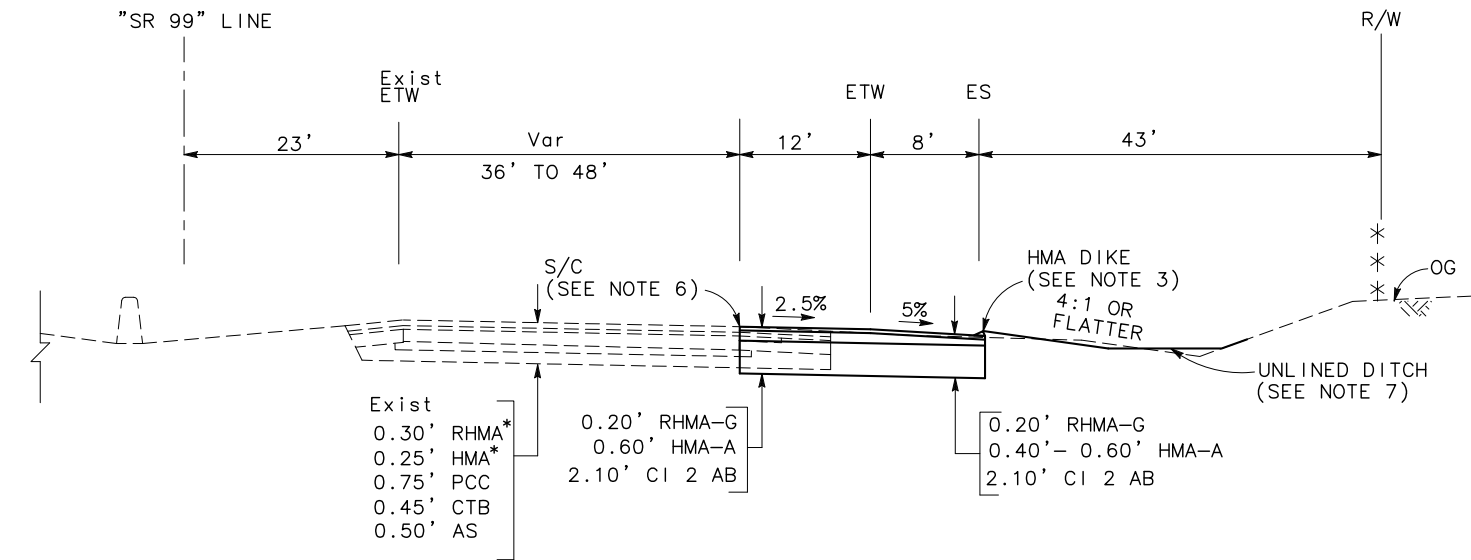
NOTES:

- DIMENSIONS OF THE PAVEMENT STRUCTURAL SECTIONS ARE SUBJECT TO TOLERANCES SPECIFIED IN THE STANDARD SPECIFICATIONS.
- SUPERELEVATION ARE SHOWN ON THE SUPERELEVATION DIAGRAMS.
- FOR LOCATION AND TYPE OF CURBS, DIKES, CONCRETE BARRIER, THRIE BEAM BARRIER, MBGR AND FENCES, SEE LAYOUT PLANS.
- FOR SOUNDWALL LOCATION, SEE SOUNDWALL PLANS.
- FOR RETAINING WALL AND CURB WALL LOCATION, SEE RETAINING WALL PLANS.
- FOR LOCATION OF SAWCUT LINES, SEE LAYOUT PLANS.
- FOR LOCATION AND ELEVATION OF DITCHES, SEE DRAINAGE PLANS.
- EXISTING PAVEMENT SECTIONS BASED ON AS-BUILT PLANS AND CONTRACT No. 10-0M8004 PLANS. ACTUAL SECTIONS MAY VARY.

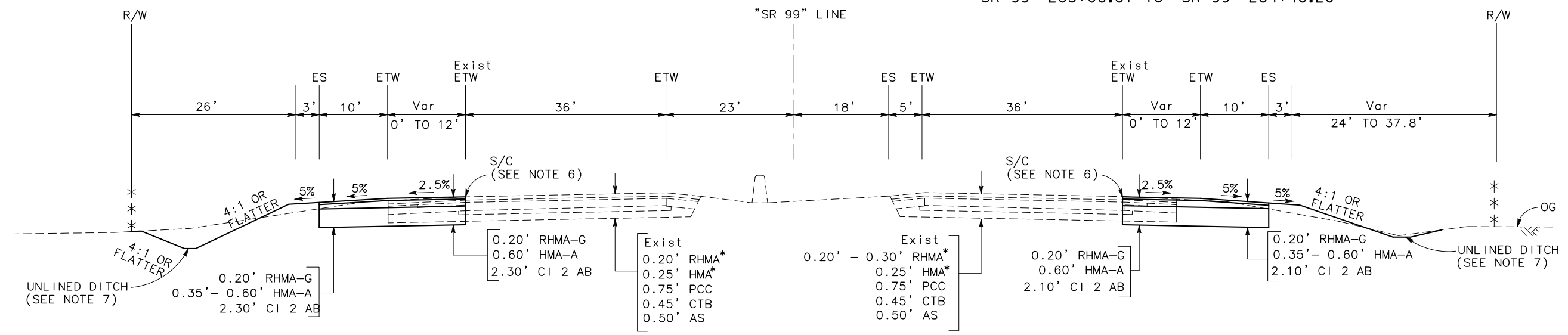
ABBREVIATIONS:

- S/C SAWCUT AND CONFORM
- RAC RUBBERIZED ASPHALT CONCRETE
- HMA-A HOT MIXED ASPHALT (TYPE A)
- RHMA-G RUBBERIZED HOT MIXED ASPHALT (TYPE G)

REVISOR: N MUMMANENT, CHUONG NGUYEN
 CHECKED BY: KEITH G. MEYER
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 DEPARTMENT OF TRANSPORTATION: STATE OF CALIFORNIA
 CALIFTRANS



NORTHBOUND
 "SR 99" 283+06.81 TO "SR 99" 284+48.20



SOUTHBOUND
 "SR 99" 269+97.39 TO "SR 99" 279+80.00

NORTHBOUND
 "SR 99" 268+49.00 TO "SR 99" 283+06.81
 "SR 99" 313+36.00 TO "SR 99" 319+36.12

ROUTE 99

TYPICAL CROSS SECTIONS

NO SCALE X-1

(*) - PER CONTRACT No. 10-0M8004 (BY OTHERS)

LAST REVISION 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

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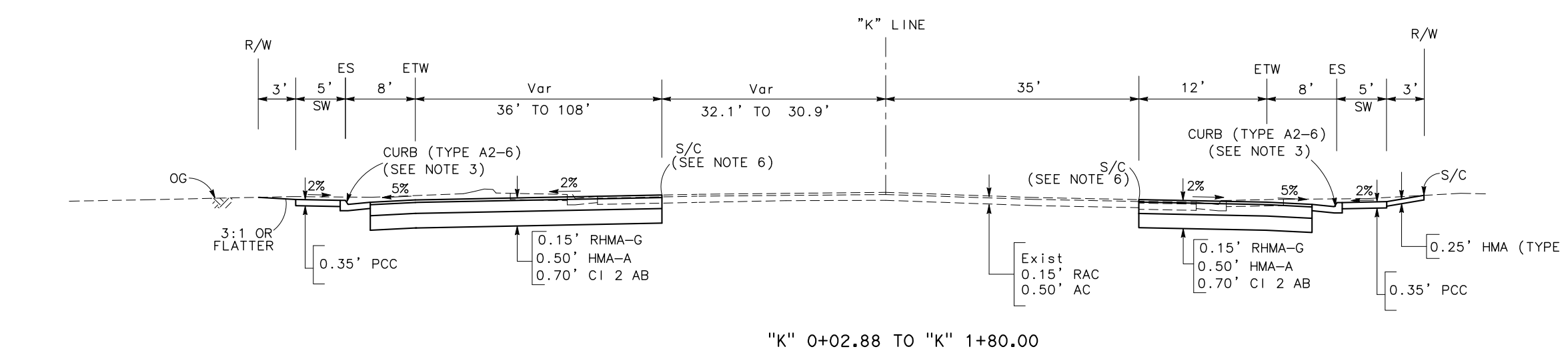
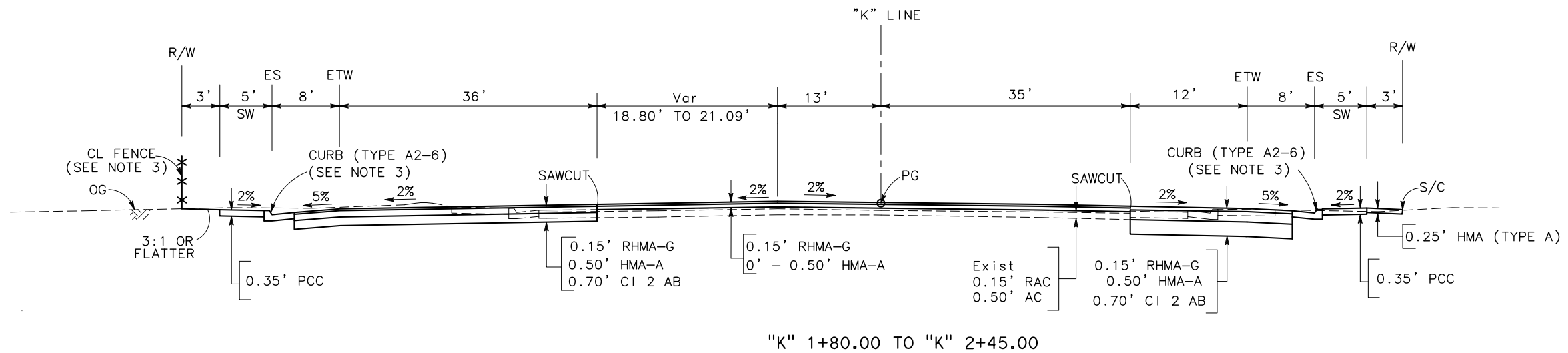
DESIGN DESIGNATION

KIERNAN Ave

ADT (2007)	22,900	D	53%
ADT (2035)	58,400	T	9.6
DHV	4,210	V	45MPH
ESAL	37,644,640	TI	12
			20

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER
 CALCULATED-DESIGNED BY CHECKED BY
 N MUMMANENT CHUONG NGUYEN
 REVISOR BY DATE REVISOR
 CHUONG NGUYEN



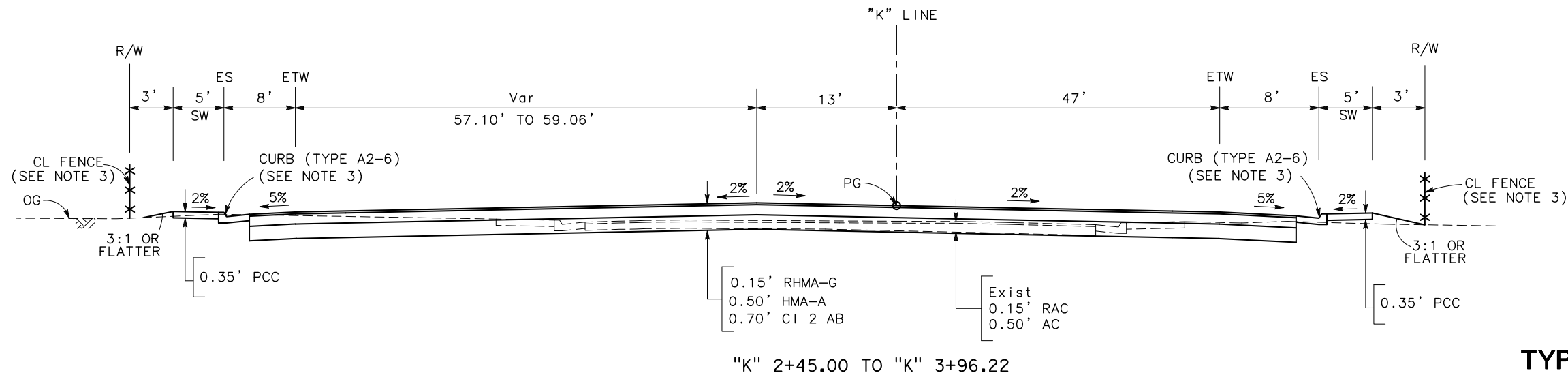
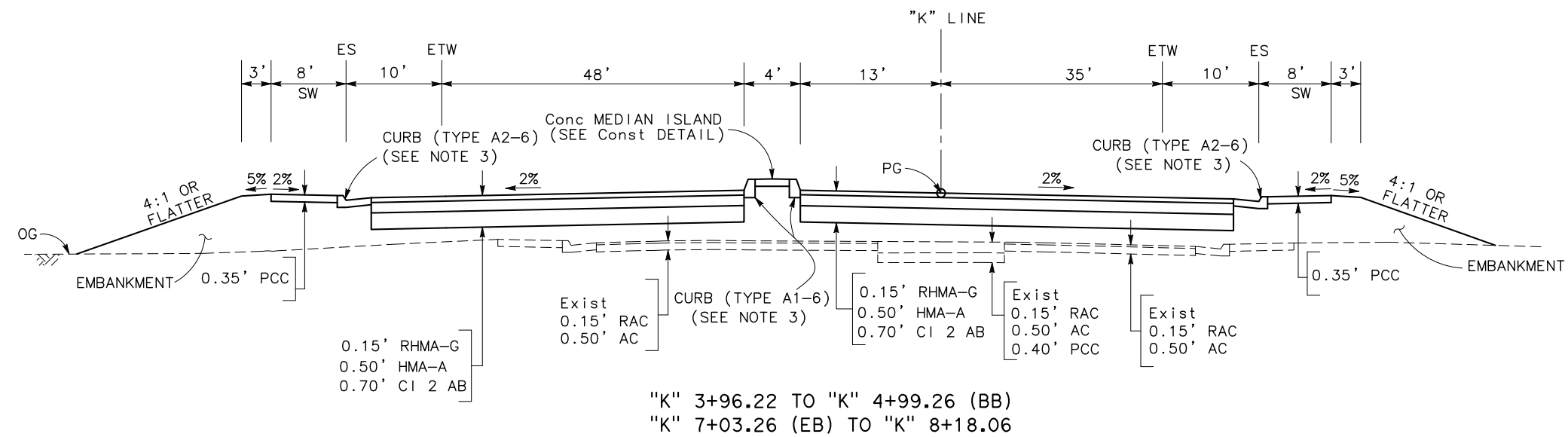
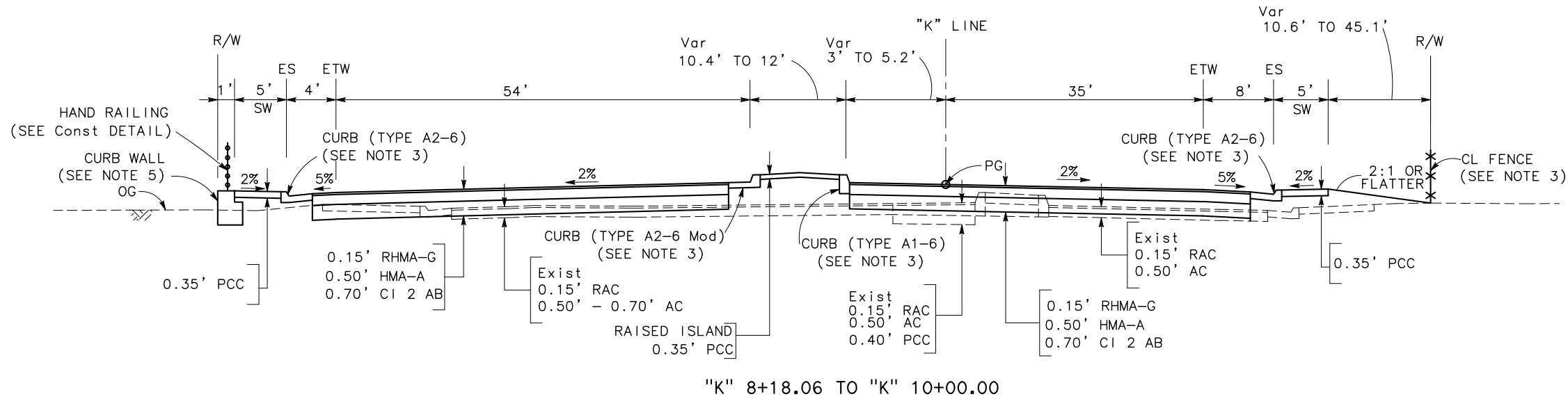
KIERNAN AVENUE

TYPICAL CROSS SECTIONS
NO SCALE X-2

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	6	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
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 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



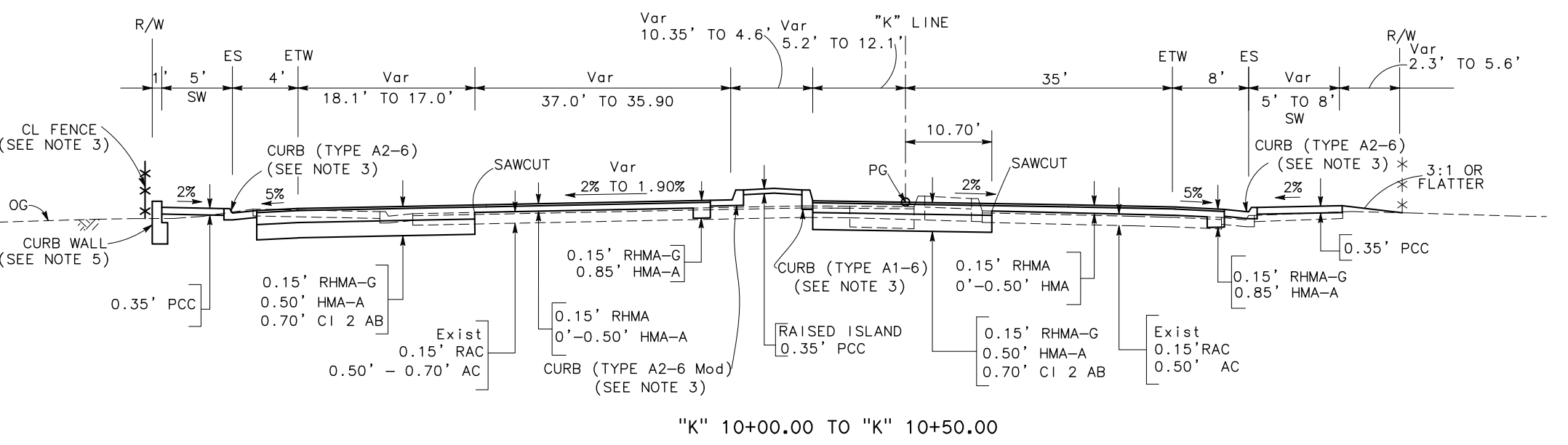
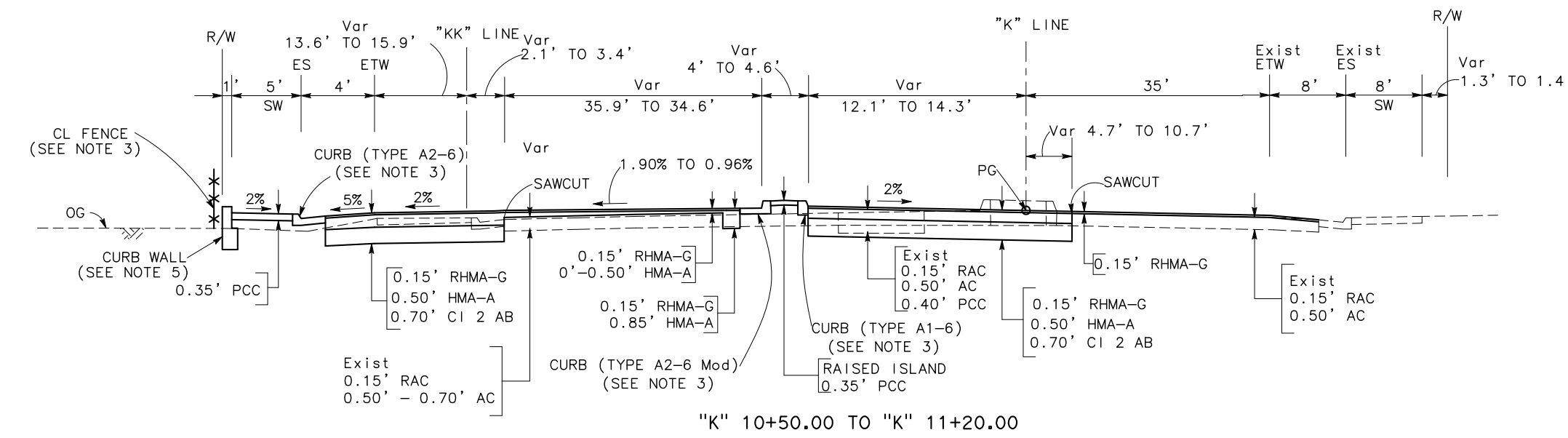
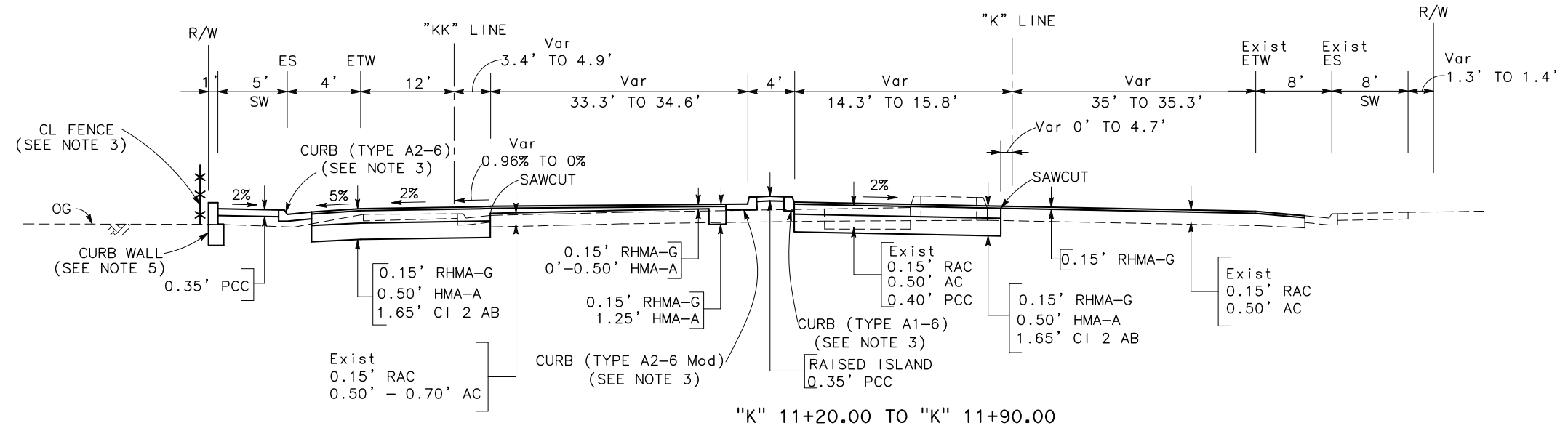
TYPICAL CROSS SECTIONS
 NO SCALE
X-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER
 CALCULATED-DESIGNED BY N MUMMANENT
 CHECKED BY CHUONG NGUYEN
 REVISED BY DATE REVISED
 N MUMMANENT CHUONG NGUYEN

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 N MUMMANENT
 REVISOR: CHUONG NGUYEN
 DATE: 05-18-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	7	423
REGISTERED CIVIL ENGINEER <i>Chuong Nguyen</i> No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA			05-18-12 DATE 9/04/12 PLANS APPROVAL DATE		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

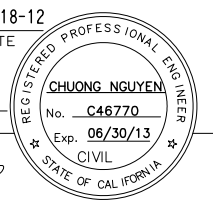


TYPICAL CROSS SECTIONS
 NO SCALE
X-4

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 06-08-12

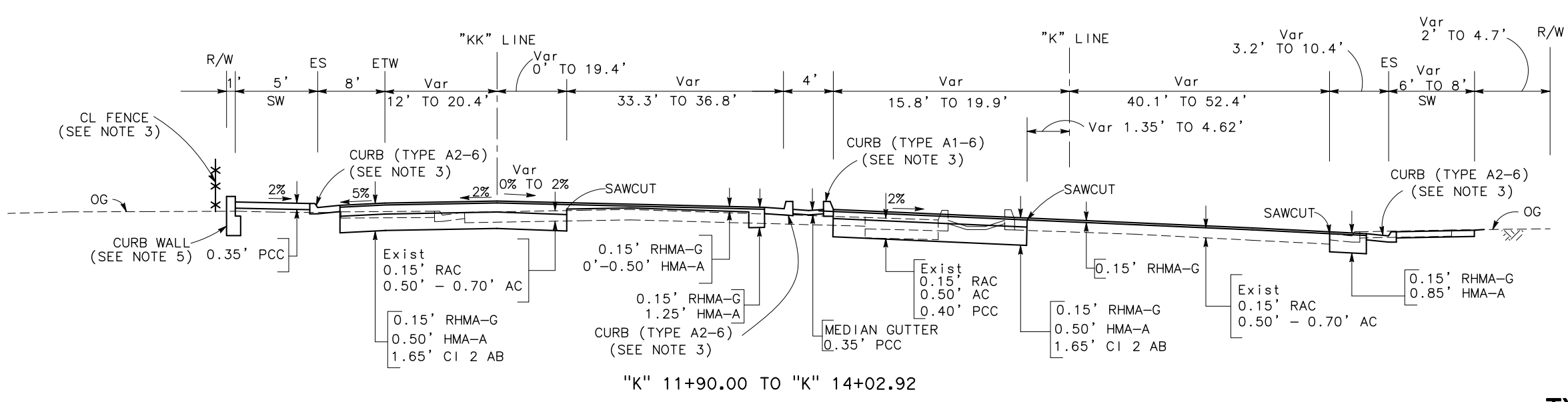
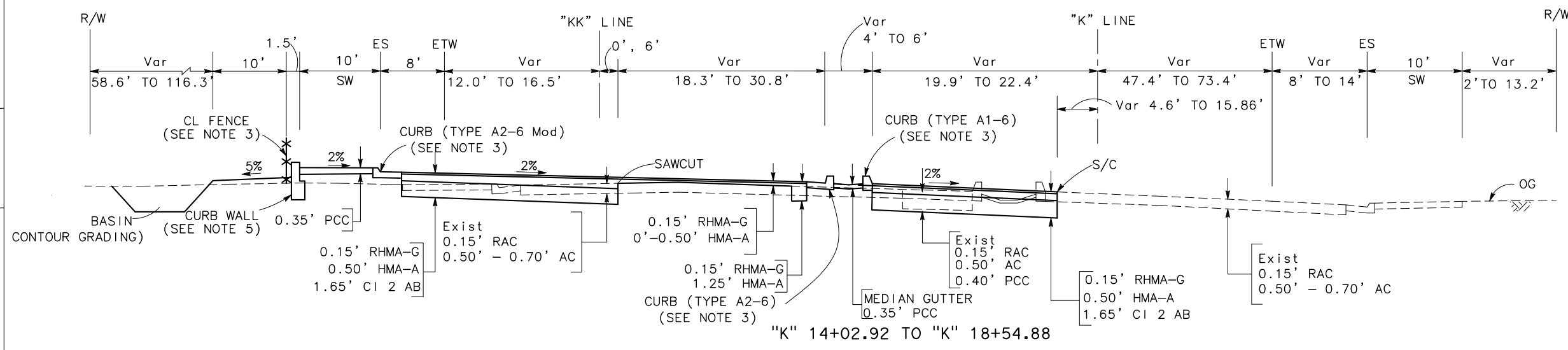
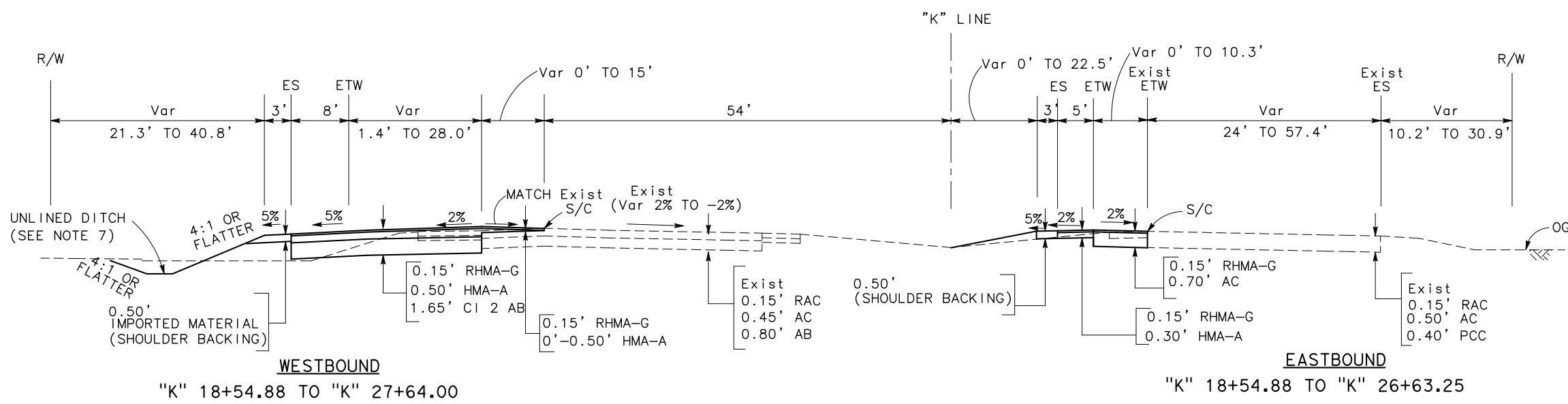
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	8	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

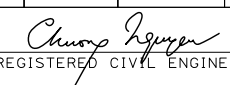
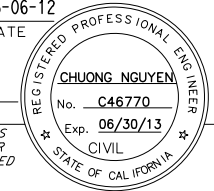
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER
 CALCULATED-DRAWN BY CHUONG NGUYEN
 N MUMMANENT
 REVISIONS BY DATE
 CHUONG NGUYEN
 DATE REVISIONS



KIERNAN AVENUE

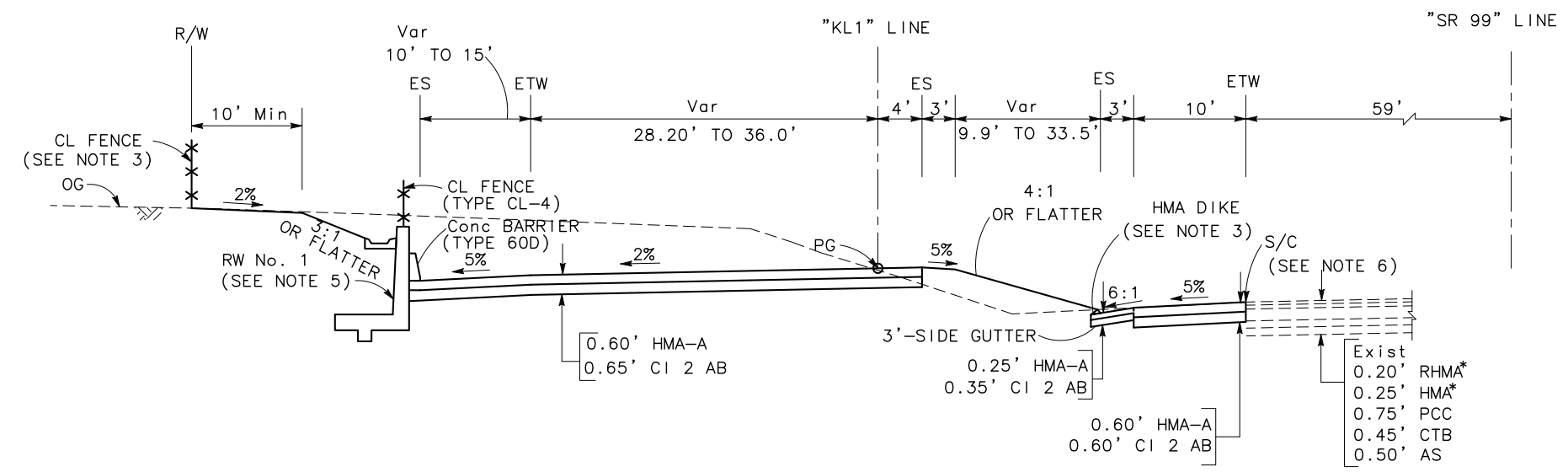
TYPICAL CROSS SECTIONS
 NO SCALE
X-5

LAST REVISION 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

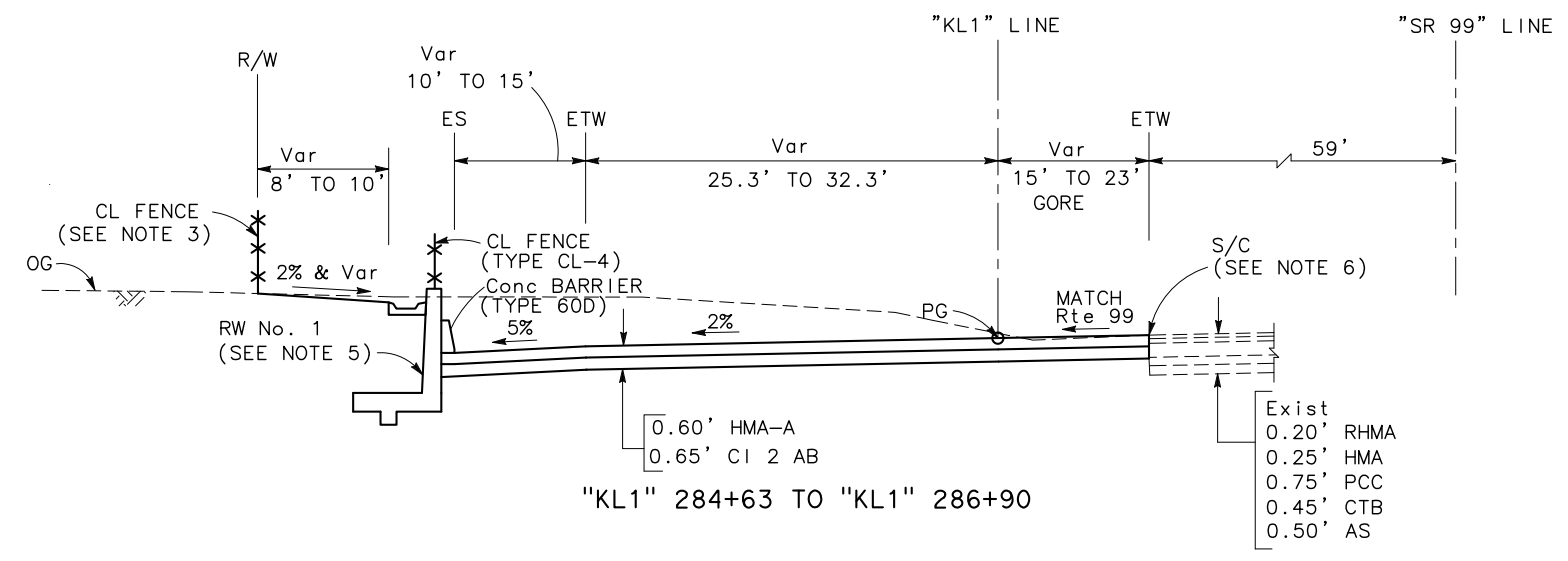
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	9	423
 REGISTERED CIVIL ENGINEER			05-06-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

**ROUTE 99 SB ON-RAMP
DESIGN DESIGNATION**

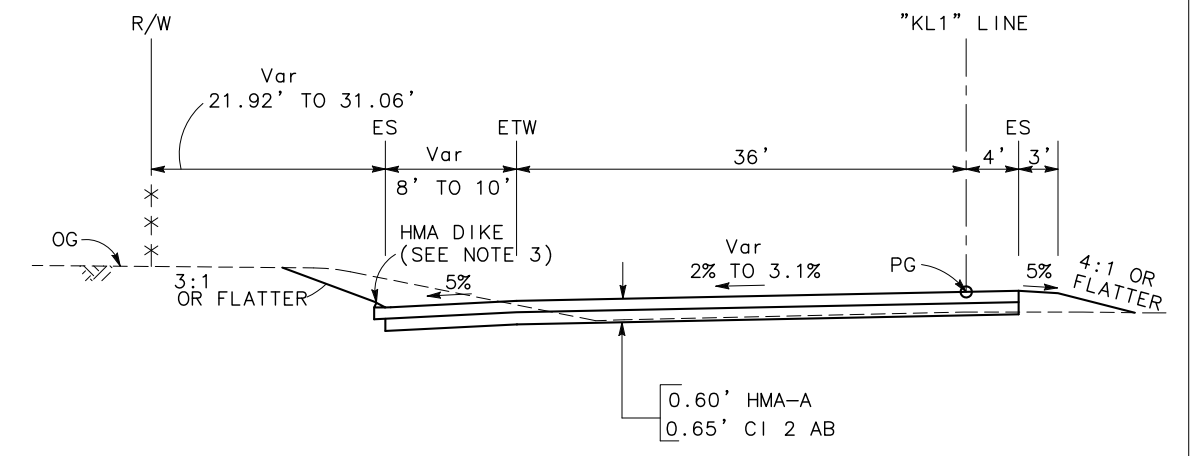
ADT (2007)	7,667	D	100%
ADT (2035)	11,920	T	9.6
DHV	1,160	TI	11
ESAL	5,087,218		20



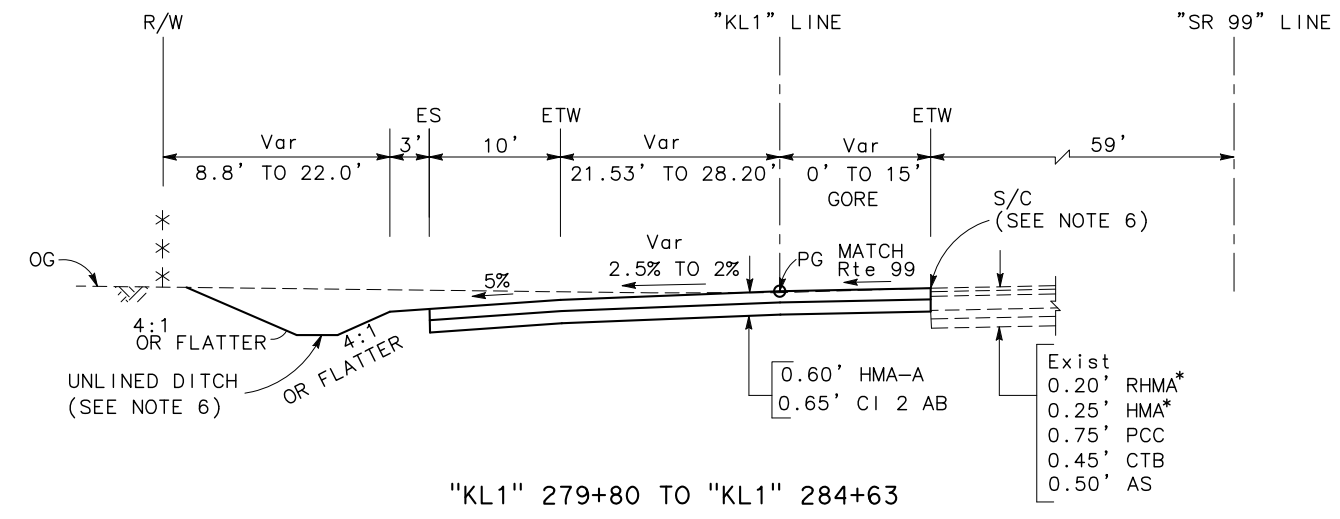
"KL1" 286+90 TO "KL1" 289+08.15



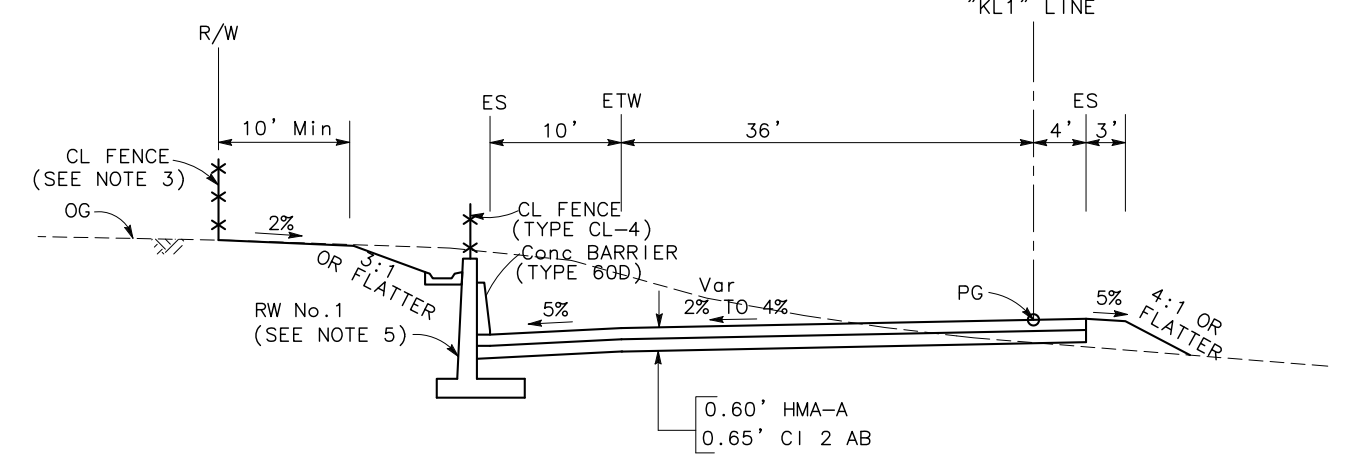
"KL1" 284+63 TO "KL1" 286+90



"KL1" 294+50 TO "KL1" 296+85.23



"KL1" 279+80 TO "KL1" 284+63



"KL1" 289+08.15 TO "KL1" 294+50

ROUTE 99 SB ON-RAMP

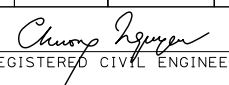
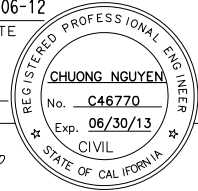
TYPICAL CROSS SECTIONS

NO SCALE

X-6

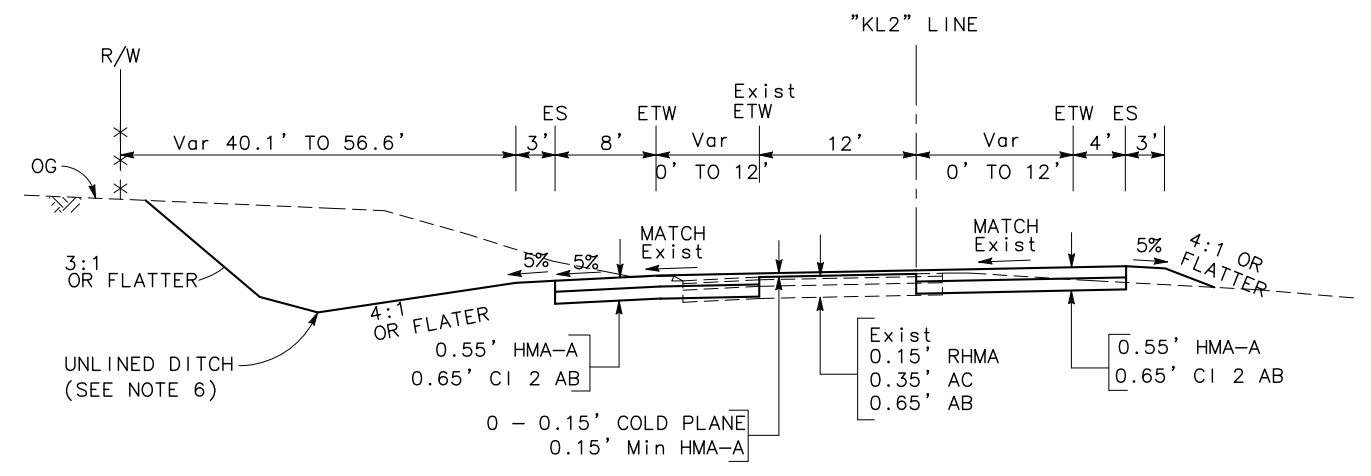
(*) - PER CONTRACT No.10-0M8004

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 CALCULATED-DESIGNED BY
 REVISOR BY
 N MUMMANENTI
 CHUONG NGUYEN
 KEITH G. MEYER
 DATE REVISOR
 DATE REVISOR

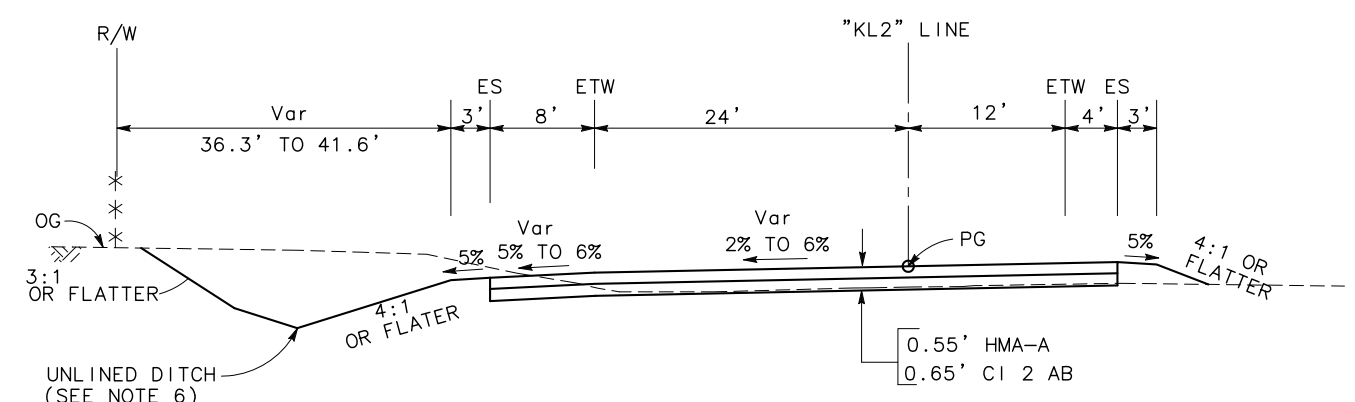
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	10	423
		 REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

ROUTE 99 SB OFF-RAMP
 DESIGN DESIGNATION

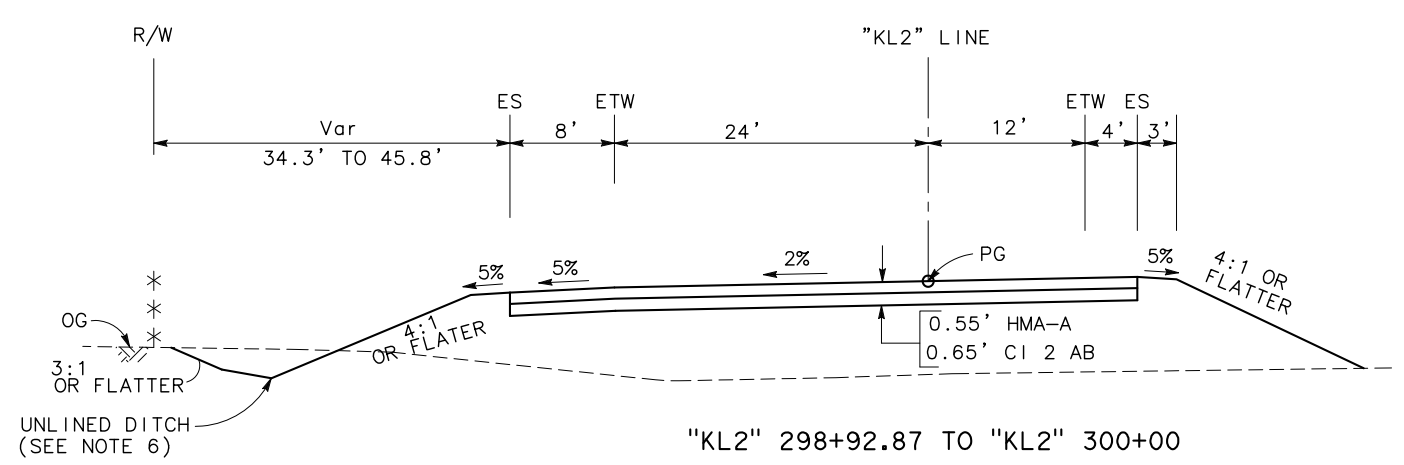
ADT (2007) 8,271 D 100%
 ADT (2035) 9,670 T 9.6
 DHV 770 Tl 10.5
 ESAL 4,126,963 20



"KL2" 303+40 TO "KL2" 304+60



"KL2" 300+00.00 TO "KL2" 303+40



"KL2" 298+92.87 TO "KL2" 300+00

ROUTE 99 SB OFF-RAMP

TYPICAL CROSS SECTIONS

NO SCALE X-7

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Caltrans

REVISOR BY
 DATE REVISOR

N MUMMANENT
 CHUONG NGUYEN

CALCULATED-DESIGNED BY
 CHECKED BY

CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Caltrans

USERNAME => \$USER
 DGN FILE => \$REQUEST



UNIT 1457

PROJECT NUMBER & PHASE 1000000100

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 05-01-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENT
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

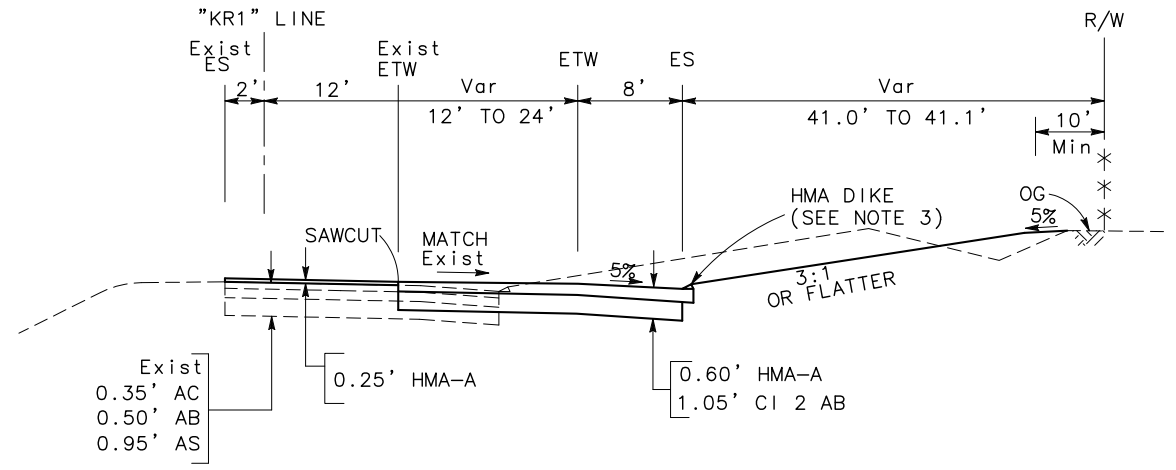
ROUTE 99 NB OFF-RAMP
 DESIGN DESIGNATION

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 DHV 1,520 TI 11.5
 ESAL 8,804,471 20

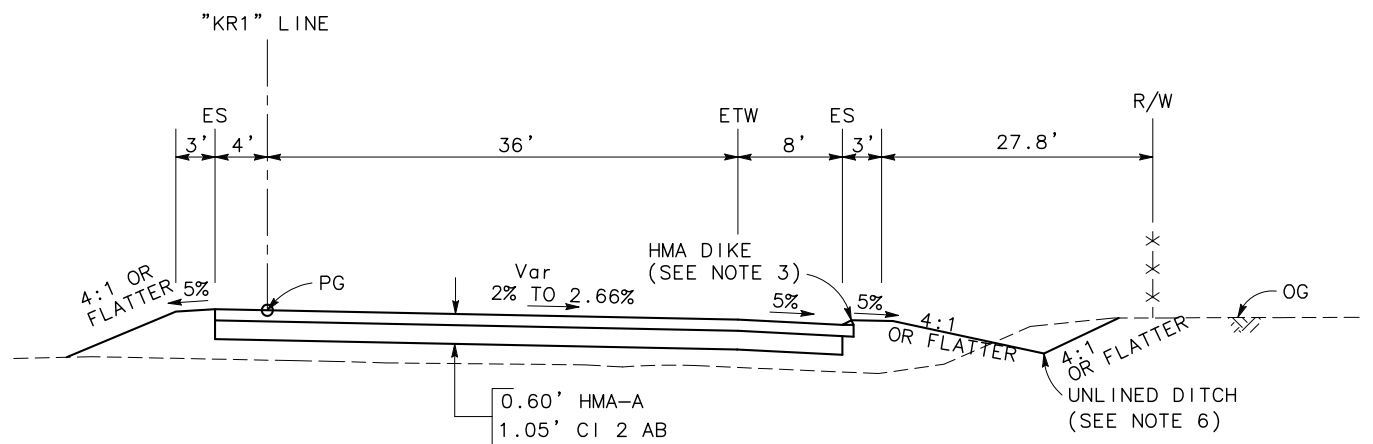
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	11	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

"SR 99" LINE

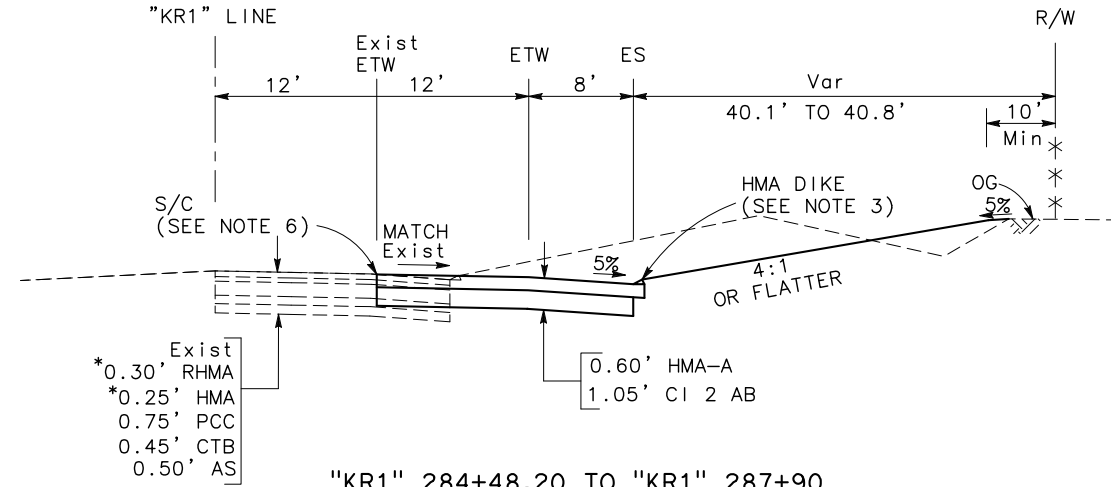


"KR1" 287+90 TO "KR1" 293+60.00

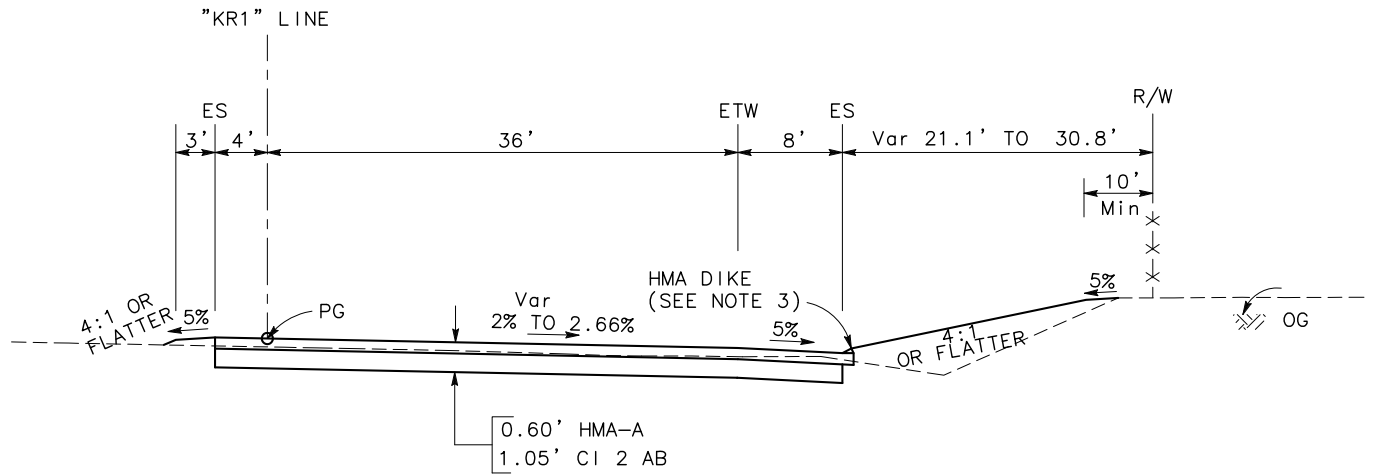


"KR1" 296+00.00 TO "KR1" 297+15.58

"SR 99" LINE



"KR1" 284+48.20 TO "KR1" 287+90



"KR1" 293+60.00 TO "KR1" 296+00.00

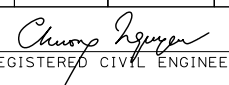
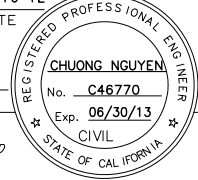
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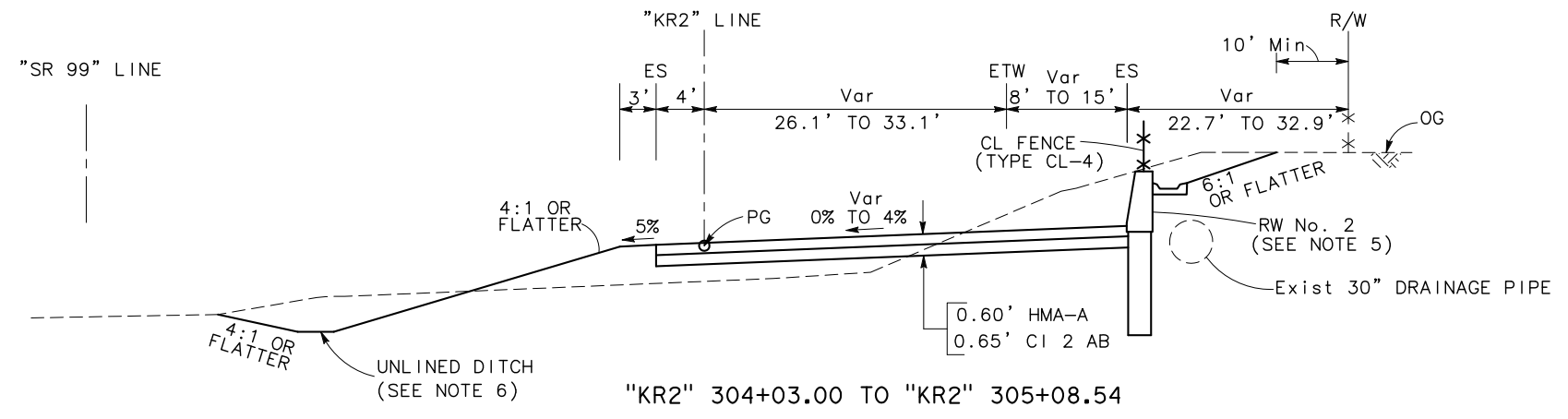
TYPICAL CROSS SECTIONS

NO SCALE X-8

(*) - PER CONTRACT No. 10-0M8004

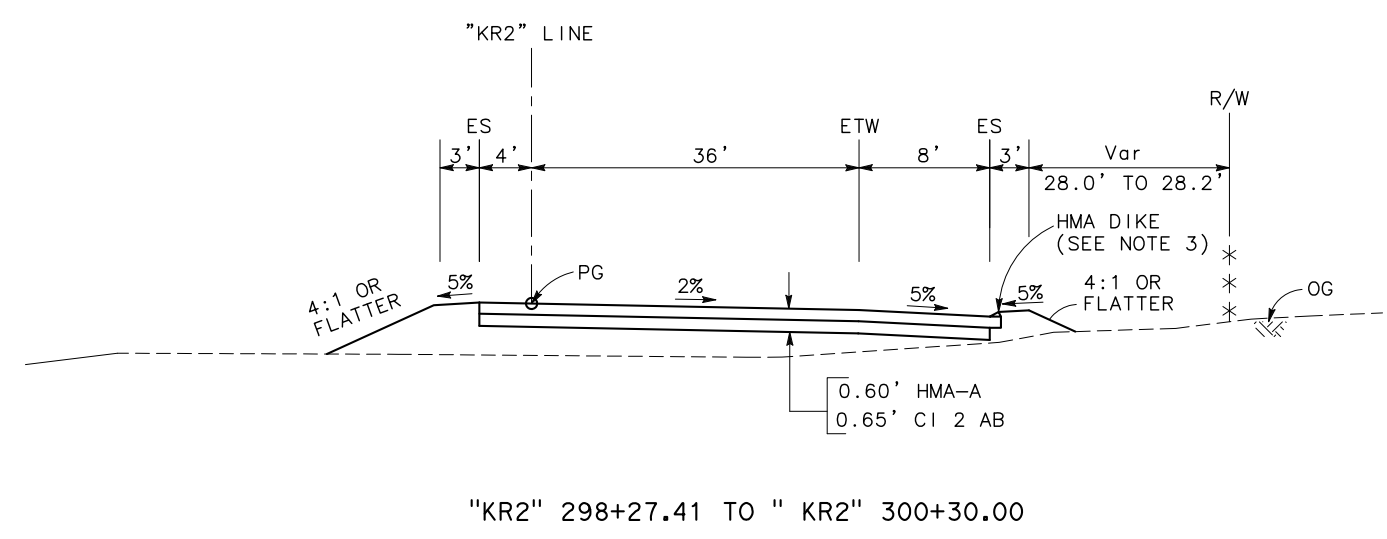
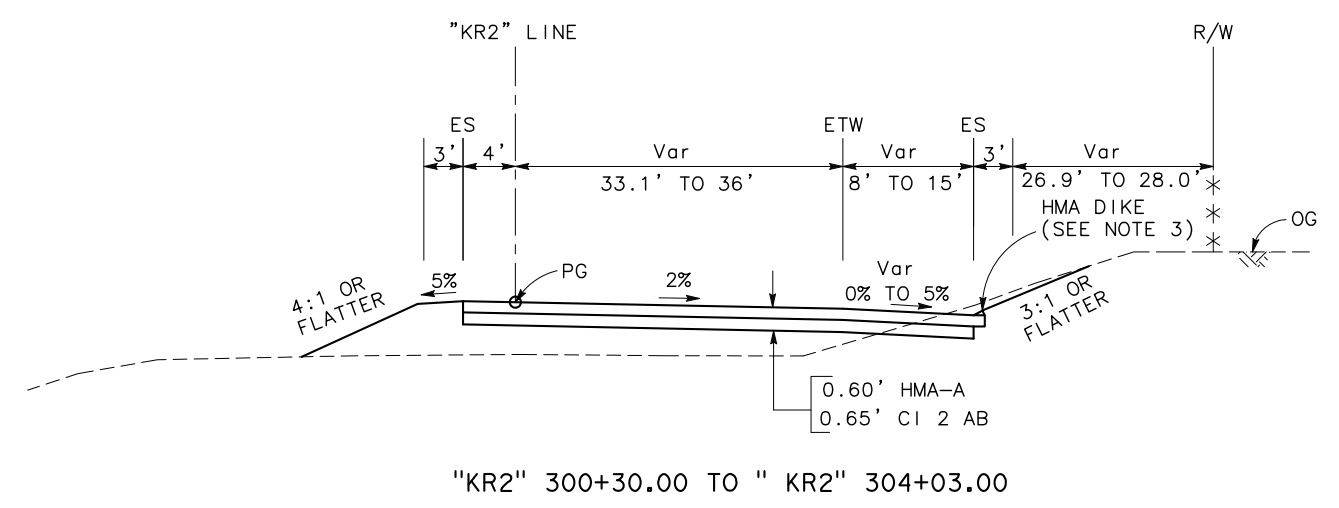
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENT
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dis+	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	12	423
 REGISTERED CIVIL ENGINEER			05-18-12	DATE	
PLANS APPROVAL DATE 9/04/12					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



**ROUTE 99 NB ON-RAMP
 DESIGN DESIGNATION**

ADT (2007)	6,350	D	100%
ADT (2035)	11,610	T	9.6
DHV	620	T _I	11
ESAL	4,954,916		20



(*) - PER CONTRACT No. 10-0M8004

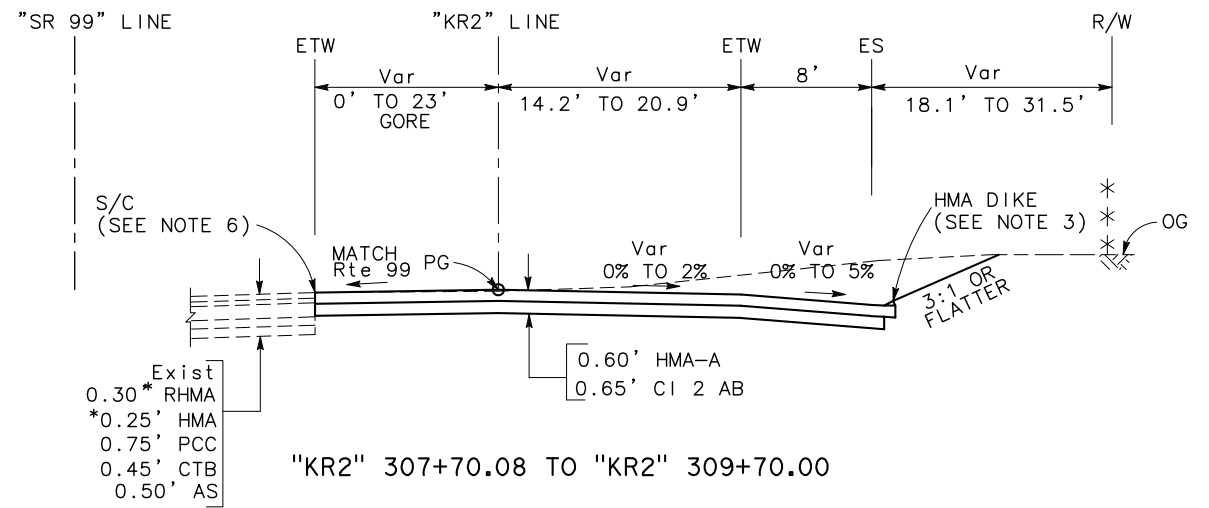
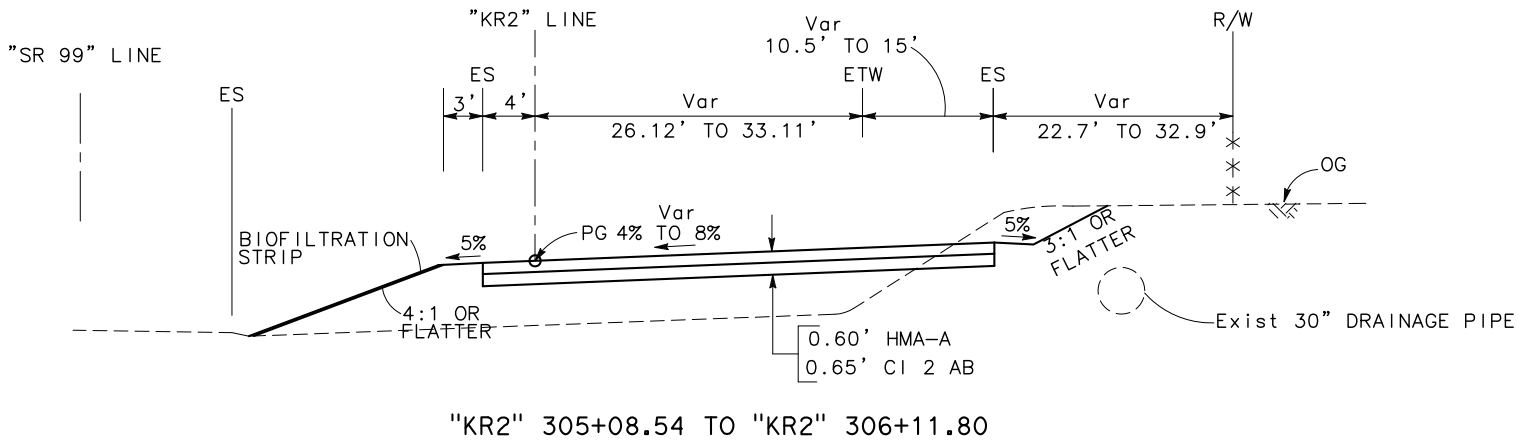
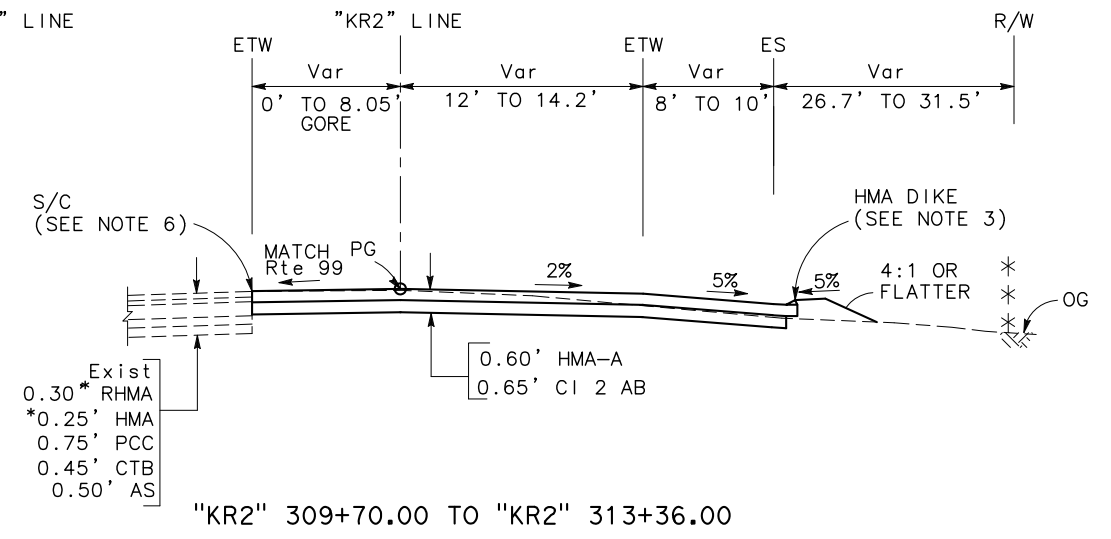
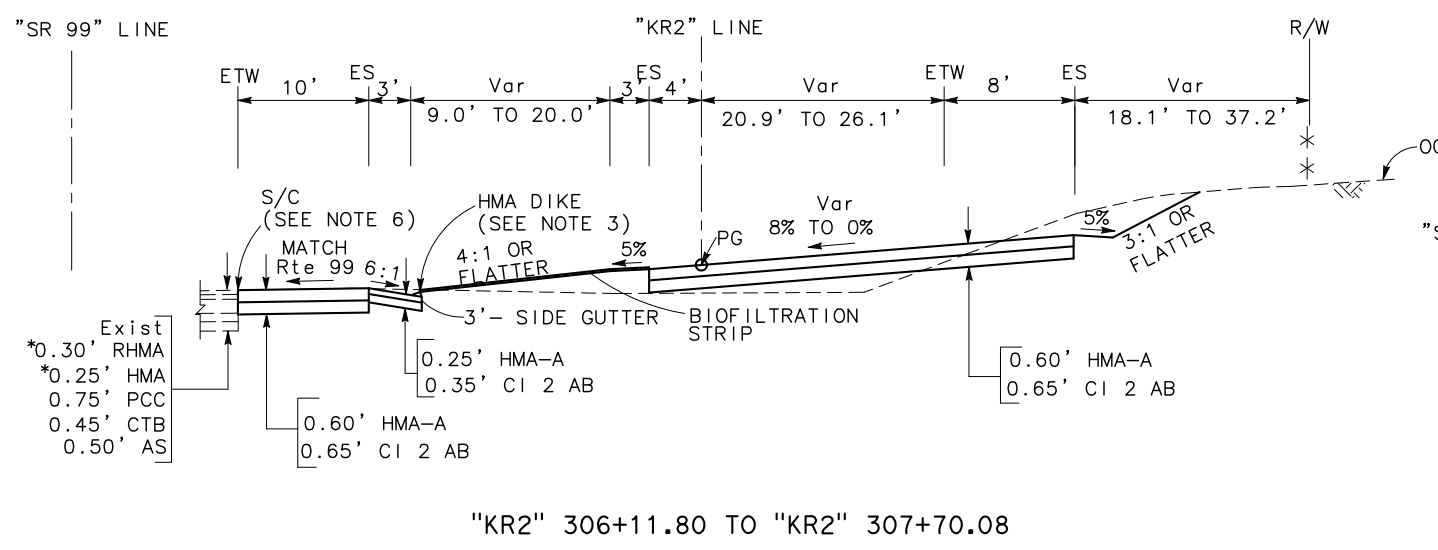
ROUTE 99 NB ON-RAMP

TYPICAL CROSS SECTIONS
 NO SCALE
X-9

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	13	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA



ROUTE 99 NB ON-RAMP

TYPICAL CROSS SECTIONS
NO SCALE X-10

(*) - PER CONTRACT No.10-0M8004

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER

REVISOR N MUMMANENT

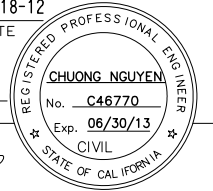
DESIGNER CHUONG NGUYEN

CHECKER CHUONG NGUYEN

DATE REVISED

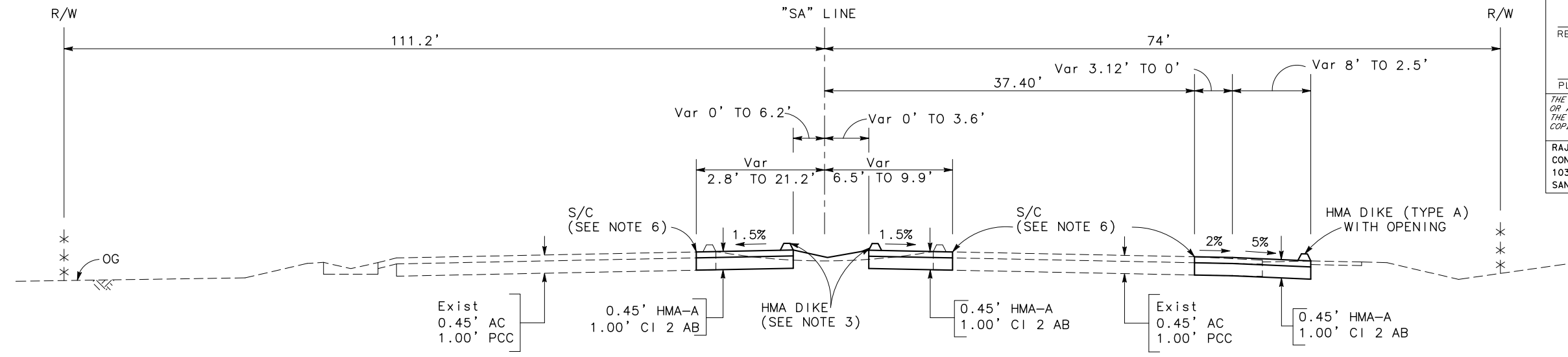
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	14	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
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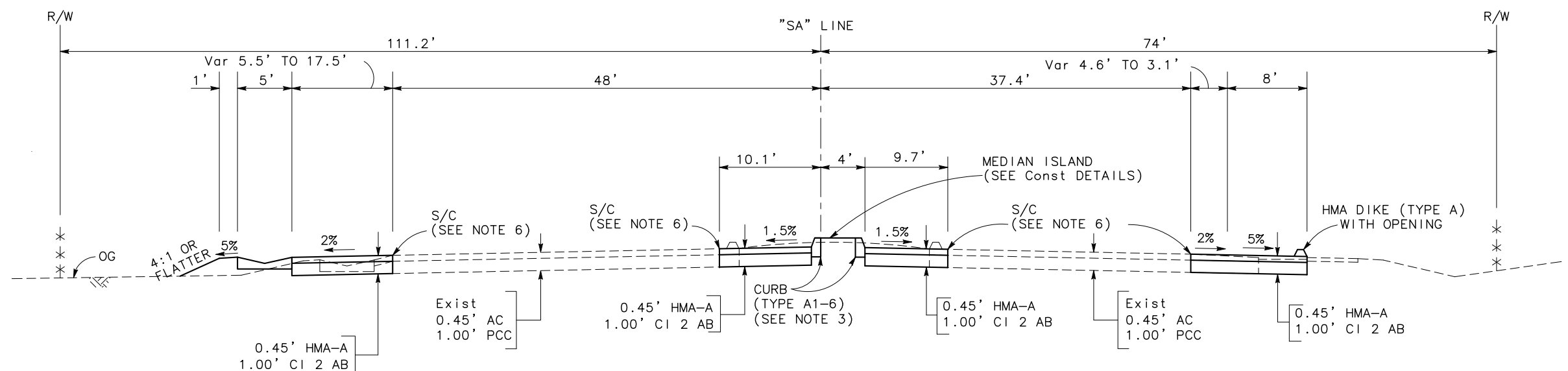


RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

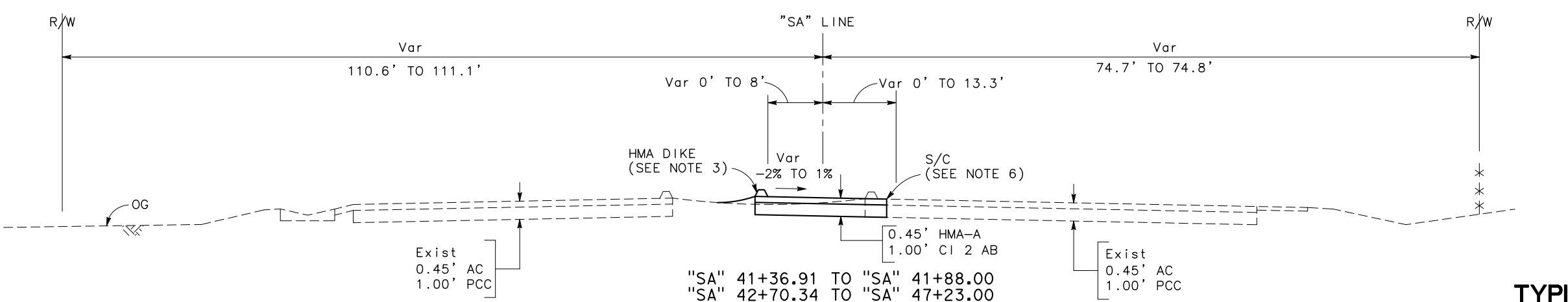
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER
 CALCULATED-DESIGNED BY N MUMMANENT
 CHECKED BY CHUONG NGUYEN
 REVISED BY DATE REVISED
 REVISIONS: x, x, x, x, x



"SA" 51+70.73 TO "SA" 55+33.09



"SA" 47+23.00 TO "SA" 51+70.73



"SA" 41+36.91 TO "SA" 41+88.00
 "SA" 42+70.34 TO "SA" 47+23.00

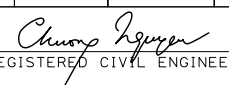
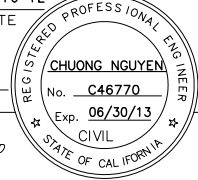
SALIDA BOULEVARD

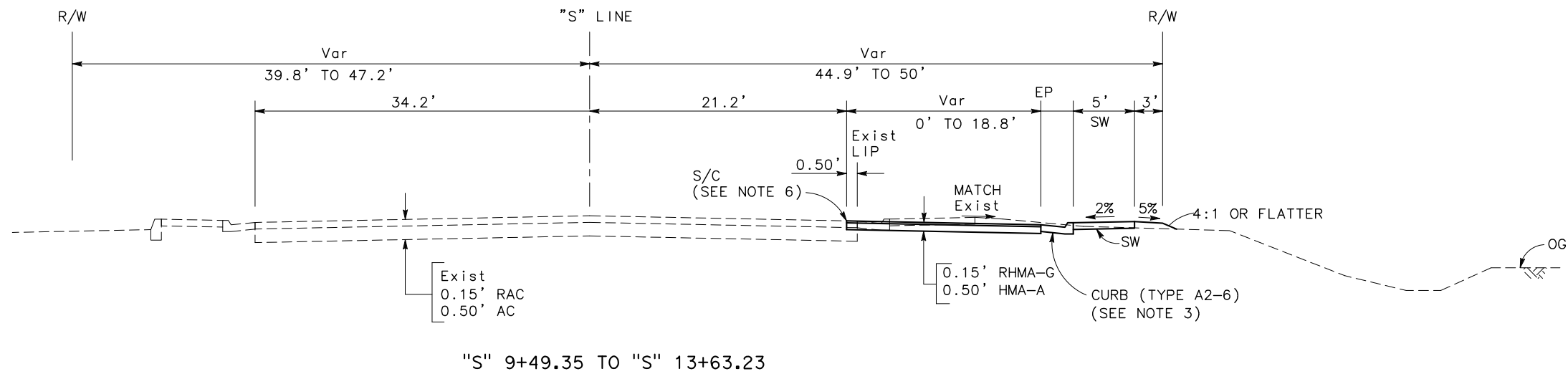
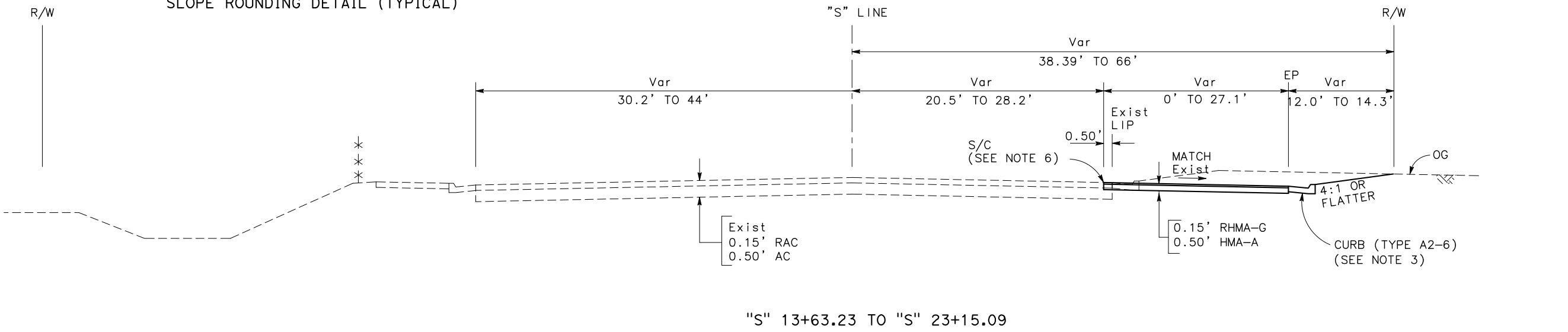
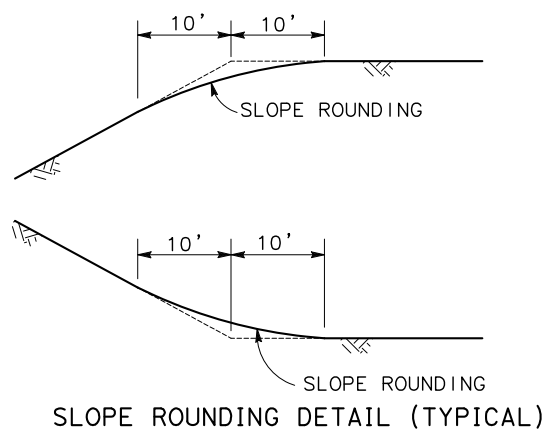
TYPICAL CROSS SECTIONS

NO SCALE X-11

DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENT
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	15	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



SISK ROAD

TYPICAL CROSS SECTIONS
 NO SCALE
X-12


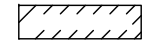
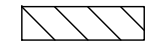

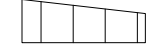
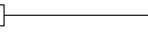

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION
 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTES:

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- ALL COORDINATE DISTANCES AND BEARINGS ARE BASED ON THE CCS83, ZONE 3. ALL ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).

LEGEND

-  PAVEMENT CONFORM TAPER (SEE CONSTRUCTION DETAILS)
-  HMA OVERLAY
-  REMOVE RAISED ISLAND & REPAVE
-  RETAINING WALL
-  CONTRASTING SURFACE AREA (SEE CONSTRUCTION DETAILS)
-  IRRIGATION CROSSOVER (SEE IRRIGATION PLANS)
-  SOUNDWALL ON BARRIER

ABBREVIATIONS:

- MVP MAINTENANCE VEHICLE PULLOUT
- LSS LIMIT OF STRUCTURAL SECTION
- Reconst RECONSTRUCT
- S/C SAWCUT & CONFORM
- TT TRANSITION TAPER
- AFTS ALTERNATIVE FLARED TERMINAL SYSTEM
- UPRR UNION PACIFIC RAILROAD
- Com COMMERCIAL

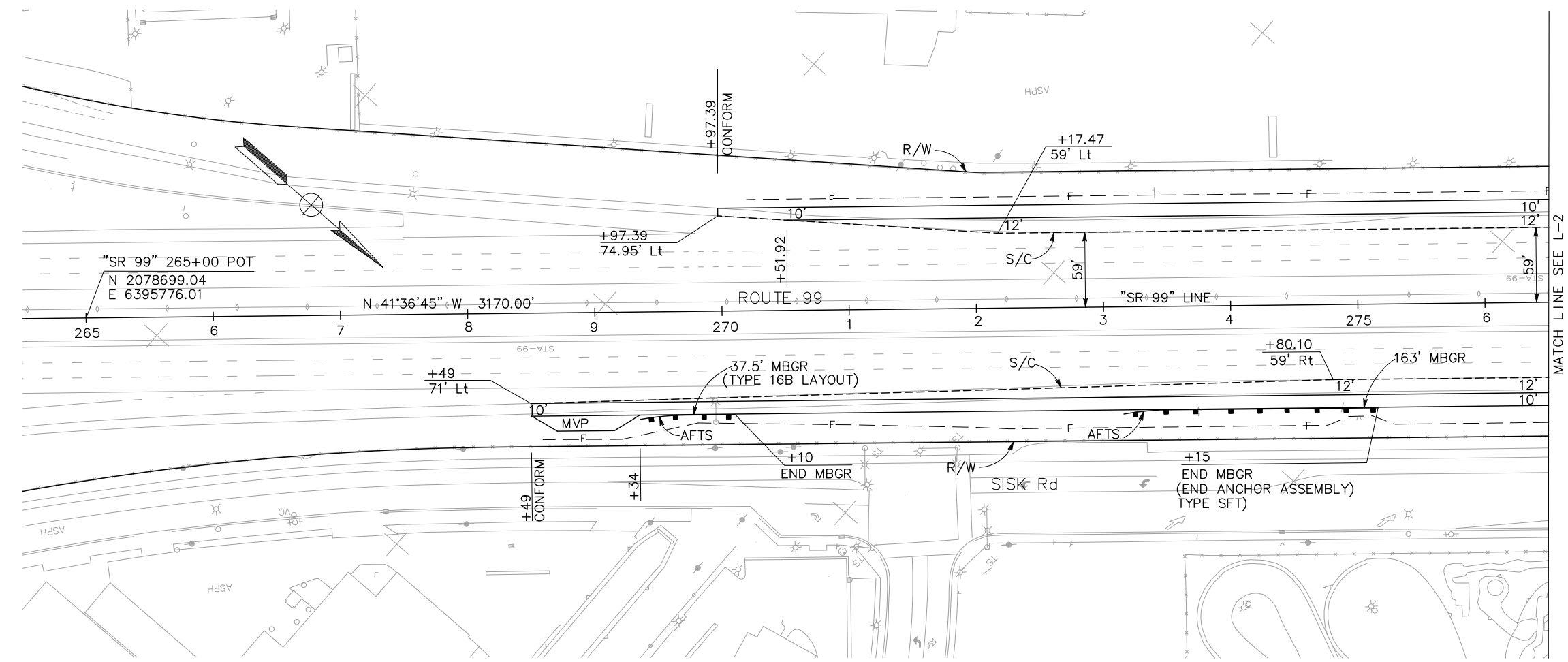
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	16	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 PLANS APPROVAL DATE 9/04/12
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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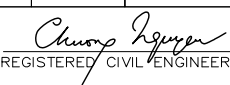
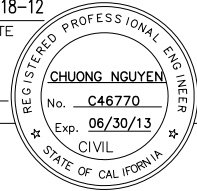
STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



MATCH LINE SEE L-2

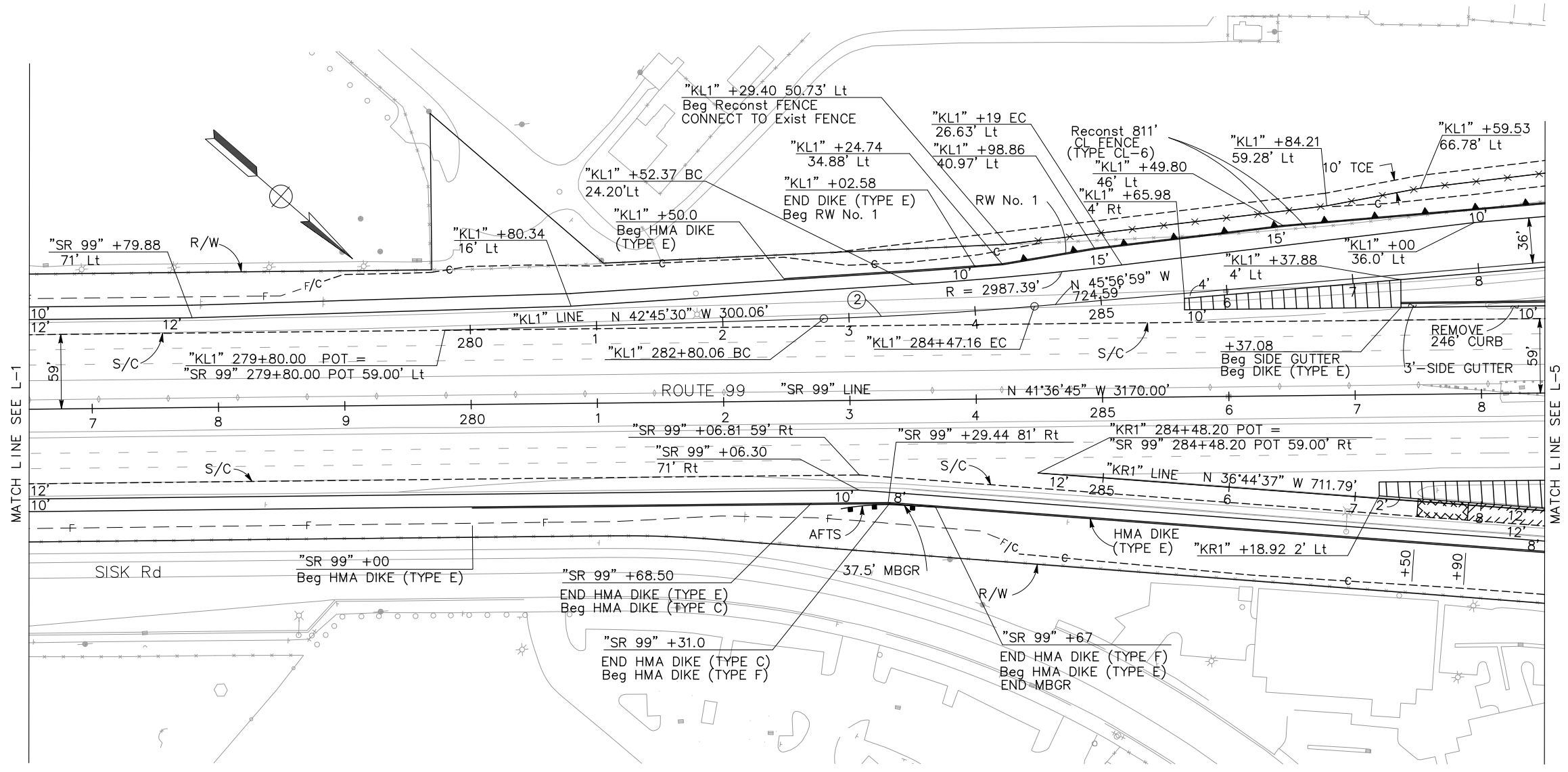
LAYOUT
 SCALE: 1" = 50' L-1

LAST REVISION 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	17	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
②	3,000.00'	03°11'29"	83.57'	167.10'	2077949.98	6392342.66



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans

CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

CALCULATED-DESIGNED BY
 CHECKED BY

N MUMMANENI
 CHUONG NGUYEN

REVISED BY
 DATE REVISED

LAYOUT
 SCALE: 1" = 50'
L-2

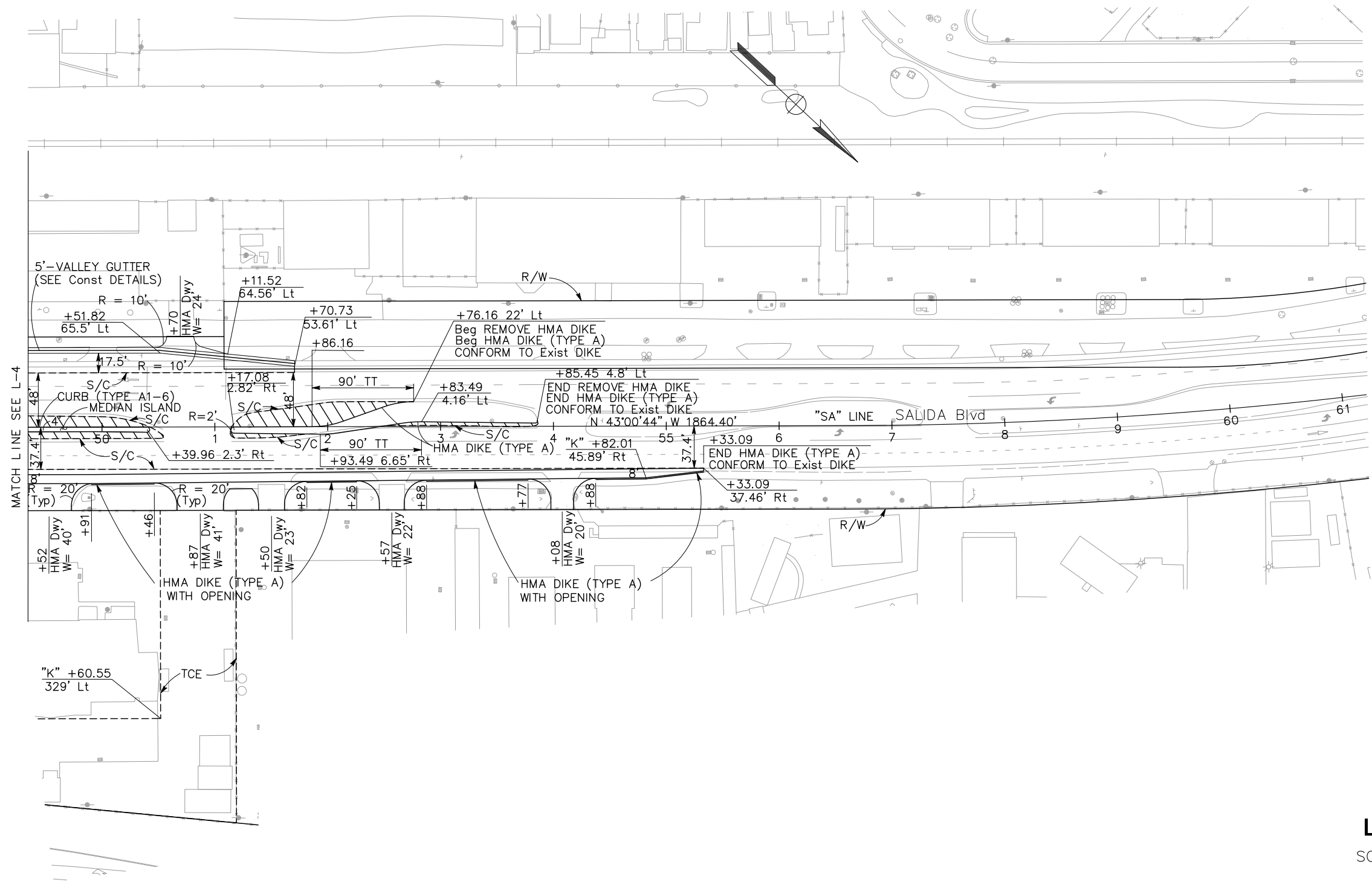
LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	18	423

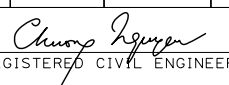

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DRAWN BY: N MUMMANENI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: DATE REVISED




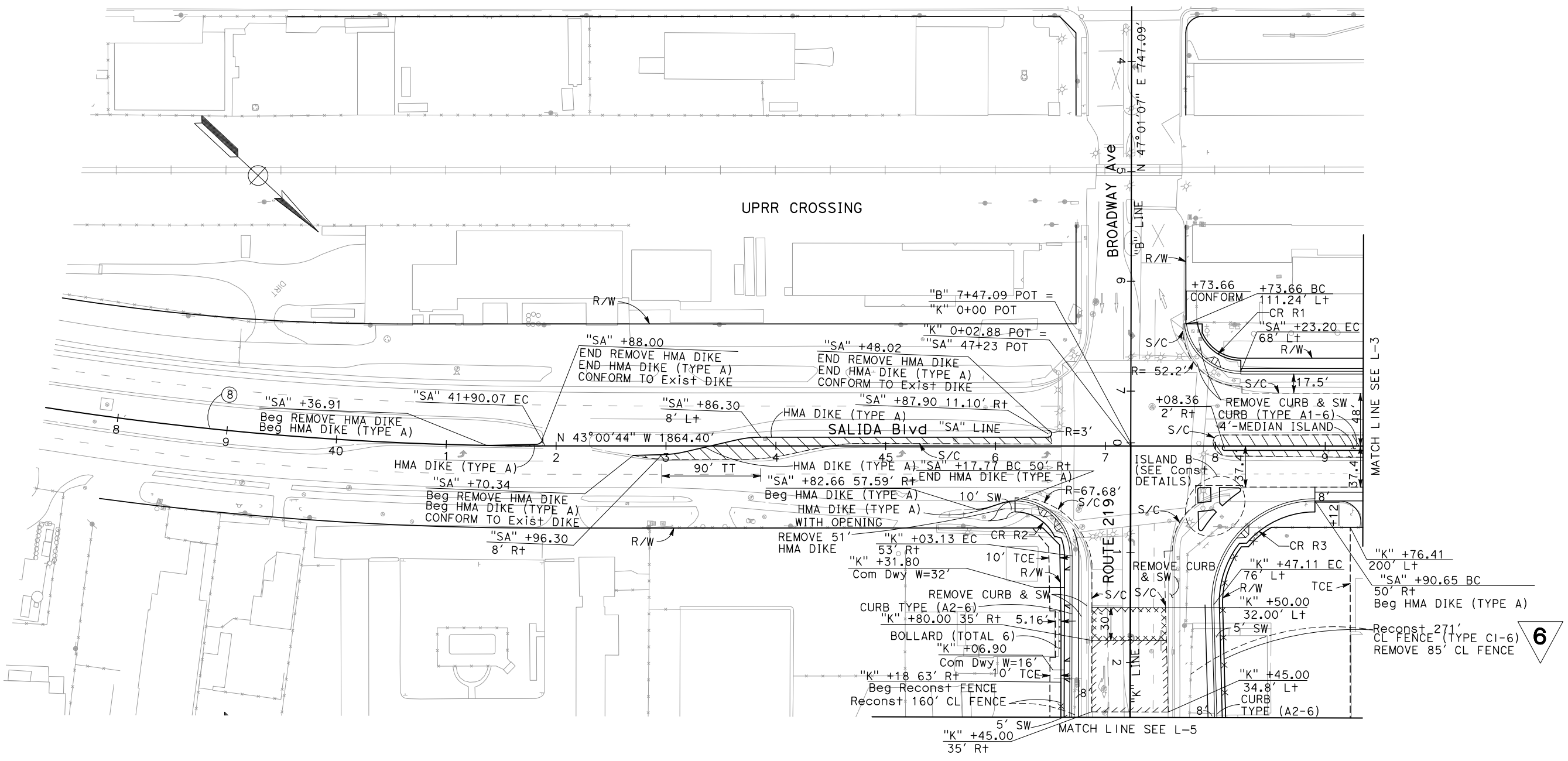
LAYOUT
 SCALE: 1" = 50' **L-3**

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	19	423
 REGISTERED CIVIL ENGINEER			05-18-12	DATE	
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
⑧	2,975.00'	09°21'04"	243.31'	485.54'	2078309.99	6391356.59

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANANI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED:



6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

LAYOUT
 SCALE: 1" = 50' L-4

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	20	423

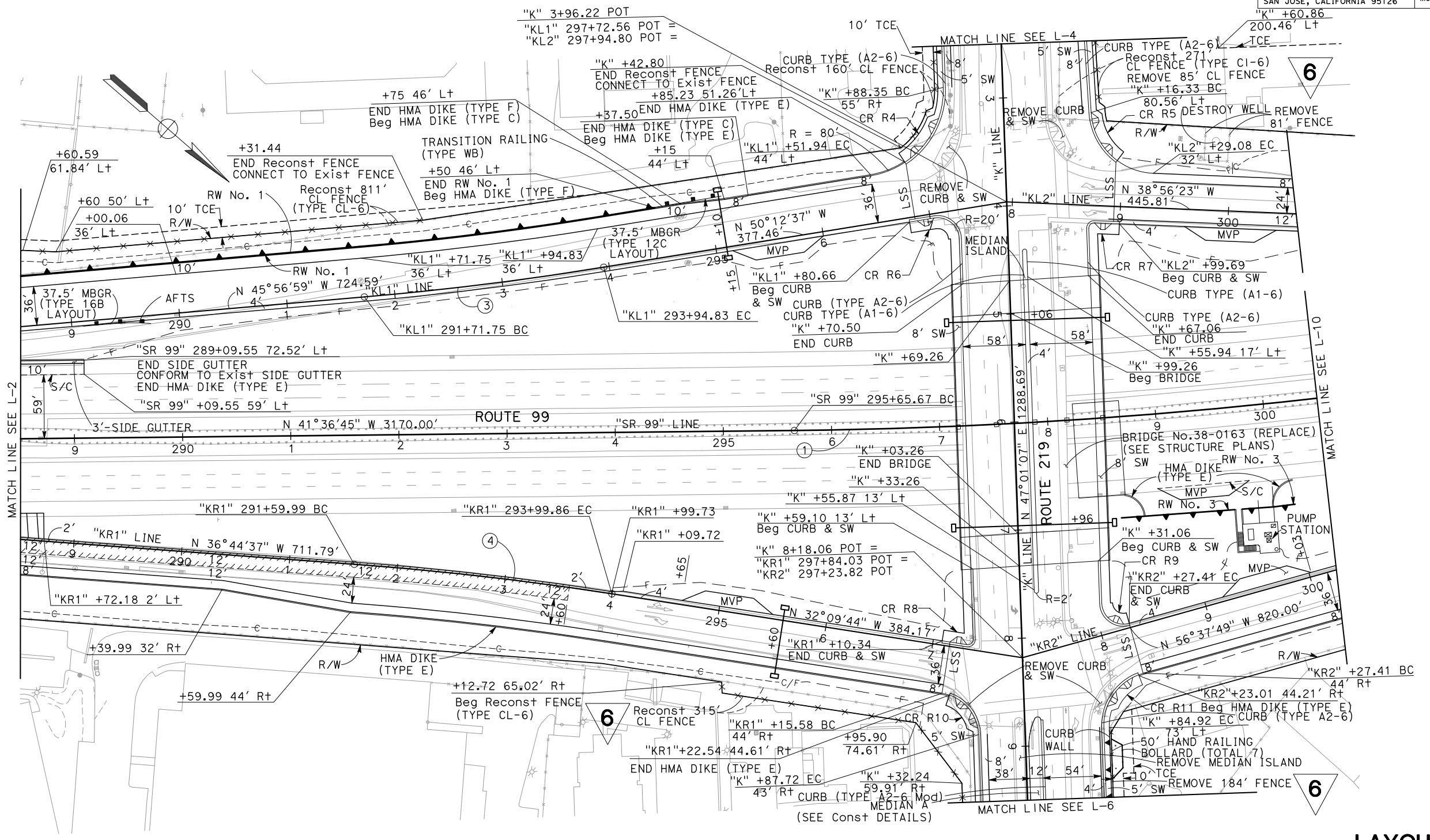
9/04/12
 REGISTERED CIVIL ENGINEER DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
③	3,000.00'	04°15'38"	111.59'	223.08'	2078453.78	6391821.88
④	3,000.00'	04°34'53"	120.00'	239.87'	2082559.88	6396504.49
①	4,750.00'	15°59'12"	667.00'	1325.34'	2077836.67	6390188.78



6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

LAYOUT
 SCALE: 1" = 50' L-5

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CHECKED BY: CHUONG NGUYEN
 DESIGNED BY: N MUMMANENI
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 REVISIONS: REVISOR, DATE, REVISION
 REVISOR: N MUMMANENI, DATE: 05-18-12, REVISION: 1
 REVISOR: CHUONG NGUYEN, DATE: 09-04-12, REVISION: 2
 REVISOR: CHUONG NGUYEN, DATE: 09-04-12, REVISION: 3
 REVISOR: CHUONG NGUYEN, DATE: 09-04-12, REVISION: 4
 REVISOR: CHUONG NGUYEN, DATE: 09-04-12, REVISION: 5
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LAST REVISION DATE PLOTTED => \$DATE
 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	21	423

REGISTERED CIVIL ENGINEER *Chuong Nguyen* DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

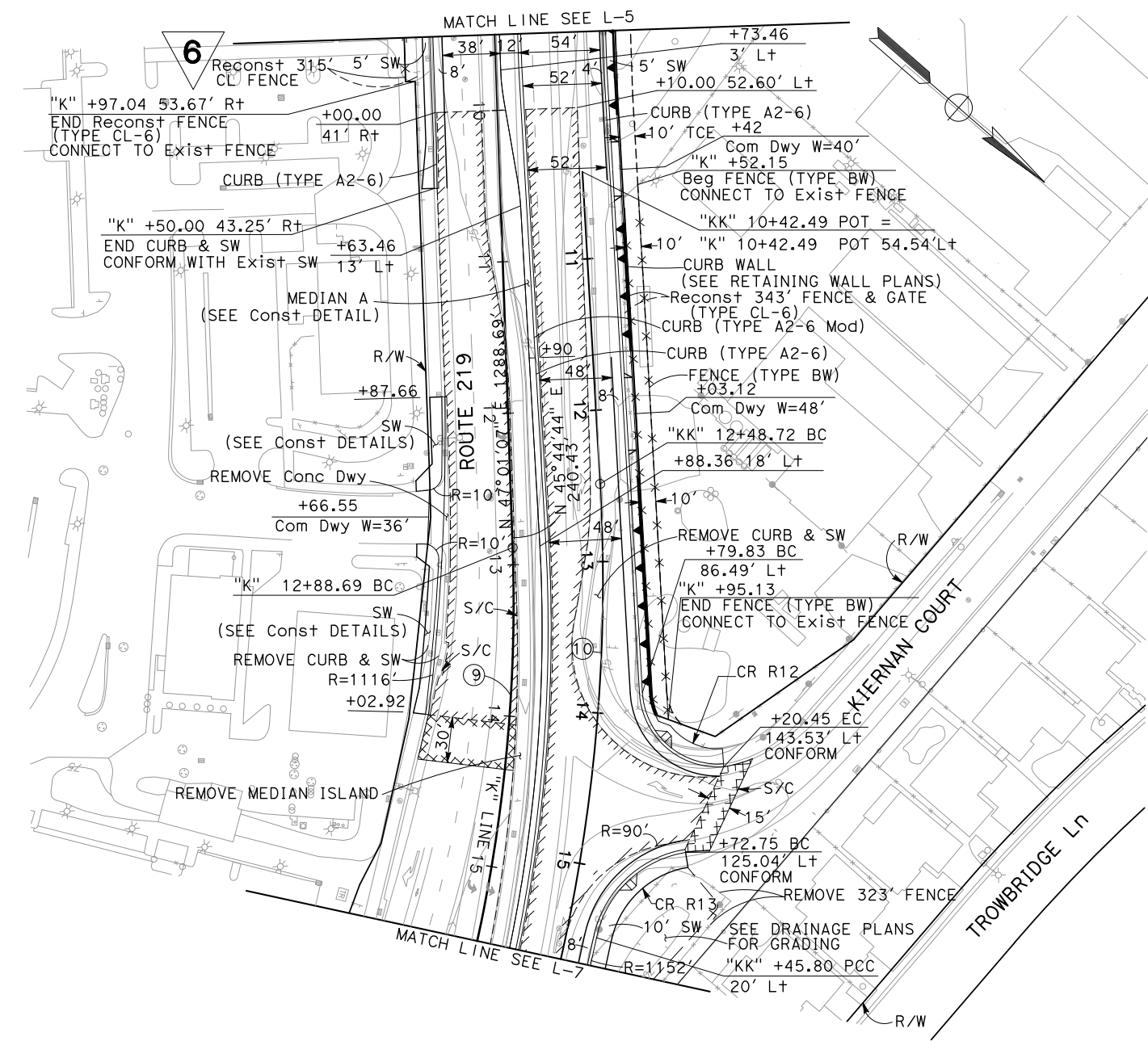
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
9	1,049.87'	42°44'24"	410.80'	783.15'	2080840.93	6394820.42
10	1,132.00'	32°47'35"	333.09'	647.85'	2080814.17	6394825.04



6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

LAYOUT
SCALE: 1" = 50' L-6

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

Caltrans

CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER

CALCULATED-DESIGNED BY: N MUMMANENTI

CHECKED BY: CHUONG NGUYEN

REVISOR: CHUONG NGUYEN

DATE REVISED:

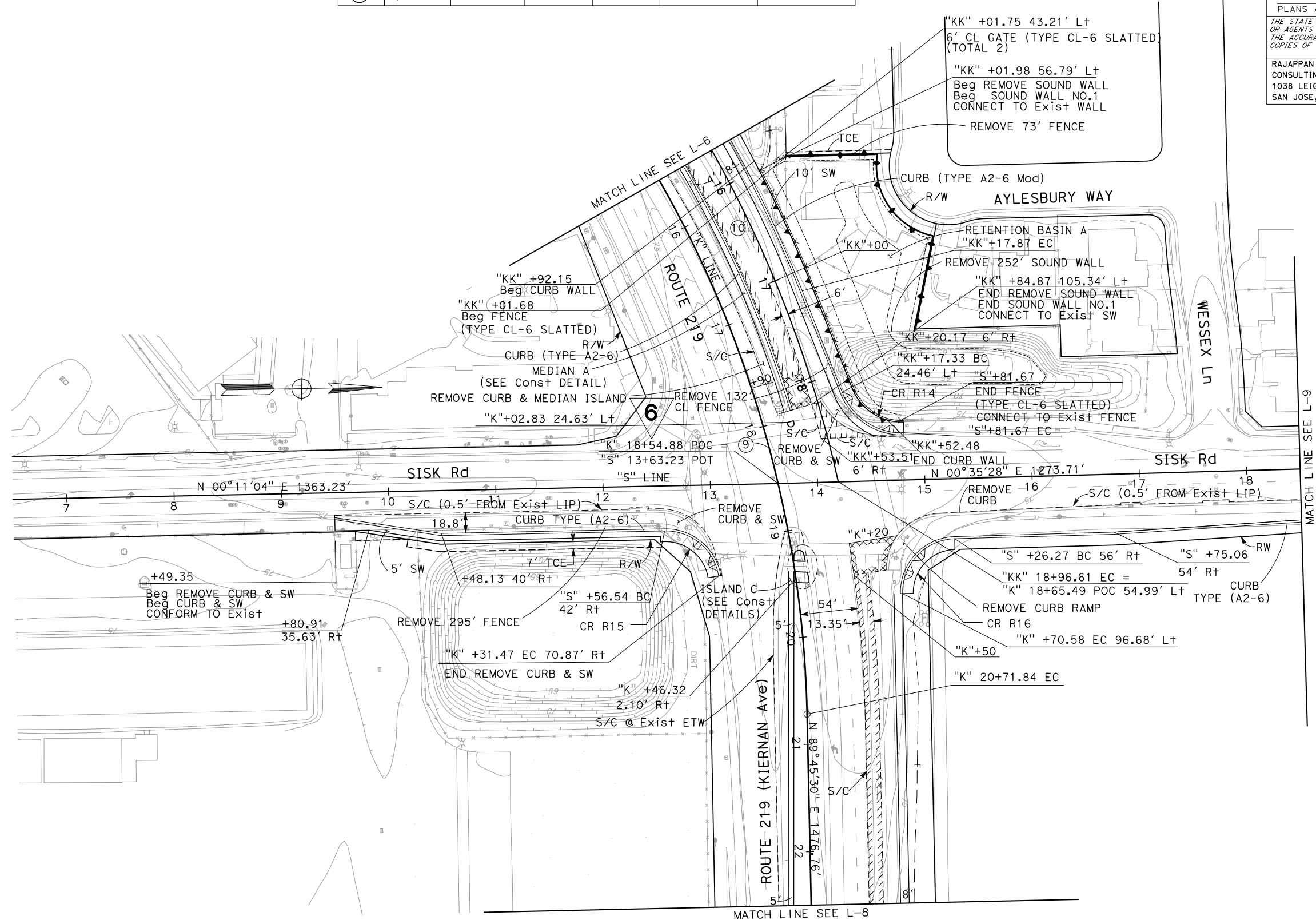
LAST REVISION DATE PLOTTED => \$DATE 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	22	423

REGISTERED CIVIL ENGINEER	DATE 05-18-12
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
9	1,049.87'	42°44'24"	410.80'	783.15'	2080840.93	6394820.42
10	1,132.00'	32°47'35"	333.09'	647.85'	2080814.17	6394825.04



6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

LAYOUT
 SCALE: 1" = 50' **L-7**

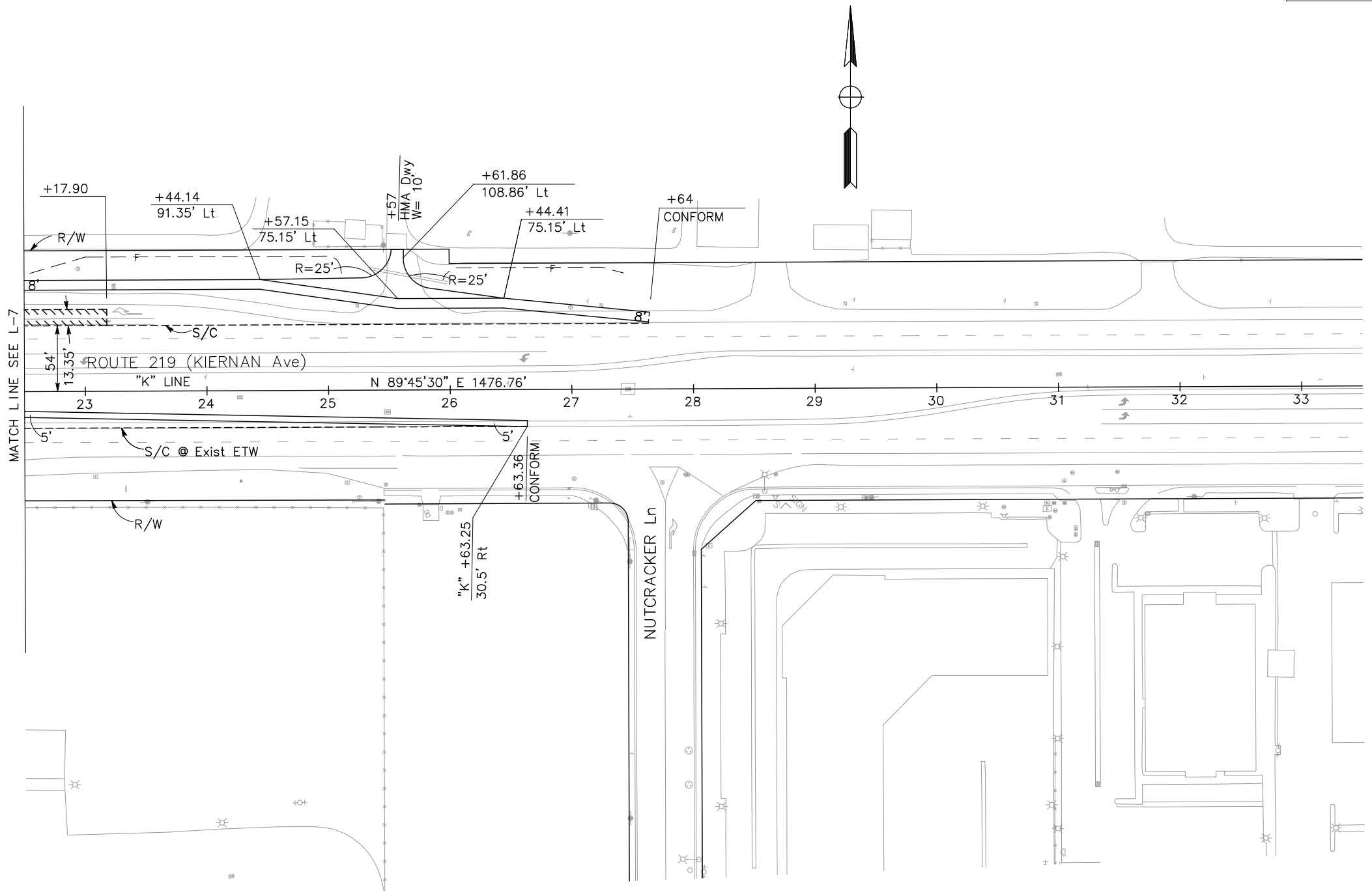
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DRAWN BY: CHUONG NGUYEN
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: N MUMMANENI, CHUONG NGUYEN
 REVISOR: N MUMMANENI, CHUONG NGUYEN
 DATE: 06-08-12

LAST REVISION DATE PLOTTED => \$DATE
 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	23	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
Caltrans	KEITH G. MEYER	CHECKED BY	N. MUMMANENI
			CHUONG NGUYEN
			DATE REVISED

LAYOUT
SCALE: 1" = 50' **L-8**

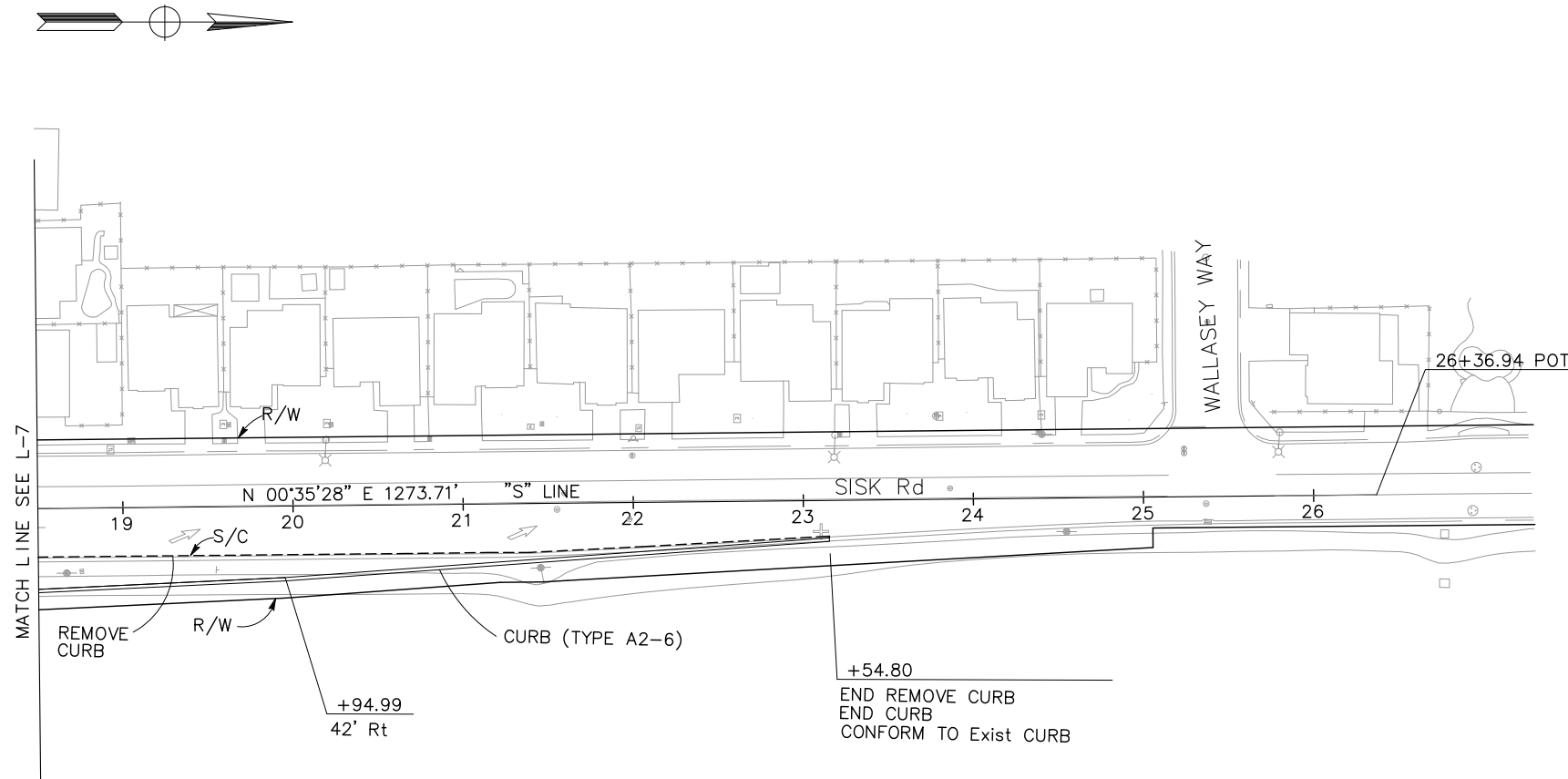
LAST REVISION 06-08-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	24	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA	REGISTERED PROFESSIONAL ENGINEER
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
CALCULATED-DRAWN BY: N MUMMANENI
CHECKED BY: CHUONG NGUYEN
REVISED BY: []
DATE REVISED: []



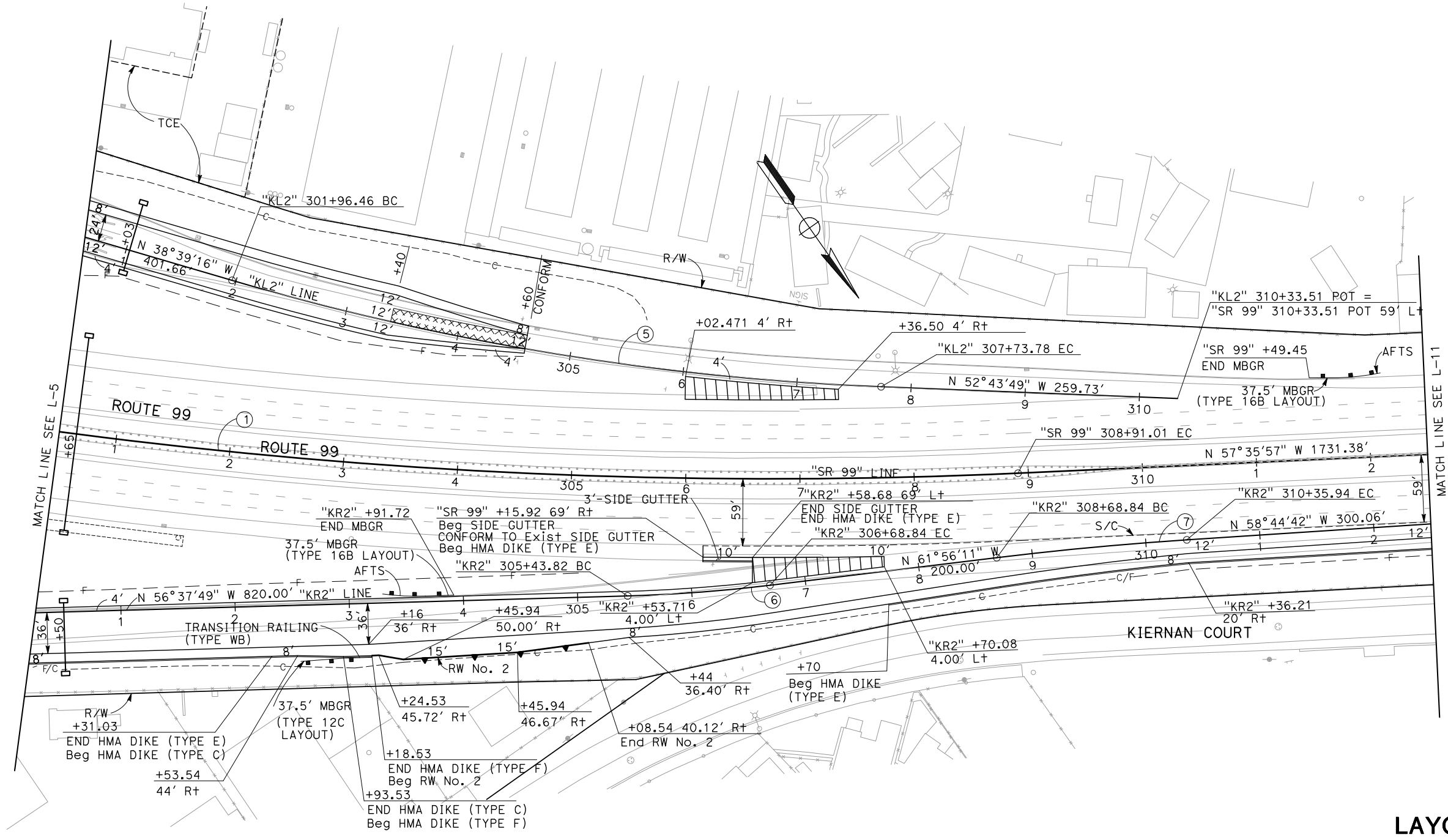
LAYOUT
SCALE: 1" = 50'
L-9

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	25	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER DATE 05-18-12	
9/04/12 PLANS APPROVAL DATE	
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
①	4,750.00'	15°59'12"	667.00'	1325.34'	2077836.67	6390188.78
⑤	2,350'	14°04'33"	290.12'	577.32'	2079846.34	6391365.70
⑥	1,350.00'	5°18'21"	62.55'	124.02'	2080611.73	6392333.00
⑦	3,000.00'	03°11'29"	83.57'	167.10'	2084544.36	6394202.99



LAYOUT
 SCALE: 1" = 50'
L-10

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANENT
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED: 06-08-12

LAST REVISION
 06-08-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

REVISIONS:
 x
 x
 x
 x
 x
 x

STATE OF CALIFORNIA – DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DRAWING BY
 DESIGNED BY
 CHECKED BY
 N MUMMANENI
 CHUONG NGUYEN
 REVISOR
 DATE
 REVISED BY
 DATE REVISED
 CHUONG NGUYEN

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	26	423

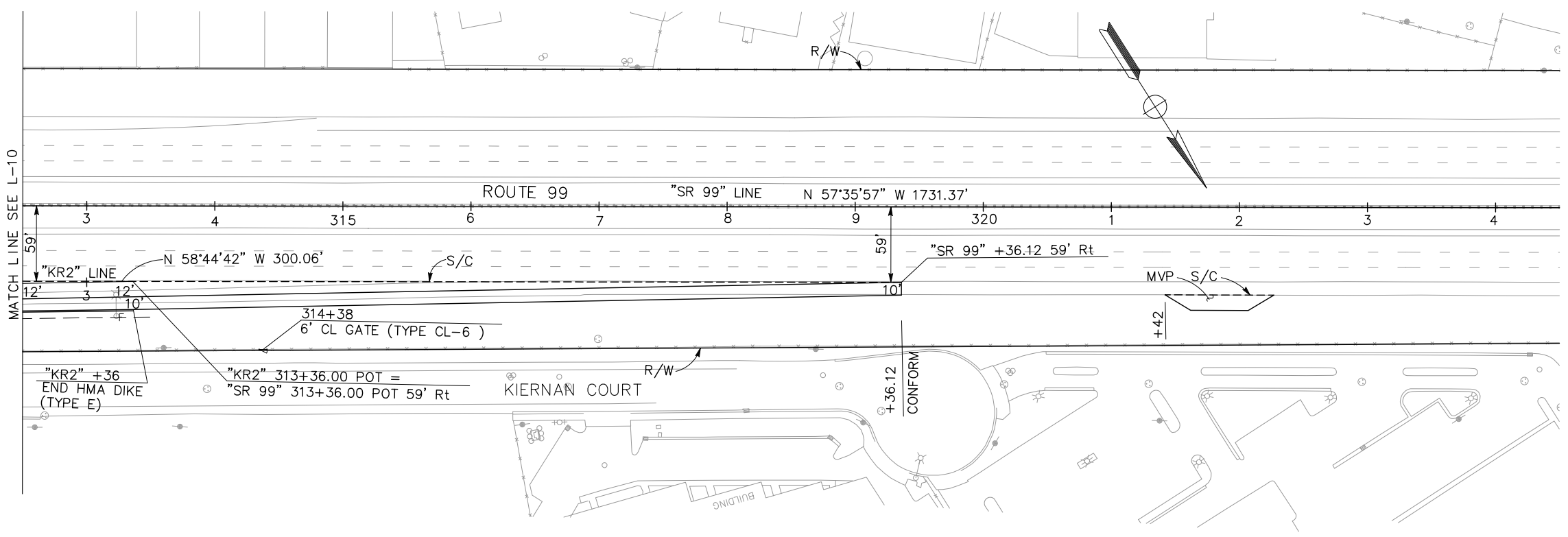
Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12

9/04/12
 PLANS APPROVAL DATE

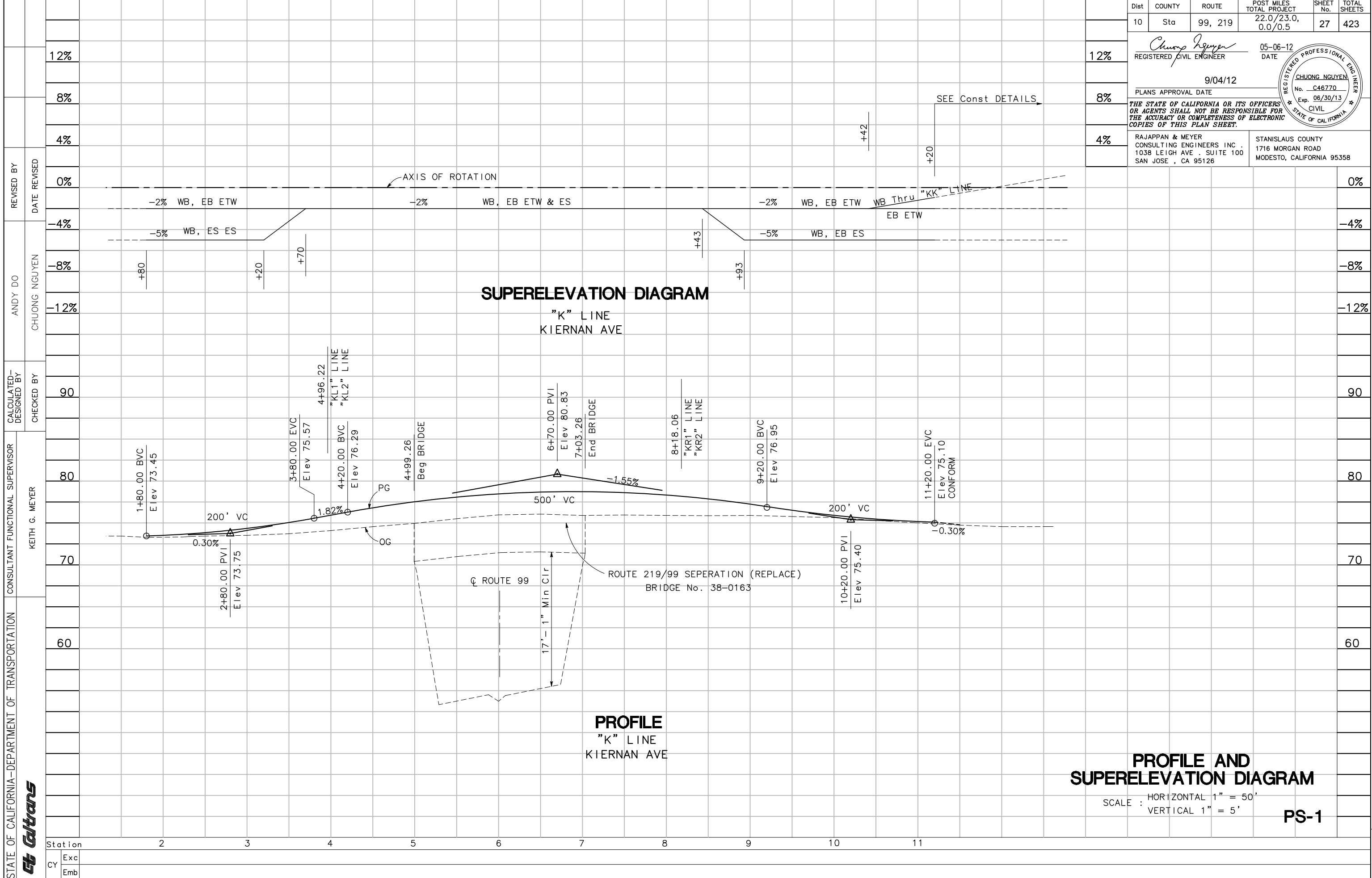
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OR COMPLETENESS OF SCANNED
 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--



LAYOUT
 SCALE: 1" = 50' **L-11**



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	27	423

REGISTERED CIVIL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS INC.
 1038 LEIGH AVE. SUITE 100
 SAN JOSE, CA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

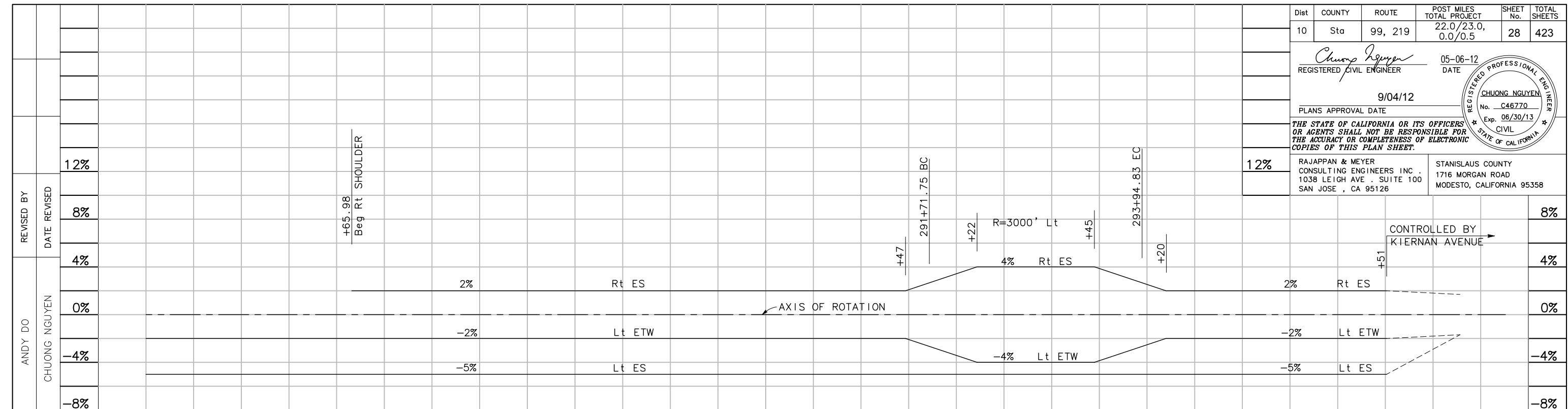
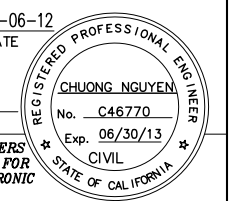
PROFILE AND SUPERELEVATION DIAGRAM

SCALE : HORIZONTAL 1" = 50'
 VERTICAL 1" = 5'

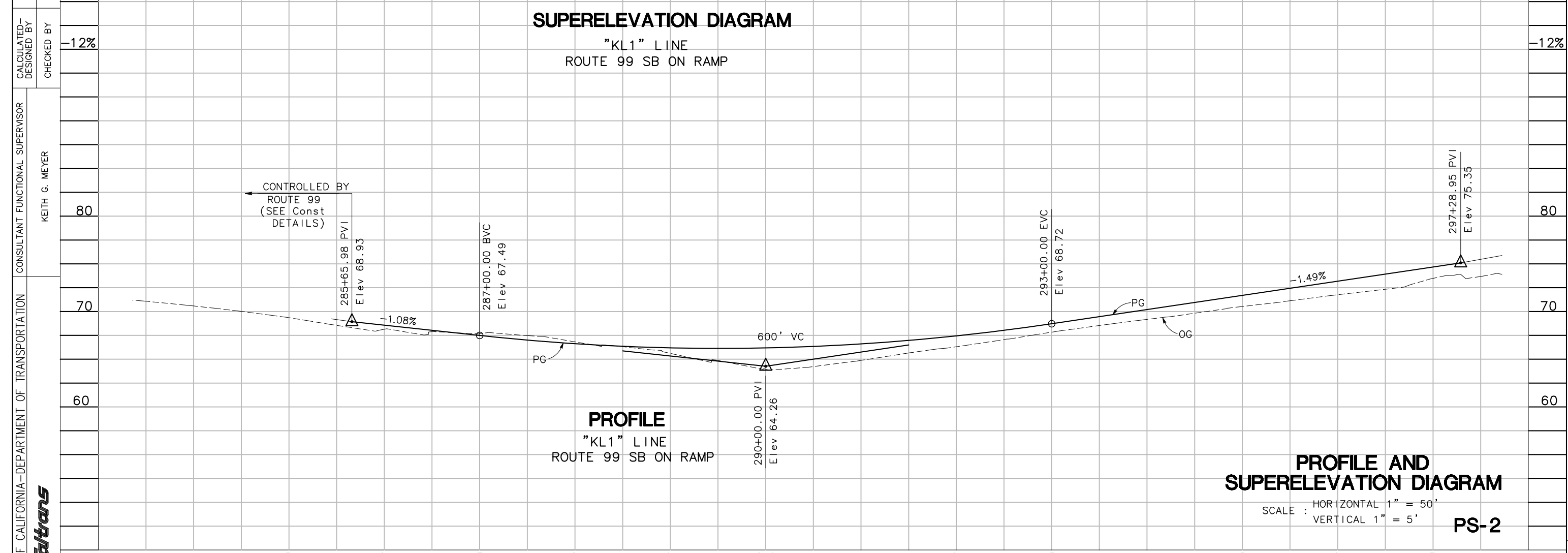
PS-1

LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	28	423
			05-06-12		
REGISTERED CIVIL ENGINEER			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE. SUITE 100 SAN JOSE, CA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



SUPERELEVATION DIAGRAM
"KL1" LINE
ROUTE 99 SB ON RAMP



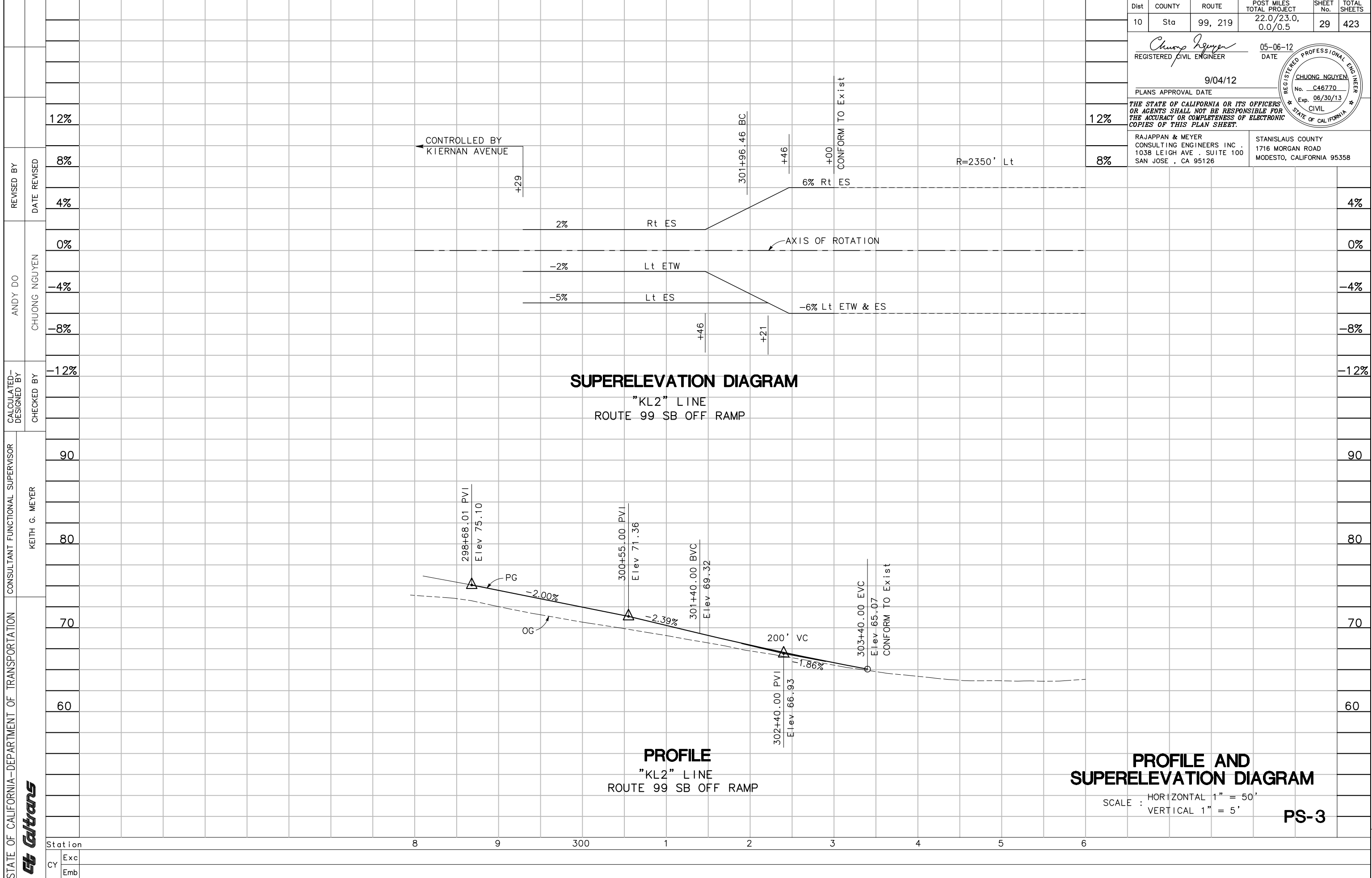
PROFILE
"KL1" LINE
ROUTE 99 SB ON RAMP

PROFILE AND SUPERELEVATION DIAGRAM

SCALE : HORIZONTAL 1" = 50'
VERTICAL 1" = 5'

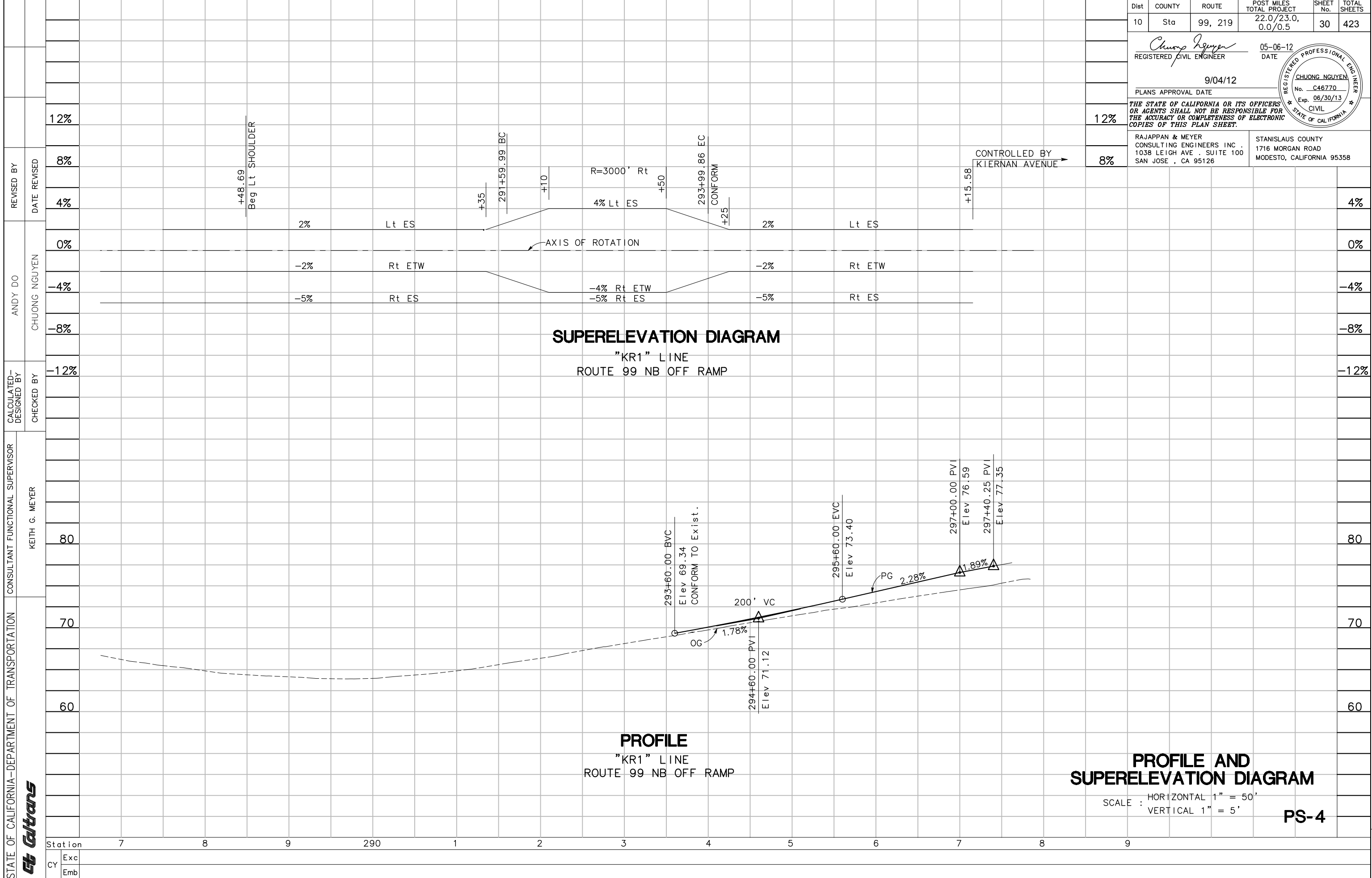
PS-2

Station	4	5	6	7	8	9	290	1	2	3	4	5	6	7
CY	Exc													
	Emb													



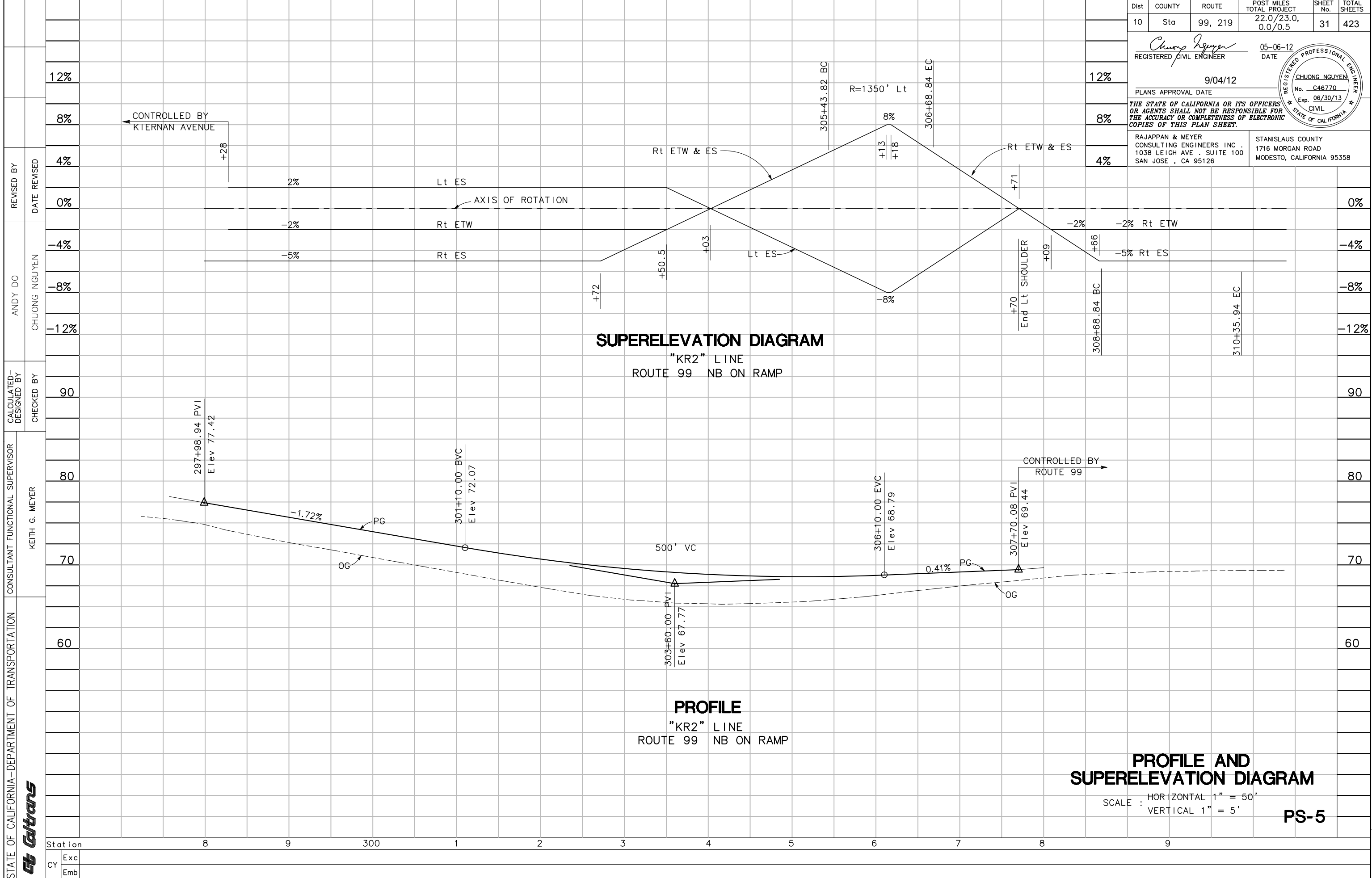
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	29	423
REGISTERED CIVIL ENGINEER <i>Chuong Nguyen</i> 9/04/12 DATE			REGISTERED PROFESSIONAL ENGINEER CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA		
PLANS APPROVAL DATE			9/04/12		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.			RAJAPPAN & MEYER CONSULTING ENGINEERS INC . 1038 LEIGH AVE . SUITE 100 SAN JOSE , CA 95126		
STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358					

REVISOR	REVISION	DATE
ANDY DO	CHUONG NGUYEN	
CALCULATED-DESIGNED BY	CHECKED BY	
KEITH G. MEYER		
CONSULTANT FUNCTIONAL SUPERVISOR		
STATE OF CALIFORNIA-DEPARTMENT OF TRANSPORTATION		
Exc	Emb	

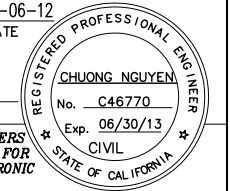


Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	30	423
REGISTERED CIVIL ENGINEER <i>Chuong Nguyen</i> DATE 05-06-12 9/04/12 PLANS APPROVAL DATE			REGISTERED PROFESSIONAL ENGINEER CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA		
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE. SUITE 100 SAN JOSE, CA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

REVISED BY	DATE	REVISION
	12%	
ANDY DO	CHUONG NGUYEN	
	0%	
CALCULATED-DESIGNED BY	CHECKED BY	
	4%	
CONSULTANT FUNCTIONAL SUPERVISOR	KEITH G. MEYER	
	8%	
STATE OF CALIFORNIA-DEPARTMENT OF TRANSPORTATION		
	8%	
CY	Exc	
	Emb	
Station	7	8
	9	290
	1	2
	3	4
	5	6
	7	8
	9	



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	31	423
			REGISTERED CIVIL ENGINEER	DATE	
			9/04/12		
PLANS APPROVAL DATE					
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE. SUITE 100 SAN JOSE, CA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	ANDY DO	REVISOR	DATE
Keith G. Meyer	90	Chuong Nguyen	12%		
			8%		
			4%		
			0%		
			-4%		
			-8%		
			-12%		

SUPERELEVATION DIAGRAM
 "KR2" LINE
 ROUTE 99 NB ON RAMP

PROFILE
 "KR2" LINE
 ROUTE 99 NB ON RAMP

PROFILE AND SUPERELEVATION DIAGRAM

SCALE : HORIZONTAL 1" = 50'
 VERTICAL 1" = 5'

PS-5

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DRAWN BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: REVISOR: DATE

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- PAVEMENT ELEVATIONS ARE AT 50 FEET INTERVAL ON STATION UNLESS OTHERWISE NOTED.
- FOR INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
- PAVEMENT ELEVATIONS ON ROUTE 99 ARE BASED ON FUTURE OVERLAY OF 0.55' ON NB 99 AND 0.45' ON SB 99 AS PROVIDED IN CONTRACT 10-0M8004. ENGINEER TO CONFIRM AT TIME OF CONSTRUCTION.

ABBREVIATIONS:

- S/C SAWCUT & CONFORM
 TW TOP OF WALL ELEVATION
 TC TOP OF CURB ELEVATION
 FL FLOWLINE

LEGEND

XX.XX PAVEMENT ELEVATIONS

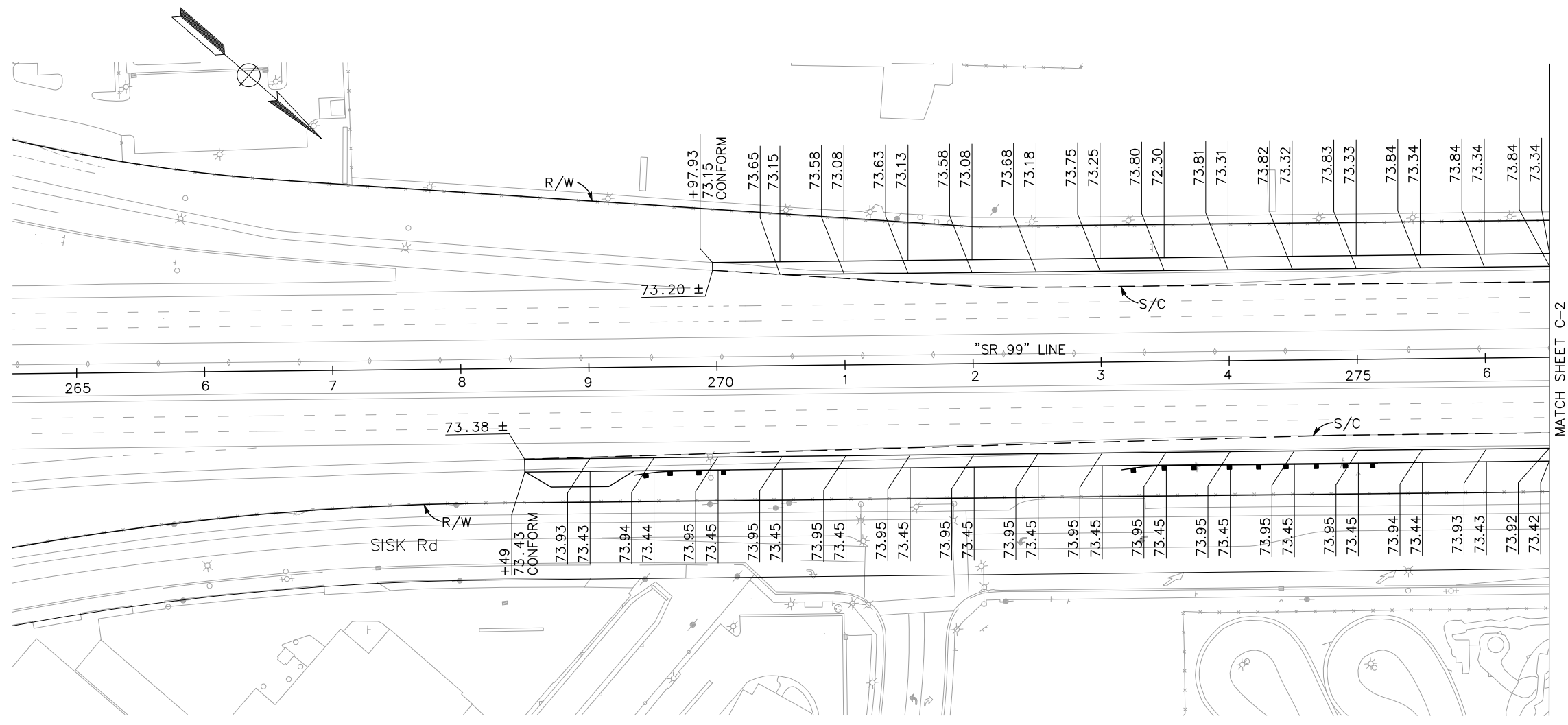
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	32	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA
 REGISTERED PROFESSIONAL ENGINEER

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RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



PAVEMENT ELEVATIONS

MATCH SHEET C-2

CONSTRUCTION DETAILS

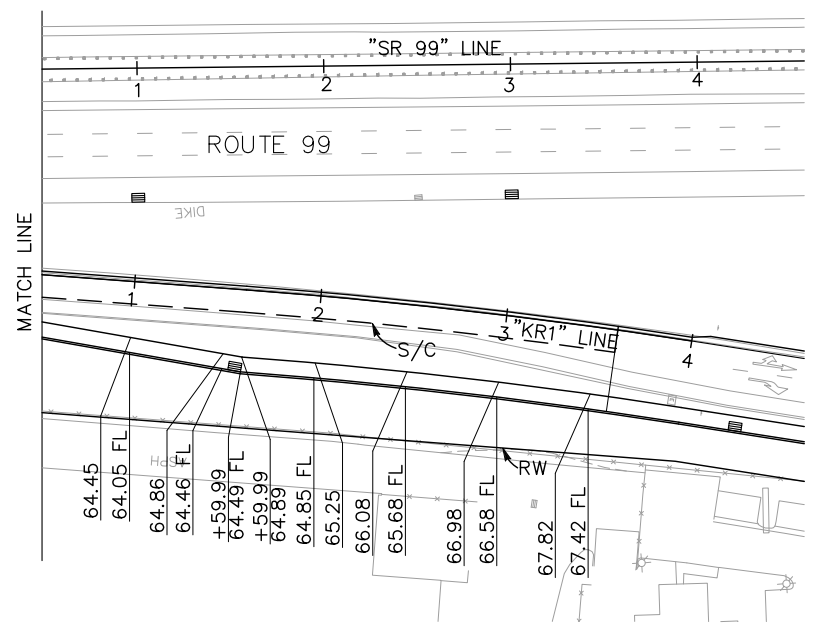
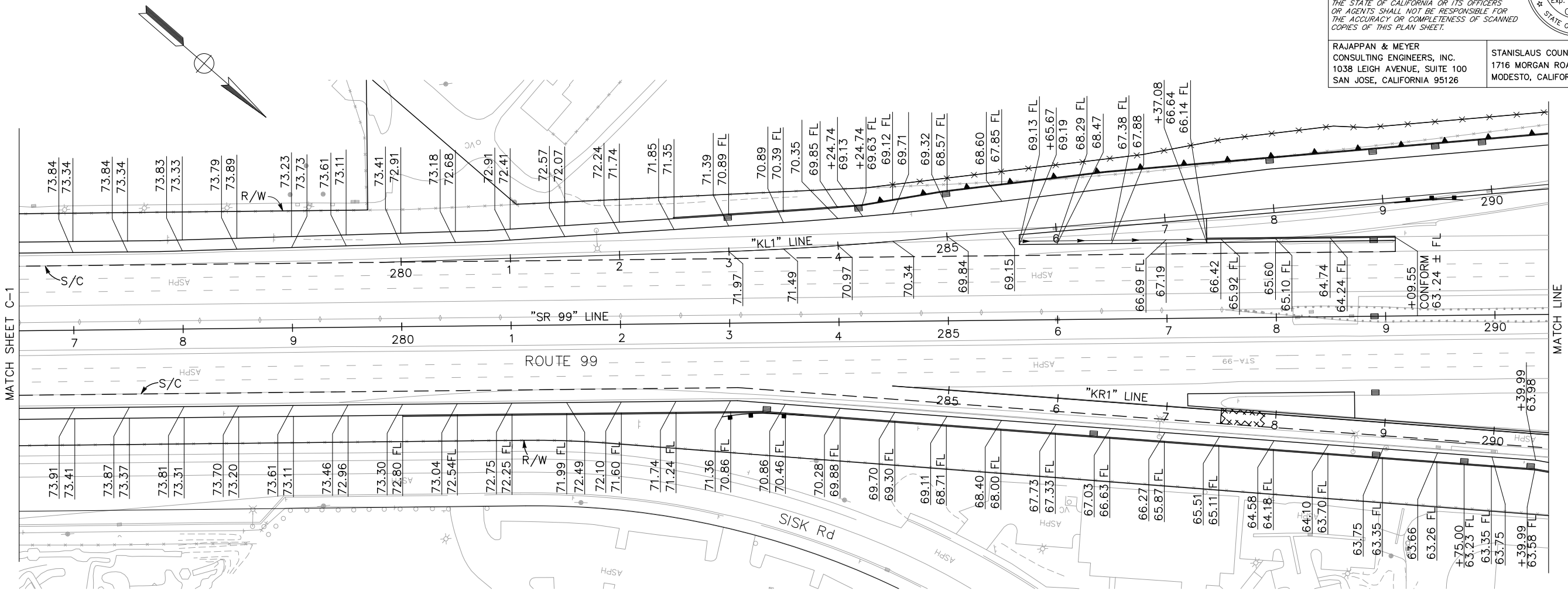
SCALE: 1" = 50' **C-1**

APPROVED FOR PAVEMENT ELEVATIONS ONLY

LAST REVISION 06-08-12 | DATE PLOTTED => \$DATE | TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: CHUONG NGUYEN
 CHECKED BY:
 DAVID LIU
 REVISOR: CHUONG NGUYEN
 DATE REVISED:

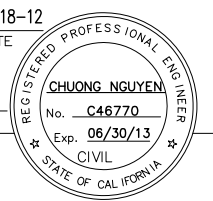
NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



PAVEMENT ELEVATIONS

APPROVED FOR PAVEMENT ELEVATIONS ONLY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	33	423
REGISTERED CIVIL ENGINEER <i>Chuong Nguyen</i> 9/04/12 PLANS APPROVAL DATE			05-18-12 DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



CONSTRUCTION DETAILS

SCALE: 1" = 50' **C-2**

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

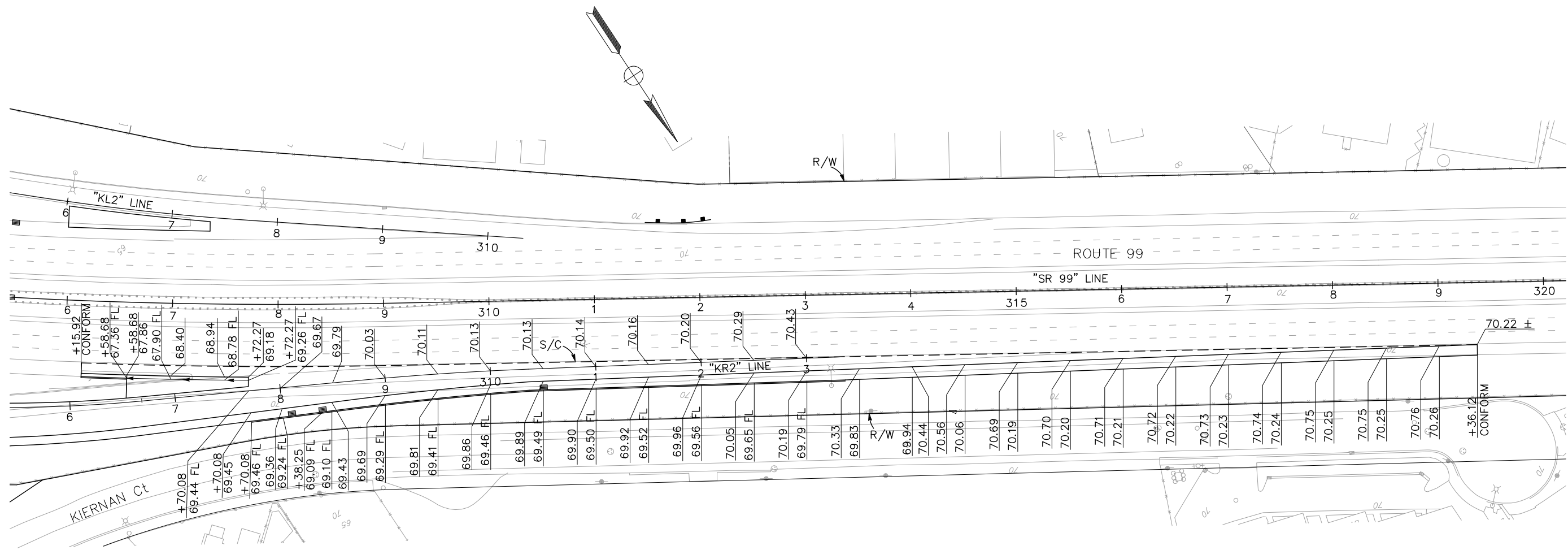
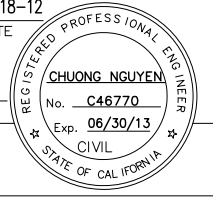
NOTE:

FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	34	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--




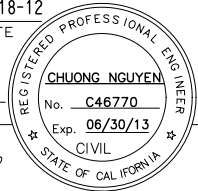
PAVEMENT ELEVATIONS

APPROVED FOR PAVEMENT ELEVATIONS ONLY

CONSTRUCTION DETAILS

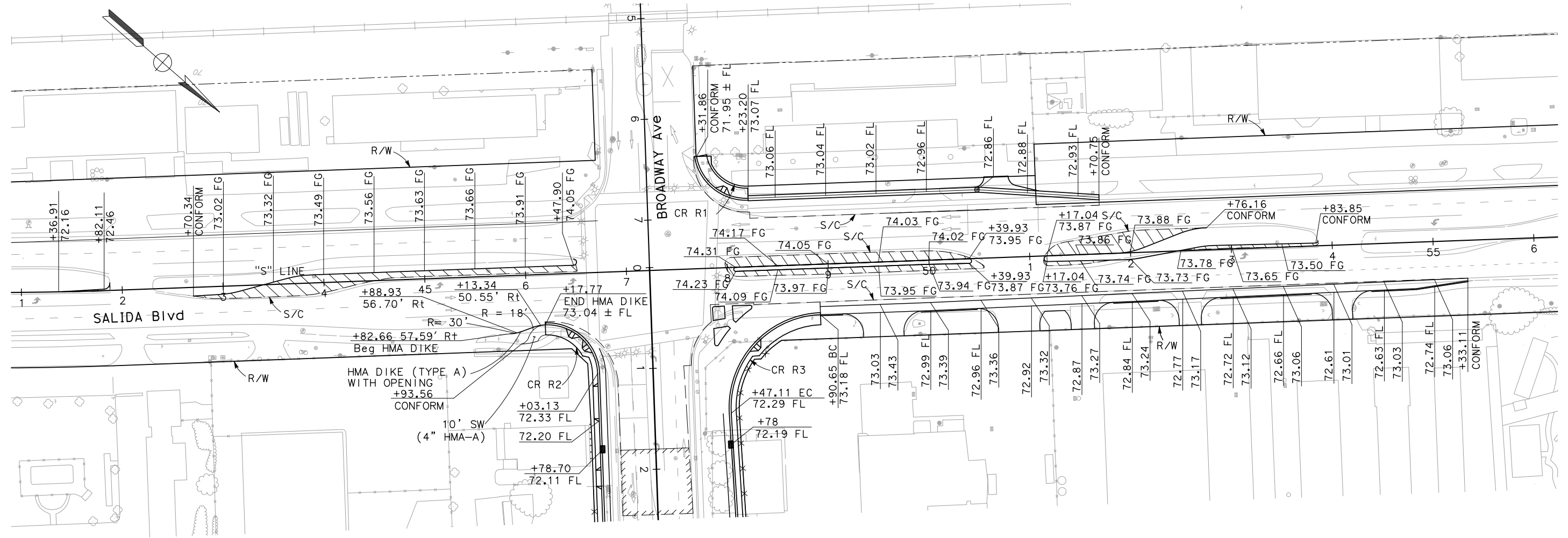
SCALE: 1" = 50' **C-3**

LAST REVISION 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	35	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
PLANS APPROVAL DATE 9/04/12			THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:

FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.




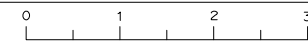
PAVEMENT ELEVATIONS

CONSTRUCTION DETAILS

SCALE: 1" = 50' **C-4**

APPROVED FOR PAVEMENT ELEVATIONS ONLY

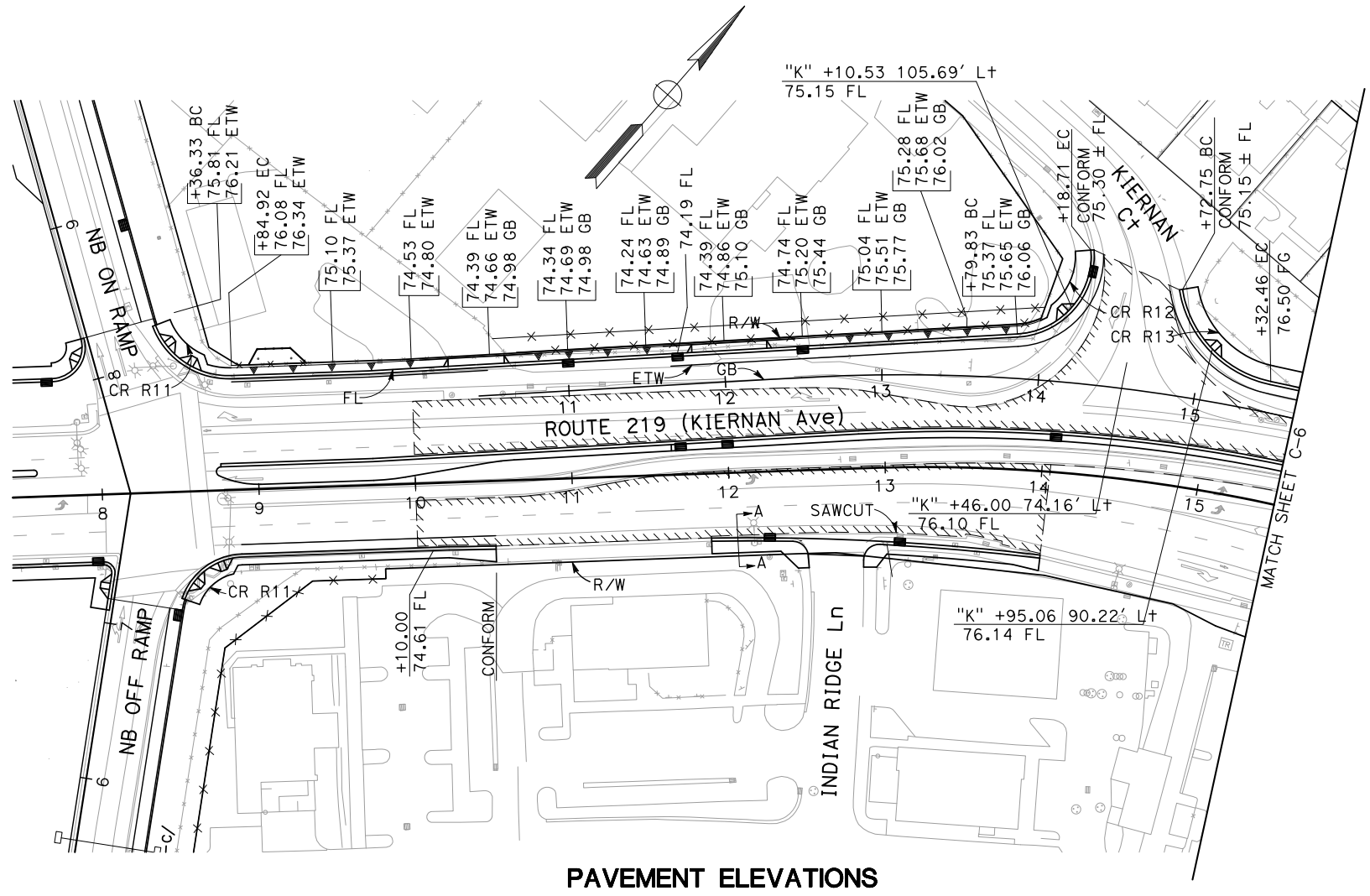
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CHECKED BY: DAVID LIU
 CHUONG NGUYEN
 REVISOR: CHUONG NGUYEN
 DATE REVISOR: 9/04/12



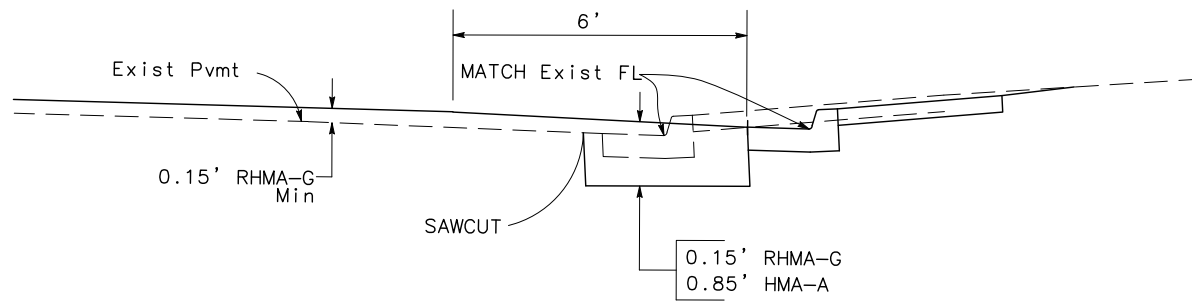
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CHECKED BY
 CHUONG NGUYEN
 DAVID LIU
 REVISIONS:
 REVISION NO. | DATE | BY | DESCRIPTION

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

ABBREVIATION:
 GB GRADE BREAK



PAVEMENT ELEVATIONS



SECTION A-A

APPROVED FOR PAVEMENT ELEVATIONS ONLY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	36	423

9/04/12
 REGISTERED CIVIL ENGINEER DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

CONSTRUCTION DETAILS
 SCALE: 1" = 50' C-5

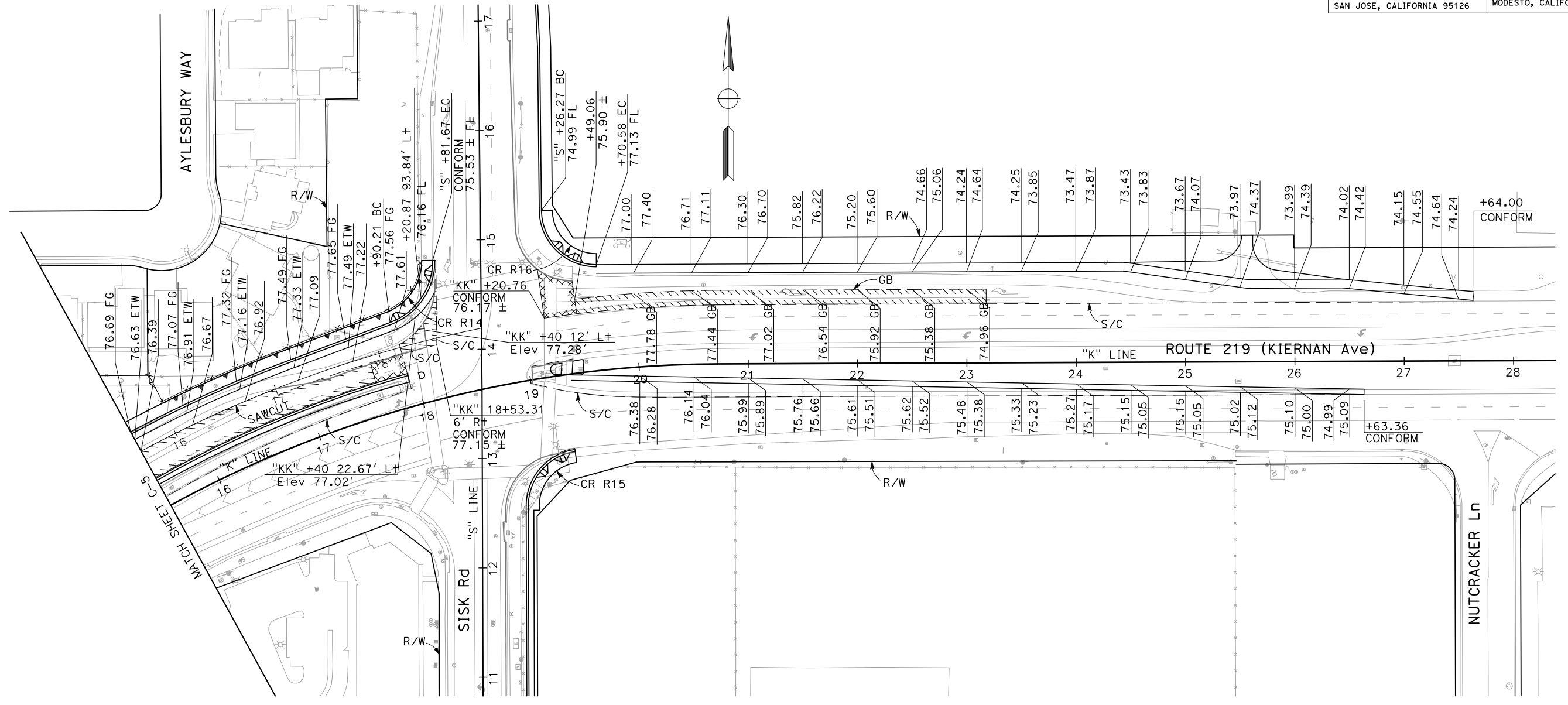
DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DRAWN BY: N. MUMMANENI
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: REVISOR: N. MUMMANENI, DATE: CHUONG NGUYEN, DATE: CHUONG NGUYEN

NOTE:
 1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
 2. PAVEMENT ELEVATIONS ARE AT 50 FEET WITH RESPECT TO "K" LINE UNLESS OTHERWISE NOTED.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	37	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen* 05-18-12
 DATE: 9/04/12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN, No. C46770, Exp. 06/30/13, CIVIL, STATE OF CALIFORNIA
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



PAVEMENT ELEVATIONS

CONSTRUCTION DETAILS
 SCALE: 1" = 50'
C-6

APPROVED FOR PAVEMENT ELEVATIONS ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 06-08-12

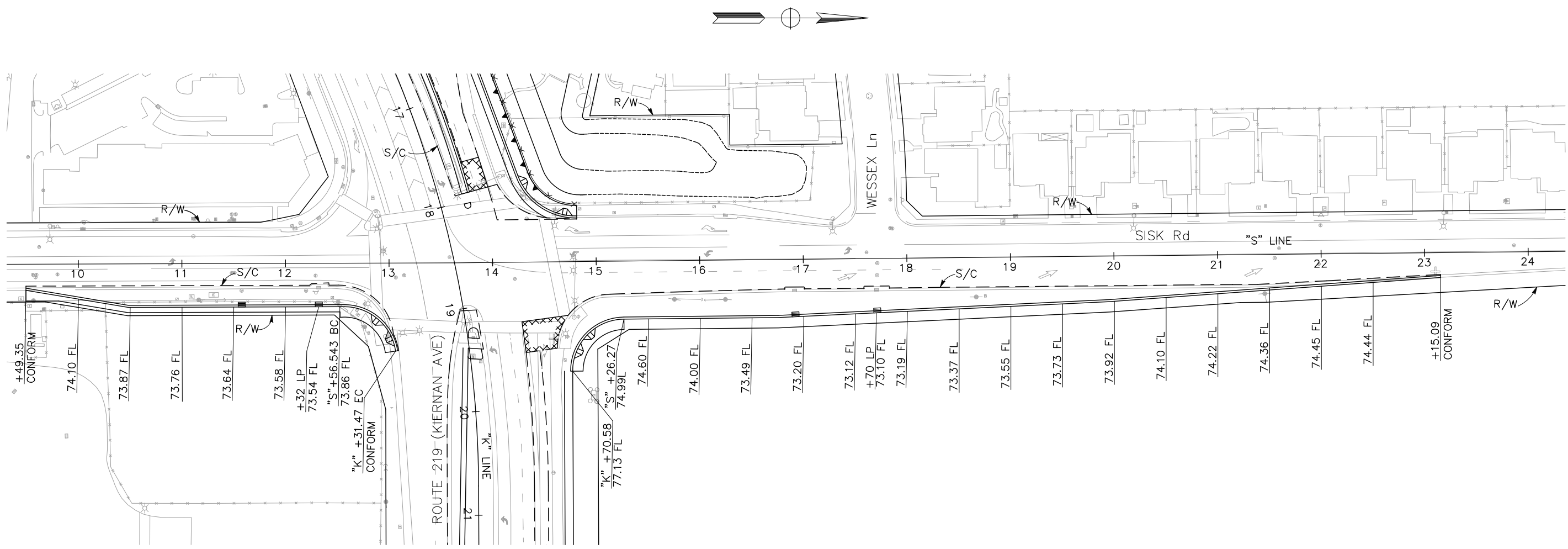
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	38	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:

FOR ACCURATE RIGHT OF WAY AND ACCESS DATA,
CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
Caltrans	KEITH G. MEYER	CHECKED BY	DAVID LIU
			CHUONG NGUYEN
			DATE REVISED



PAVEMENT ELEVATIONS

APPROVED FOR PAVEMENT ELEVATIONS ONLY

CONSTRUCTION DETAILS

SCALE: 1" = 50' **C-7**

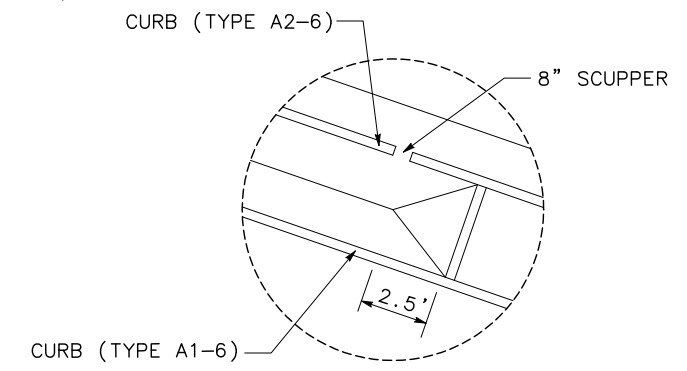
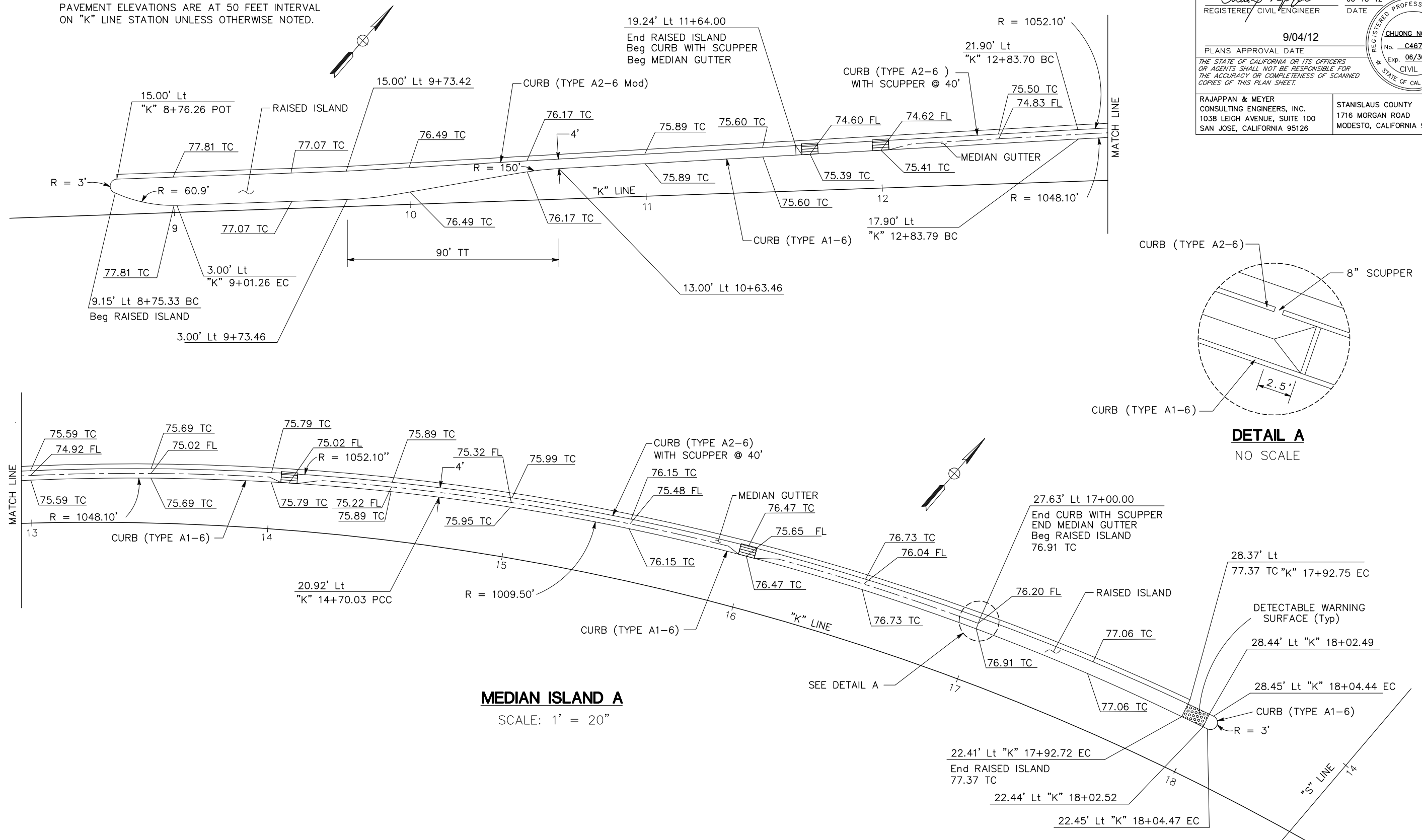
LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	39	423

REGISTERED CIVIL ENGINEER	DATE
CHUONG NGUYEN	05-18-12
PLANS APPROVAL DATE	
9/04/12	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:

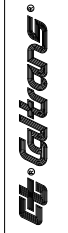
PAVEMENT ELEVATIONS ARE AT 50 FEET INTERVAL ON "K" LINE STATION UNLESS OTHERWISE NOTED.



MEDIAN ISLAND A
SCALE: 1" = 20"

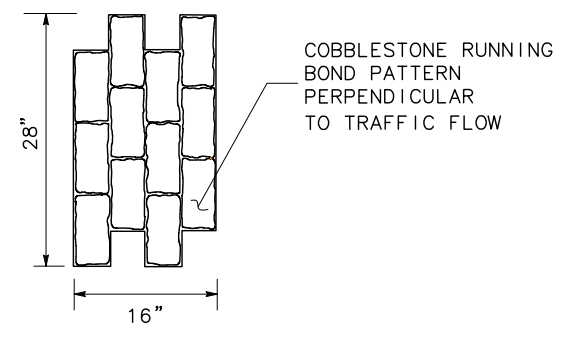
CONSTRUCTION DETAILS
SCALE AS SHOWN **C-8**

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT SUPERVISOR
 KEITH G. MEYER
 REVISED BY
 ANDY CHAN
 DATE REVISION
 CHUONG NGUYEN
 CALCULATED-DESIGNED BY
 CHECKED BY

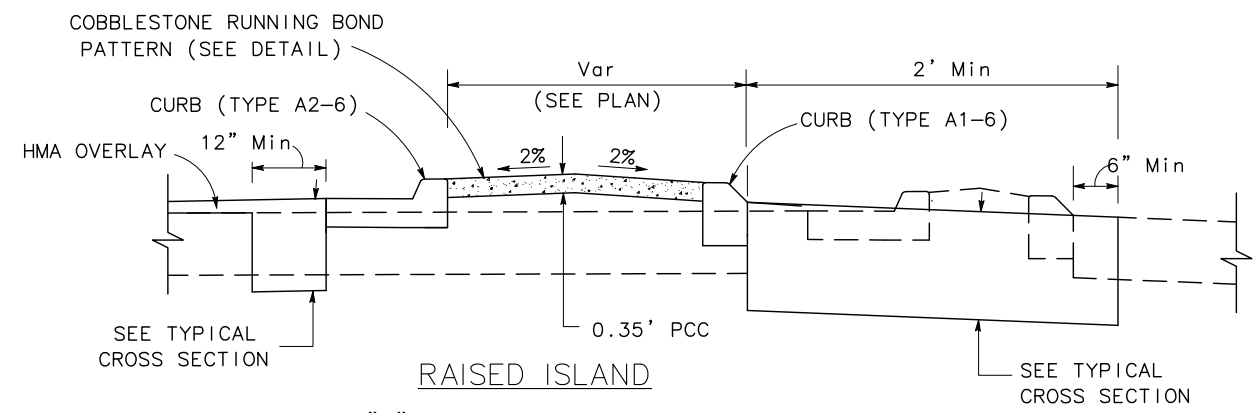


Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	40	423

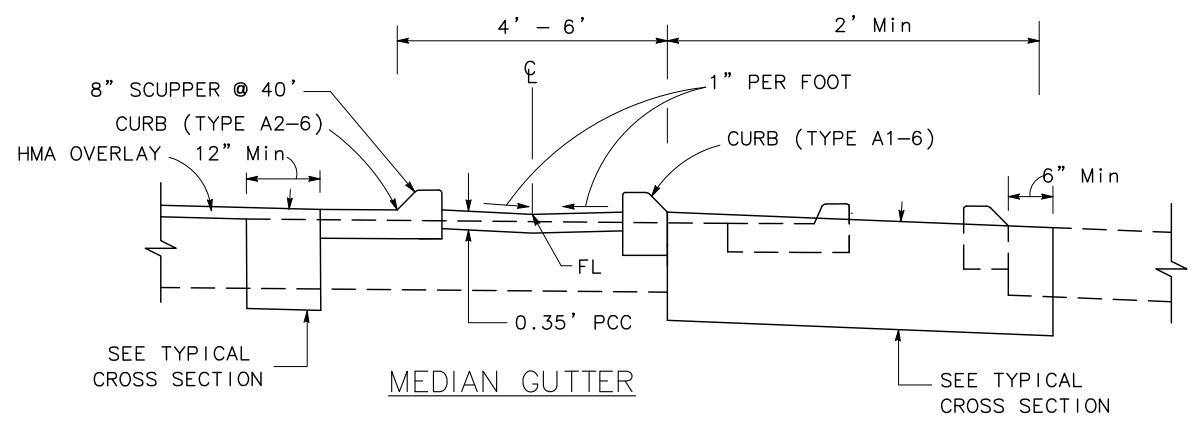
<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



COBBLESTONE RUNNING BOND PATTERN DETAIL

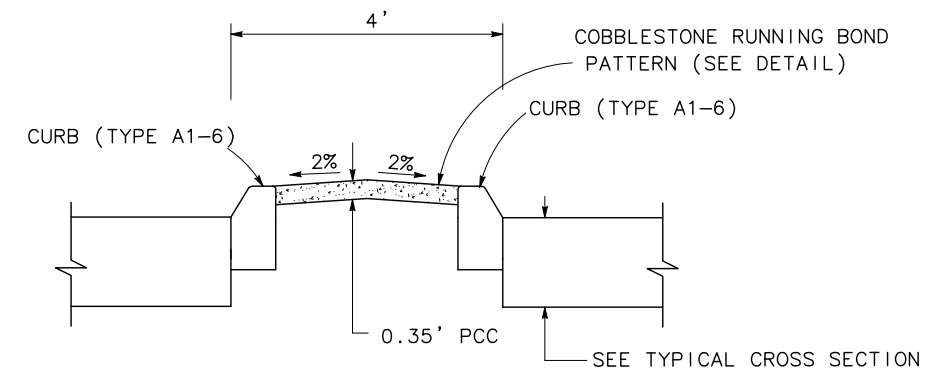


RAISED ISLAND
Sta "K" 10+00.00 TO 11+90.00
Sta "K" 17+00.00 TO 18+02.93



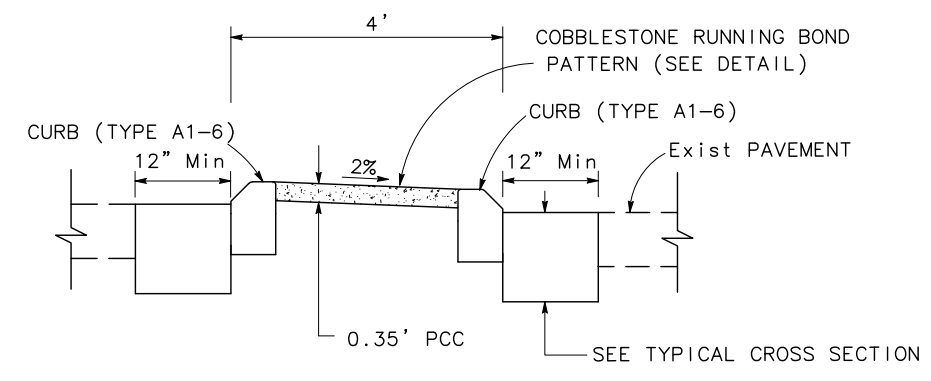
MEDIAN GUTTER
Sta "K" 11+64.00 TO 17+00.00

MEDIAN A



MEDIAN ISLAND

Sta "K" 4+40.40 TO "K" 4+68.87
Sta "K" 7+02.75 TO "K" 7+58.90



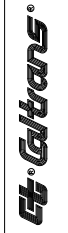
MEDIAN ISLAND

Sta "SA" 48+06.36 TO "SA" 50+42.26

CONSTRUCTION DETAILS

NO SCALE **C-9**

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT SUPERVISOR
 KEITH G. MEYER
 CALCULATED/DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 DAVID LIU
 REVISOR BY
 DATE REVISED

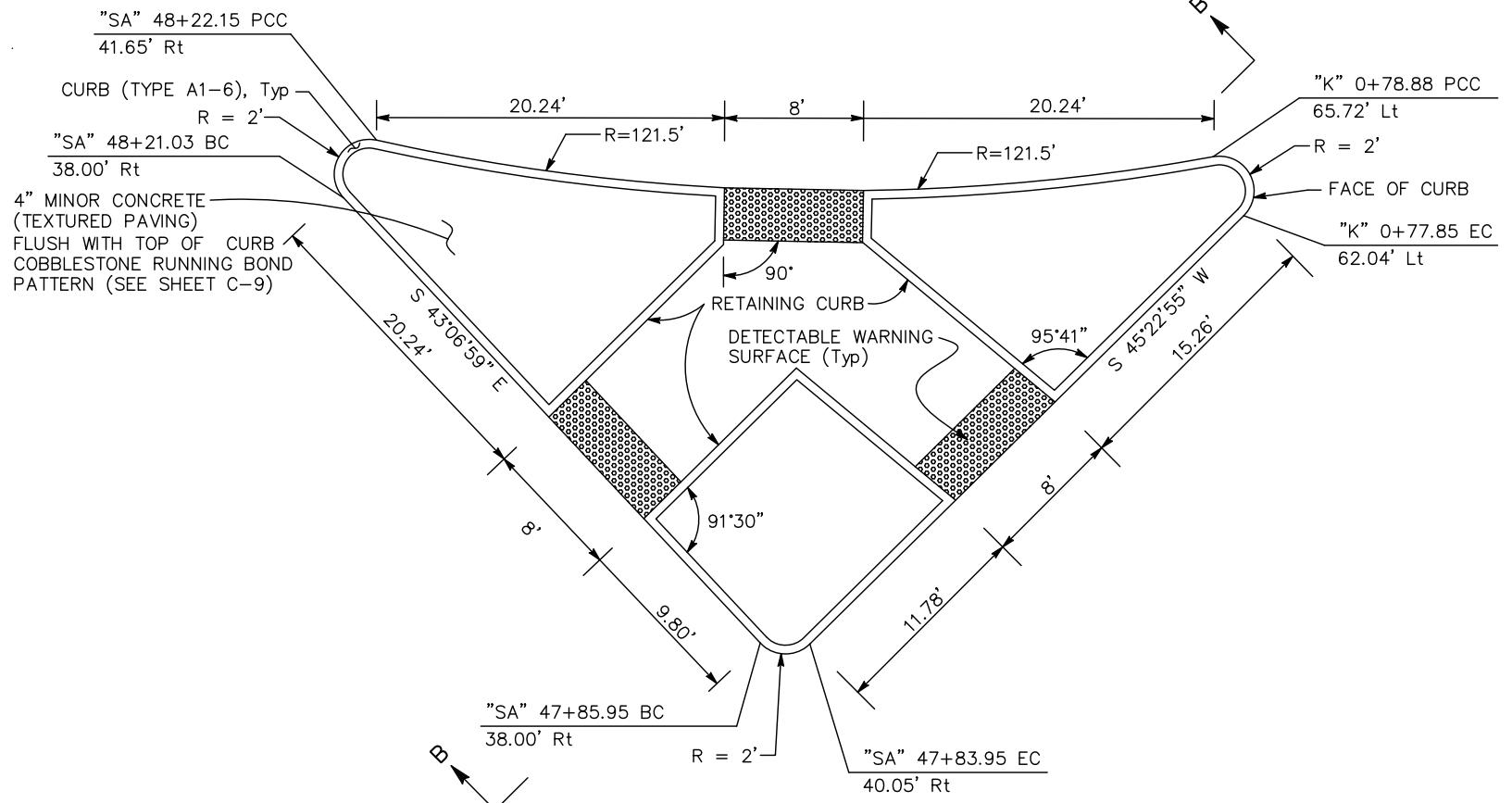


LAST REVISION
 06-08-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

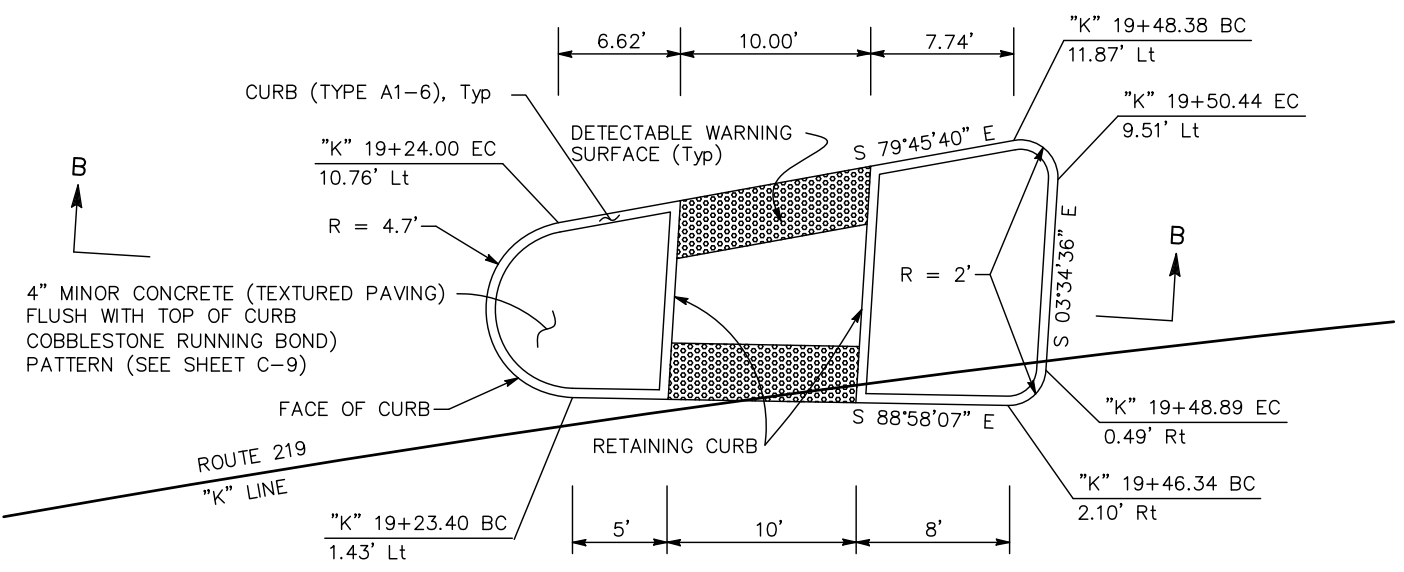
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	41	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA	05-18-12 DATE 9/04/12 PLANS APPROVAL DATE
--	--

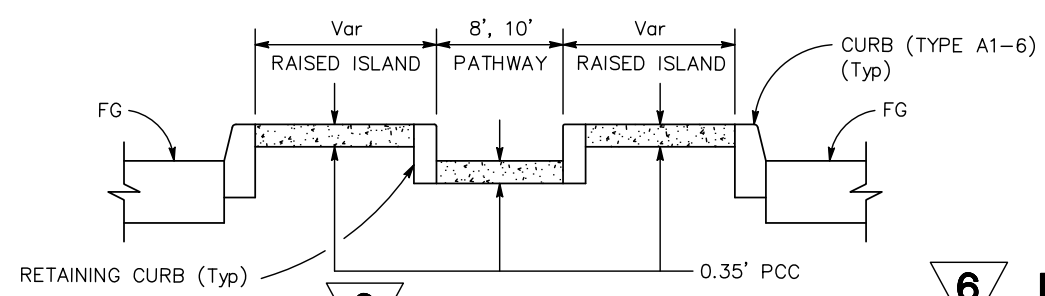
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--



ISLAND B

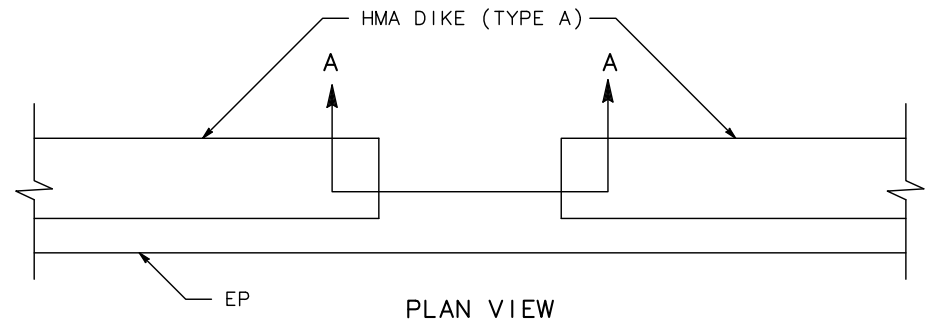


ISLAND C

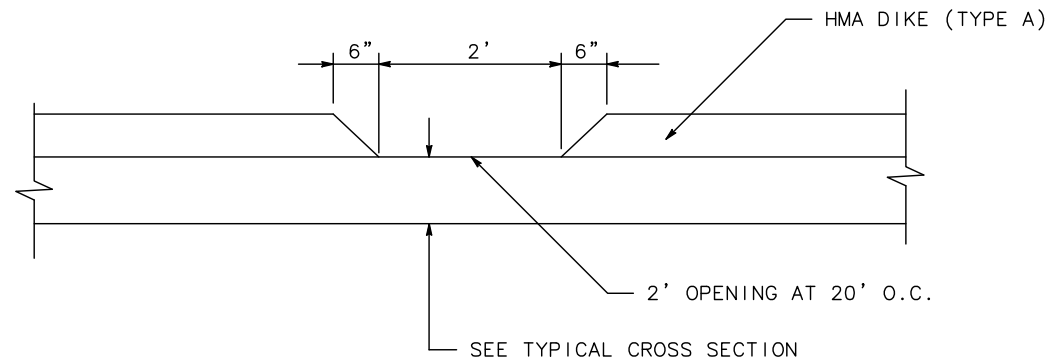


SECTION B-B

- NOTES:**
- HMA DIKE LOCATIONS SHOWN ON LAYOUT SHEETS.
 - 2' OPENINGS AT 20' CENTERS TO ALLOW FOR DRAINAGE.



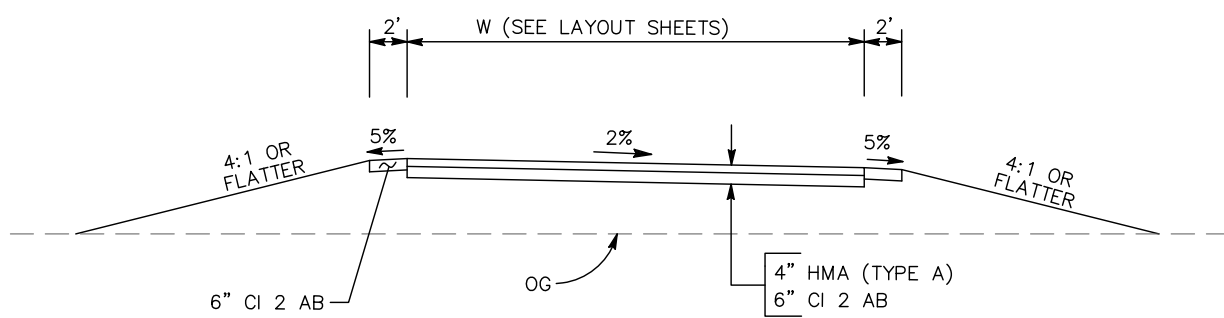
PLAN VIEW



SECTION A-A

HMA DIKE (TYPE A) WITH OPENING DETAIL

Sta "SA" 48+90.05 TO 55+33.12



HMA DRIVEWAY

CONSTRUCTION DETAILS


SCALE: 1" = 5' **C-10**

REVISION 6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT SUPERVISOR: KEITH G. MEYER
 REVISOR: DAVID LIU
 DESIGNER: CHUONG NGUYEN

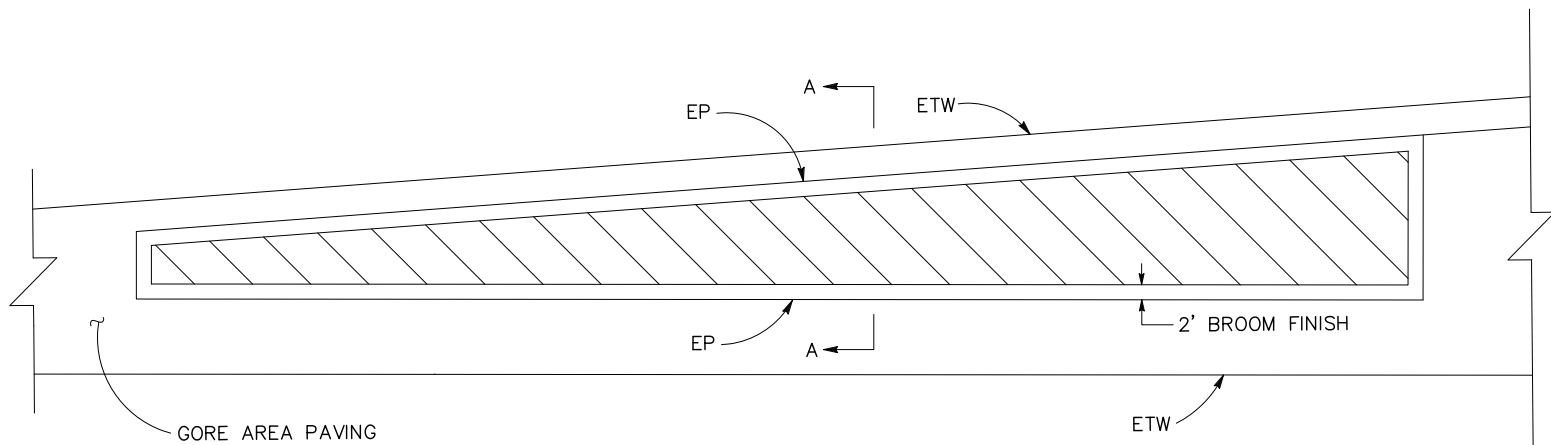
DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	42	423

 REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

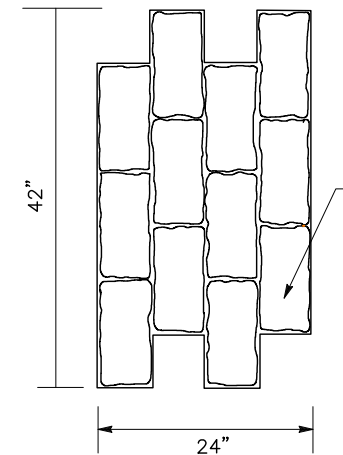
LEGEND

 MINOR CONCRETE (TEXTURED PAVING)

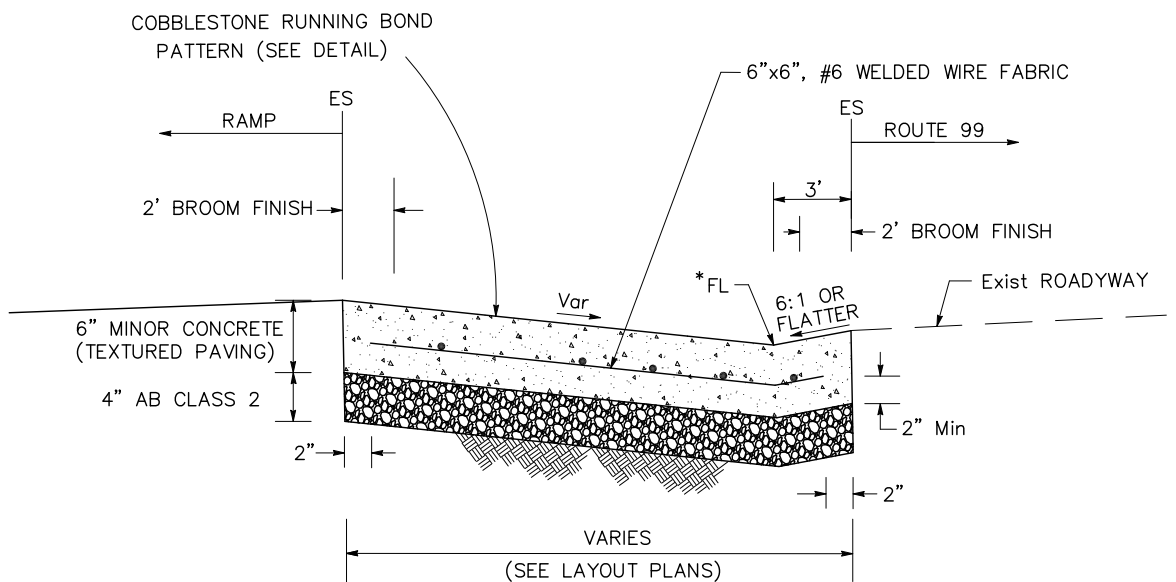


SECTION A-A

NOTES:
 1. MINOR CONCRETE (TEXTURED PAVING) TO INCLUDE SAND COLOR HARDENER AND RUST BROWN RELEASE AGENT.

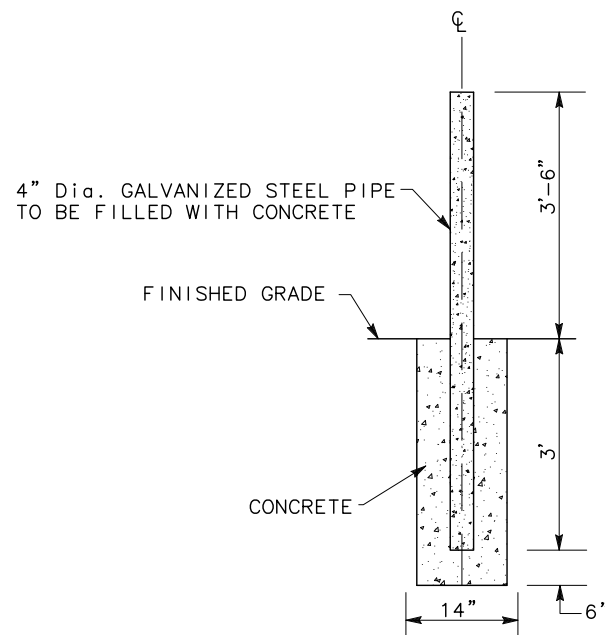


COBBLESTONE RUNNING BOND PATTERN DETAIL

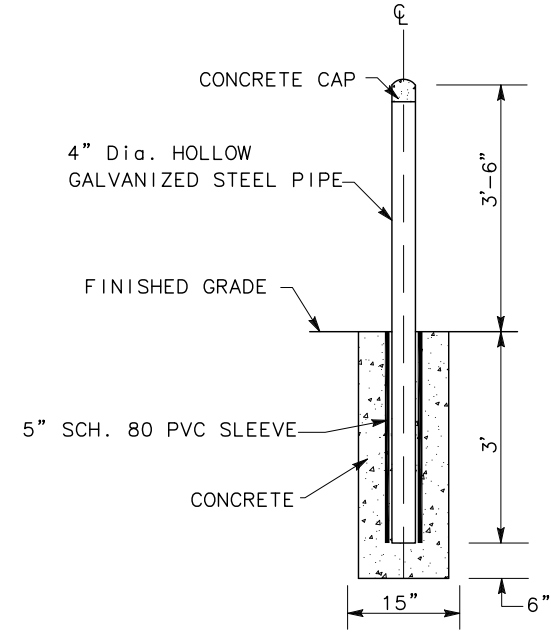


AT SB OFF-RAMP
 SB ON-RAMP
 NB OFF-RAMP
 NB ON-RAMP

* FLOW LINE AT NB OFF RAMP AND SB OFF RAMP SHOULD MATCH EXISTING FLOWLINE ELEVATIONS. SEE PAVEMENT ELEVATION FOR FLOW LINE AT NB ON RAMP AND SB ON RAMP.



STATIONARY POST



REMOVABLE POST

BOLLARD DETAIL

CONSTRUCTION DETAILS

NO SCALE

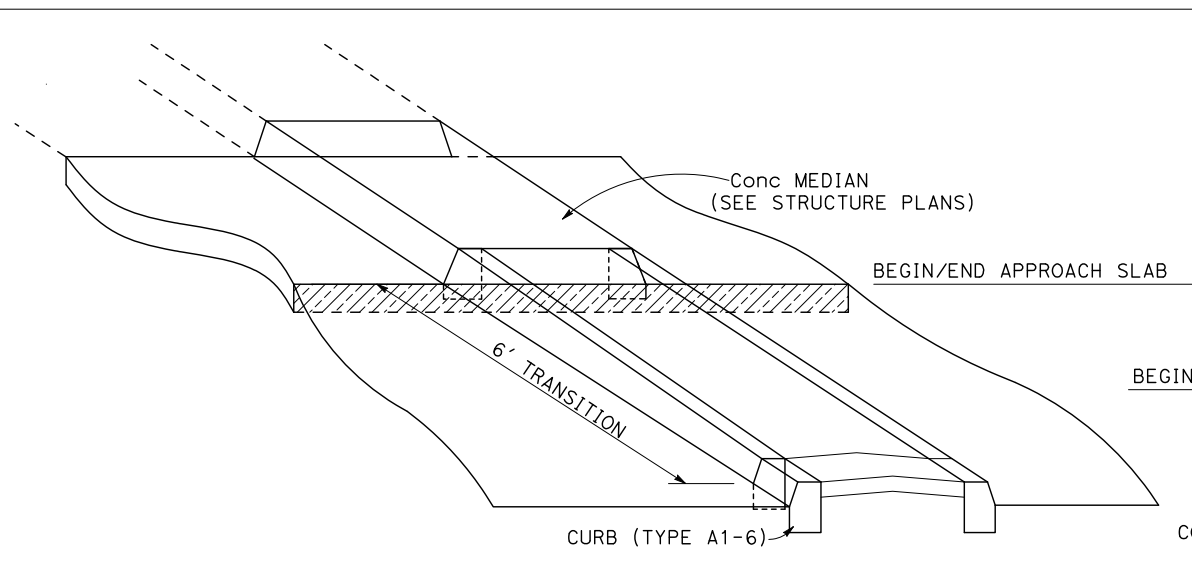
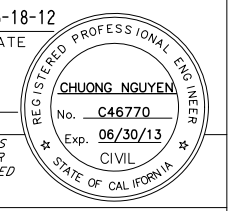
C-11

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISED BY
 ANDY CHAN
 CHUONG NGUYEN
 CALCULATED-DESIGNED BY
 CHECKED BY
 \$TRANS

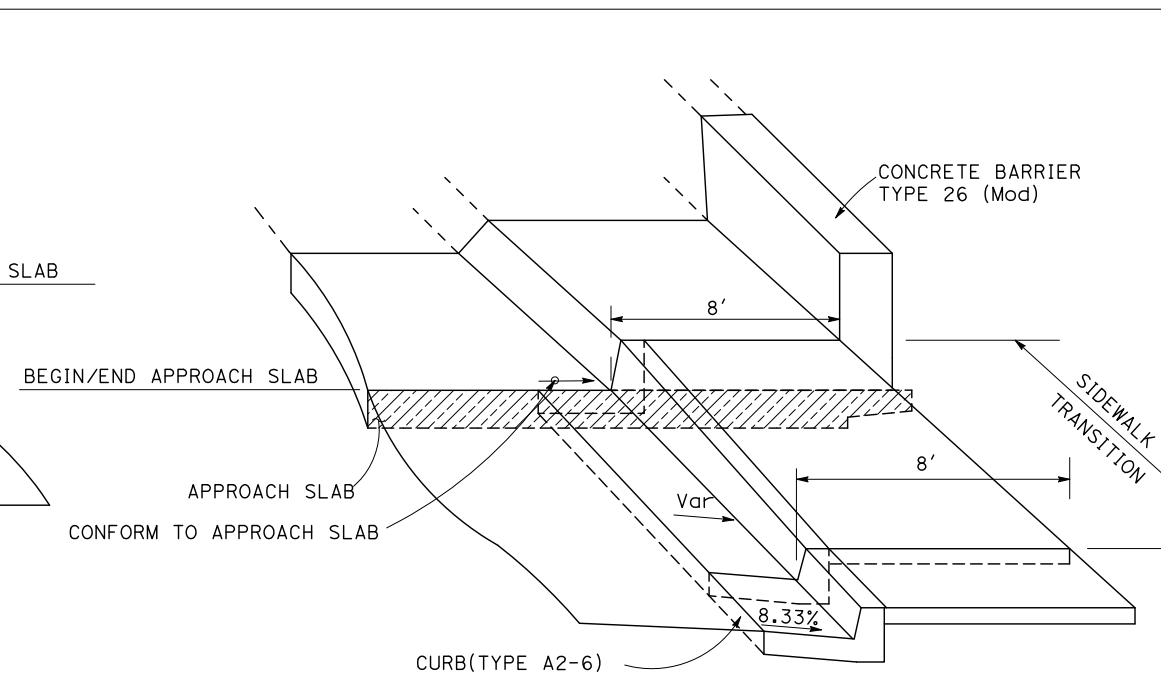
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	43	423

REGISTERED CIVIL ENGINEER	DATE
9/04/12	05-18-12
PLANS APPROVAL DATE	

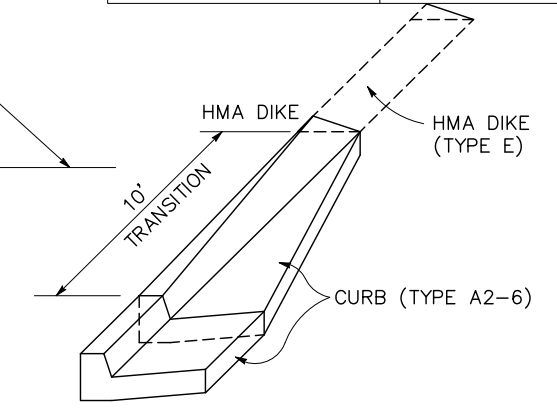
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
---	--



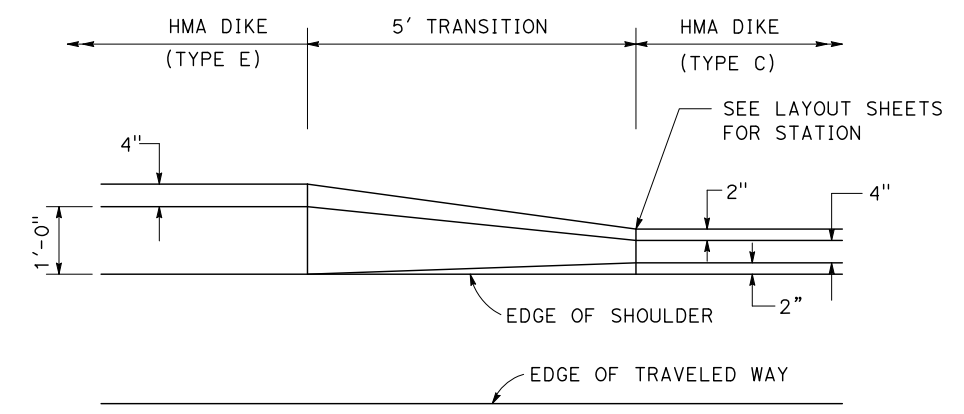
3 CURB TRANSITION DETAIL AT BRIDGE



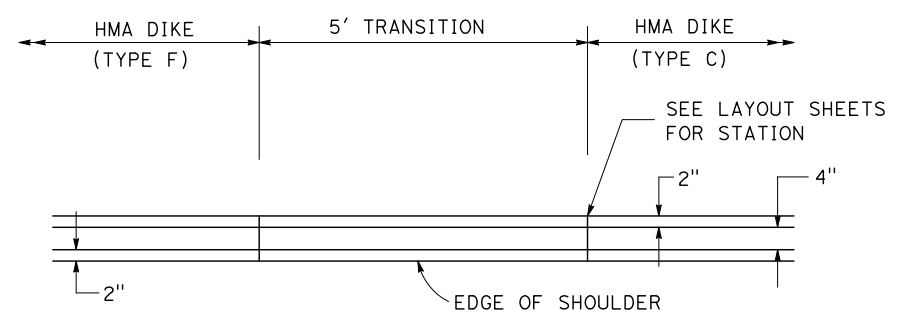
3 SIDEWALK TRANSITION DETAIL AT BRIDGE



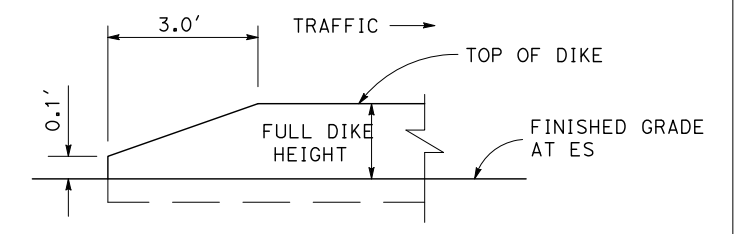
TRANSITION FROM CURB (TYPE A2-6) TO HMA DIKE (TYPE E)



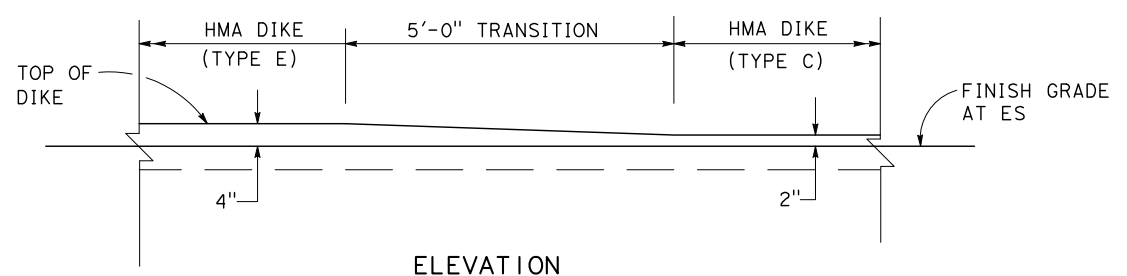
PLAN



PLAN

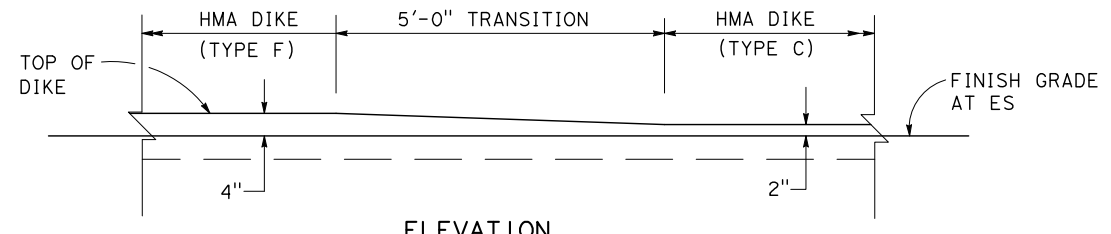


DIKE APPROACH TRANSITION



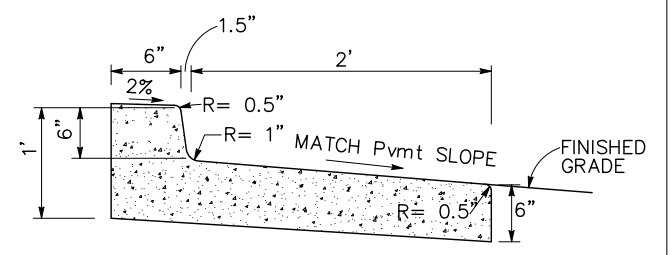
ELEVATION

TRANSITION FROM HMA DIKE TYPE E TO TYPE C



ELEVATION

TRANSITION FROM HMA DIKE TYPE F TO TYPE C



CURB (TYPE A2-6 MOD)

CONSTRUCTION DETAILS

NO SCALE **C-12**

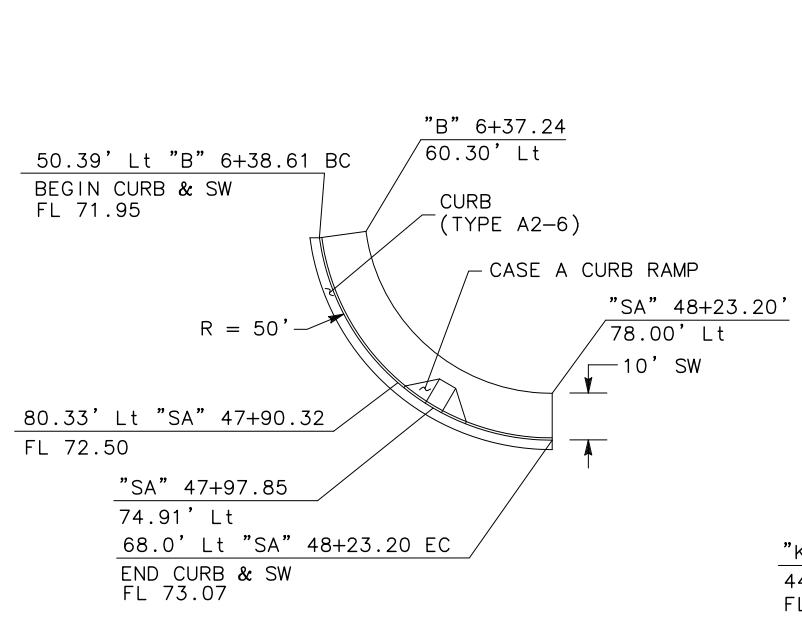
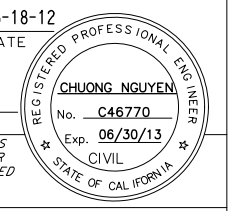
3 REVISED PER ADDENDUM No. 3 DATED OCTOBER 10, 2012

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED/DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 DAVID LIU
 REVISOR
 DATE
 REVISIONS
 DATE
 REVISIONS

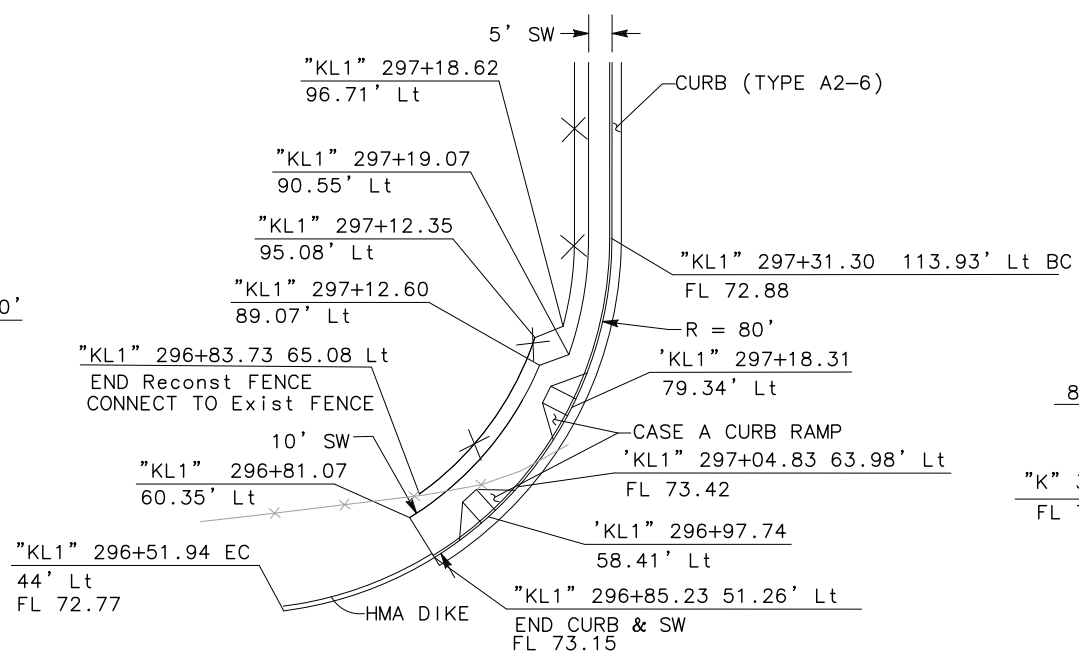
LAST REVISION DATE PLOTTED => \$DATE
 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	44	423

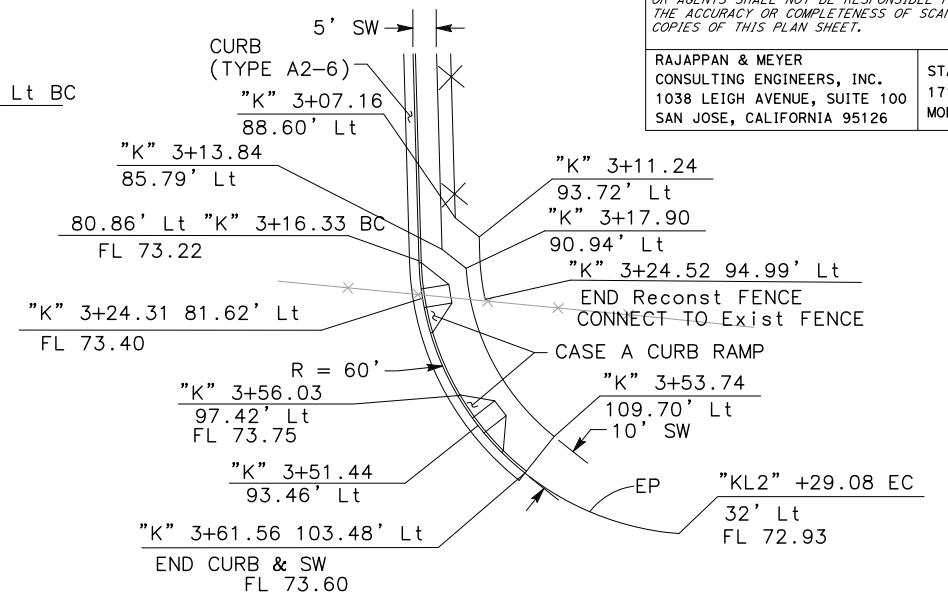
REGISTERED CIVIL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



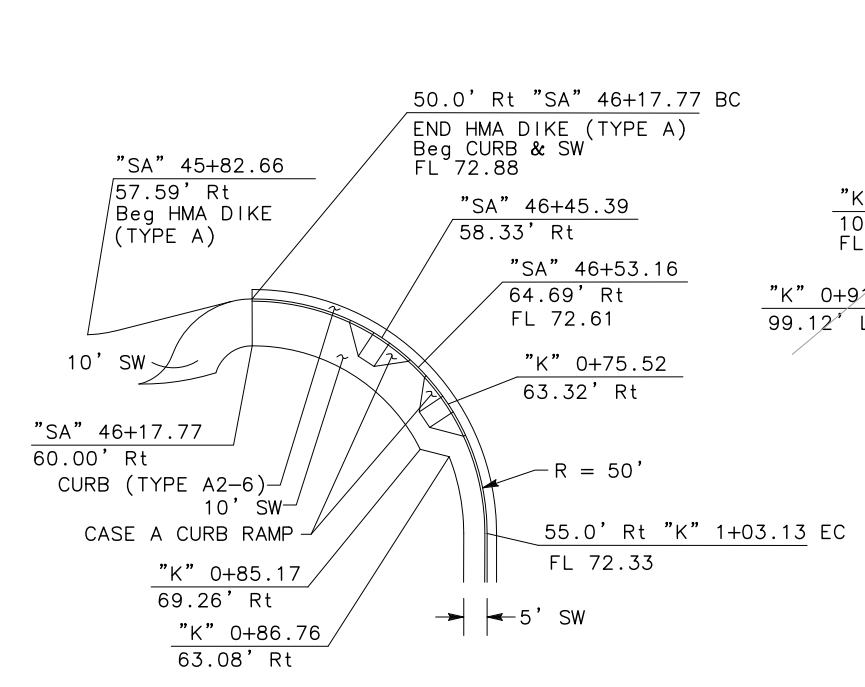
CURB RAMP CR 1
CASE A ONE-RAMP CORNER INSTALLATION



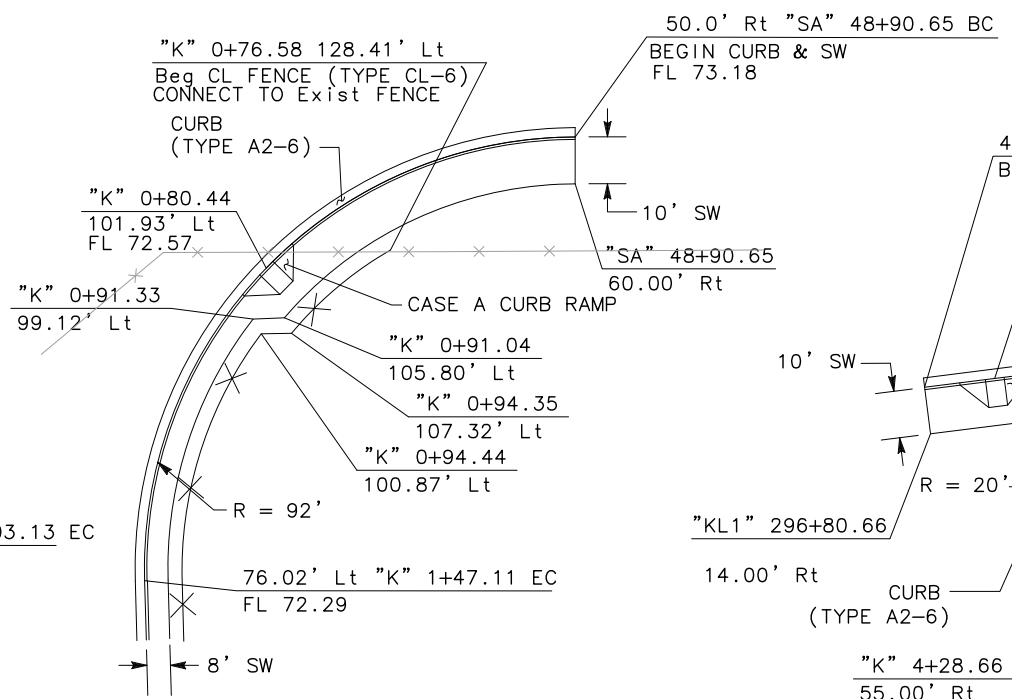
CURB RAMP CR 4
CASE A TWO-RAMP CORNER INSTALLATION



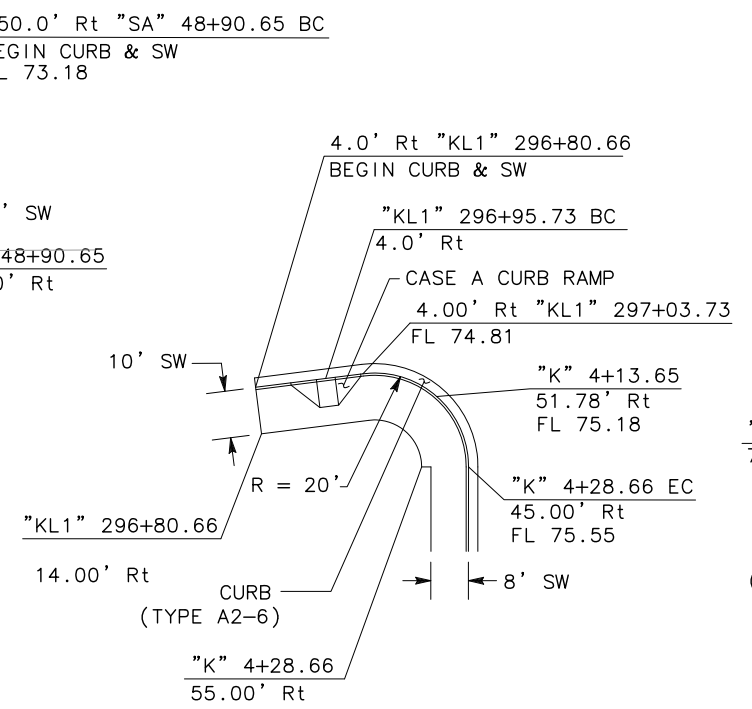
CURB RAMP CR 5
CASE A TWO-RAMP CORNER INSTALLATION



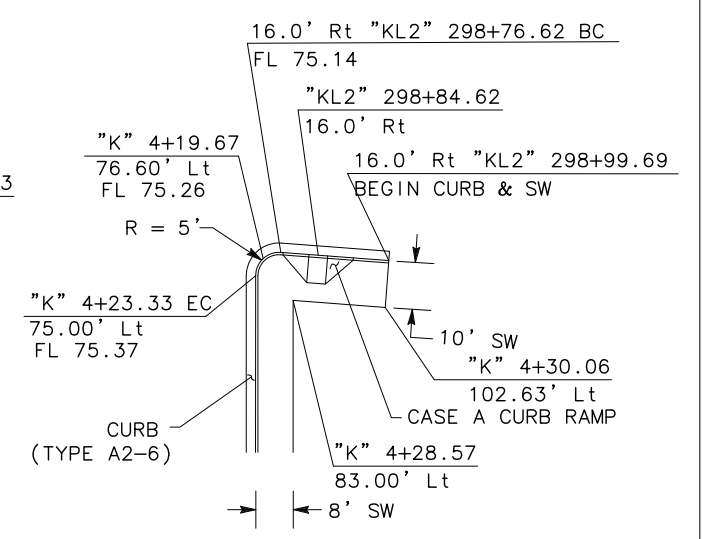
CURB RAMP CR 2
CASE A ONE-RAMP CORNER INSTALLATION



CURB RAMP CR 3
CASE A ONE-RAMP CORNER INSTALLATION



CURB RAMP CR 6
CASE A TWO-RAMP CORNER INSTALLATION




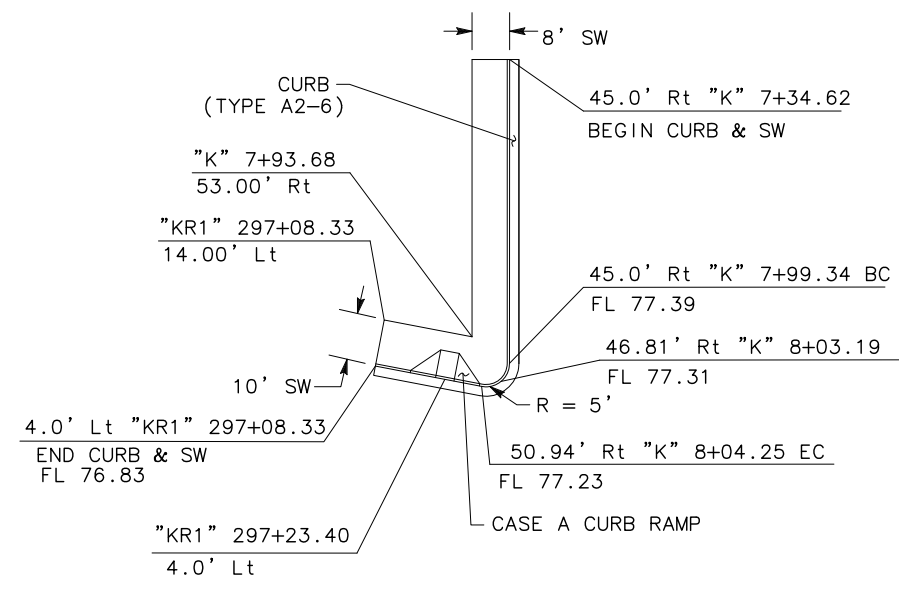
CURB RAMP CR 7
CASE A ONE-RAMP CORNER INSTALLATION

REVISOR: N MUMMANENT
 DATE: CHUONG NGUYEN
 CALCULATED/DESIGNED BY: KEITH G. MEYER
 CHECKED BY:
 CONSULTANT FUNCTIONAL SUPERVISOR:
 DEPARTMENT OF TRANSPORTATION
 STATE OF CALIFORNIA
 Galtrans

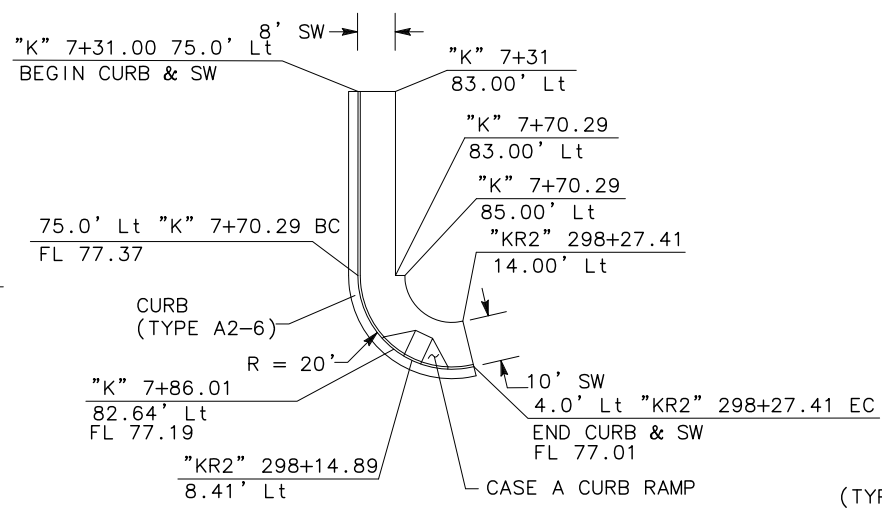
LAST REVISION DATE PLOTTED => \$DATE
 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	45	423

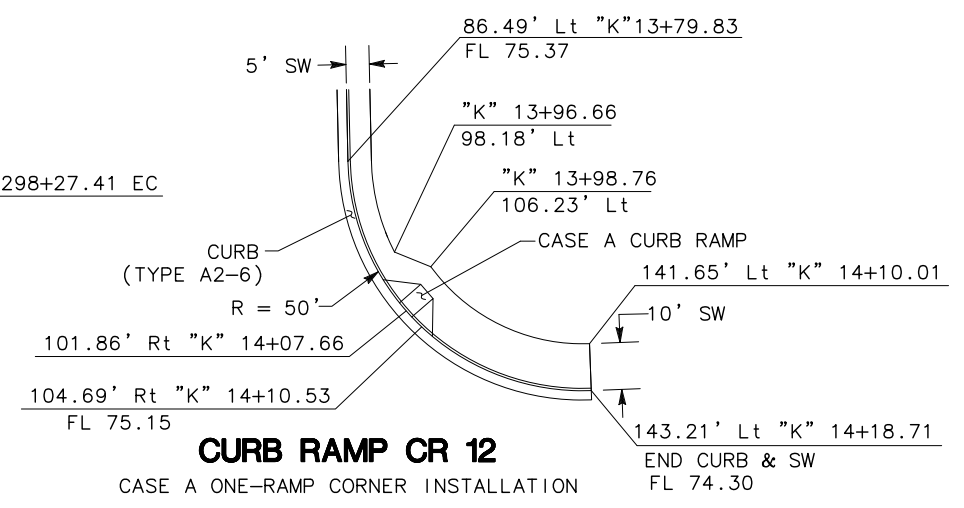
 REGISTERED CIVIL ENGINEER		05-18-12 DATE
9/04/12 PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



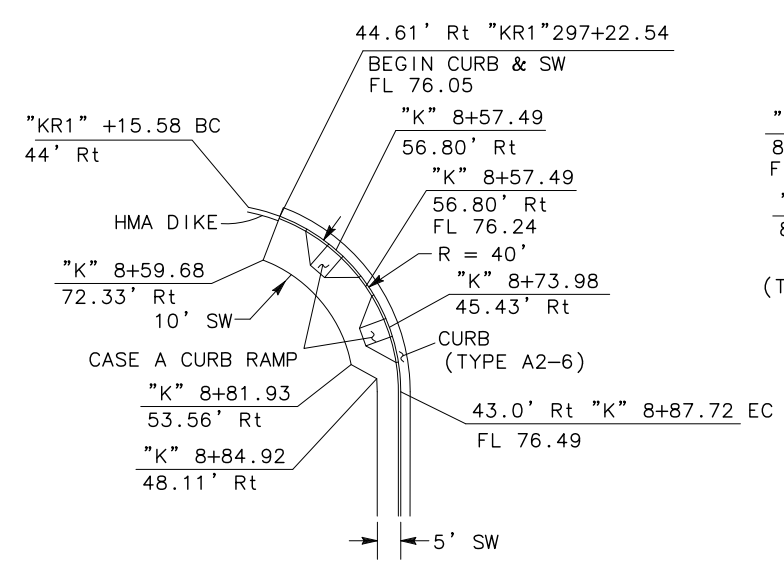
CURB RAMP CR 8
CASE A ONE-RAMP CORNER INSTALLATION



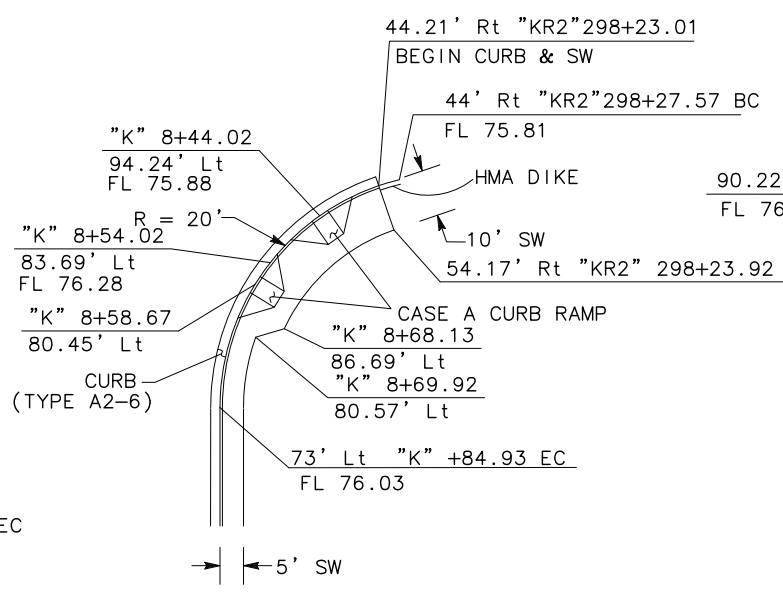
CURB RAMP CR 9
CASE A ONE-RAMP CORNER INSTALLATION



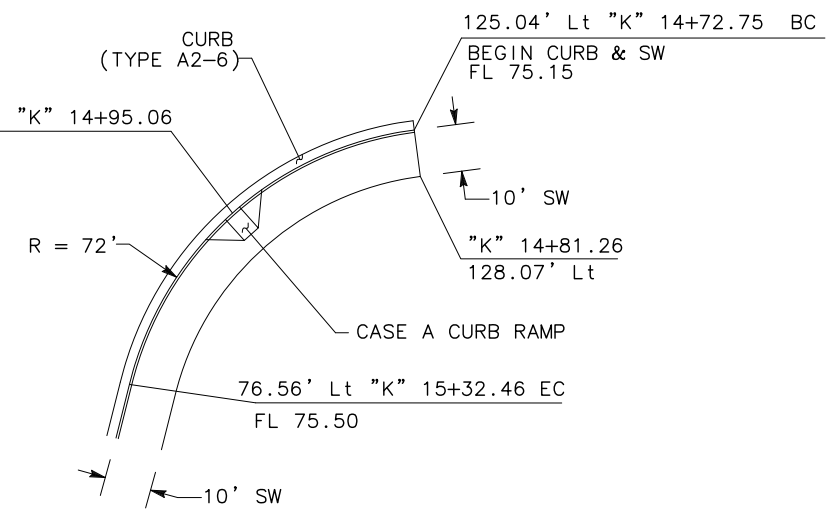
CURB RAMP CR 12
CASE A ONE-RAMP CORNER INSTALLATION



CURB RAMP CR 10
CASE A TWO-RAMP CORNER INSTALLATION



CURB RAMP CR 11
CASE A TWO-RAMP CORNER INSTALLATION



CURB RAMP CR 13
CASE A ONE-RAMP CORNER INSTALLATION

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 N MUMMANENI
 CHUONG NGUYEN
 KEITH G. MEYER
 REVISIONS: REVISED BY, DATE, REVISIONS
 CALCULATED/DESIGNED BY, CHECKED BY
 \$t Galtrans

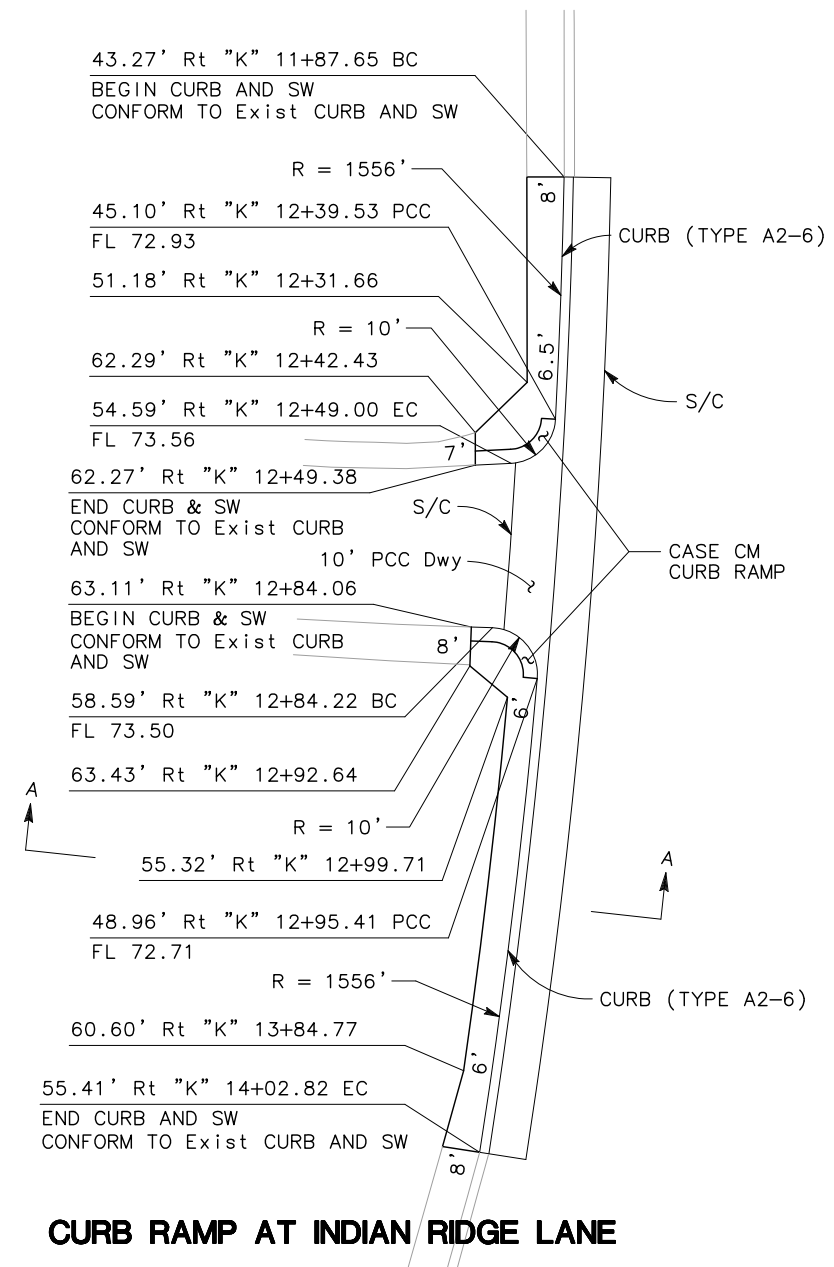
CONSTRUCTION DETAILS
NO SCALE **C-14**

LAST REVISION: 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

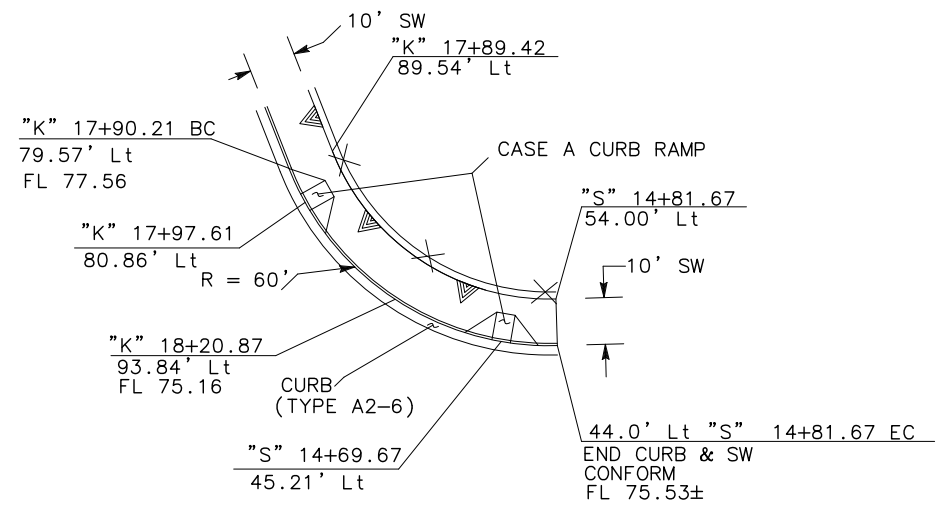
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 REVISOR: N. MUMMANENT
 DATE REVISED: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	46	423

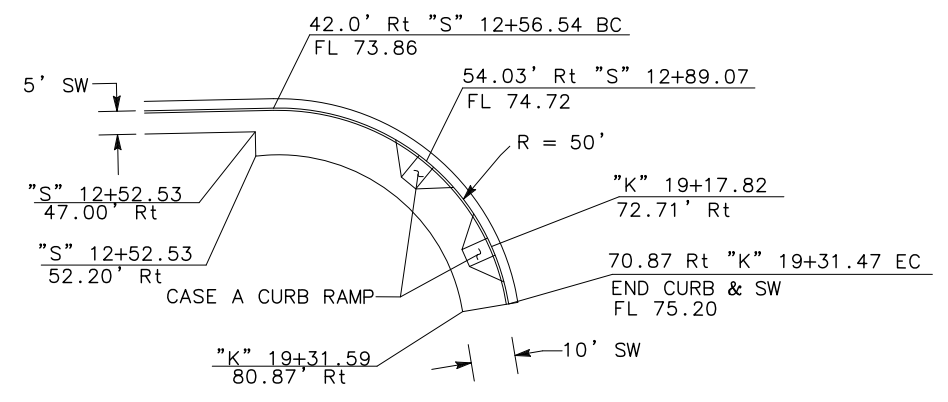
Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



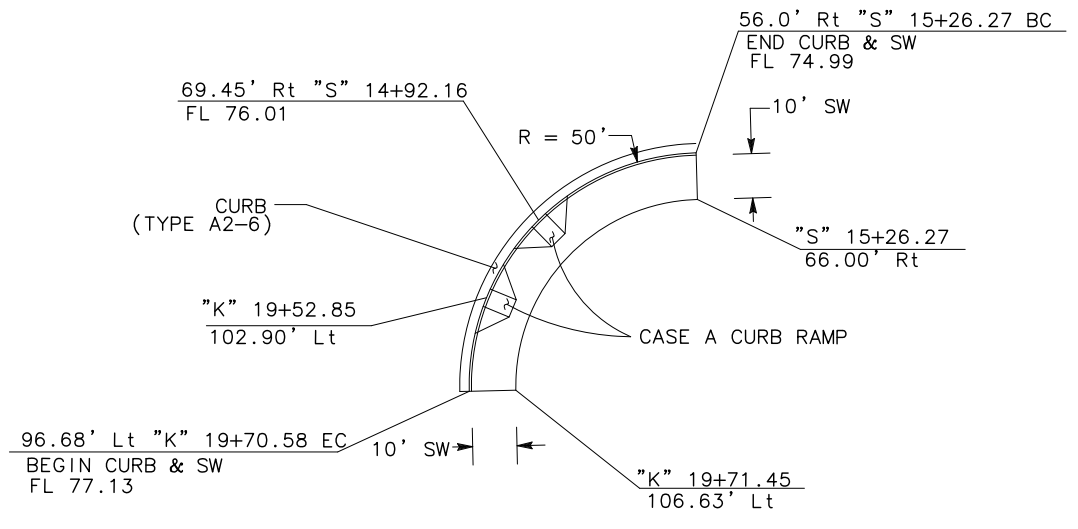
CURB RAMP AT INDIAN RIDGE LANE



CURB RAMP CR 14
 CASE A TWO-RAMP CORNER INSTALLATION



CURB RAMP CR 15
 CASE A TWO-RAMP CORNER INSTALLATION



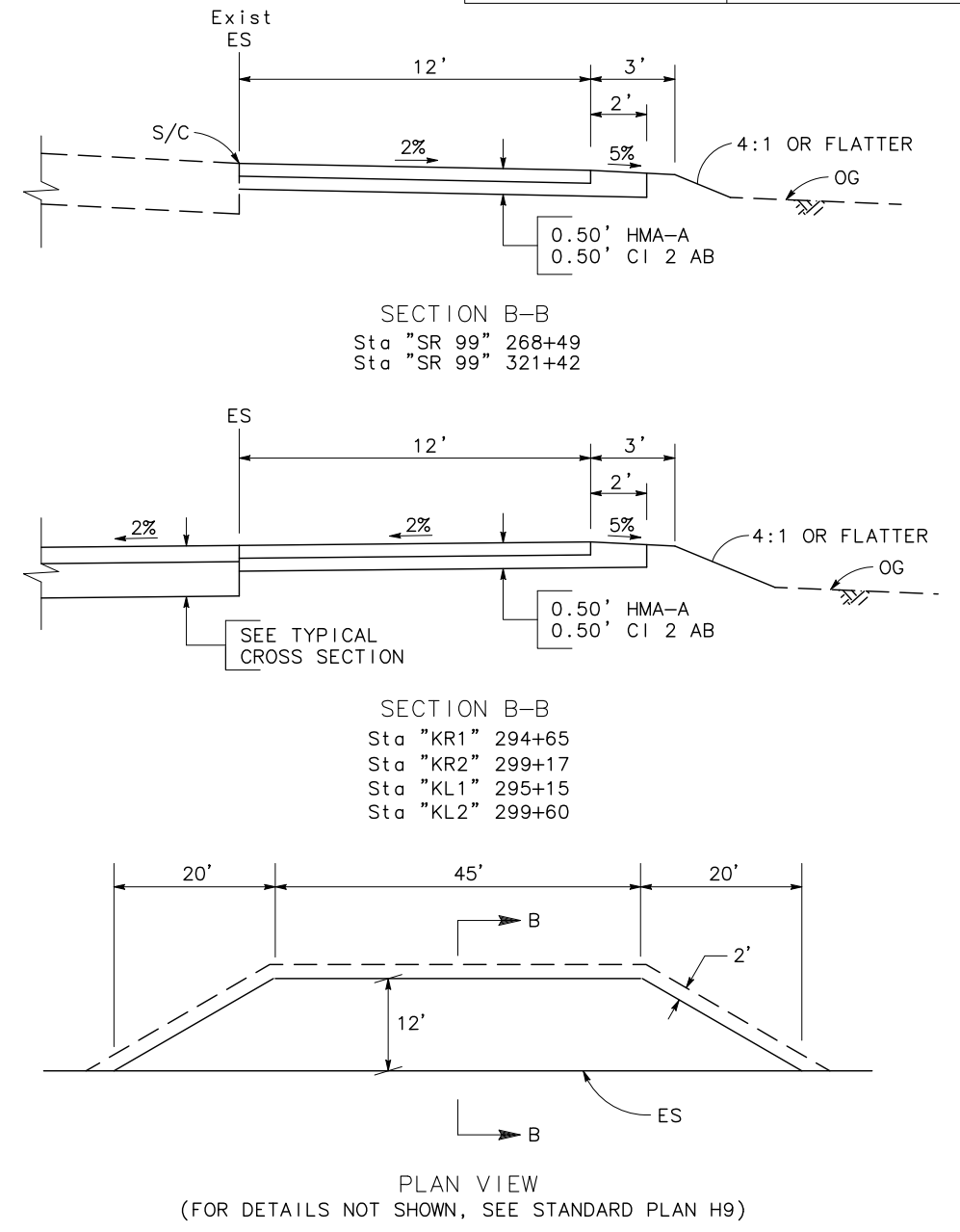
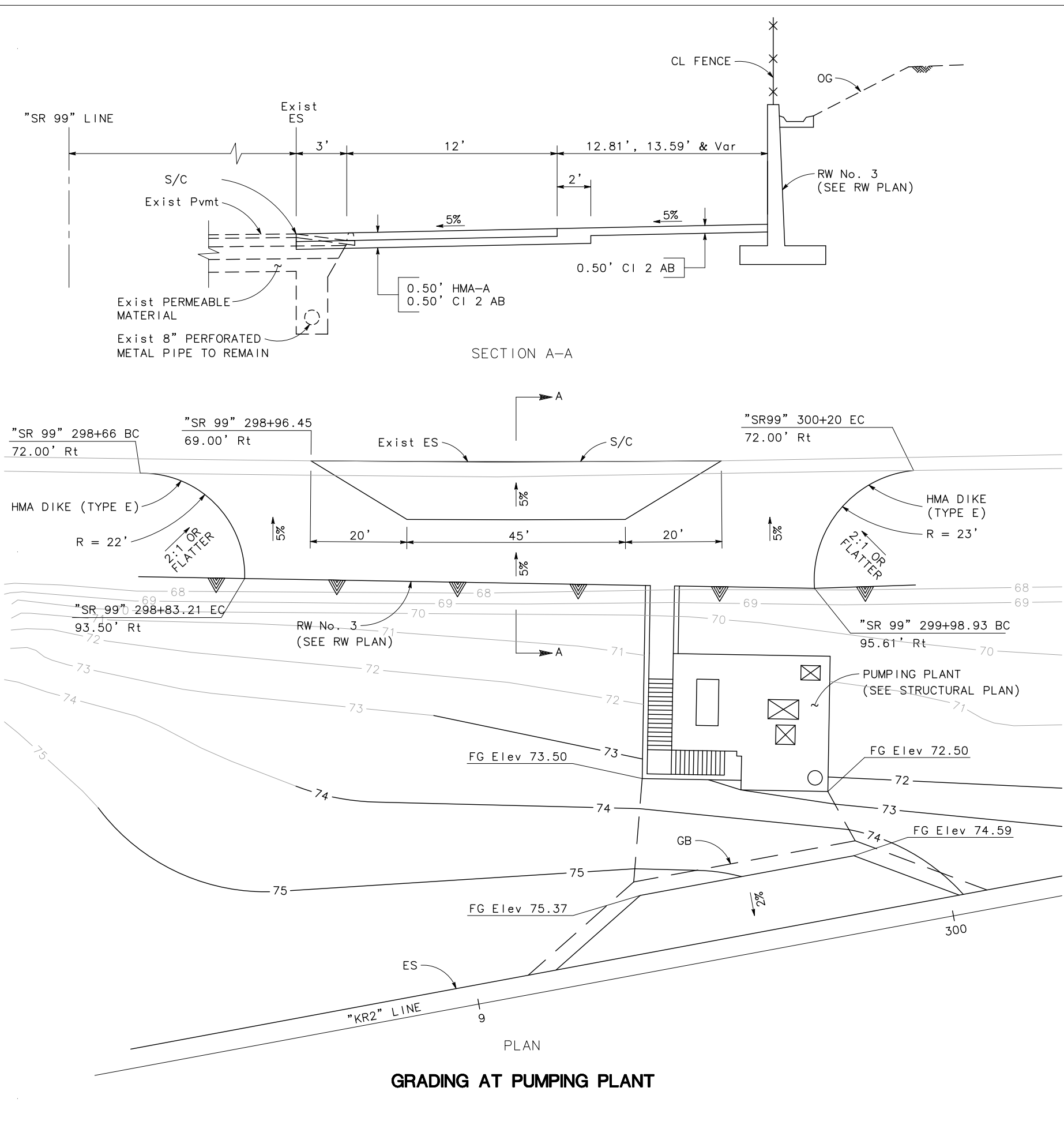
CURB RAMP CR 16
 CASE A TWO-RAMP CORNER INSTALLATION

CONSTRUCTION DETAILS
 NO SCALE
C-15

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	47	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



MAINTENANCE VEHICLE PULLOUT

CONSTRUCTION DETAILS

NO SCALE

C-16

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

Caltrans

CONSULTANT SUPERVISOR: KEITH G. MEYER

DESIGNED BY: ANDY DO

CHECKED BY: CHUONG NGUYEN

REVISIONS:

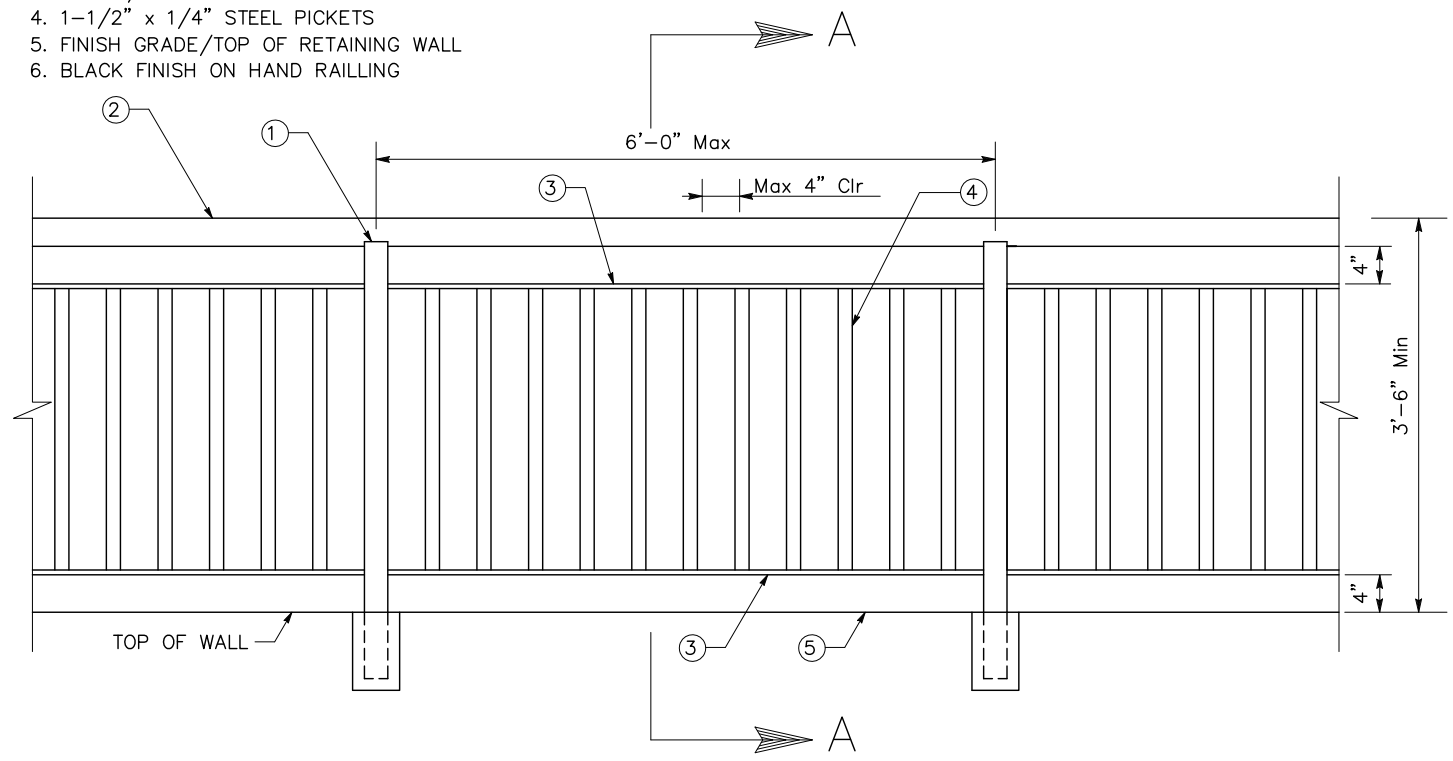
NO.	DESCRIPTION	DATE

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

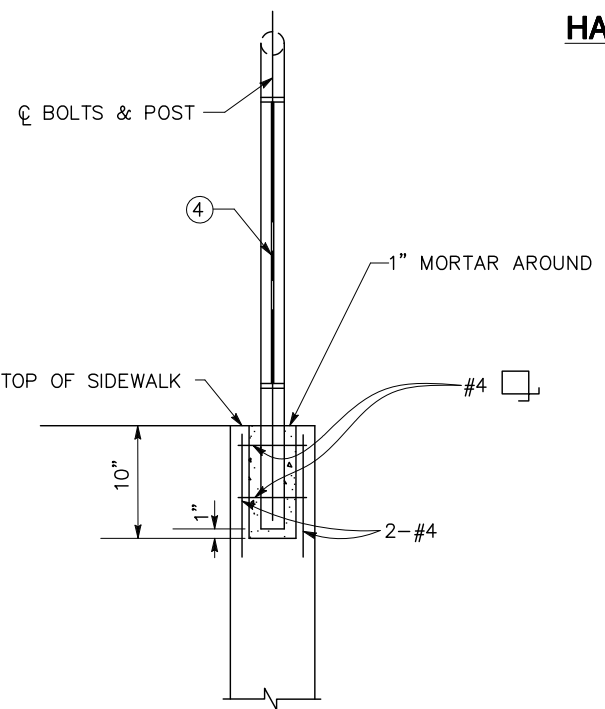
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	48	423

REGISTERED CIVIL ENGINEER	DATE
CHUONG NGUYEN	05-18-12
PLANS APPROVAL DATE	
9/04/12	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

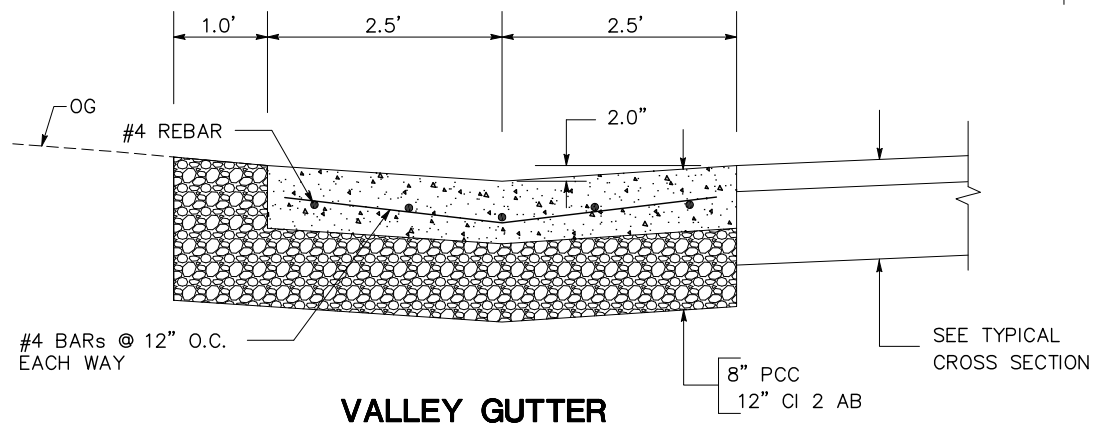
- LEGEND:**
- 2-1/2" SQ TUBULAR STEEL POST
 - 3" o TUBULAR STEEL RAIL
 - 2" x 1/2" STEEL BAR
 - 1-1/2" x 1/4" STEEL PICKETS
 - FINISH GRADE/TOP OF RETAINING WALL
 - BLACK FINISH ON HAND RAILLING



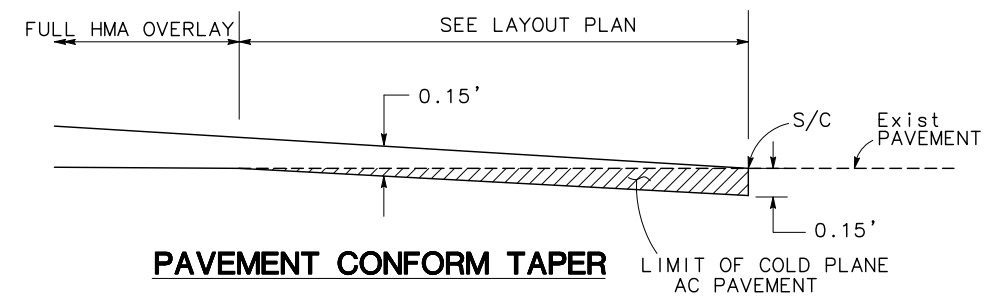
HAND RAILLING



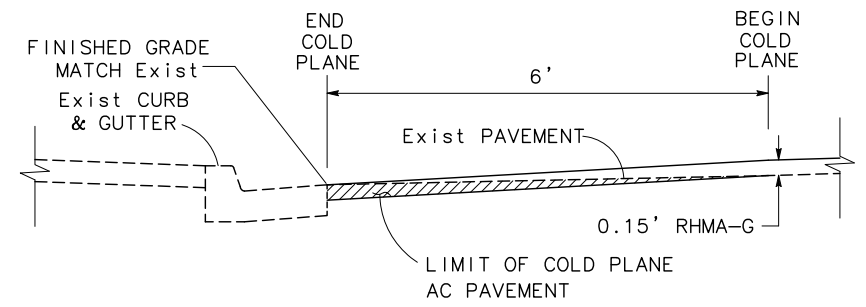
SECTION A-A



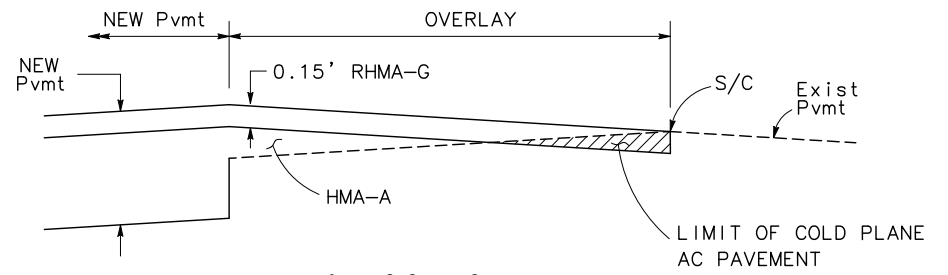
VALLEY GUTTER



PAVEMENT CONFORM TAPER



CONFORM AT EXISTING GUTTER



TRANVERSE CONFORM DETAIL

Sta "K" 17+92.84 TO 18+24.42
 Sta "K" 19+49.30 TO 23+17.90

CONSTRUCTION DETAILS

NO SCALE

C-17

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS: 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	49	423

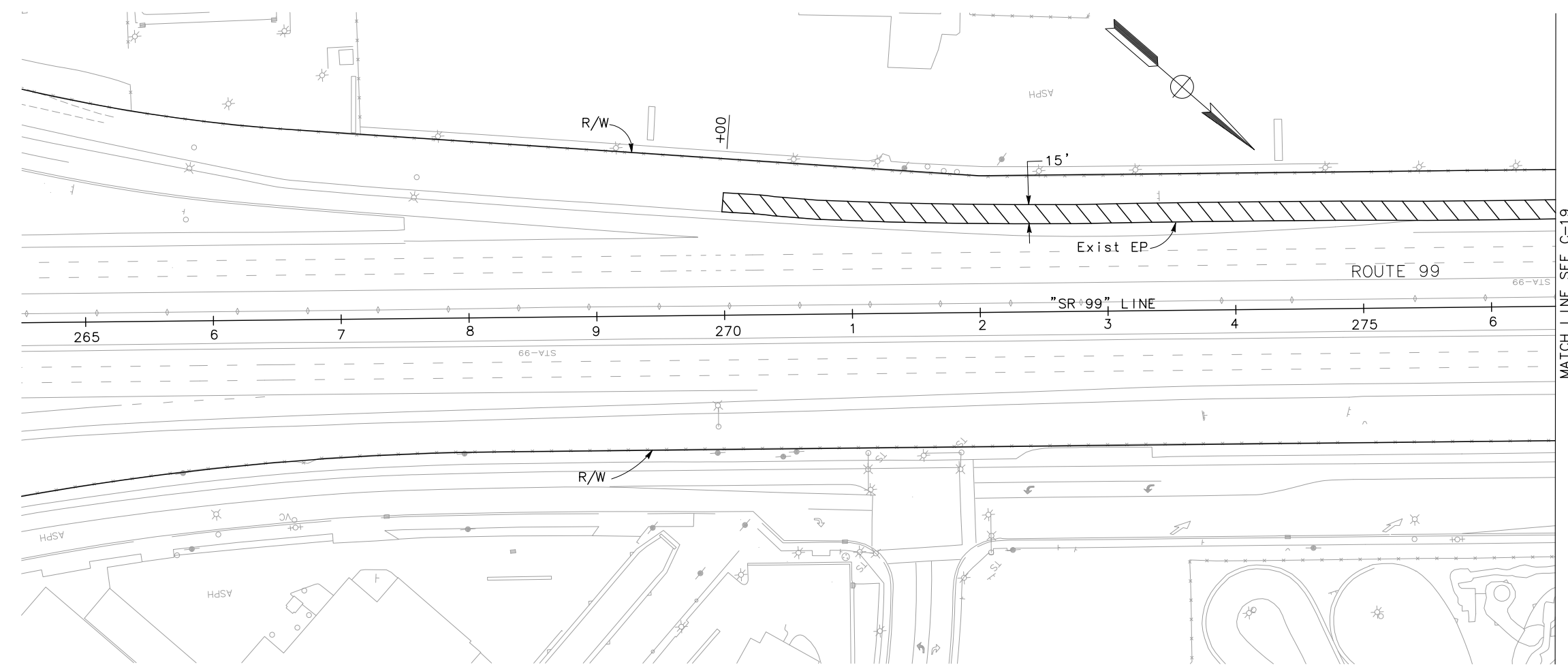
<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTES:

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- EXCAVATED SOIL FROM THE AREAS DELINEATED ON THESE SHEETS MUST BE TRANSPORTED TO AND DISPOSED OF AT A CA CLASS 1 DISPOSAL SITE.

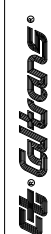
LEGEND

 0' - 1.0' ROADWAY EXCAVATION (TYPE Z-2) (AERIALLY DEPOSITED LEAD)



CONSTRUCTION DETAILS
AERIALLY DEPOSITED LEAD
SCALE 1" = 50'
C-18

APPROVED FOR AERIALLY DEPOSITED LEAD WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

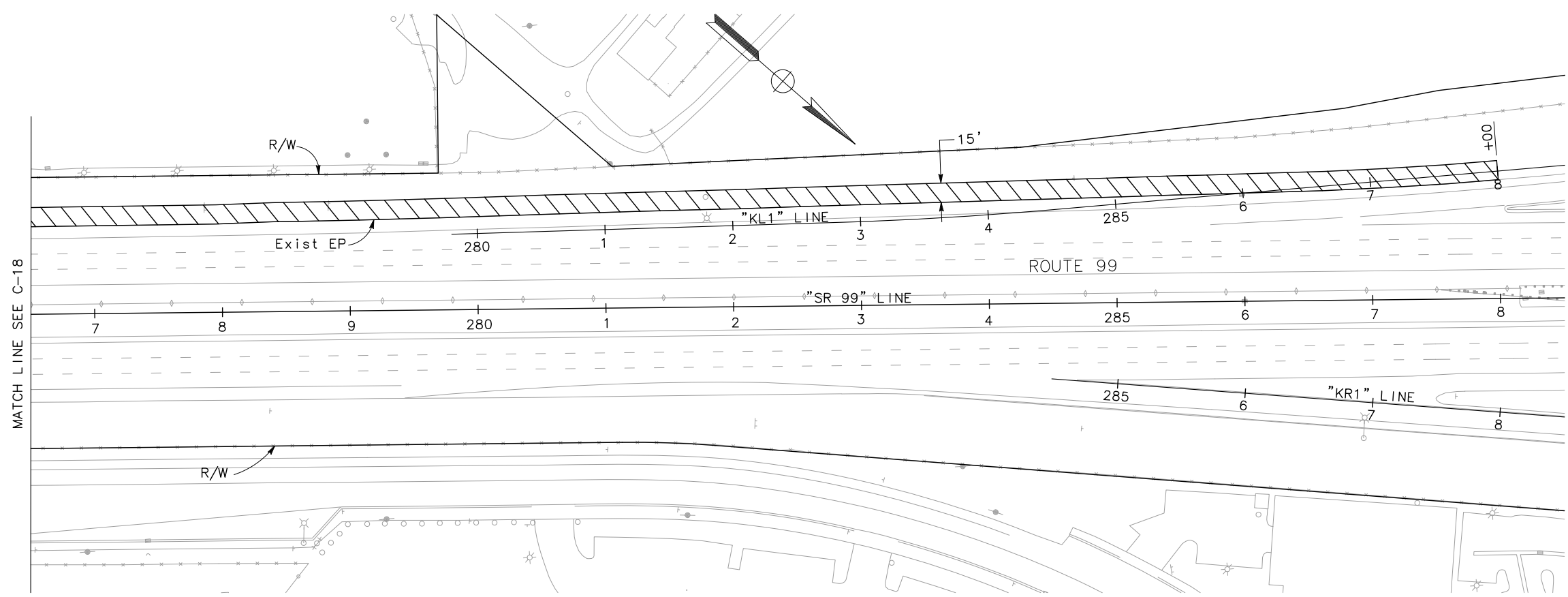
LAST REVISION
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	50	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	DATE REVISED
			ANDY CHAN
			CHUONG NGUYEN


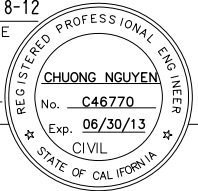


CONSTRUCTION DETAILS
AERIALLY DEPOSITED LEAD
SCALE 1" = 50'

C-19

APPROVED FOR AERIALLY DEPOSITED LEAD WORK ONLY











LAST REVISION
06-08-12
DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	51	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTES:

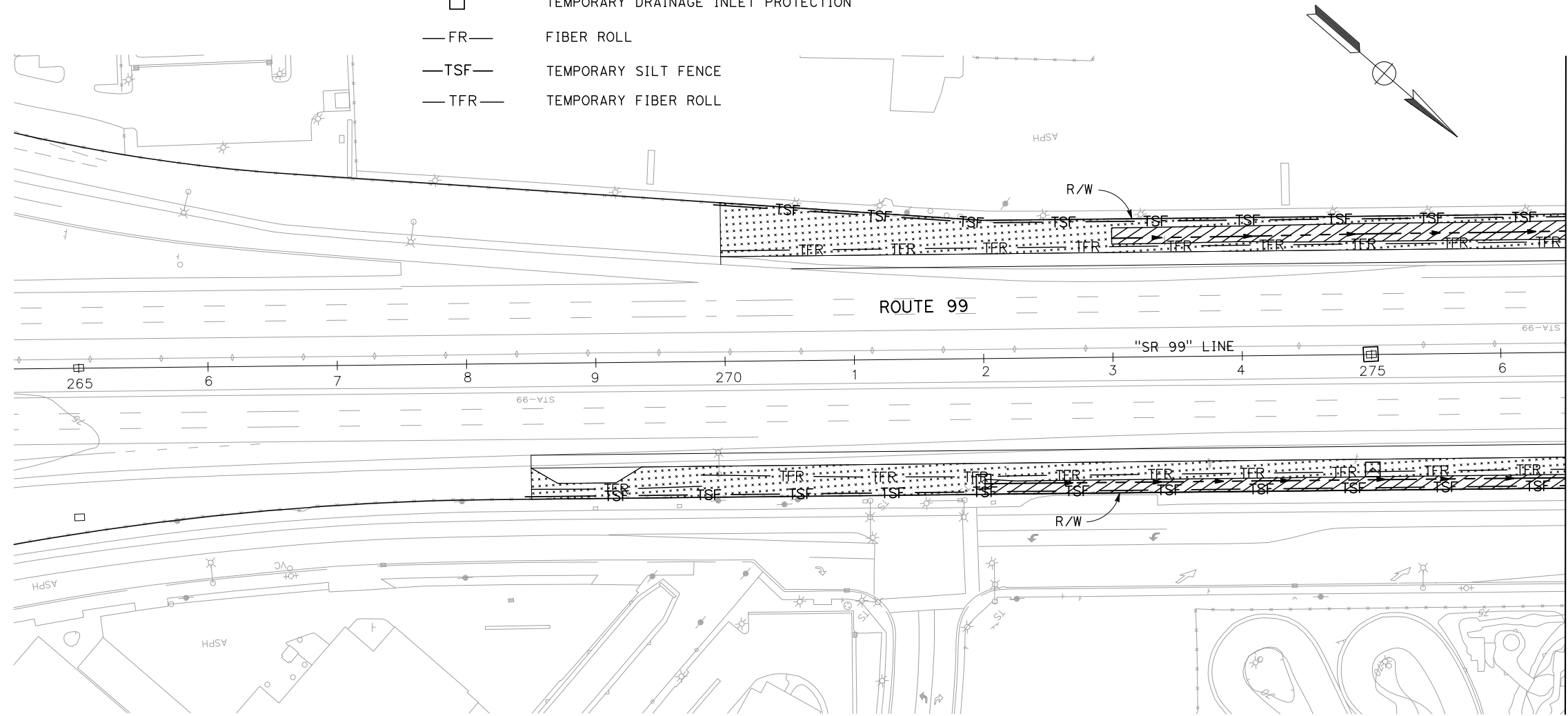
1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
2. SEEDING AND PLANTING WORK FOR EROSION CONTROL SHALL BE PERFORMED WHEN OPTIMAL.
3. ALL PROPOSED DRAINAGE INLETS ADJACENT TO PEDESTRIAN FACILITIES SHALL BE STENCILED DURING CONSTRUCTION.
4. FOR LOCATION OF BIOFILTRATION SWALES AND BIOFILTRATION STRIP, SEE DRAINAGE PLANS.

LEGEND

-  TEMPORARY CONSTRUCTION ENTRANCE
-  HYDROSEED
-  TEMPORARY HYDRAULIC MULCH (BONDED FIBER MATRIX)
-  BIOFILTRATION STRIP
-  BIOFILTRATION SWALE
-  TEMPORARY EROSION CONTROL BLANKET
-  TEMPORARY DRAINAGE INLET PROTECTION
-  FIBER ROLL
-  TEMPORARY SILT FENCE
-  TEMPORARY FIBER ROLL

ABBREVIATION

BWS BIOFILTRATION SWALE





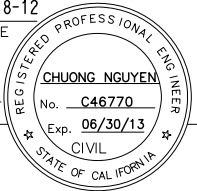
EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'


EC-1

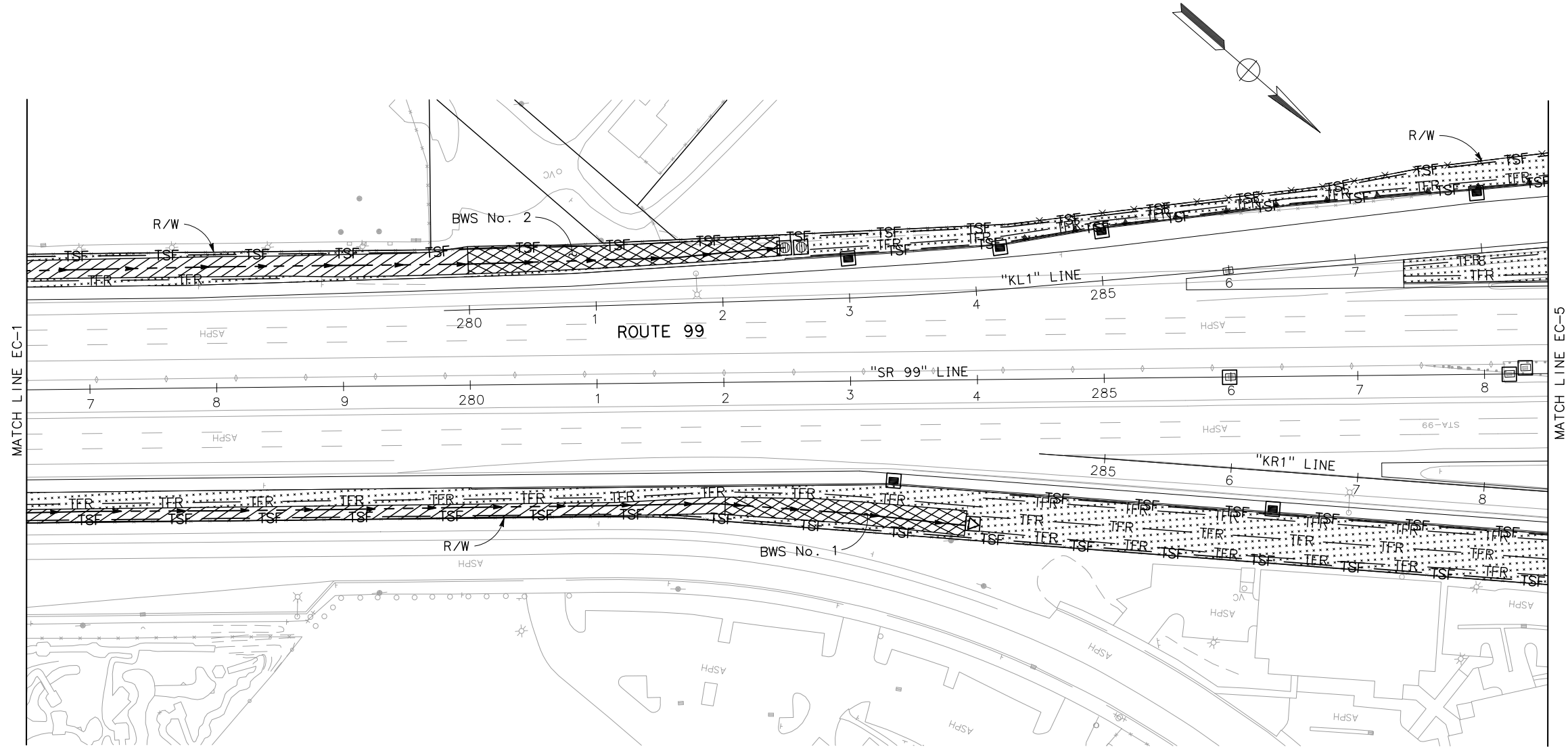
APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 ANDY DO
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	52	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
	KEITH G. MEYER	CHECKED BY	DATE REVISED
	ANDY DO		
	CHUONG NGUYEN		



EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'

EC-2

APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

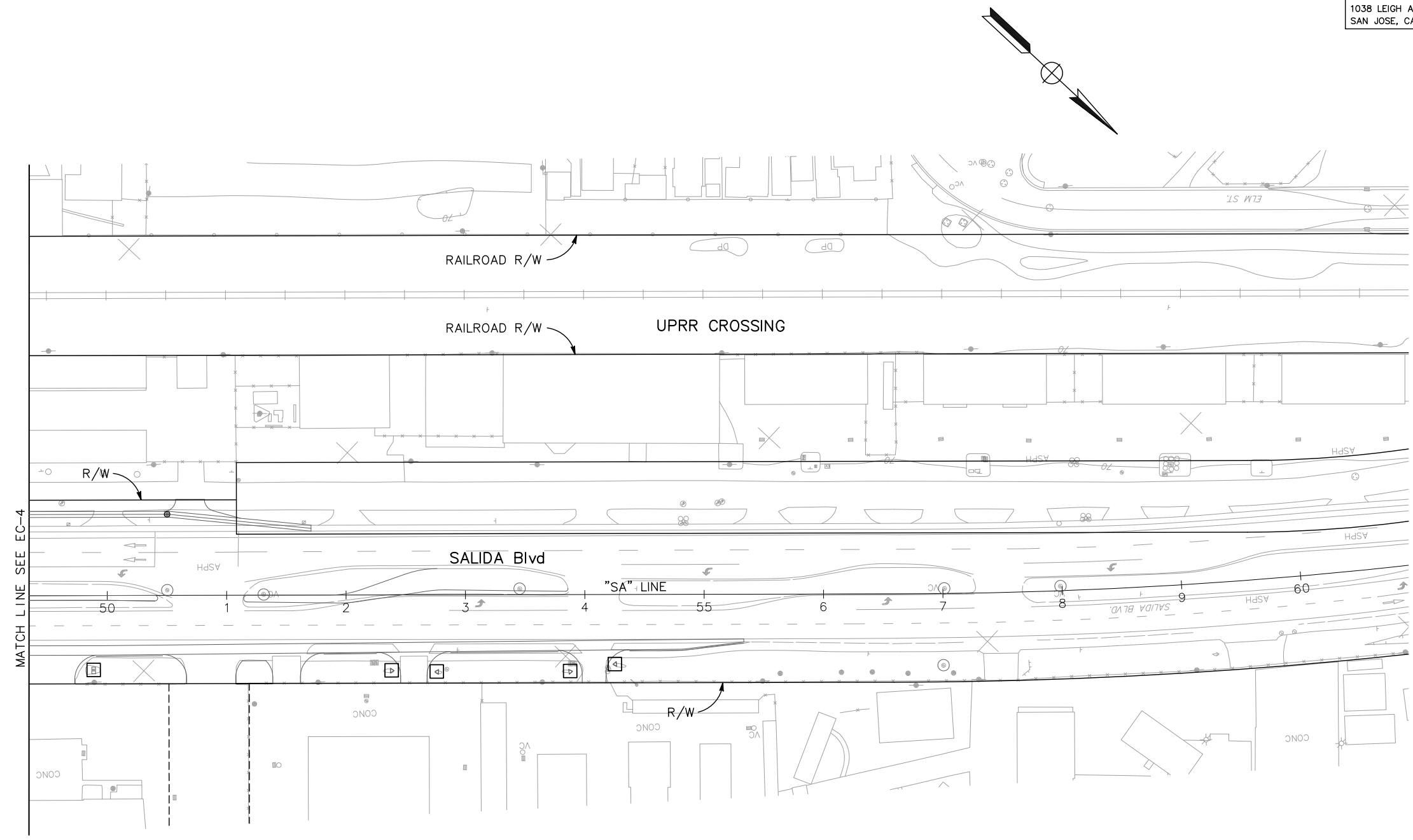
LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	53	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
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NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	DATE REVISED
	ANDY DO		
	CHUONG NGUYEN		



EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'

EC-3

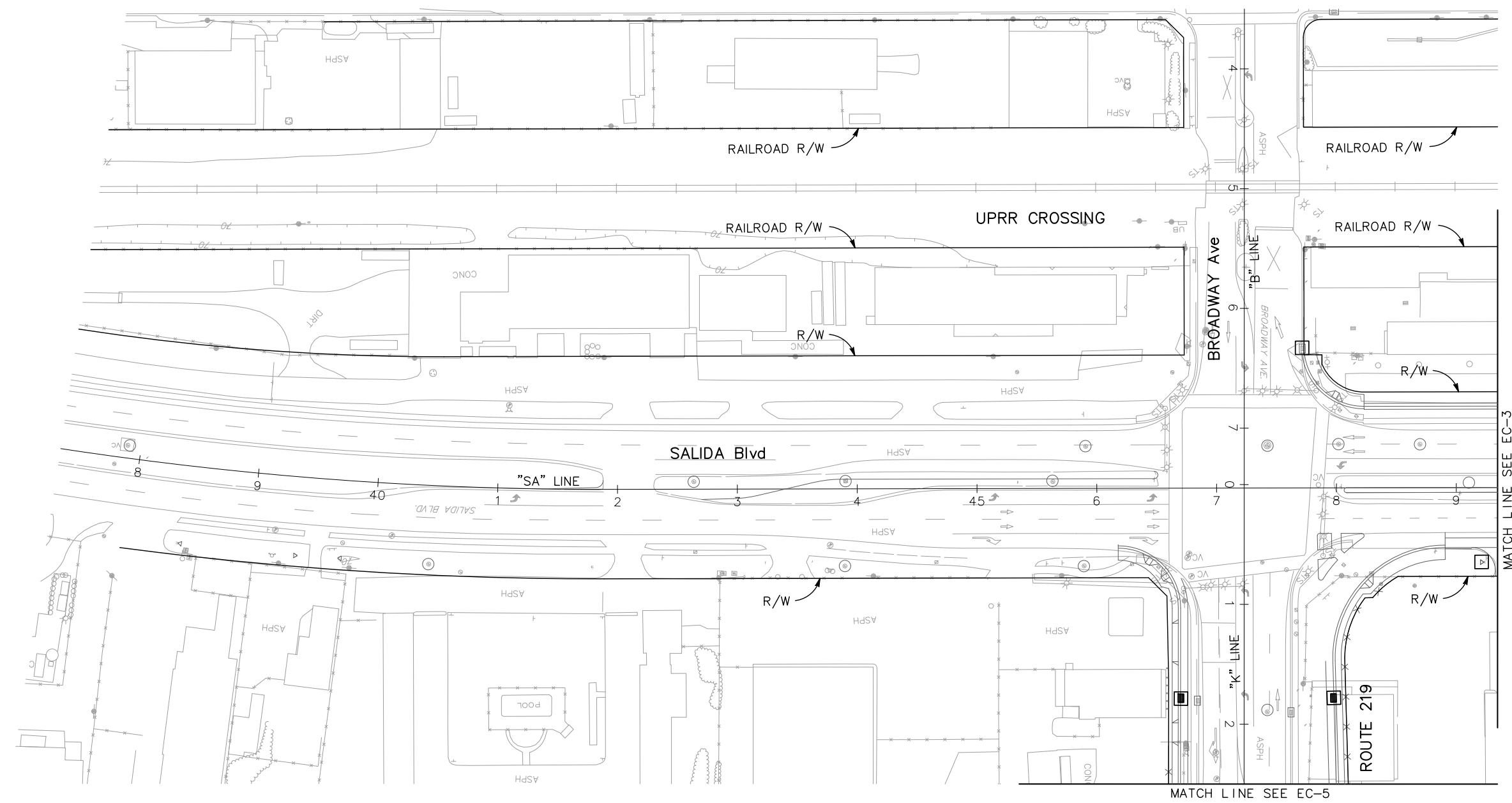
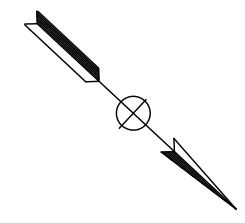
APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME
LAST REVISION
05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	54	423
		<i>Chung Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:

FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



**EROSION CONTROL AND WATER
POLLUTION CONTROL PLANS**

SCALE: 1" = 50'

EC-4

APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

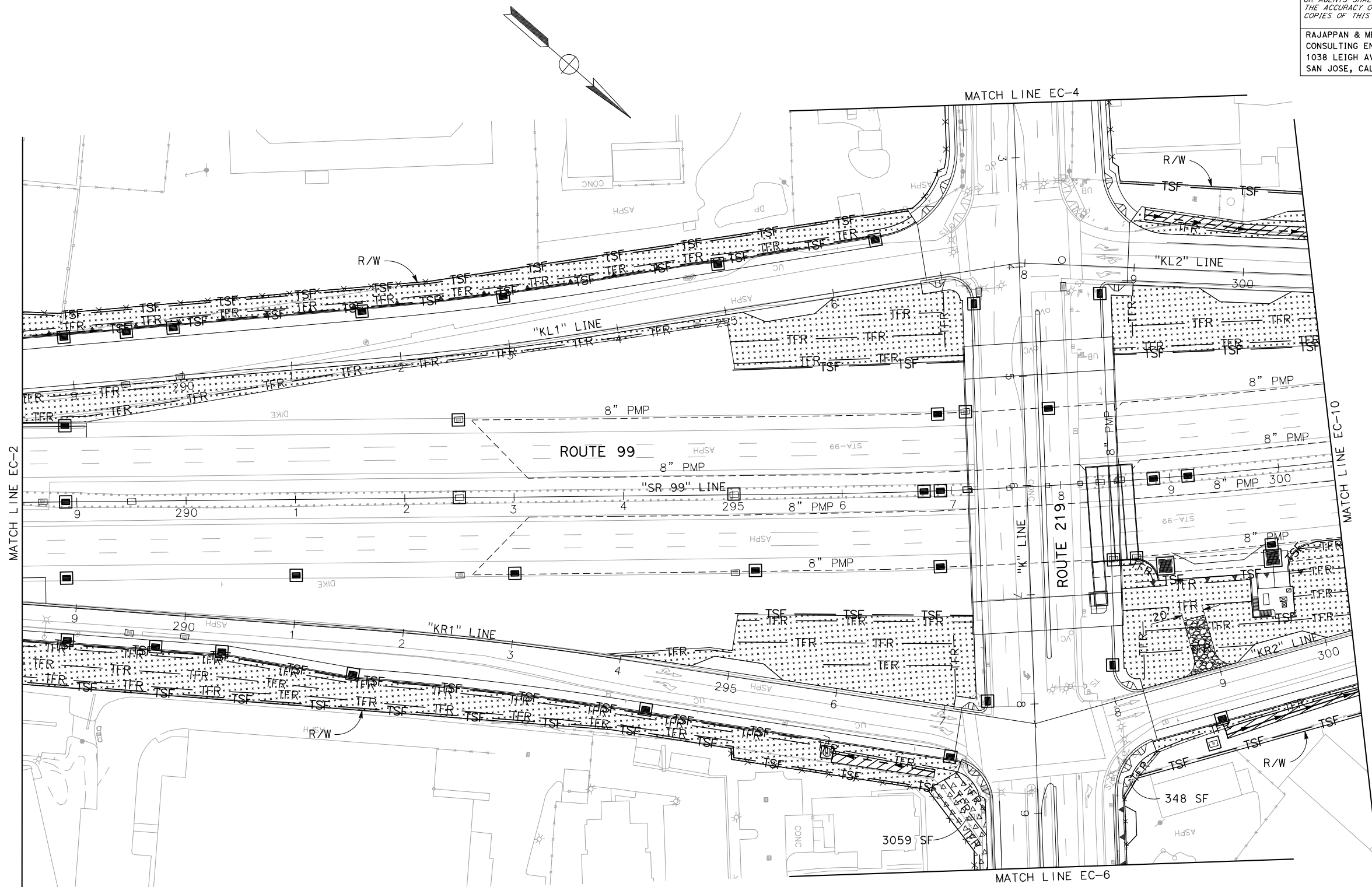
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	REVISOR BY
	KEITH G. MEYER	ANDY DO	CHUONG NGUYEN

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME
LAST REVISION 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	55	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'

EC-5

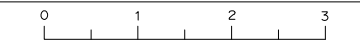
APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 ANDY DO
 REVISOR: CHUONG NGUYEN
 DATE REVISOR: CHUONG NGUYEN

BORDER LAST REVISED 7/2/2010

USERNAME => \$USER
 DGN FILE => \$REQUEST

RELATIVE BORDER SCALE IS IN INCHES



UNIT 1457

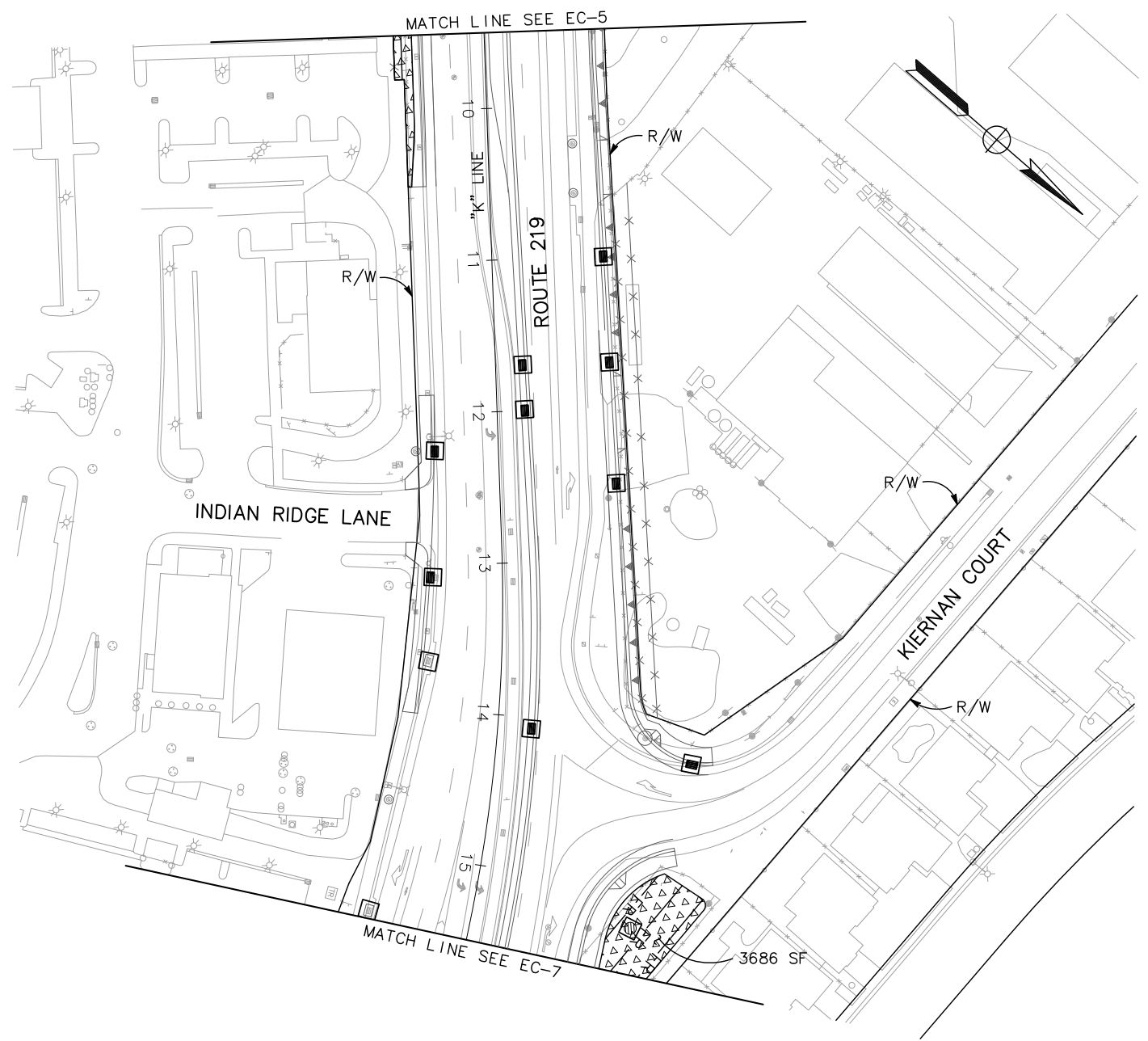
PROJECT NUMBER & PHASE 100000100

LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	56	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAT DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



**EROSION CONTROL AND WATER
POLLUTION CONTROL PLANS**

SCALE: 1" = 50'

EC-6

APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

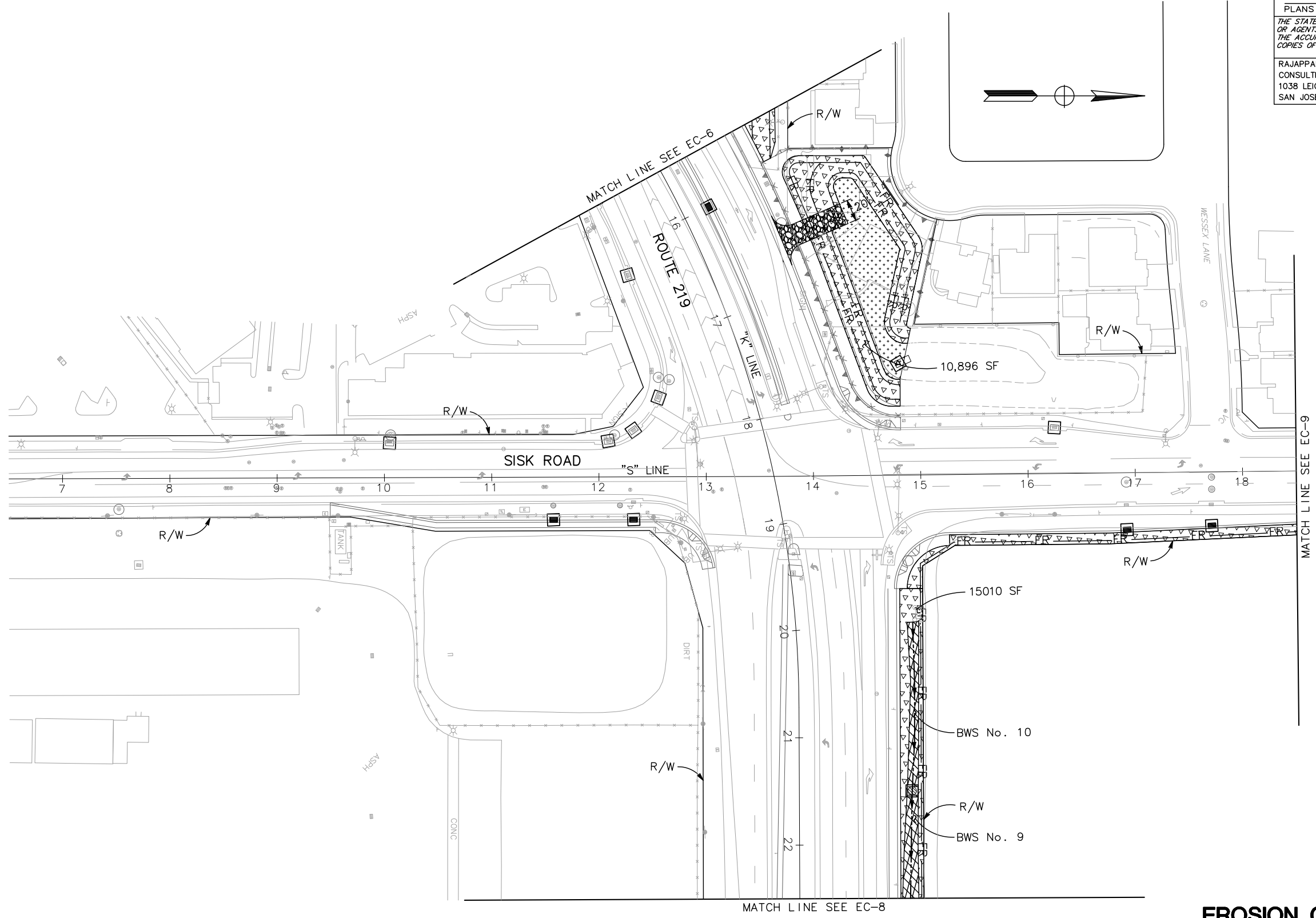
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISOR BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED

LAST REVISION 05-01-12
DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	57	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



**EROSION CONTROL AND WATER
POLLUTION CONTROL PLANS**

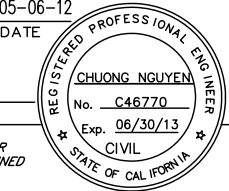
SCALE: 1" = 50'

EC-7

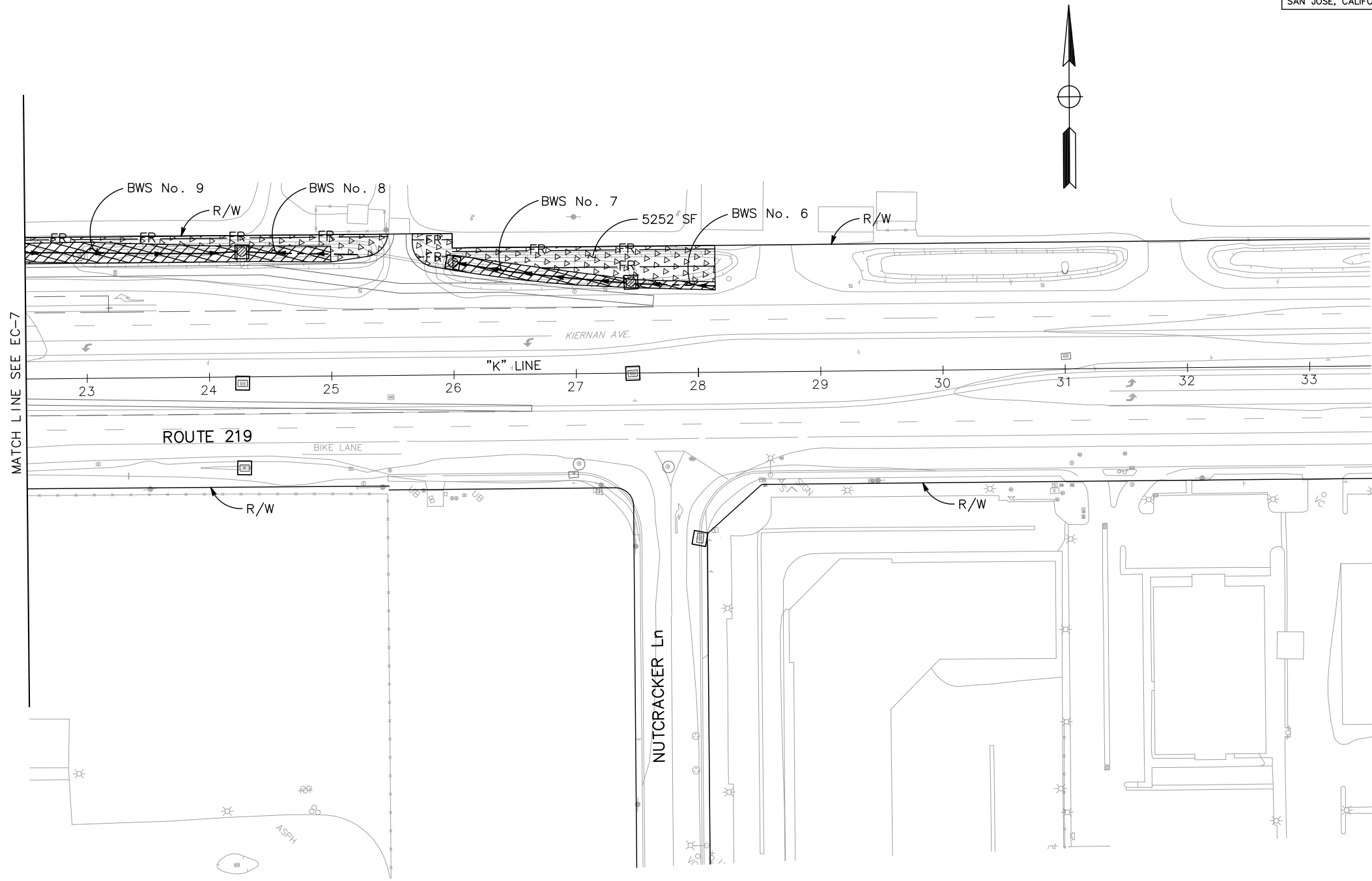
APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME
LAST REVISION
05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	58	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.




EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

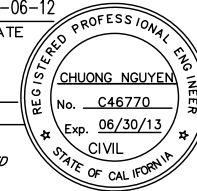
SCALE: 1" = 50'

EC-8


APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

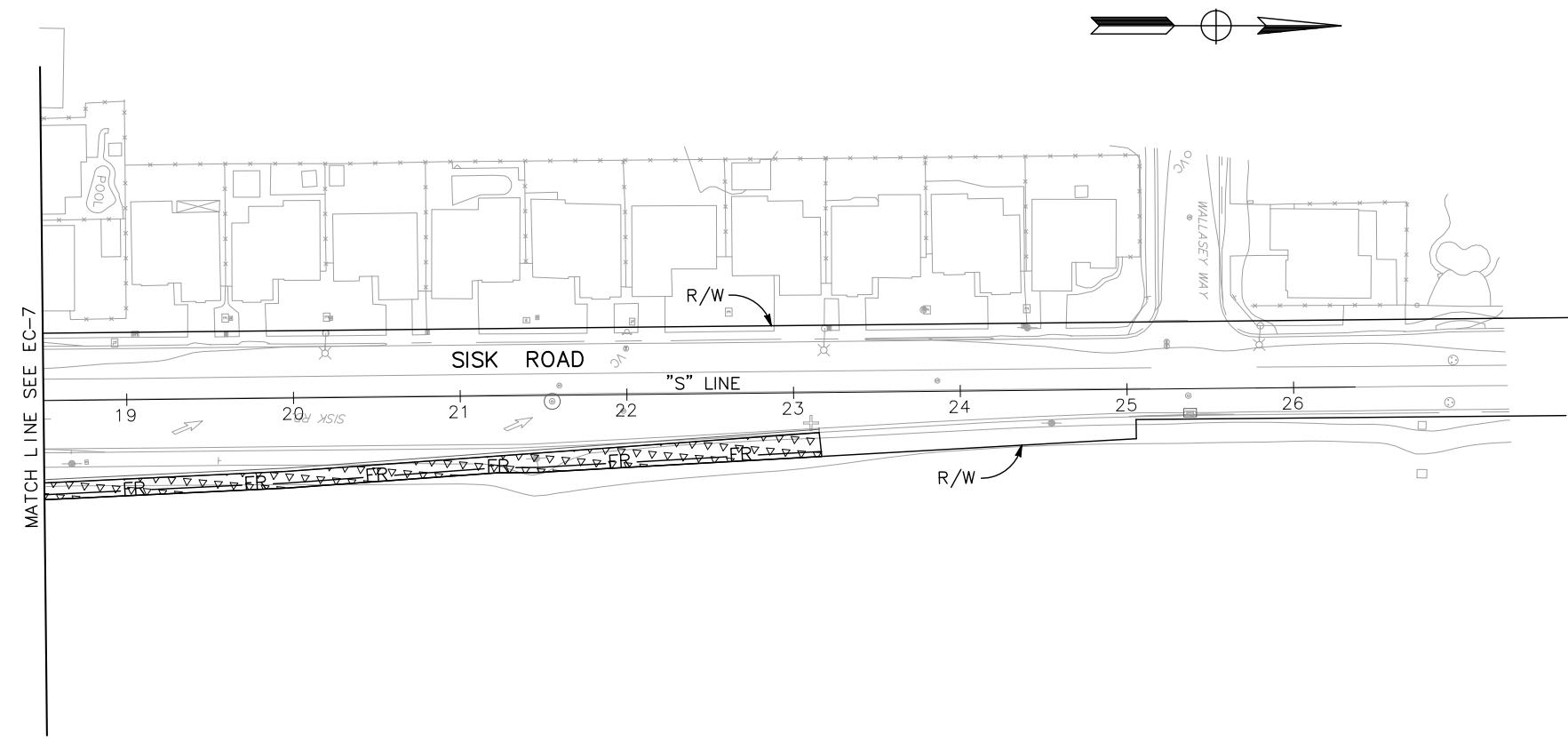
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED— DESIGNED BY	REVISED BY
	KEITH G. MEYER	CHECKED BY	DATE REVISED
	ANDY DO		
	CHUONG NGUYEN		

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	59	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

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STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED- DESIGNED BY	ANDY DO	REVISED BY	
	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED	



EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'

EC-9

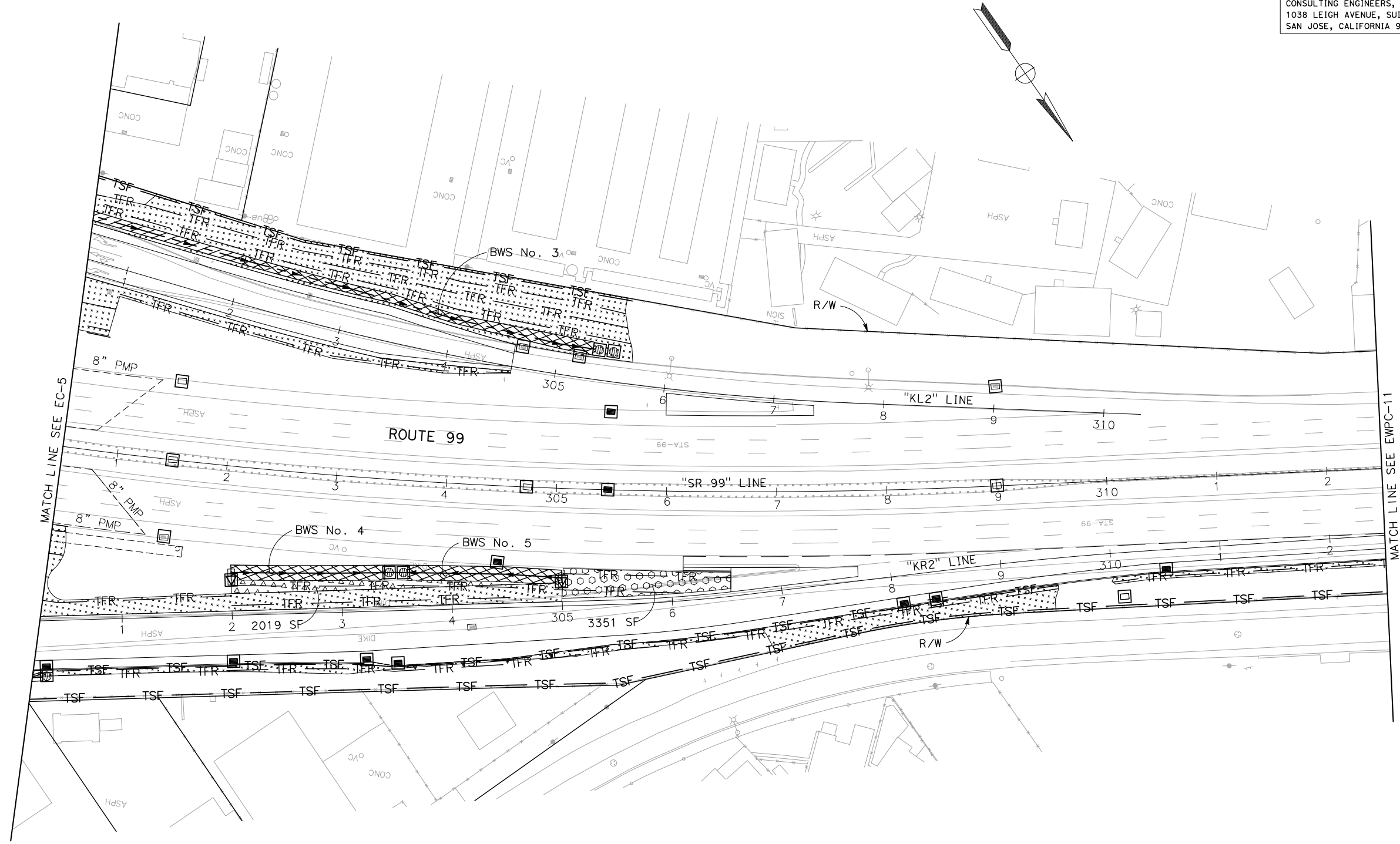
APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION
 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	60	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
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RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'

EC-10

APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISOR
	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISOR
Caltrans				

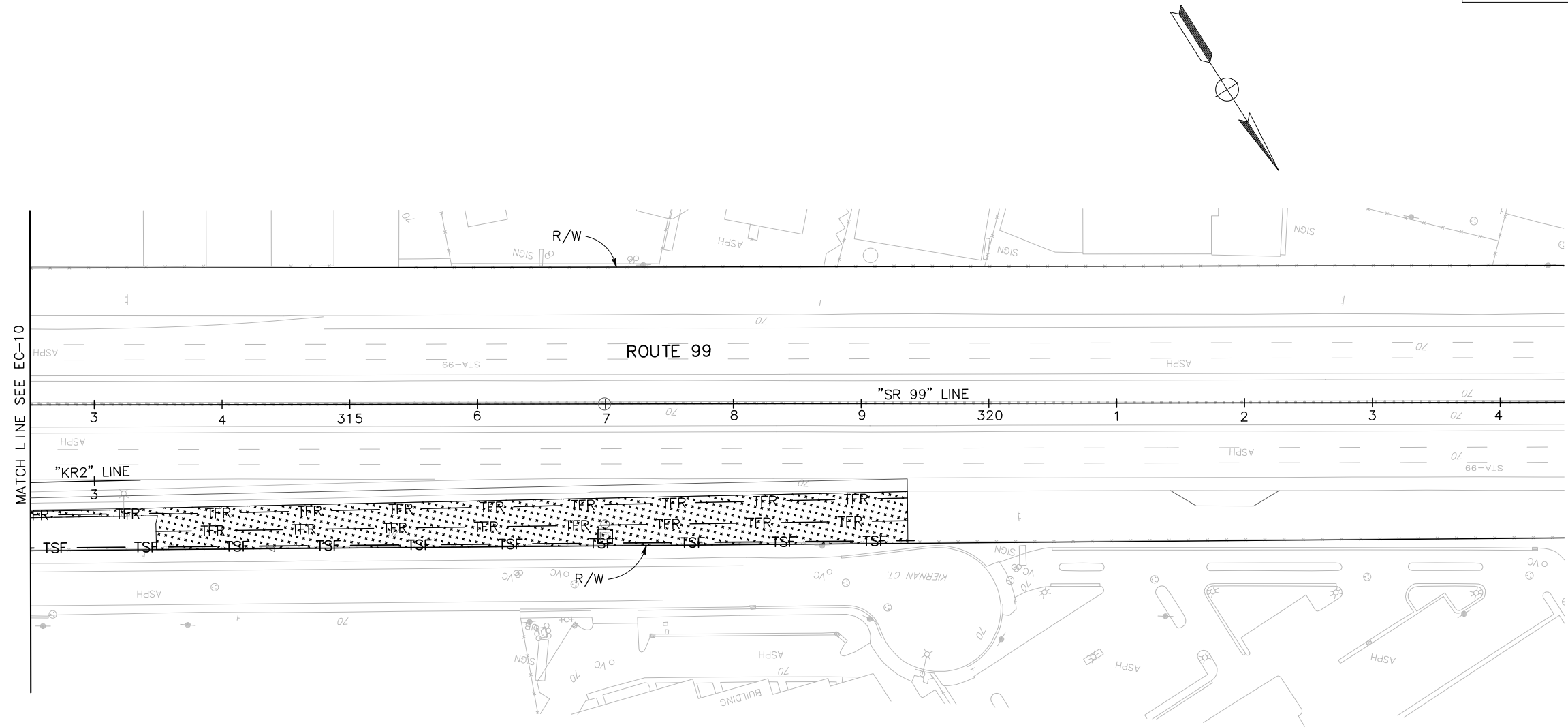
LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	61	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

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STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED— DESIGNED BY	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	DATE REVISED
	ANDY DO		
	CHUONG NGUYEN		



EROSION CONTROL AND WATER POLLUTION CONTROL PLANS

SCALE: 1" = 50'

EC-11

APPROVED FOR EROSION CONTROL AND WATER POLLUTION CONTROL WORK ONLY

LAST REVISION: 05-18-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY DO
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

TEMPORARY DRAINAGE INLET PROTECTION

SHEET No.	TEMPORARY DRAINAGE INLET PROTECTION
	EA
EC-1	1
EC-2	9
EC-3	5
EC-4	4
EC-5	42
EC-6	12
EC-7	13
EC-8	7
EC-10	26
EC-11	1
TOTAL	120

TEMPORARY SILT FENCE

SHEET No.	STATION	Loc	LENGTH
			LF
EC-1,2,5	"SR99" 269+97 TO "KL1" 296+79	L+	4,013
EC-1,2,5	"SR99" 268+49 TO "KR1" 296+96	R+	4,267
EC-2,5	"KL1" 282+70 TO 296+46	L+	2,056
EC-2,5	"SR99" 284+25 TO "KR1" 295+74	R+	1,716
EC-5	"KR1" 294+97 TO 297+13	L+	327
EC-5	"KL1" 294+98 TO 297+09	R+	318
EC-5,10	"KL2" 298+84 TO 301+02	R+	328
EC-5,10	"SR99" 298+50 TO 300+64	R+	498
EC-5,10,11	"KR2" 298+03 TO "SR99" 319+36	R+	3,213
EC-10	"KR2" 300+40 TO 309+50	R+	1,370
EC-5,10	"KL2" 298+84 TO 305+61	L+	999
TOTAL			19,105

MOVE IN/MOVE OUT (TEMPORARY EROSION CONTROL)

SHEET No.	MOVE IN/MOVE OUT (TEMPORARY EROSION CONTROL)
	EA
EC-1	2
EC-5	8
EC-10	1
EC-11	1
TOTAL	12

BIOFILTRATION

SHEET No.	STATION	Loc	BIOFILTRATION SWALE	BIOFILTRATION STRIP	COMPOST
			SQFT		
EC-2	"SR99" 282+00 TO 284+00	R+	3,291		3,291
EC-2	"KL1" 280+00 TO 282+49	L+	4,432		4,432
EC-10	"KL2" 302+00 TO 305+33	L+	2,915		2,915
EC-10	"KR2" 302+00 TO 303+40	L+	1,894		1,894
EC-10	"KR2" 303+60 TO 305+00	L+	1,872		1,872
EC-10	"KR2" 305+00 TO 306+54	L+		3,351	
EC-7	"K" 20+00 TO 21+46	L+	1,369		1,369
EC-7,8	"K" 21+54 TO 24+23	L+	3,872		3,872
EC-8	"K" 24+31 TO 25+00	L+	878		878
EC-8	"K" 26+04 TO 27+41	L+	1,516		1,516
EC-8	"K" 27+49 TO 28+14	L+	423		423
SUBTOTAL			22,462	3,351	22,462
TOTAL			25,813*		22,462

(*) - QUANTITY INCLUDED IN HYDROSEED QUANTITIES TABLE.

HYDROSEED

SHEET No.	STATION	Loc	AREA	STRAW (N)	FIBER (N)	SEED (N)	COMMERCIAL FERTILIZER (N)	TACKIFIER (N)
			SQFT	TON	LB			
EC-5,6	"K" 8+60 TO 10+50	R+	3,059	0.14	37	5	9	9
EC-5	"K" 8+47 TO 8+90	L+	348	0.01	4	1	1	1
EC-6,7	"K" 14+87 TO 15+86	L+	3,686	0.17	45	5	11	11
EC-7	"K" 15+92 TO 18+20	L+	10,896	0.50	133	16	33	31
EC-7,8,9	"S" 23+15 TO "K" 25+47	R+ & L+	15,010	0.69	183	22	46	43
EC-8	"K" 25+67 TO 28+14	L+	5,252	0.24	64	8	16	15
EC-10	"KR2" 302+00 TO 304+77	L+	2,019	0.09	25	3	6	6
FROM BIOFILTRATION TABLE			25,813	1.19	315	39	79	74
TOTAL			66,083	3.03	806	99	201	190

(N) - NOT A SEPARATE PAY ITEM, FOR INFORMATION ONLY.

FIBER ROLL

SHEET No.	STATION	Loc	LENGTH
			LF
EC-5,6	"K" 8+60 TO 9+79	R+	231
EC-6,7	"K" 14+86 TO 18+20	L+	1,217
EC-7,8,9	"K" 28+14 TO "S" 23+16	L+/R+	1,846
TOTAL			3,294

TEMPORARY FIBER ROLL

SHEET No.	STATION	Loc	LENGTH
			LF
EC-1,2,5	"SR99" 269+97 TO "KL1" 296+79	L+	2,825
EC-1,2,5	"SR99" 268+49 TO "KR1" 297+21	R+	5,812
EC-2,5	"KL1" 287+37 TO 297+19	R+	1,927
EC-5	"KR1" 294+00 TO 297+13	L+	858
EC-5,10	"KL2" 299+00 TO 305+61	L+	1,790
EC-5,10	"KL2" 298+79 TO 304+60	R+	1,022
EC-5,10	"KR2" 298+04 TO 309+50	R+	1,450
EC-5,10	"SR99" 298+52 TO 306+59	R+	2,203
EC-10,11	"SR99" 310+00 TO 319+36	R+	1,904
TOTAL			19,791

TEMPORARY HYDRAULIC MULCH (BONDED FIBER MATRIX)

SHEET No.	STATION	Loc	AREA
			SQYD
EC-1,2,5	"SR99" 269+97 TO "KL1" 296+85	L+	7,262
EC-1,2,5	"SR99" 268+49 TO "KR1" 297+22	R+	11,311
EC-2,5	"KL1" 287+38 TO 297+16	R+	3,993
EC-5	"KR1" 293+88 TO 297+27	L+	2,588
EC-5,10	"KL2" 298+80 TO 304+60	R+	2,299
EC-5,10	"KL2" 298+95 TO 305+69	L+	3,468
EC-5,10	"KR2" 298+16 TO 305+00	L+	3,985
EC-5,10,11	"KR2" 298+23 TO "SR99" 319+36	R+	5,474
EC-7	"K" 16+25 TO 17+98	L+	995
TOTAL			41,375

TEMPORARY EROSION CONTROL BLANKET

SHEET No.	STATION	Loc	AREA
			SQYD
EC-1,2	"SR99" 273+00 TO "KL1" 282+50	L+	2,502
EC-1,2	"SR99" 272+00 TO 283+89	R+	2,305
EC-5	"KR1" 296+05 TO 297+00	R+	140
EC-5,10	"KL2" 299+08 TO 305+33	L+	888
EC-5,10	"KR2" 298+90 TO 300+26	R+	173
EC-10	"KR2" 302+00 TO 305+00	L+	675
EC-7,8	"K" 20+00 TO 28+14	L+	1,391
TOTAL			8,074

TEMPORARY CONSTRUCTION ENTRANCE

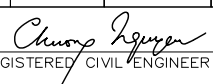

SHEET No.	STATION	Loc	TEMPORARY CONSTRUCTION ENTRANCE
			EA
EC-5	"KR2" 298+90	L+	1
EC-7	"KR2" 16+54	L+	1
TOTAL			2

EROSION CONTROL AND WATER POLLUTION CONTROL QUANTITIES

ECQ-1

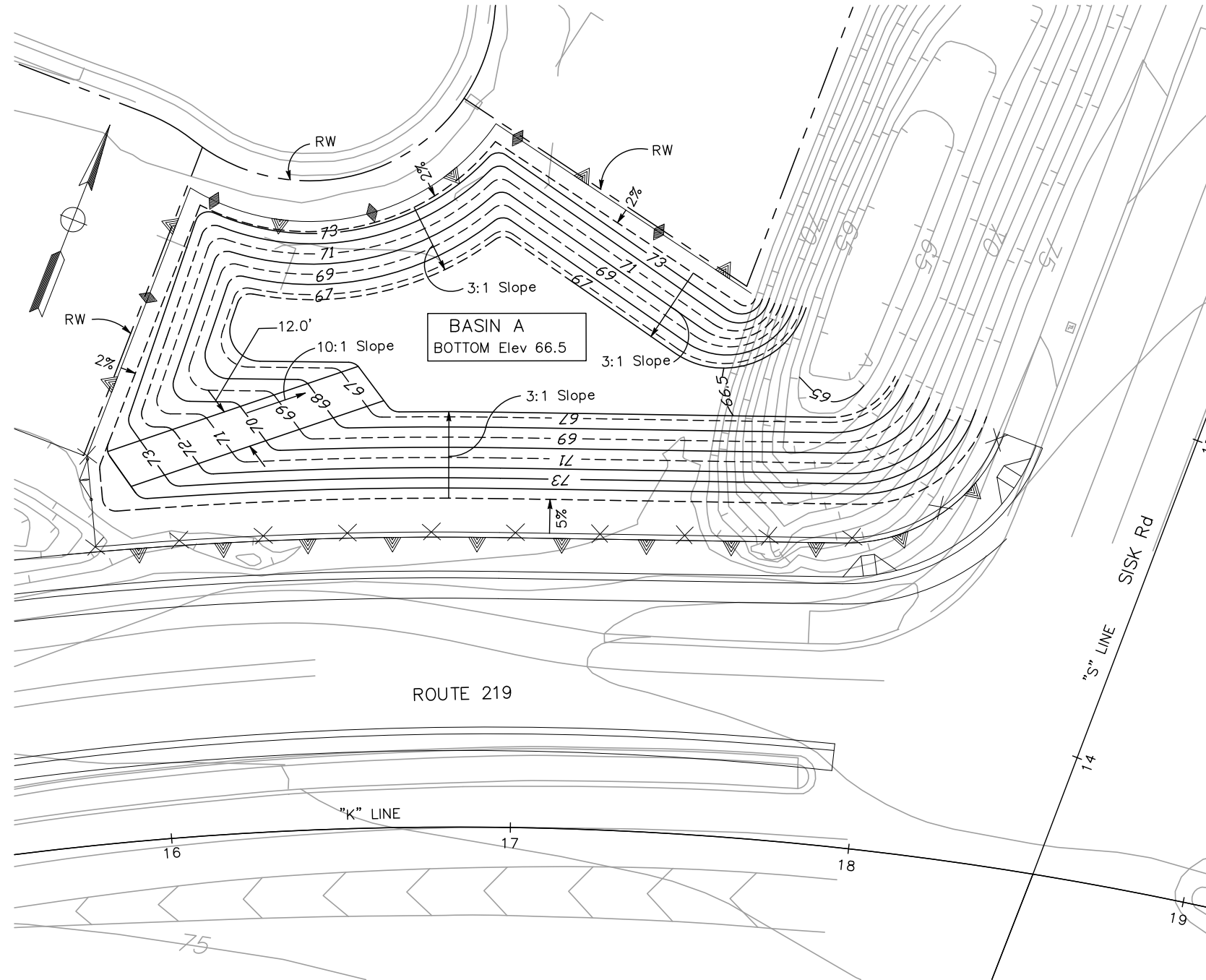
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	62	423

REGISTERED CIVIL ENGINEER *Chuong Nguyen* 05-18-12 DATE
 9/04/12 PLANS APPROVAL DATE
 No. C46770 Exp. 06/30/13
 CHUONG NGUYEN REGISTERED PROFESSIONAL ENGINEER CIVIL STATE OF CALIFORNIA
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 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	63	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
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NOTE :

FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

DESIGNED BY
 CHECKED BY
 TINH TRUONG
 CHUONG NGUYEN

REVISOR
 DATE
 REVISED BY
 DATE REVISED

CONTOUR GRADING
 SCALE: 1" = 20' **G-1**

APPROVED FOR CONTOUR GRADING WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

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 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
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 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTE :

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR PUMP STATION DETAILS, SEE PUMPING PLANT PLANS.
- FOR UNLINED DITCH, BIOFILTRATION SWALE DETAILS, BIOFILTRATION STRIP DETAILS, SEE DRAINAGE DETAIL PLANS.
- PROVIDE DRAINAGE INLET MARKERS PER STANDARD PLANS DETAIL D71 FOR ALL INLETS ADJACENT TO PEDESTRIAN FACILITIES.

LEGEND

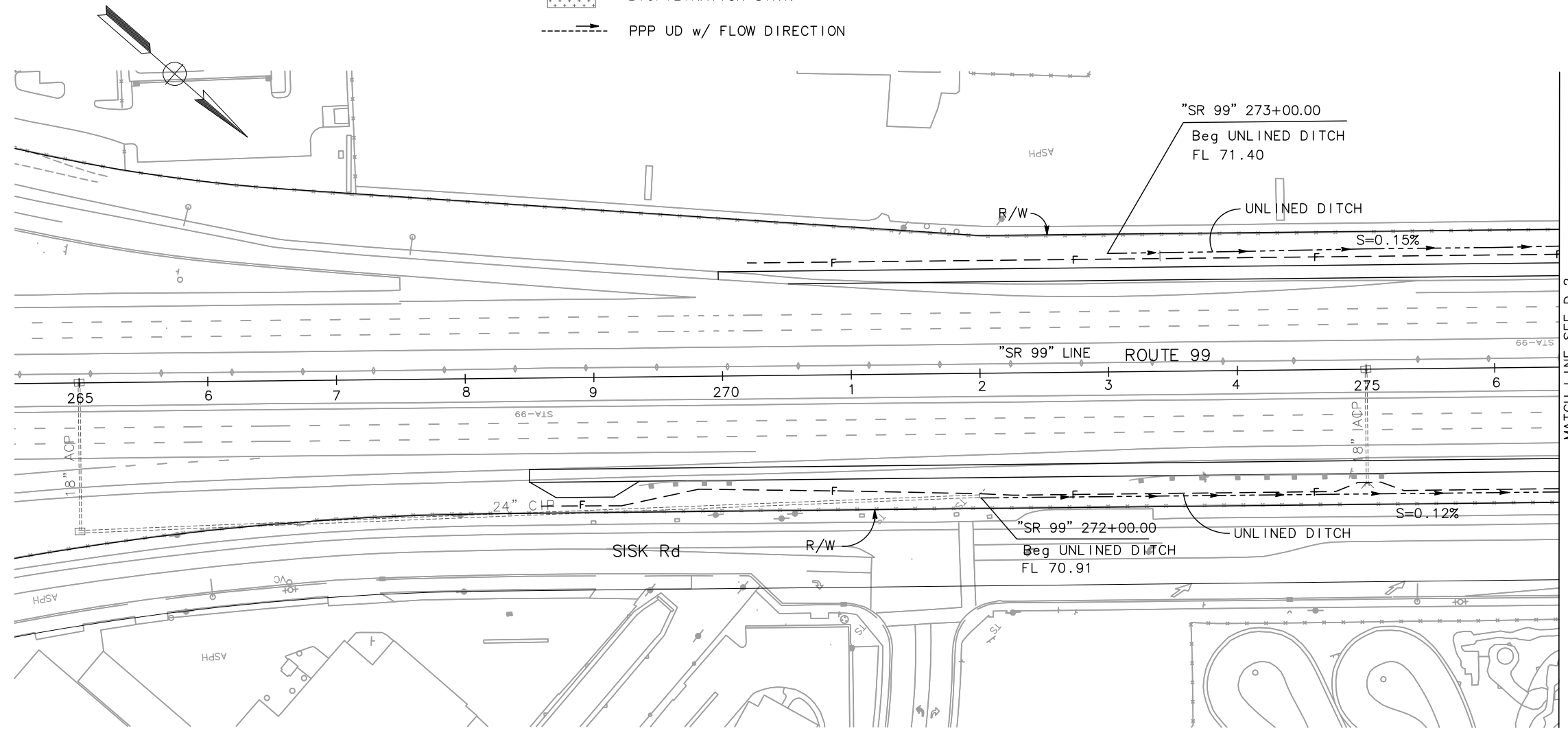
- EXISTING DRAINAGE INLET
- ==== EXISTING DRAINAGE CULVERT
- ==/== ABANDON CULVERT
- X-X- REMOVE CULVERT
- CULVERT
- - - - DITCH
- ▷ FES
- ⊙ GCP INLET
- INLET
- ⊕ MANHOLE
- ⊗ MODIFY INLET TO MANHOLE
- RSP
- ▨ BIOFILTRATION STRIP
- - - -> PPP UD w/ FLOW DIRECTION

ABBREVIATIONS

- BWS BIOFILTRATION SWALE
- SGD STANDARD GUTTER DEPRESSION

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	64	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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DRAINAGE PLAN
 SCALE: 1" = 50' D-1

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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: TINH TRUONG
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

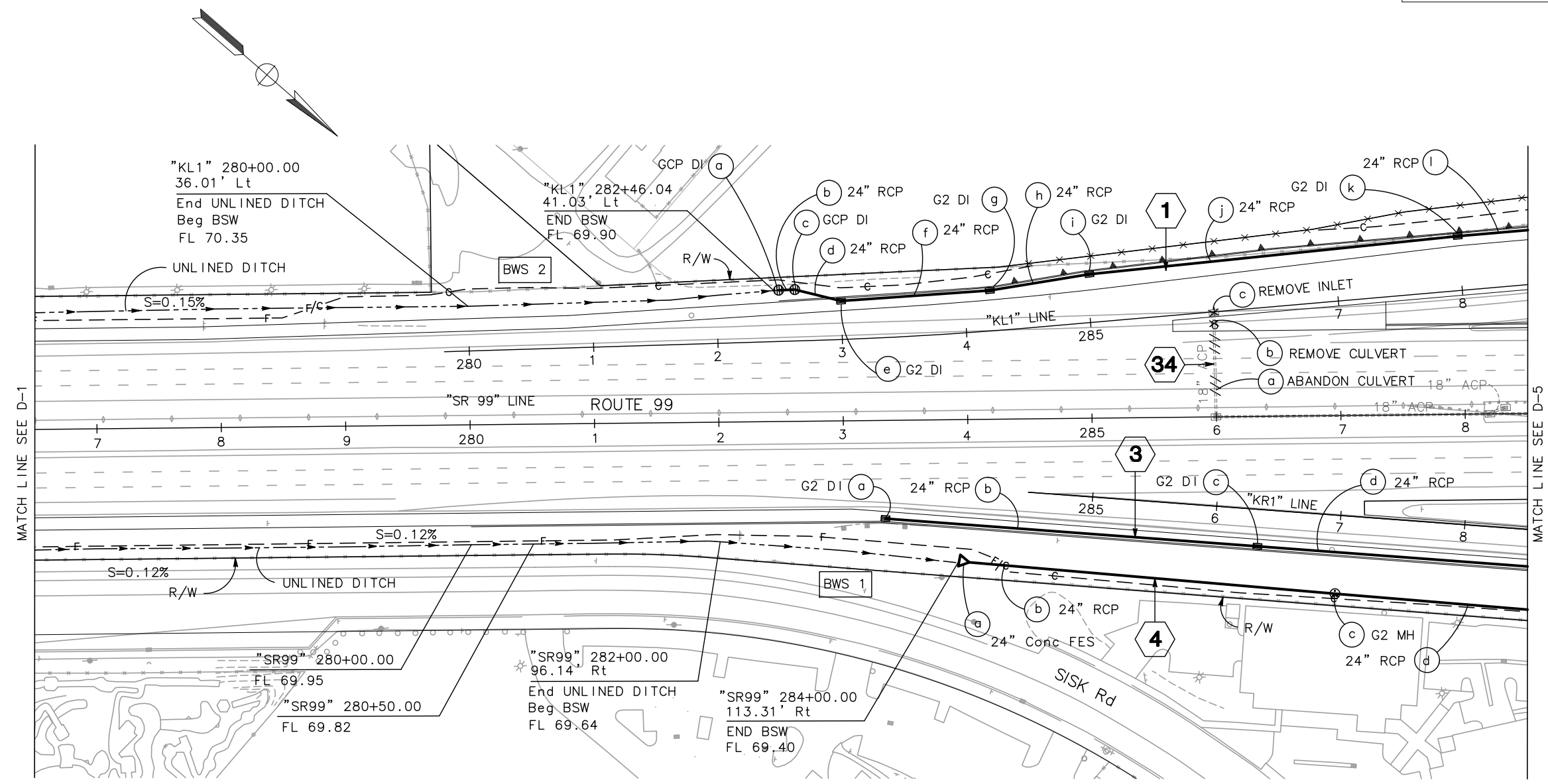
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	65	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



DRAINAGE PLAN
 SCALE: 1" = 50' D-2

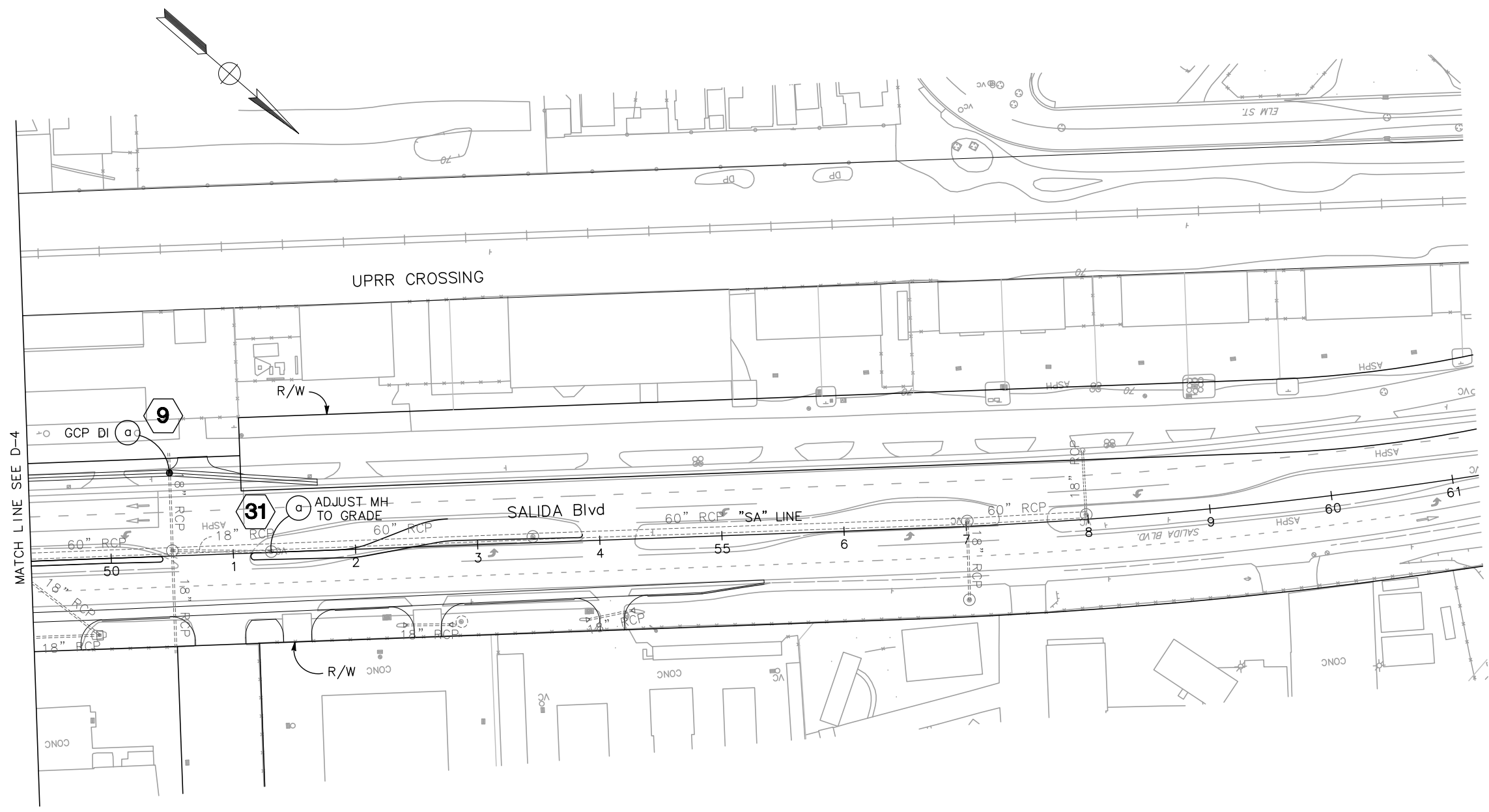
APPROVED FOR DRAINAGE WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 TINH TRUONG
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	66	423
			REGISTERED CIVIL ENGINEER CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA		05-18-12 DATE
			9/04/12 PLANS APPROVAL DATE		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



DRAINAGE PLAN
 SCALE: 1" = 50' **D-3**

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Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: TINH TRUONG
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

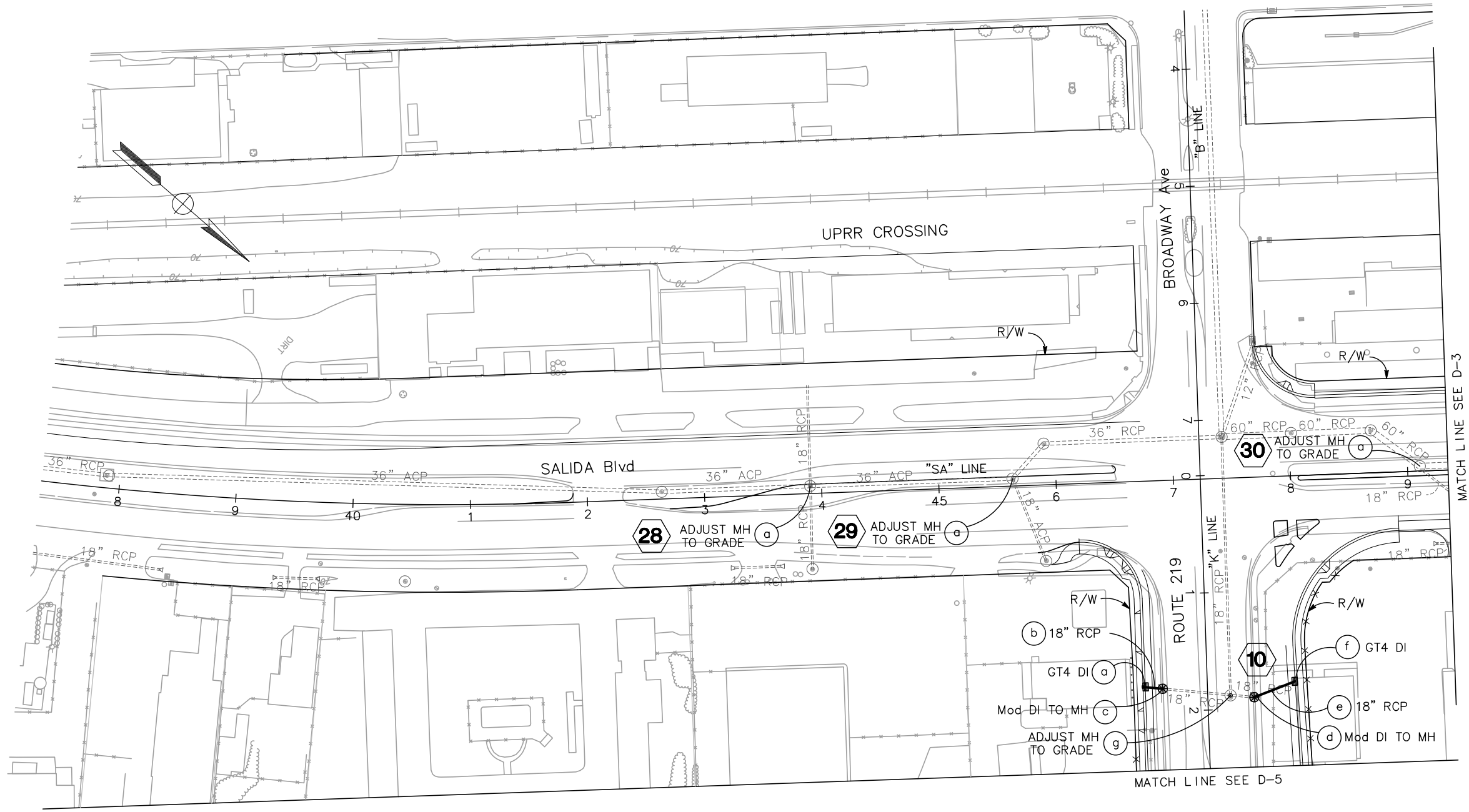
NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	67	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 9/04/12
 PLANS APPROVAL DATE
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 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358




DRAINAGE PLAN
 SCALE: 1" = 50' **D-4**

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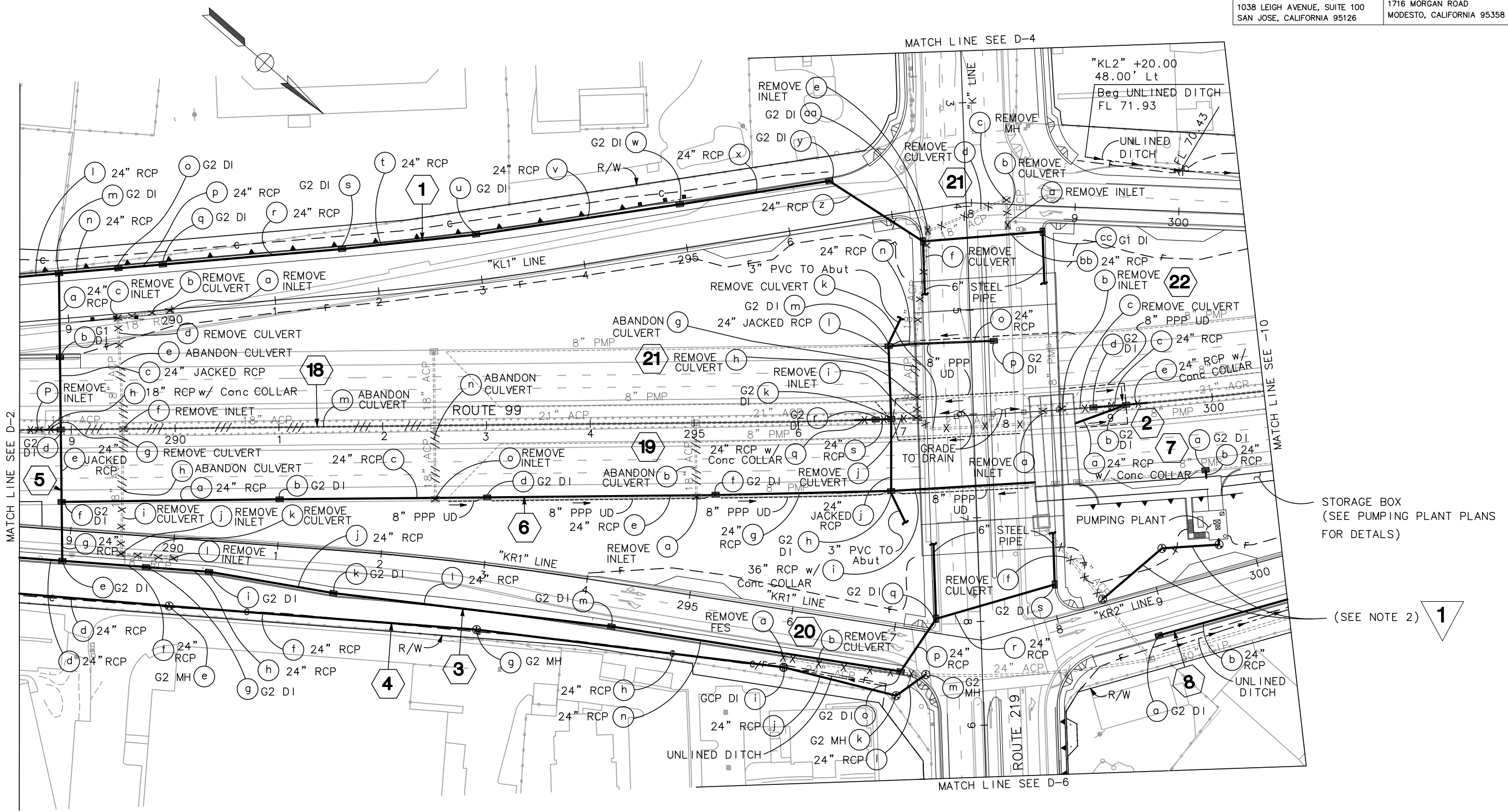
LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	68	423

 REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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NOTE :

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR INFORMATION OF DISCHARGE PIPES AND MANHOLES FROM PUMPING PLANT TO THE EXISTING SD PIPE, SEE PUMPING PLANT PLANS.



1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

DRAINAGE PLAN
SCALE: 1" = 50' **D-5**

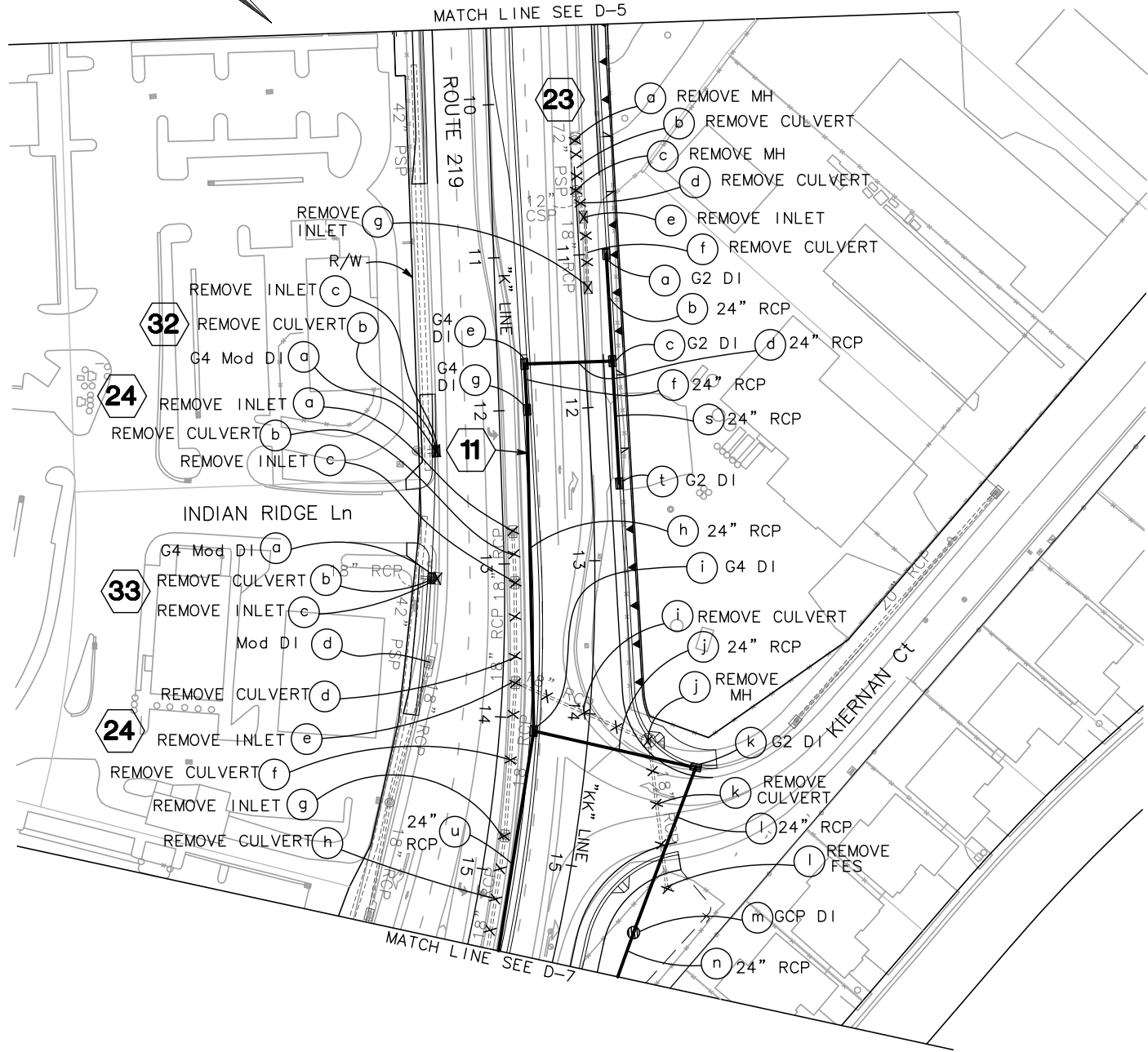
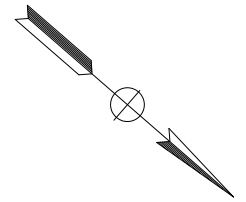
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 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DRAWN BY
 TINH TRUONG
 CHECKED BY
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
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 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
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 TINH TRUONG CHUONG NGUYEN
 REVISED BY: DATE REVISED:

NOTE:
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 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	69	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

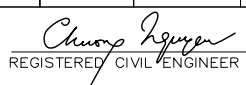
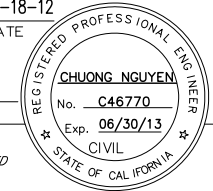
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 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

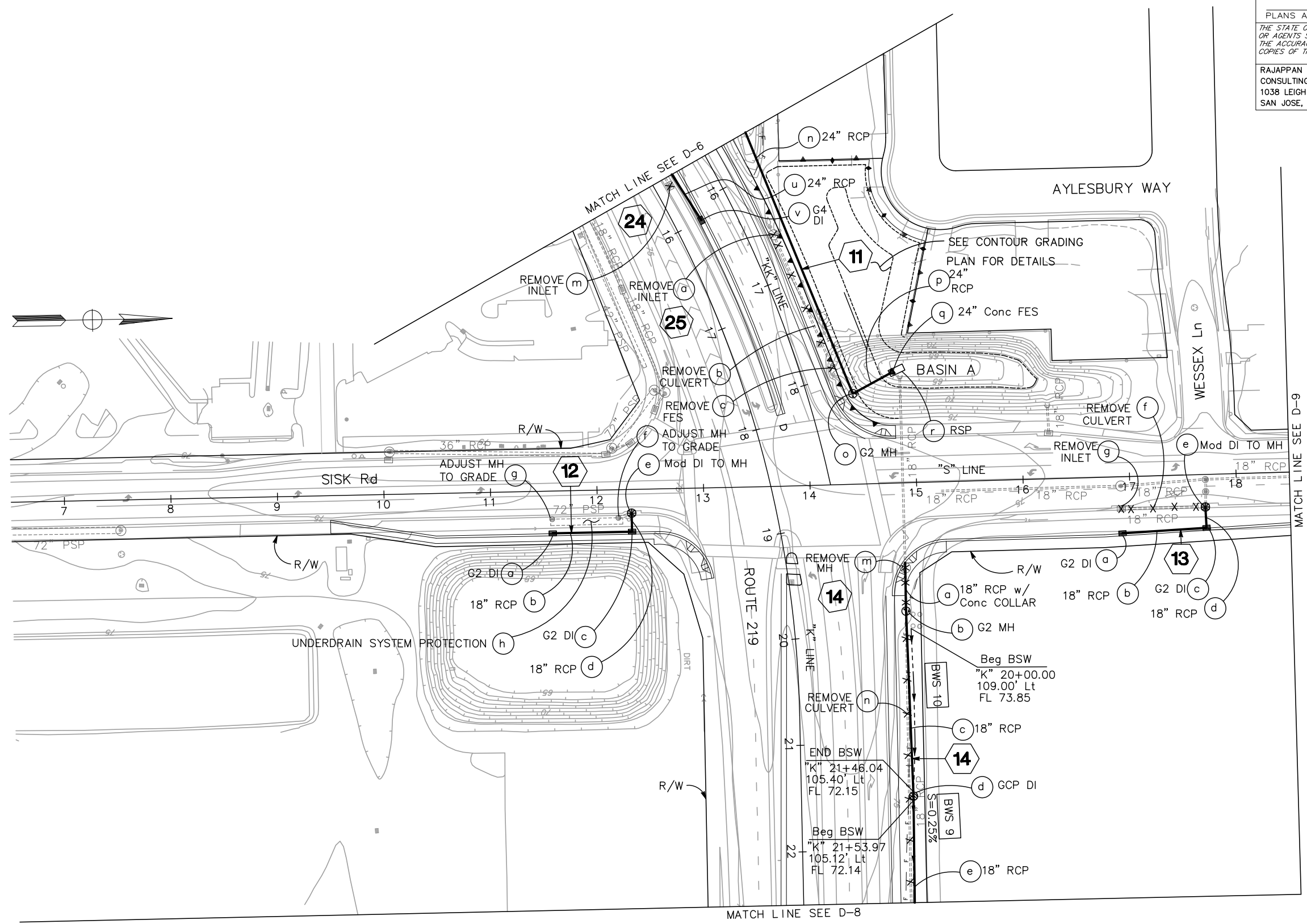
DRAINAGE PLAN
 SCALE: 1" = 50' **D-6**

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LAST REVISION: 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	70	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
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 DATE REVISED: []

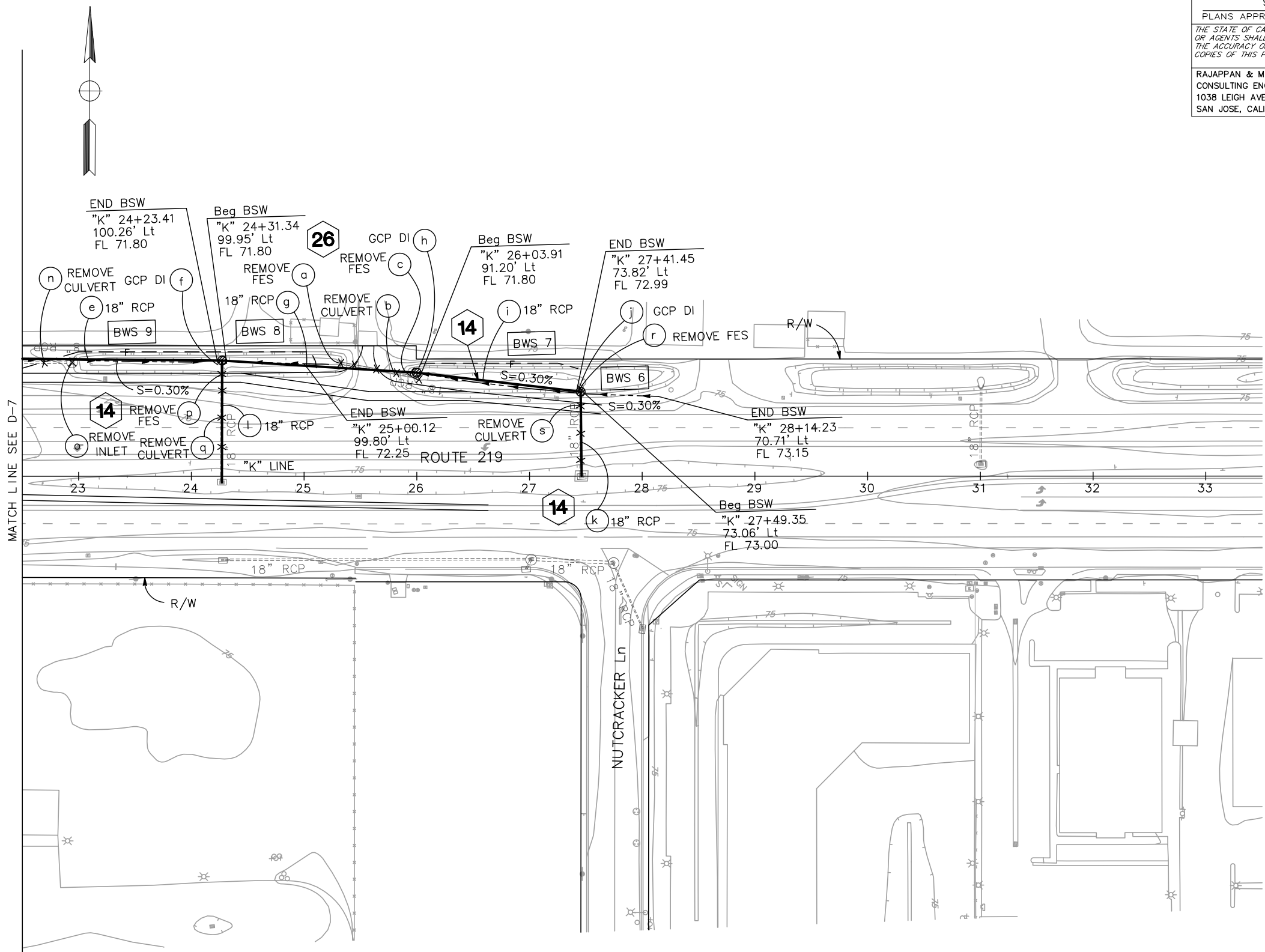
DRAINAGE PLAN
 SCALE: 1" = 50' D-7

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 06-08-12
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: TINH TRUONG
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	71	423

REGISTERED CIVIL ENGINEER <i>Chuong Nguyen</i> No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA	DATE 05-18-12 9/04/12 PLANS APPROVAL DATE
--	--

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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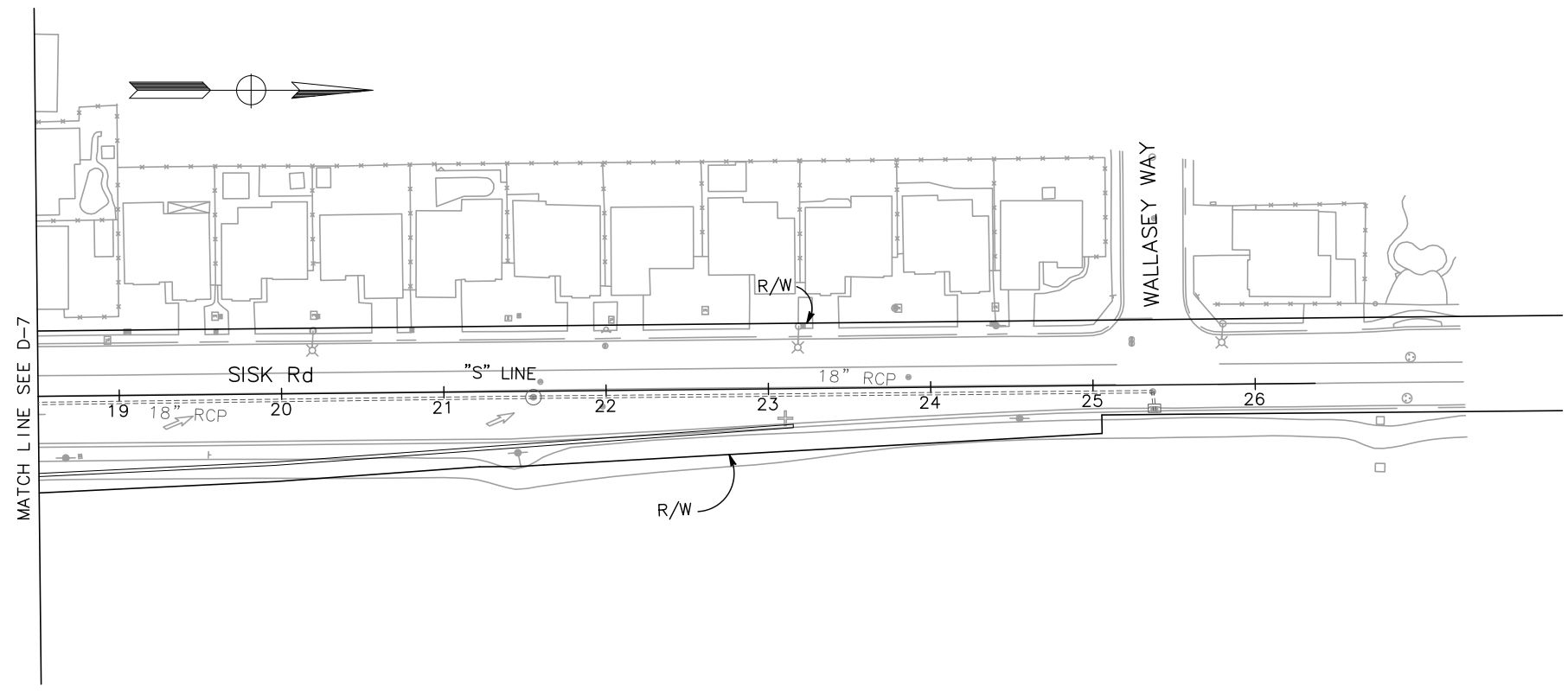
DRAINAGE PLAN
 SCALE: 1" = 50' D-8

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LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	72	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA
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DRAINAGE PLAN
 SCALE: 1" = 50' **D-9**

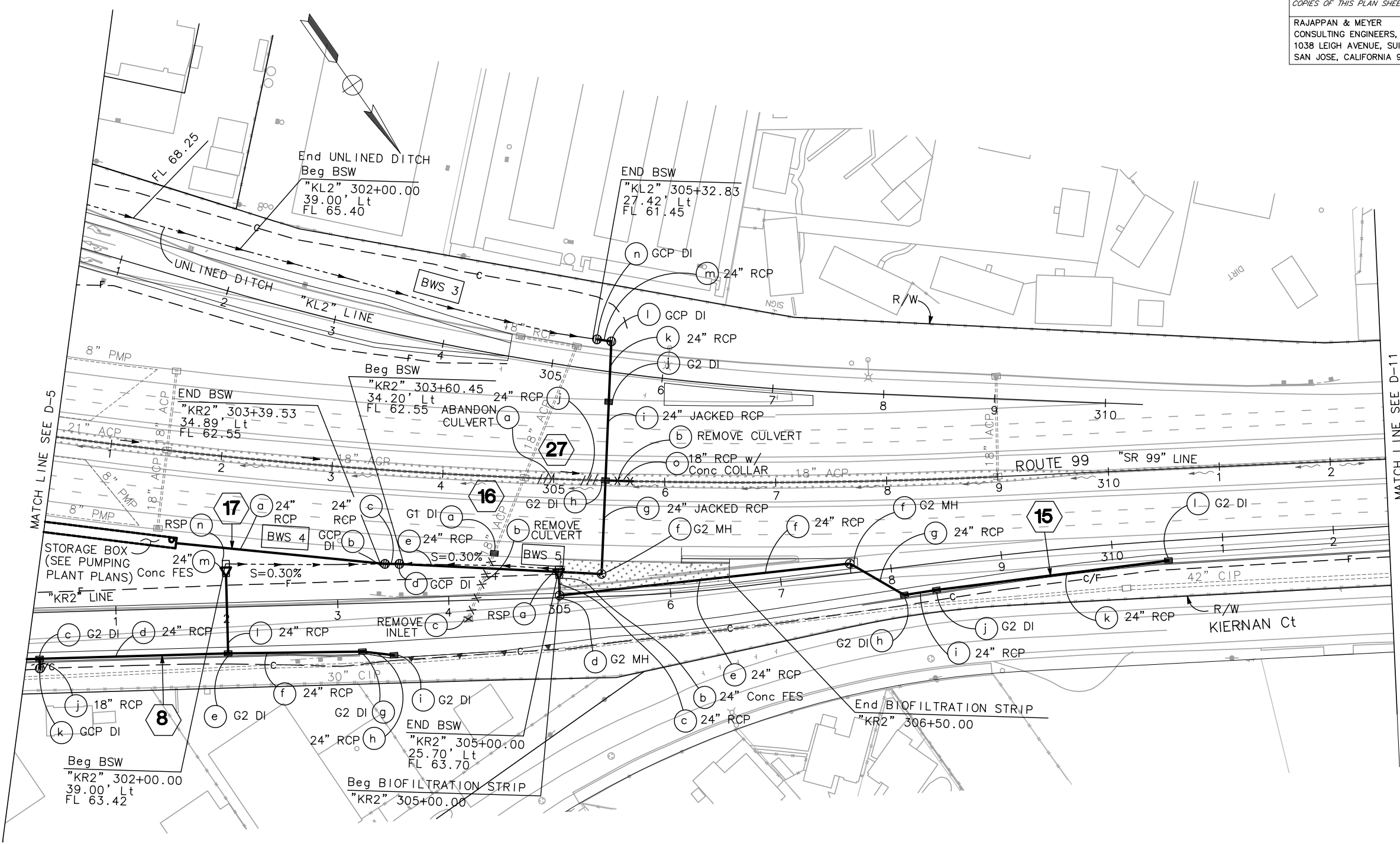
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 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	73	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-18-12 DATE
9/04/12 PLANS APPROVAL DATE		
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NOTE:
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 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
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 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED—DESIGNED BY: CHUONG NGUYEN
 REVISOR: TINH TRUONG
 DATE REVISOR: CHUONG NGUYEN

DRAINAGE PLAN
 SCALE: 1" = 50' D-10

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 TIME PLOTTED => \$TIME
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Caltrans
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 TINH TRUONG
 CHUONG NGUYEN
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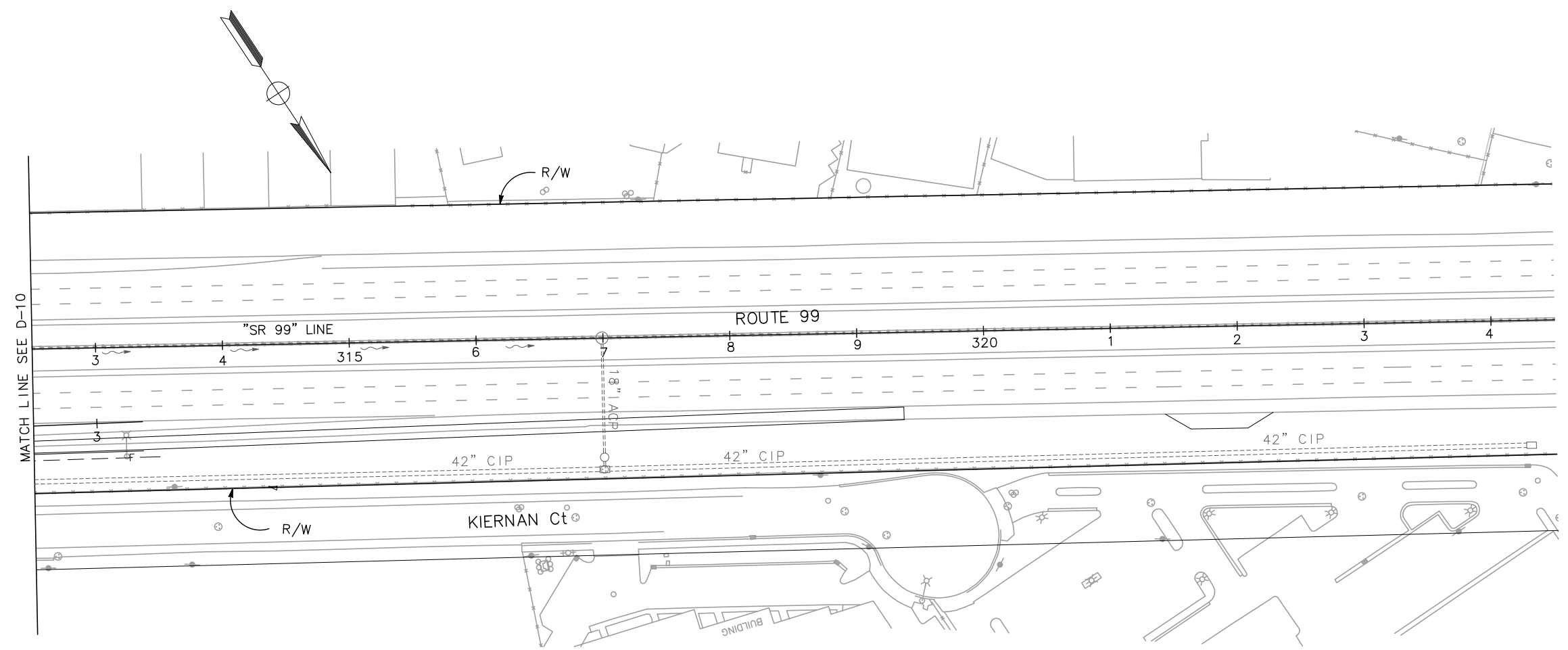
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	74	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

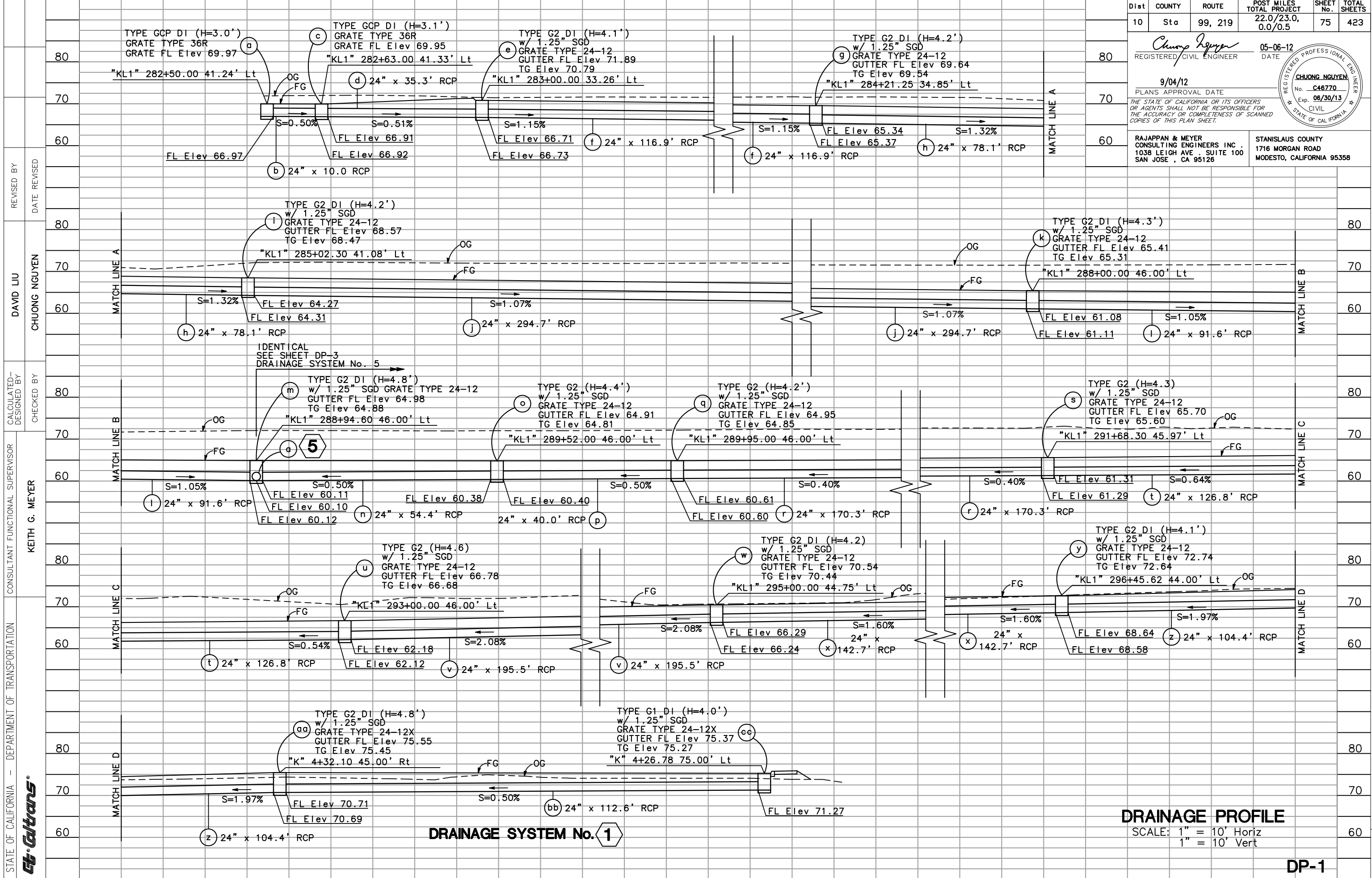
STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



DRAINAGE PLAN
 SCALE: 1" = 50' **D-11**

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Dist	COUNTY	ROUTE	POST MILES	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	75	423

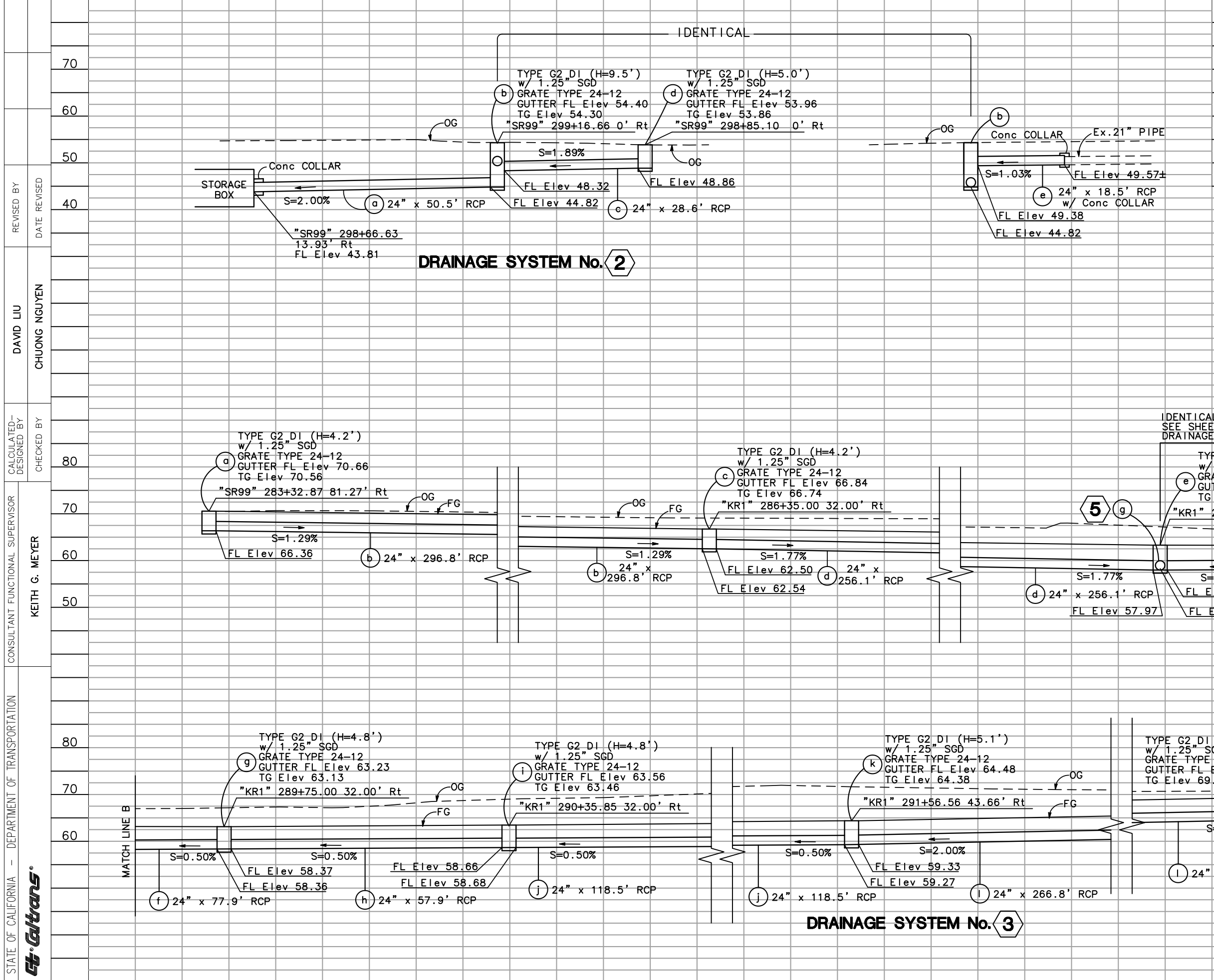
<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER DATE 05-06-12 9/04/12 PLANS APPROVAL DATE <small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
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REVISOR: DAVID LIU, CHUONG NGUYEN, KEITH G. MEYER
 CHECKED BY: CHUONG NGUYEN, KEITH G. MEYER
 CALCULATED/DESIGNED BY: CHUONG NGUYEN, KEITH G. MEYER
 CONSULTANT SUPERVISOR: KEITH G. MEYER
 DEPARTMENT OF TRANSPORTATION
 STATE OF CALIFORNIA

DRAINAGE SYSTEM No. 1

DRAINAGE PROFILE
 SCALE: 1" = 10' Horiz
 1" = 10' Vert

DP-1



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	76	423

		05-06-12
REGISTERED CIVIL ENGINEER		DATE
9/04/12		
PLANS APPROVAL DATE		
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RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

REVISIONS: 70, 60, 50, 40, 80, 70, 60, 50, 80, 70, 60

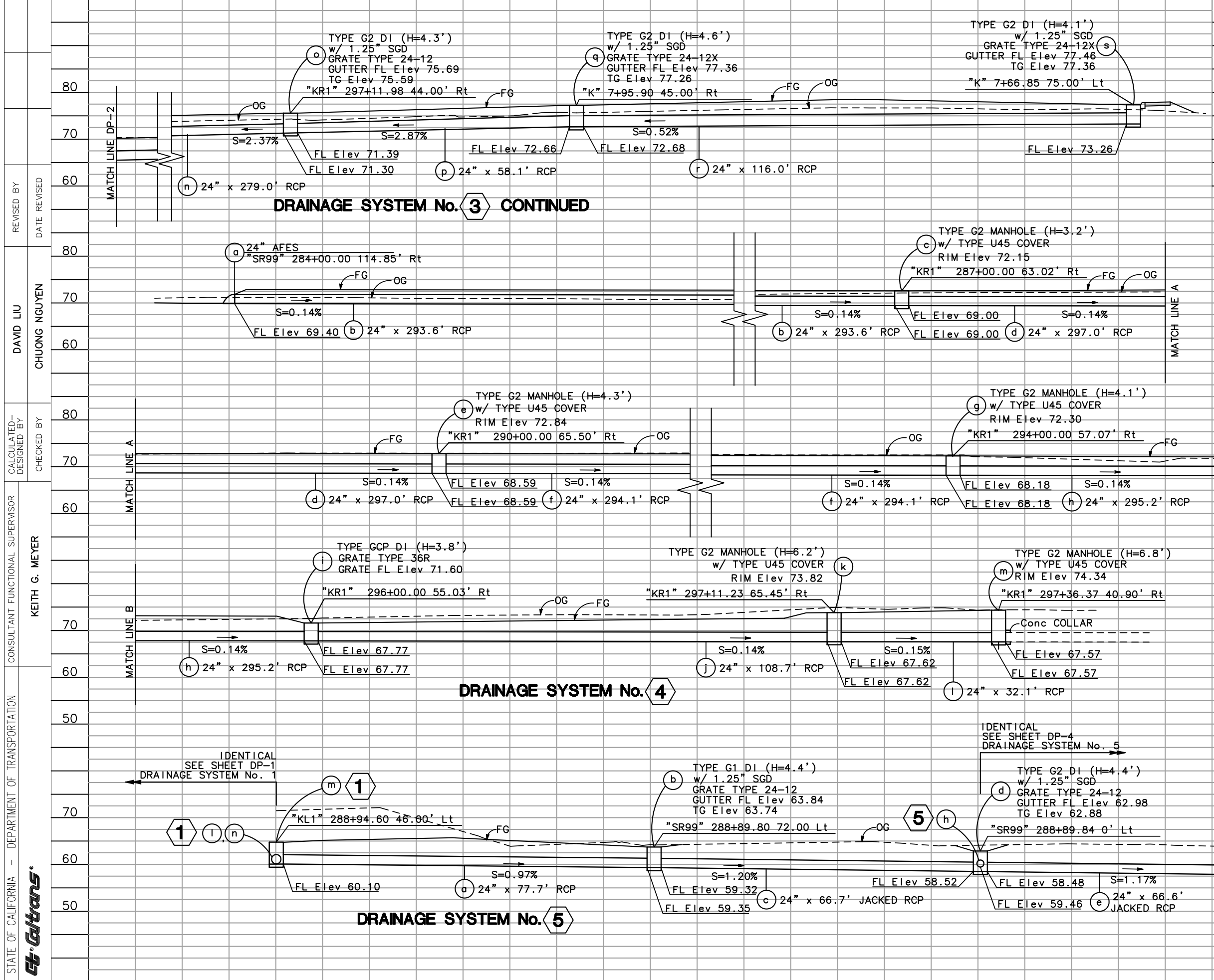
REVISOR: DAVID LIU, CHUONG NGUYEN, KEITH G. MEYER

CHECKED BY: CHUONG NGUYEN

CALCULATED/DESIGNED BY: CHUONG NGUYEN

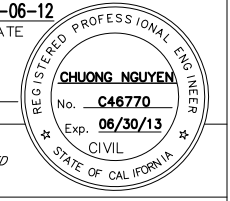
CONSULTANT SUPERVISOR: KEITH G. MEYER

DEPARTMENT OF TRANSPORTATION



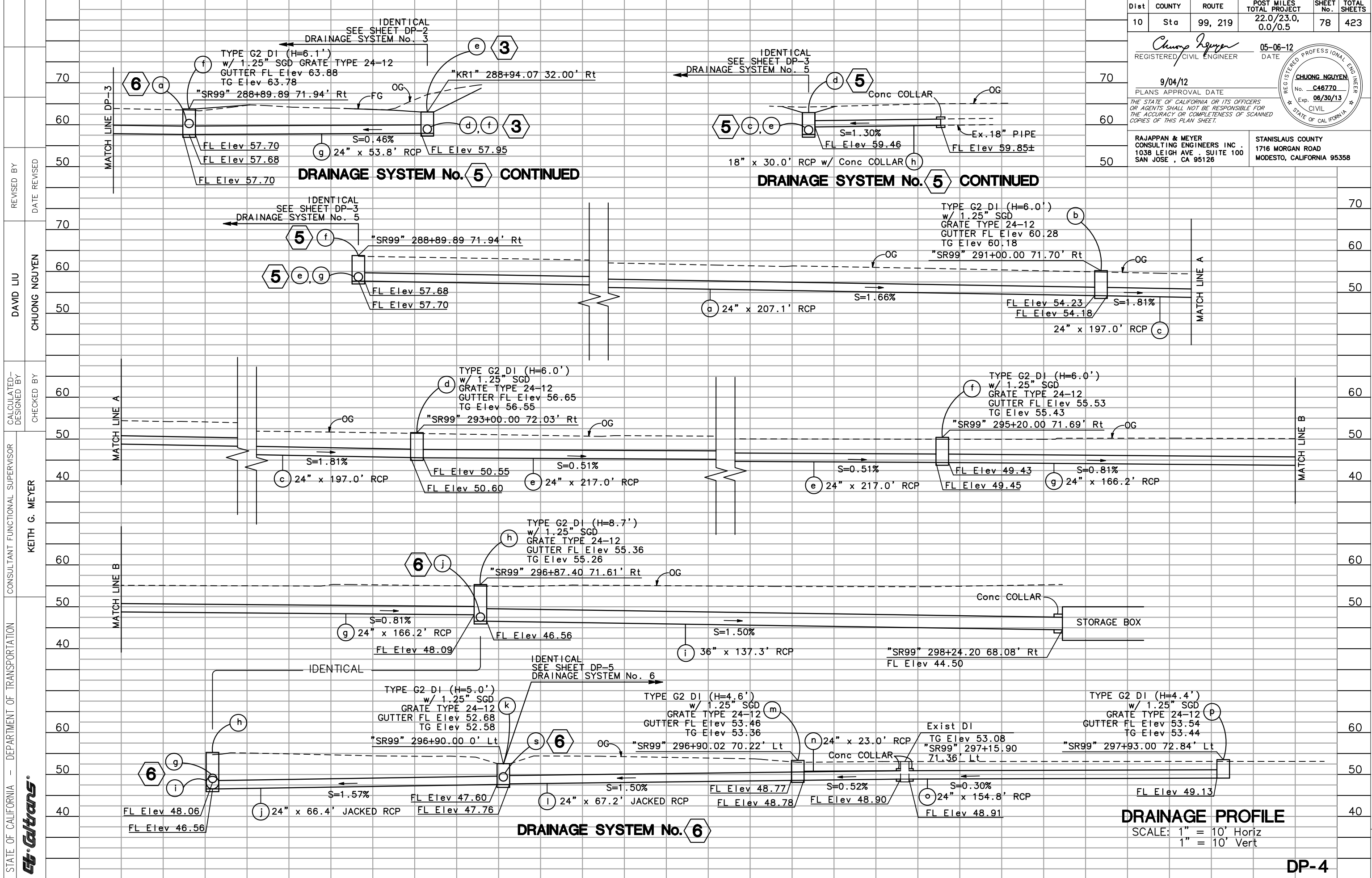
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	77	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
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 RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



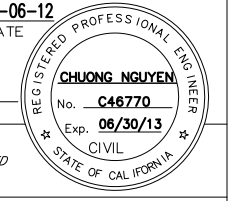
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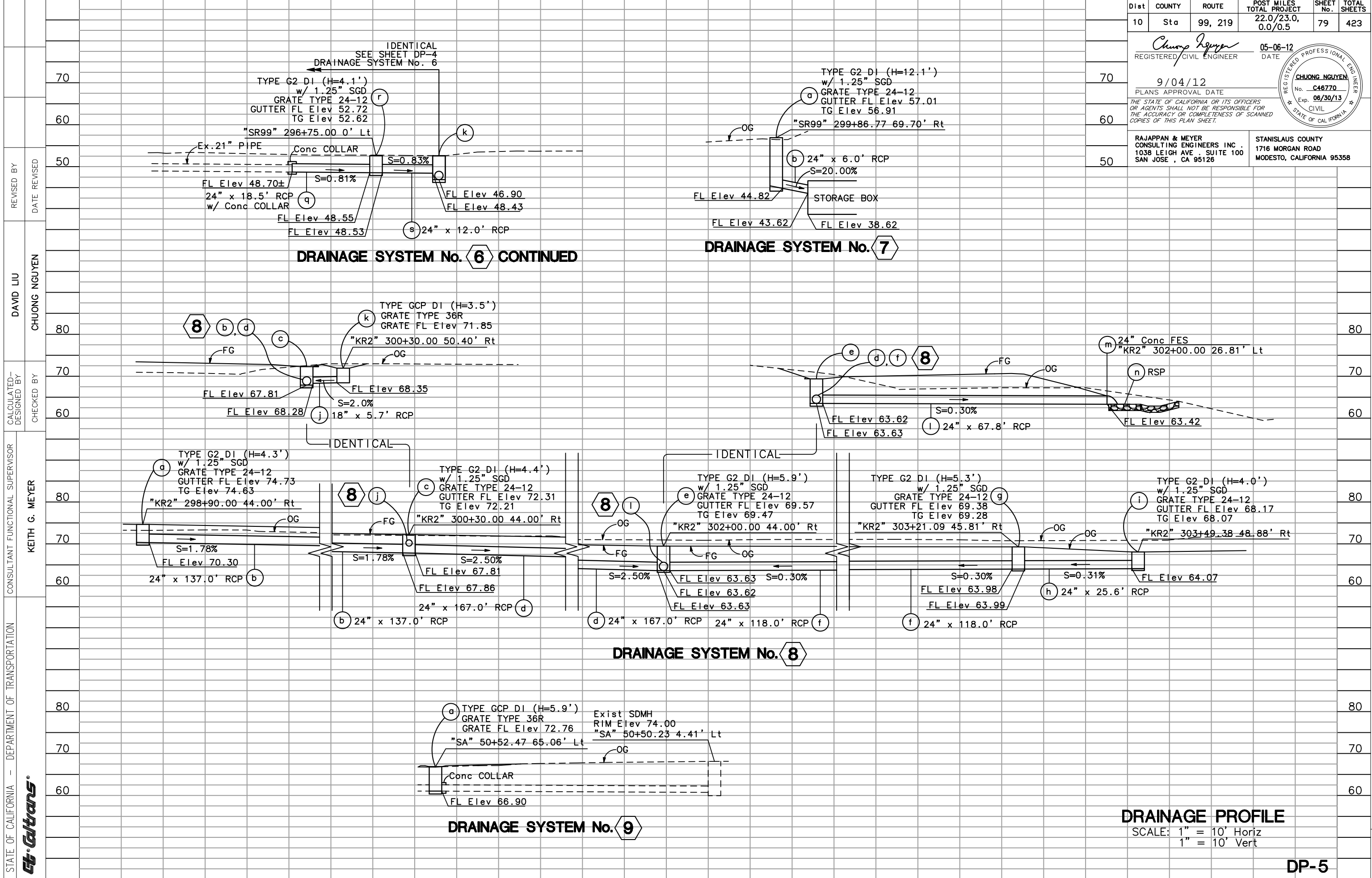
DRAINAGE PROFILE
 SCALE: 1" = 10' Horiz
 1" = 10' Vert
DP-3



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
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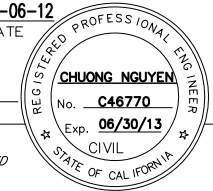
<i>Chuong Nguyen</i>		05-06-12
REGISTERED CIVIL ENGINEER		DATE
9/04/12		
PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.		
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358





Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	79	423

REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



REVISIONS: 70, 60, 50, 80, 70, 60, 80, 70, 60, 80, 70, 60

REVISIONS: 80, 70, 60, 80, 70, 60, 80, 70, 60, 80, 70, 60

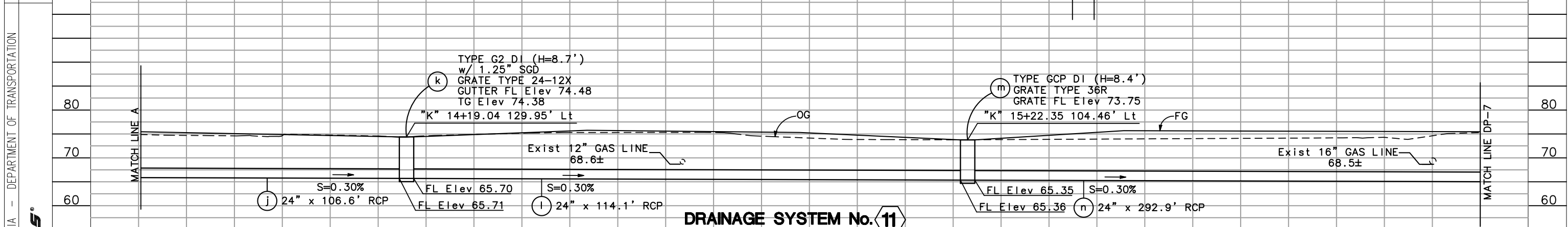
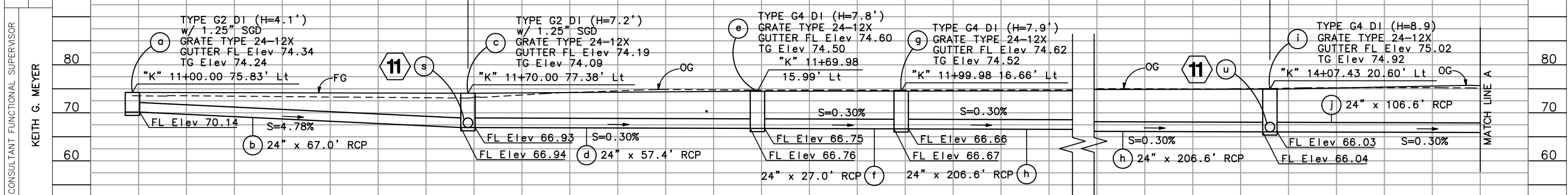
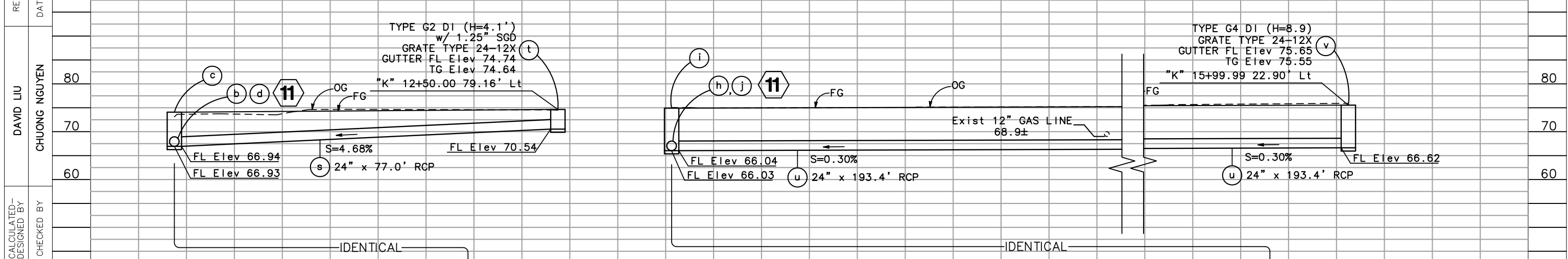
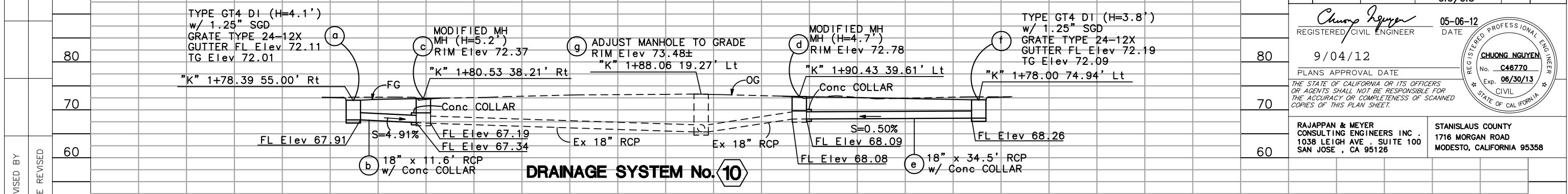
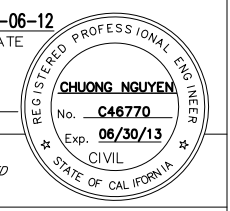
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REVISIONS: 80, 70, 60, 80, 70, 60, 80, 70, 60, 80, 70, 60

DRAINAGE PROFILE
 SCALE: 1" = 10' Horiz
 1" = 10' Vert

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	80	423

REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

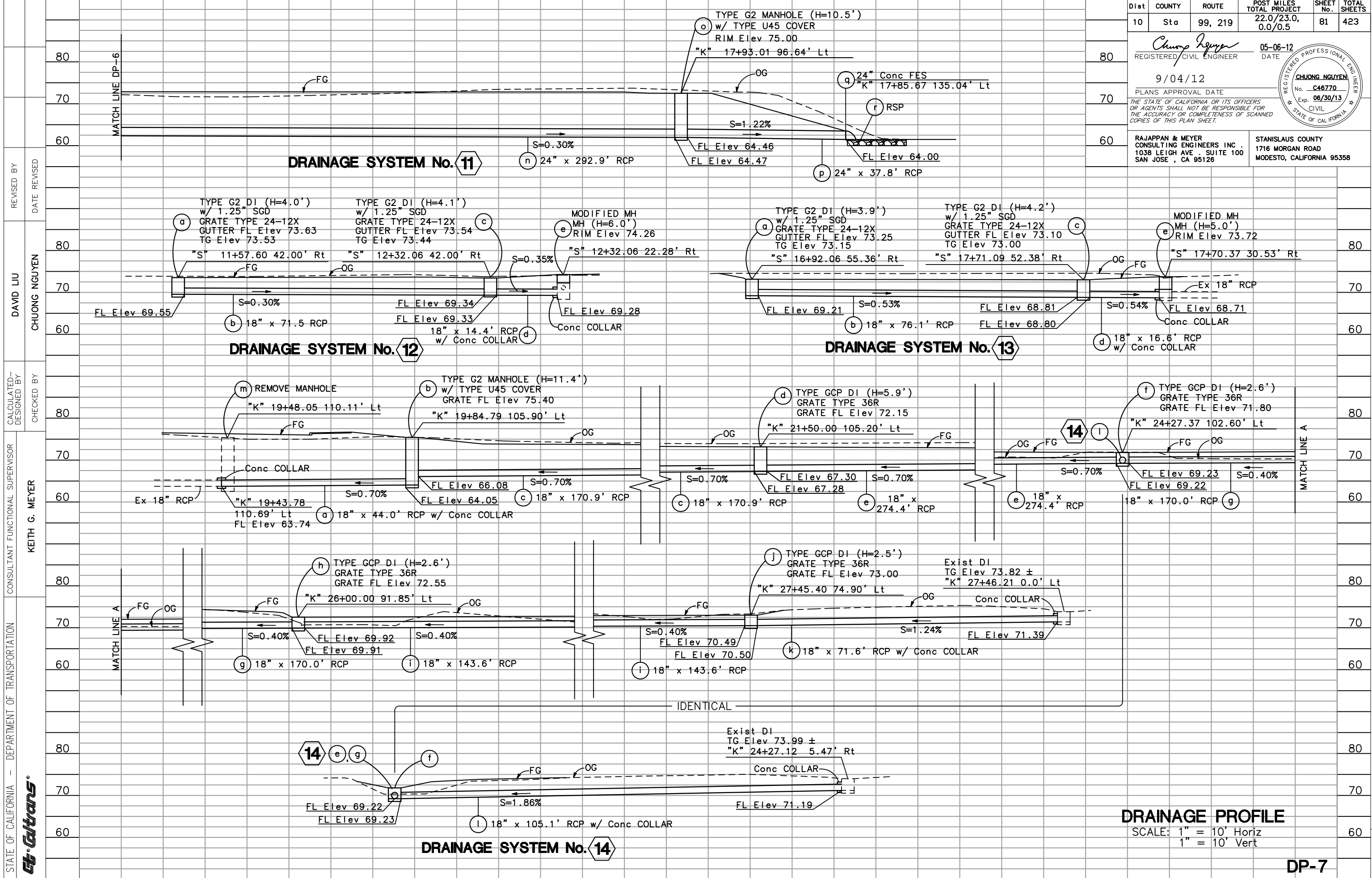


DRAINAGE PROFILE
 SCALE: 1" = 10' Horiz
 1" = 10' Vert

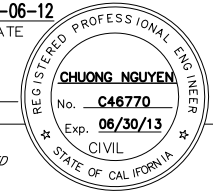
DP-6

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12



Dist	COUNTY	ROUTE	POST MILES	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	81	423
REGISTERED CIVIL ENGINEER			DATE	05-06-12	
9/04/12			PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



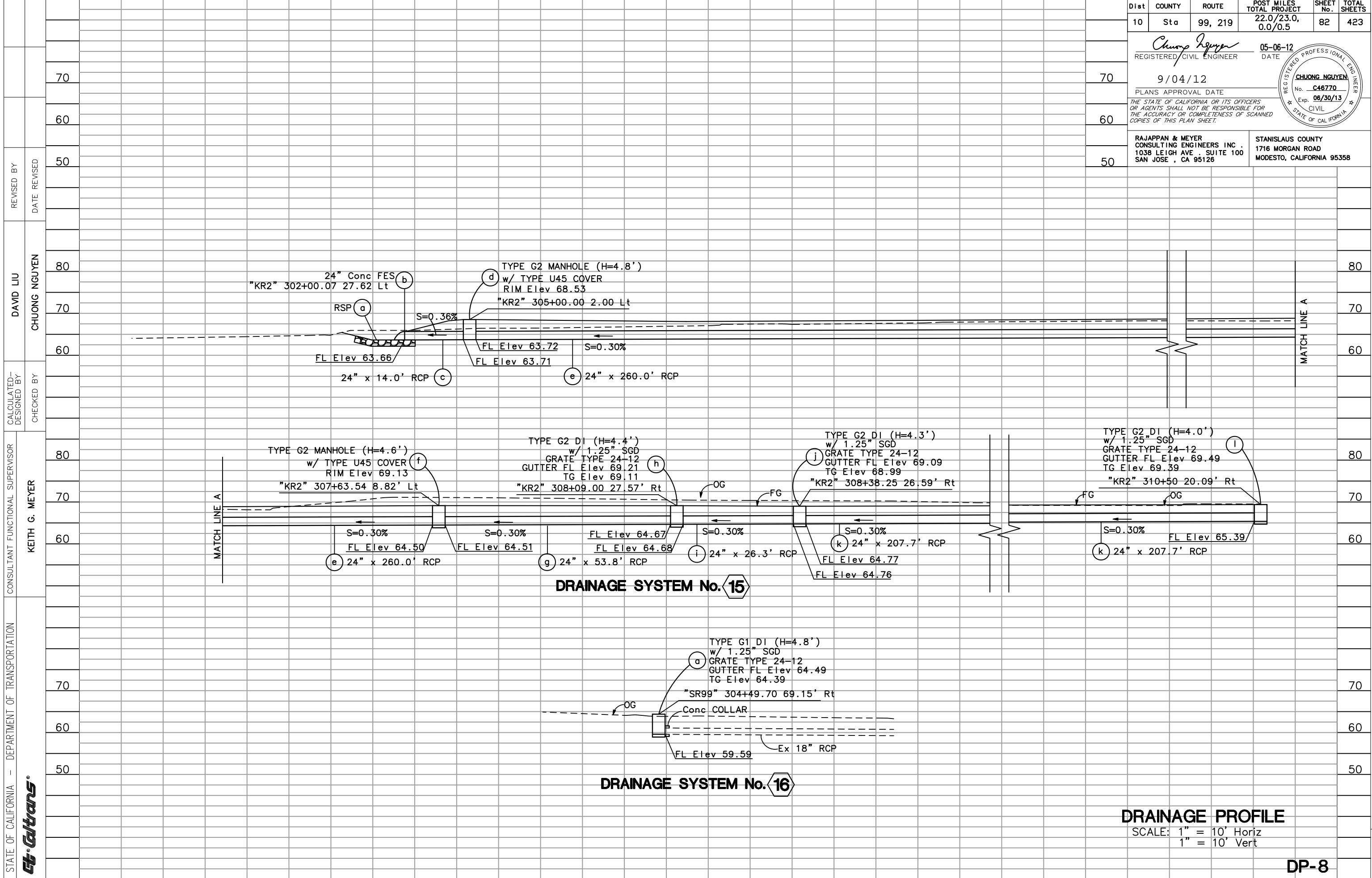
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

REVISOR: DAVID LIU, CHUONG NGUYEN, KEITH G. MEYER

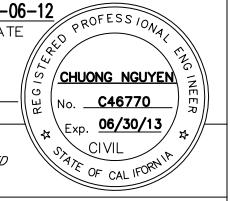
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DRAINAGE PROFILE
 SCALE: 1" = 10' Horiz
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DP-7



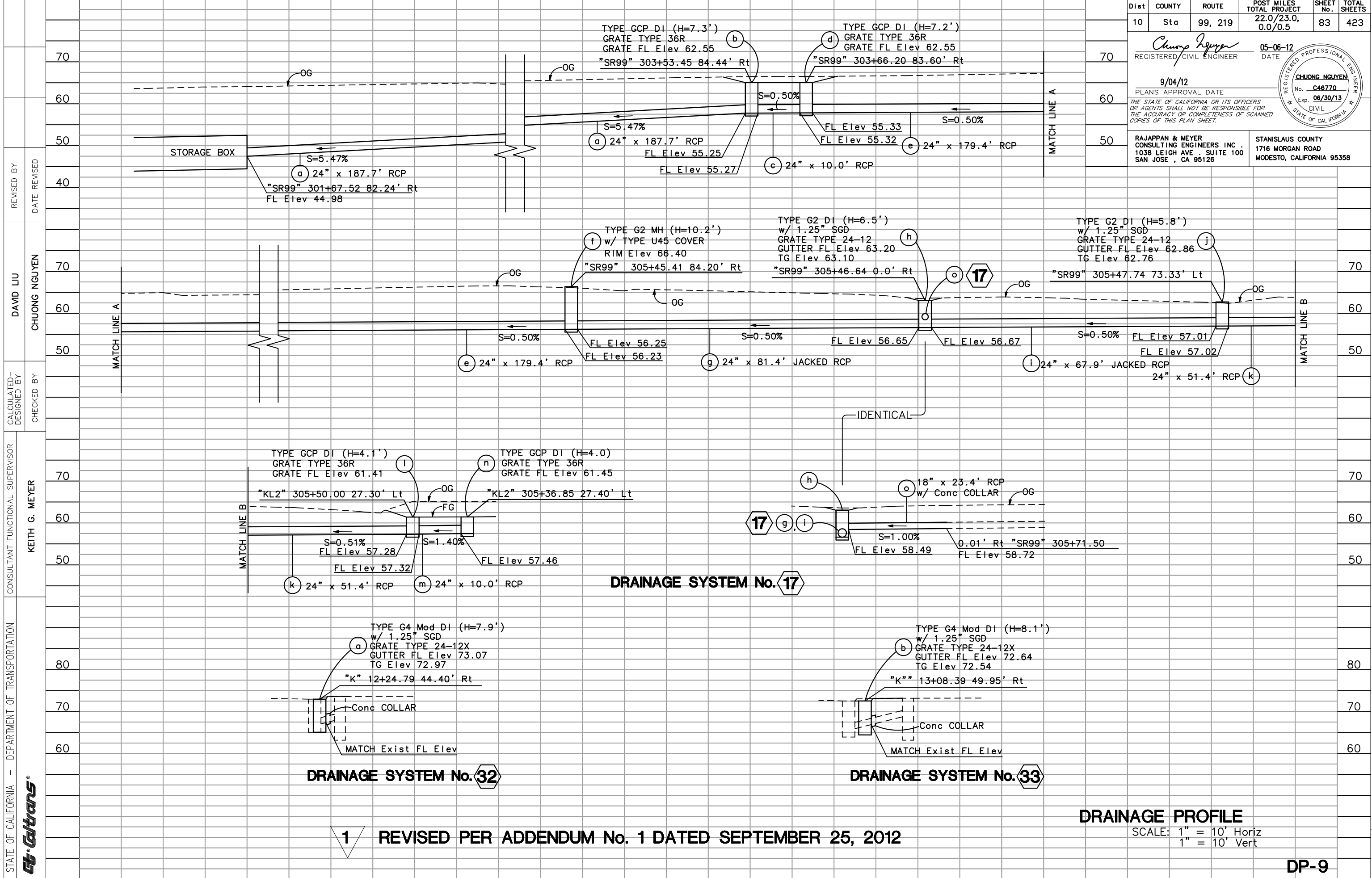
Dist	COUNTY	ROUTE	POST MILES	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	82	423
			05-06-12		
			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



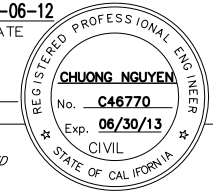
DRAINAGE PROFILE
 SCALE: 1" = 10' Horiz
 1" = 10' Vert

DP-8

DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME



Dist	COUNTY	ROUTE	POST MILES	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	83	423
9/04/12 REGISTERED CIVIL ENGINEER			05-06-12 DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
RAJAPPAN & MEYER CONSULTING ENGINEERS INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

Caltrans

CONSULTANT SUPERVISOR: KEITH G. MEYER

CHECKED BY: CHUONG NGUYEN

DESIGNED BY: CHUONG NGUYEN

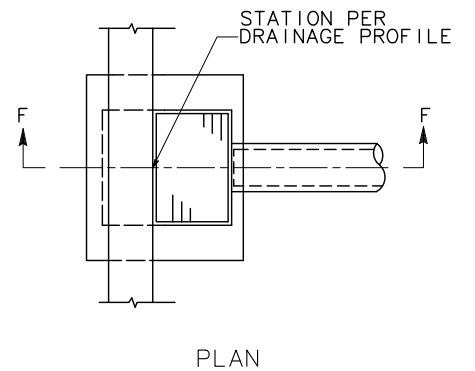
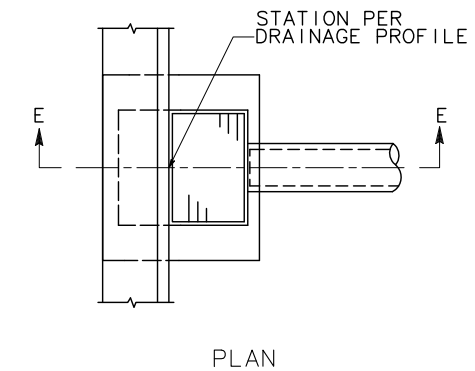
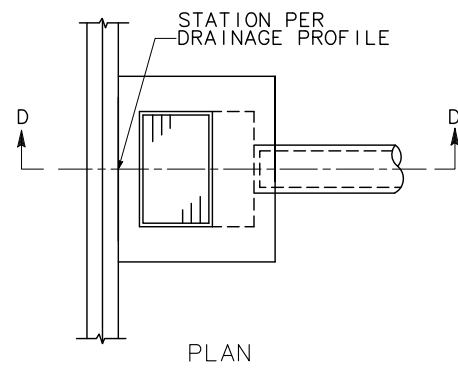
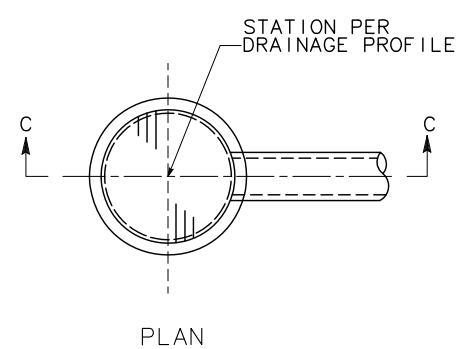
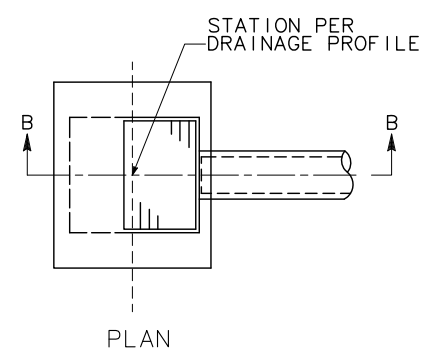
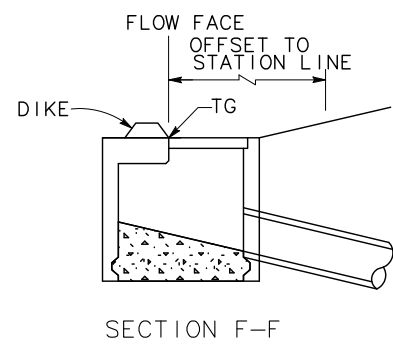
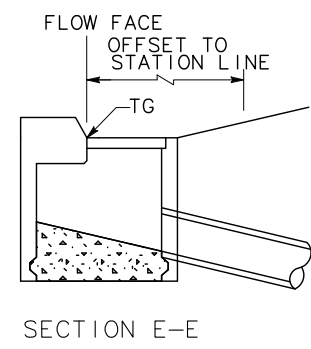
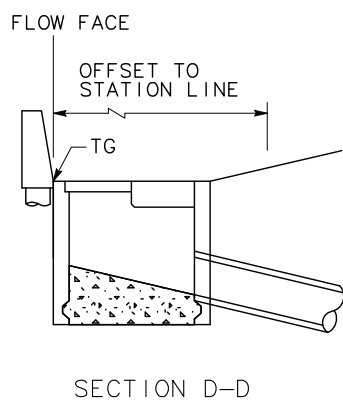
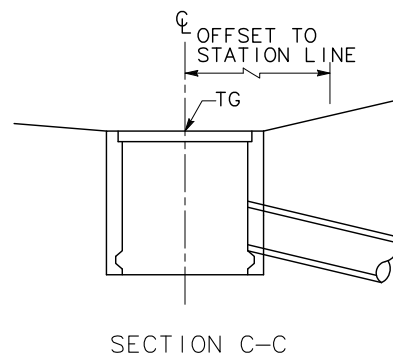
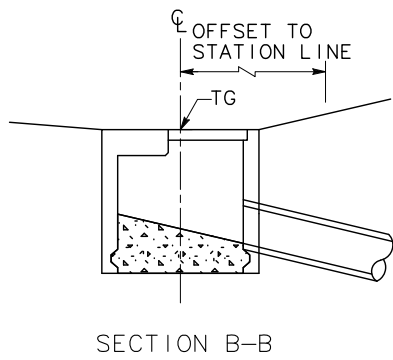
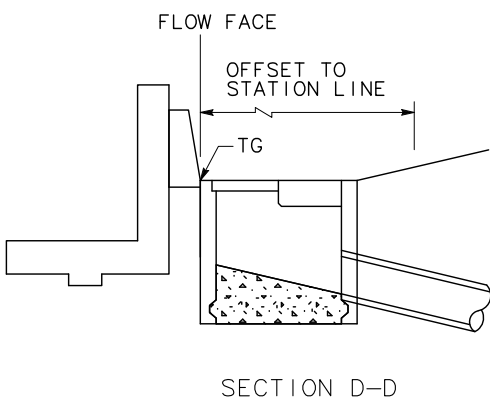
REVISIONS:

NO.	DATE	REVISION
1	09/25/12	REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	84	423

<i>Chung Nguyen</i> REGISTERED CIVIL ENGINEER		05-18-12 DATE
9/04/12 PLANS APPROVAL DATE		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		
STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



FIELD INLET

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INLET ADJACENT TO CURB

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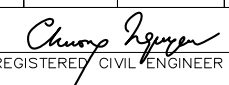
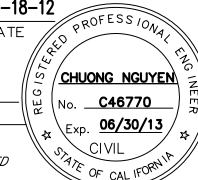
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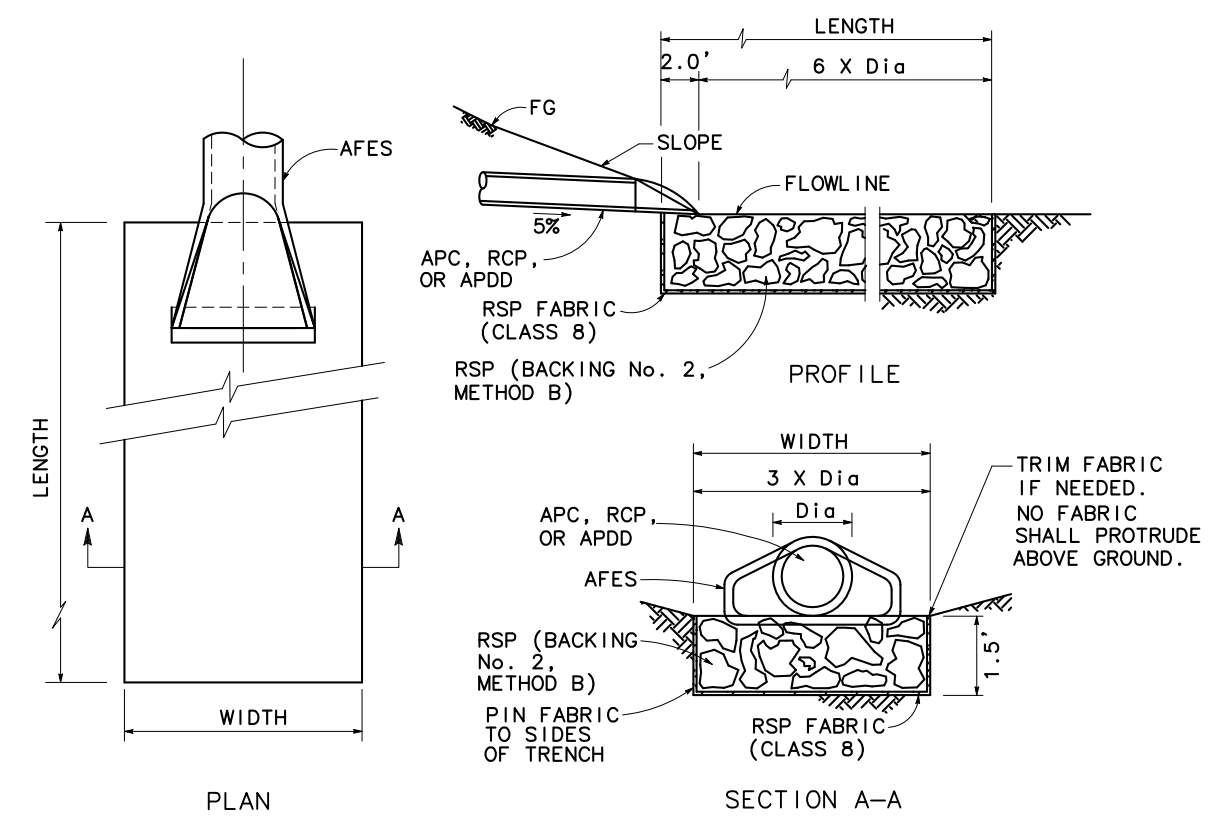
DRAINAGE DETAILS
NO SCALE

DD-1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: (None listed)

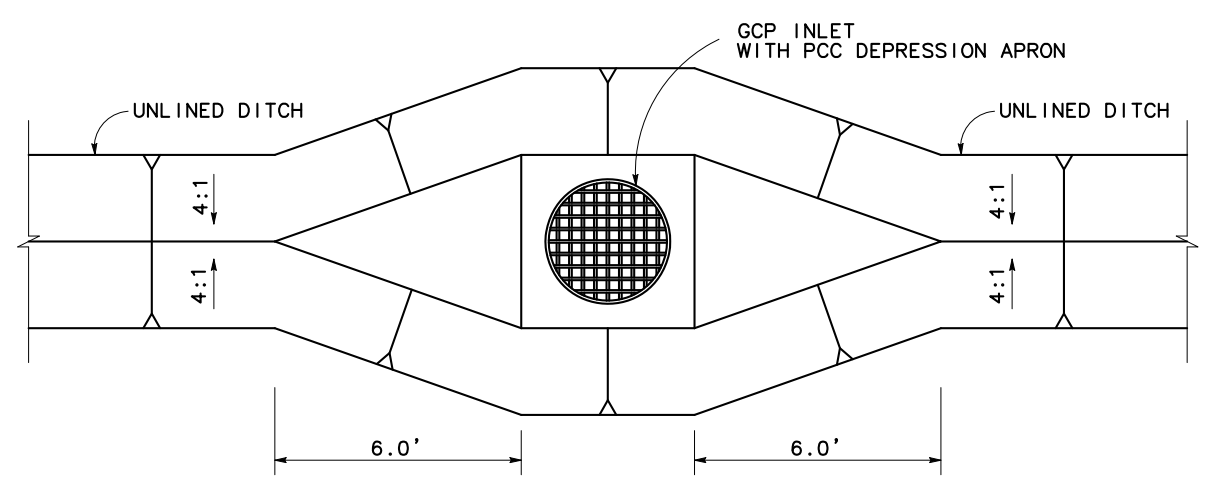
LAST REVISION: 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	85	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



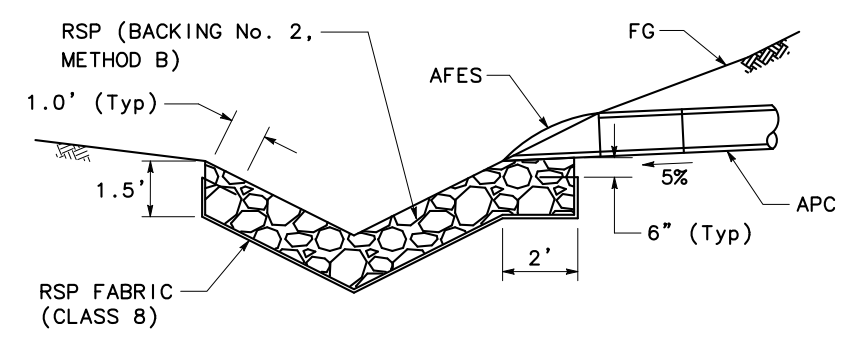
ROCK SLOPE PROTECTION AT CULVERT

11 r



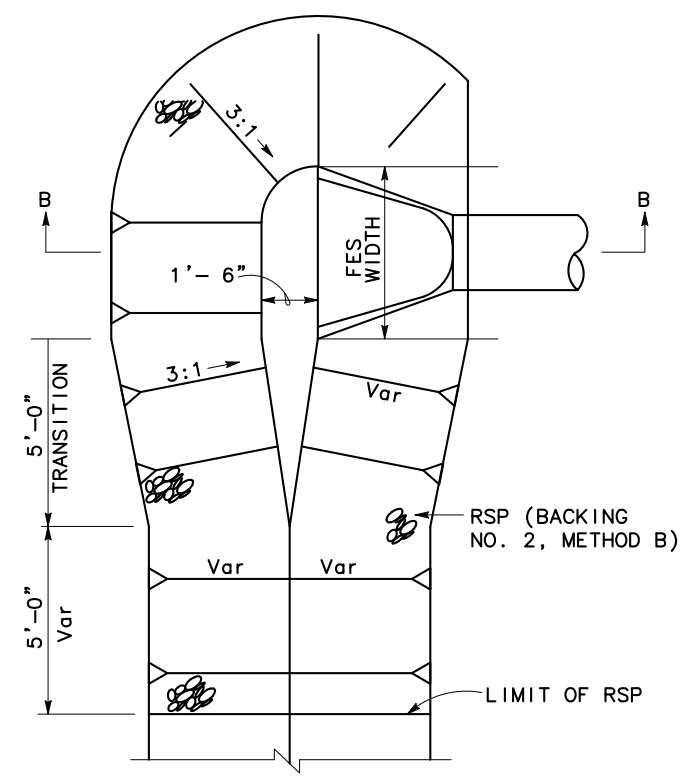
DITCH TO INLET TRANSITION

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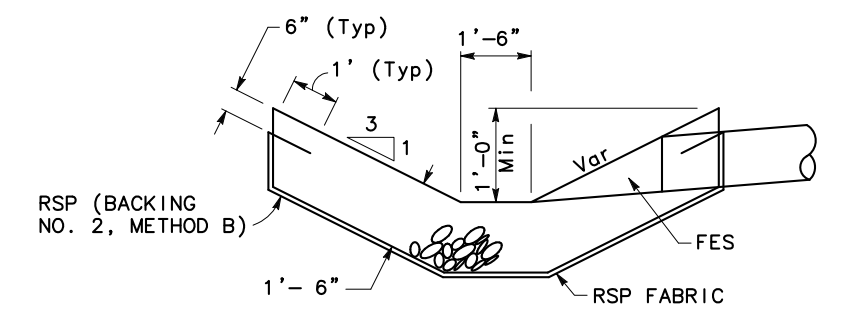


ROCK SLOPE PROTECTION AT DITCH

8 n 15 a



DITCH TRANSITION



SECTION B-B

DRAINAGE DETAILS
NO SCALE

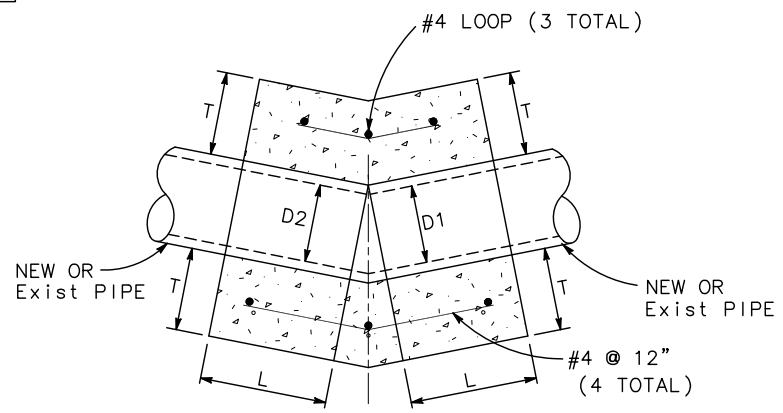
DD-2

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS: DAVID LIU, CHUONG NGUYEN, KEITH G. MEYER
 REVISED BY: DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	86	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

LEGEND:

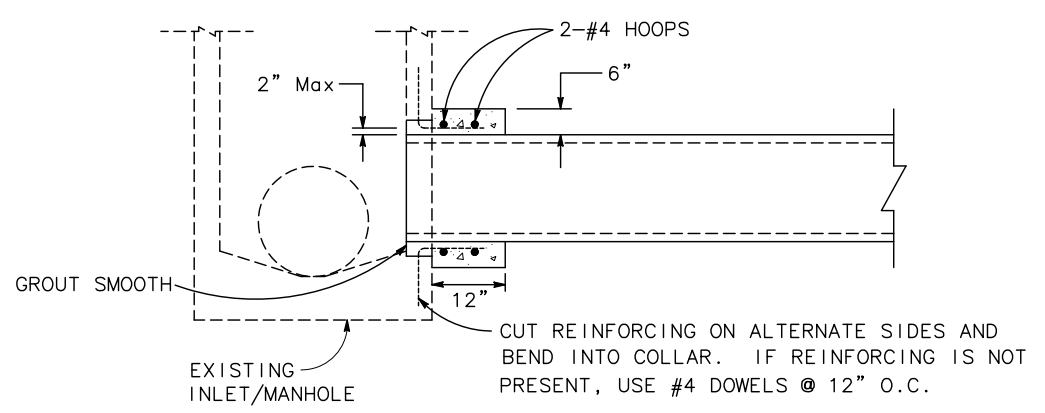
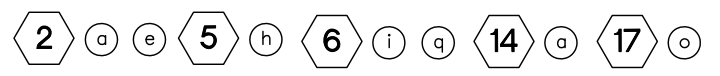


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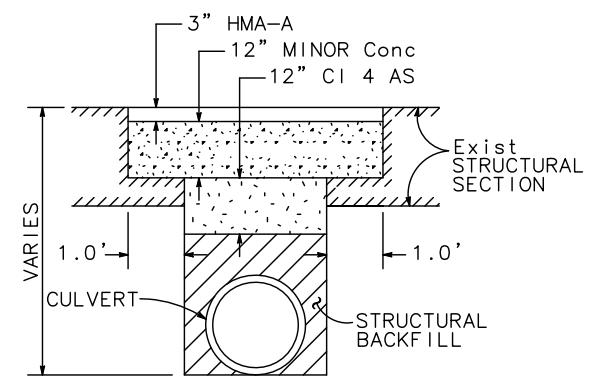
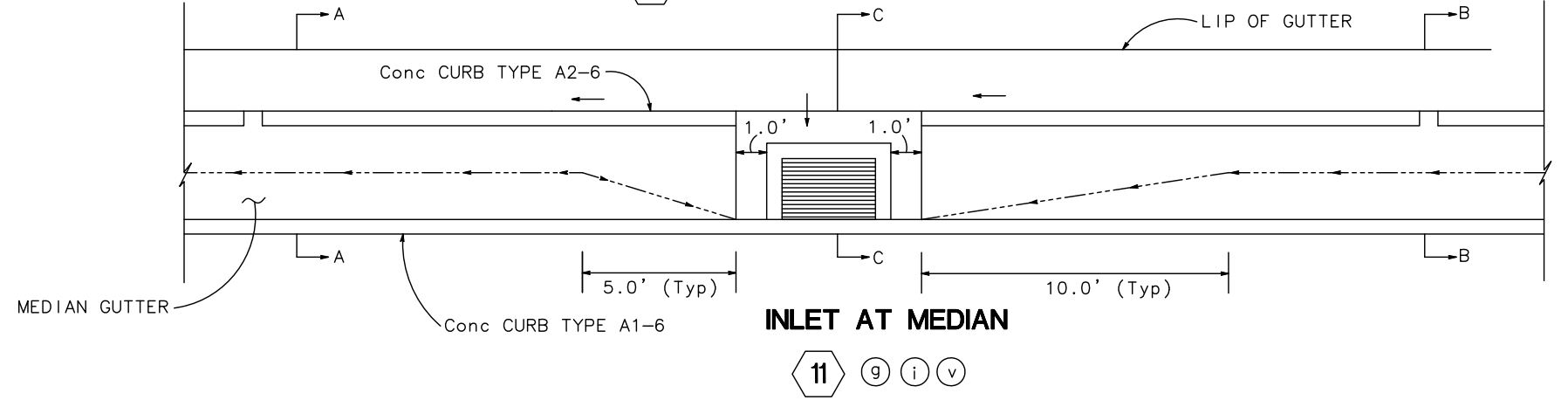
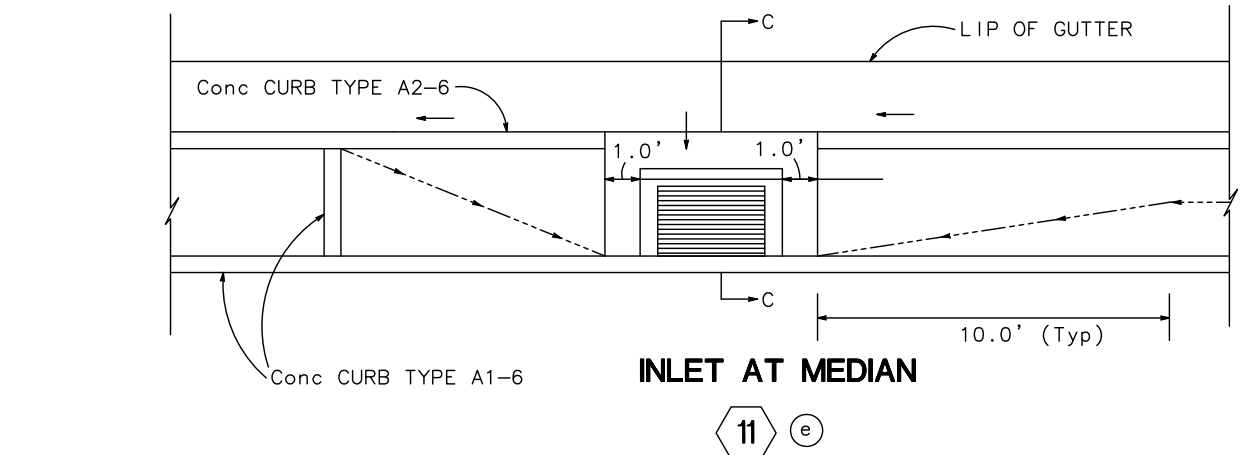
1. WHERE PIPES OF DIFFERENT DIAMETERS ARE JOINED WITH A CONCRETE COLLAR, L AND T SHALL BE THOSE OF THE LARGER PIPE. D = D1 OR D2, WHICHEVER IS GREATER.
2. WHEN D1 IS EQUAL OR LESS THAN D2, JOIN INVERTS. WHEN D1 IS GREATER THAN D2, JOIN SOFFITS.
3. FOR PIPE SIZES NOT LISTED USE NEXT LARGER SIZE.

D	L	T
12"	12"	6"
18"	12"	6"
24"	12"	6"

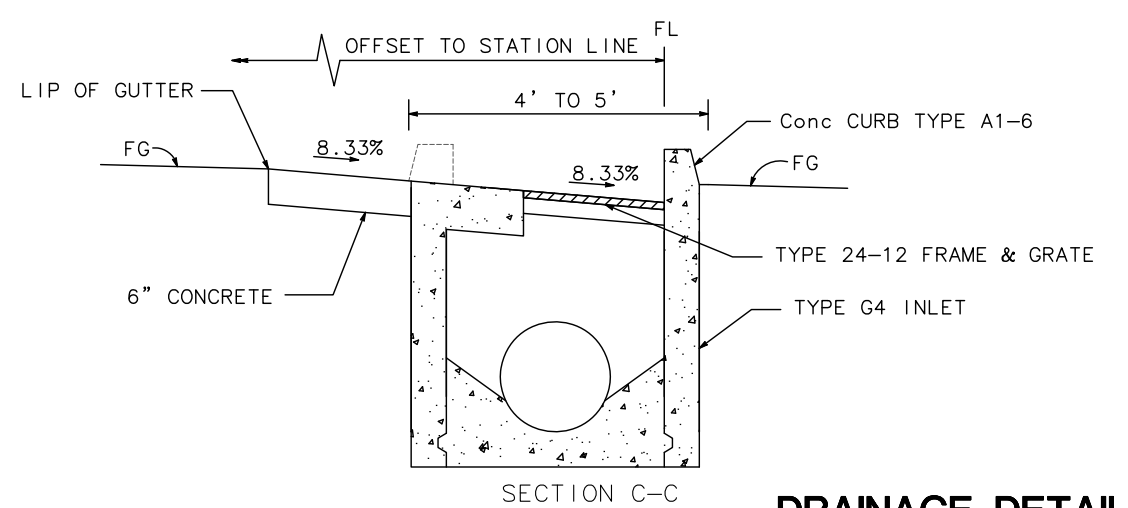
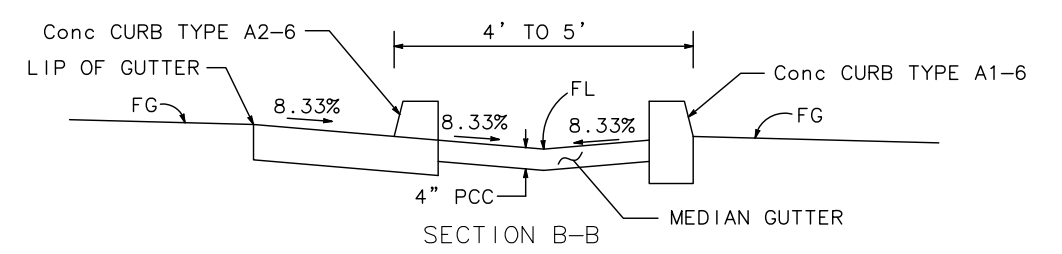
CONCRETE COLLAR



PIPE CONNECTION TO EXISTING DRAINAGE STRUCTURE



STREET TRENCH PAVING



DRAINAGE DETAILS
NO SCALE

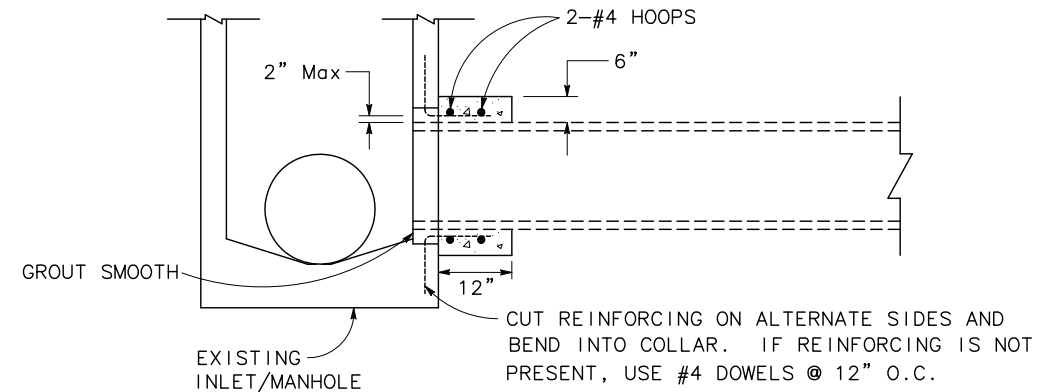
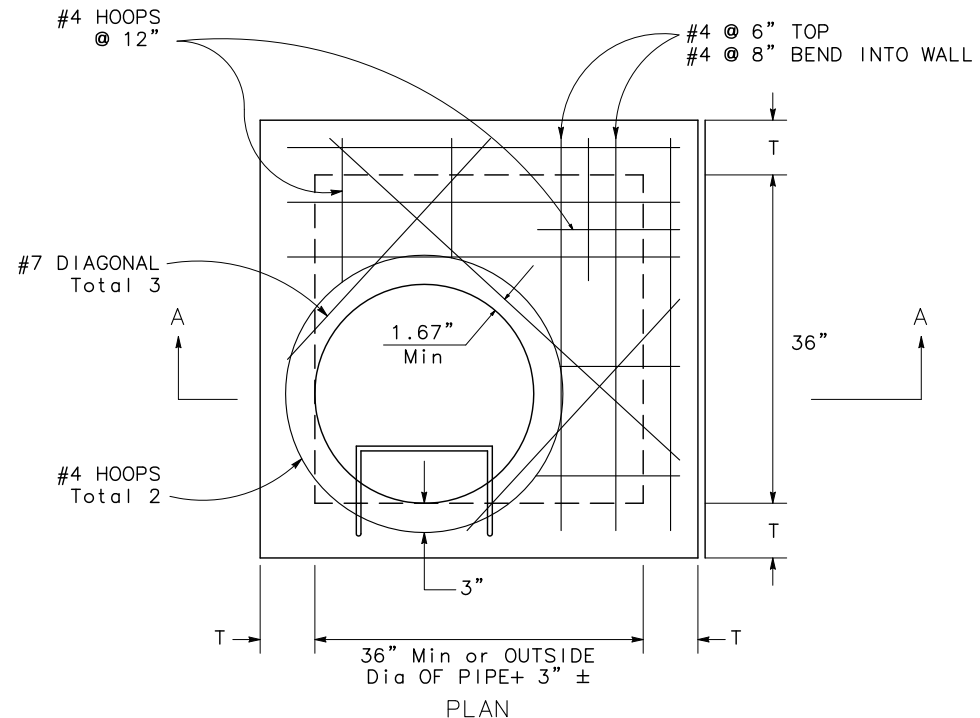
DD-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: KEITH G. MEYER
 DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: REVISED BY: DATE REVISED: 9/04/12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	87	423

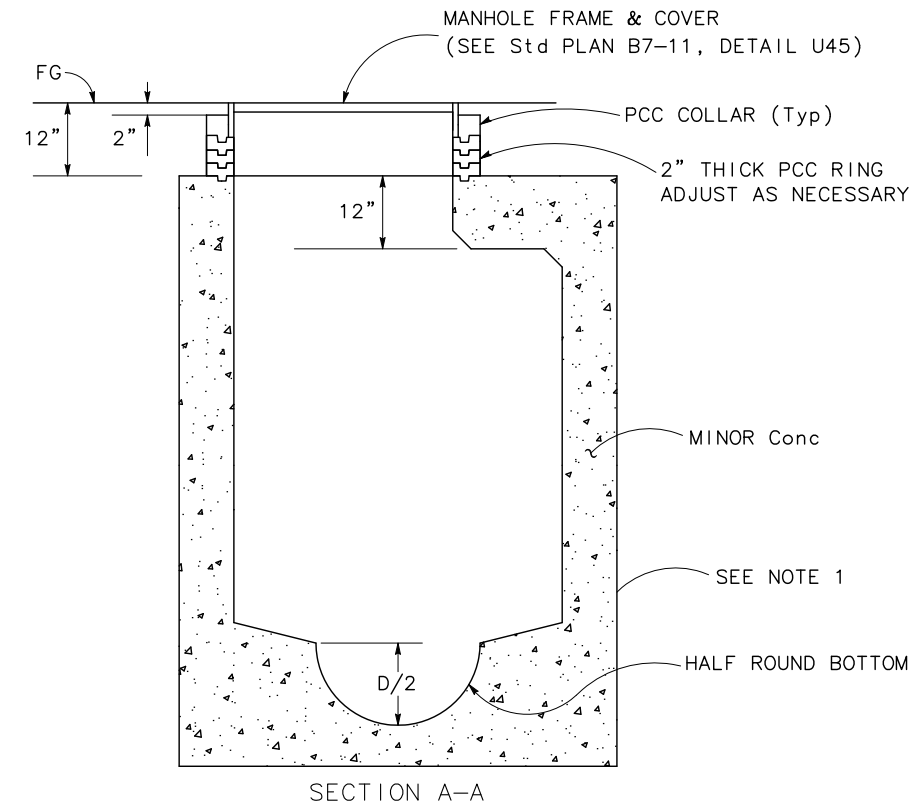
REGISTERED CIVIL ENGINEER	DATE
<i>Chuong Nguyen</i>	05-18-12
PLANS APPROVAL DATE	
9/04/12	

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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CONNECT EXISTING PIPE TO DRAINAGE STRUCTURE

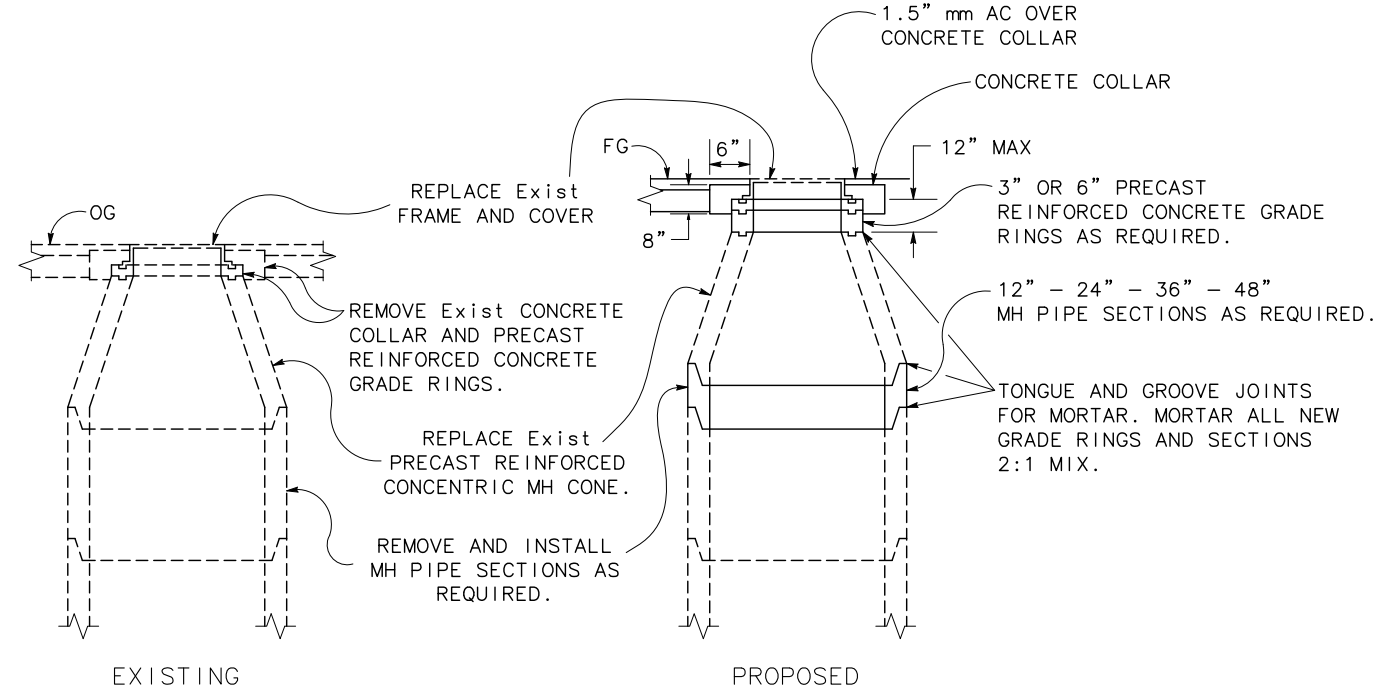
4 m 9 a 16 a 32 a 33 a



TYPE G2 MANHOLE

4 c e g k m 11 o 14 b 15 d f 17 f

NOTE:
1. FOR DETAILS NOT SHOWN, SEE Std PLAN D73 - TYPE G2 INLET.



ADJUST MANHOLE TO GRADE

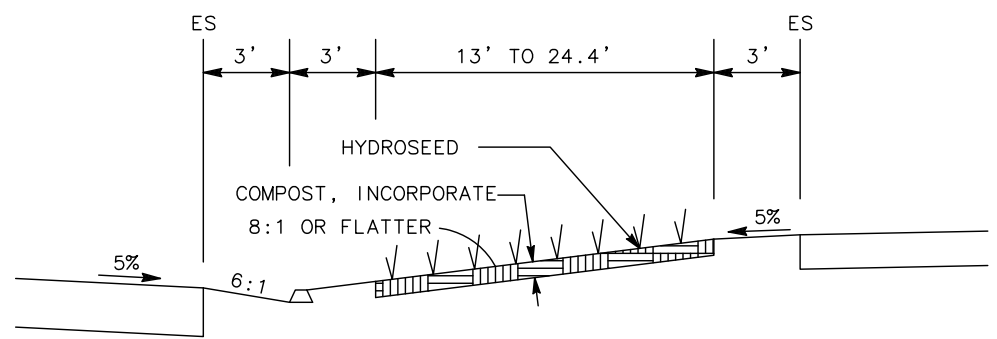
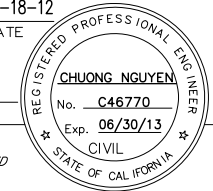
10 c d 12 e 13 e 28 a 29 a 30 a 31 a

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 REVISIONS: DAVID LIU, CHUONG NGUYEN, DAVID LIU, CHUONG NGUYEN, KEITH G. MEYER
 REVISIONS: REVISOR, DATE, REVISIONS, DATE, REVISIONS

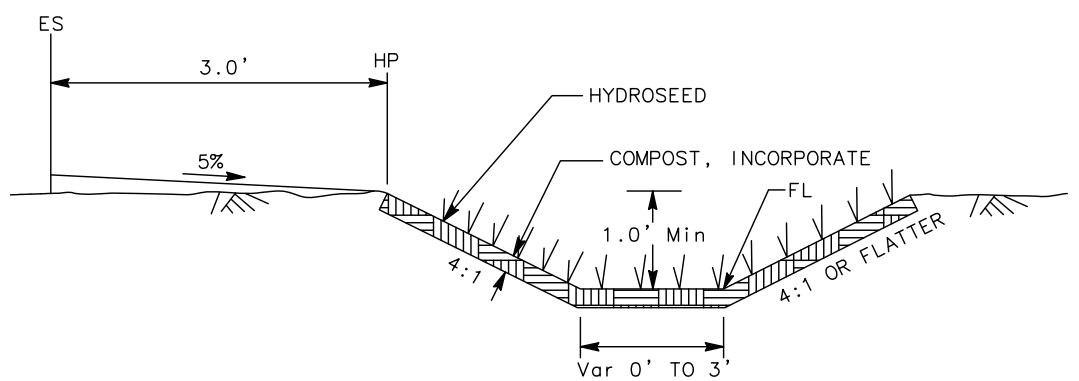
DATE PLOTTED => \$DATE
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 LAST REVISION 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
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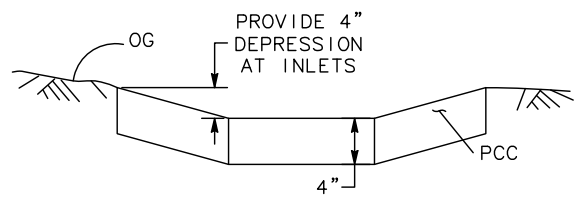
<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



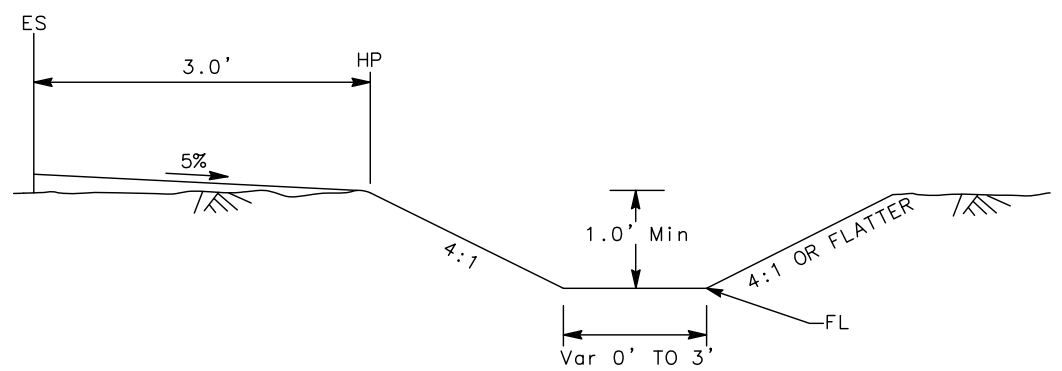
BIOFILTRATION STRIP Typ SECTION



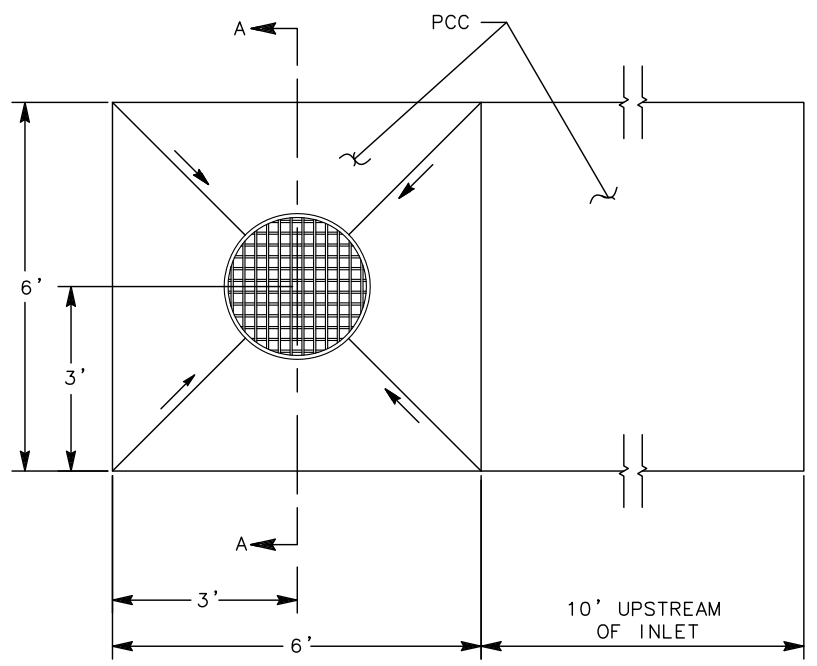
BIOFILTRATION SWALE Typ SECTION



SECTION A-A



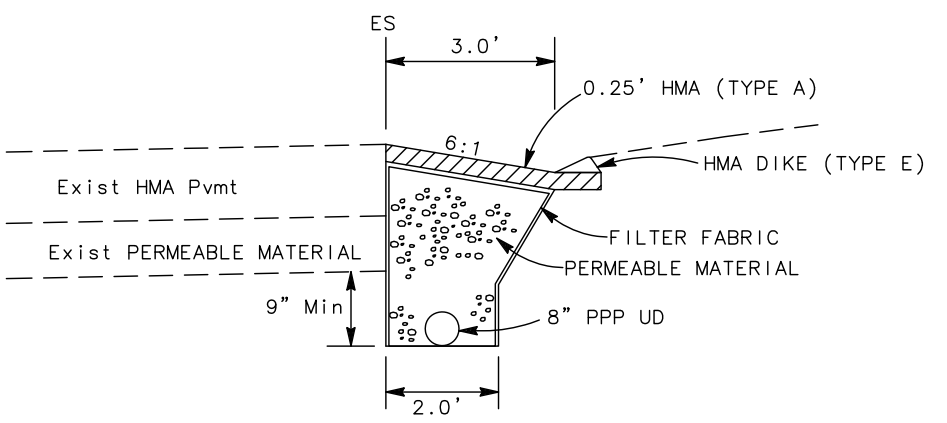
UNLINED DITCH Typ SECTION



PLAN

PCC APRON

- 1
- a
- c
- 4
- i
- 8
- k
- 14
- d
- f
- h
- j
- 17
- b
- d
- l
- n



UNDERDRAIN RECONSTRUCTION DETAIL

NB "SR 99" Sta 292+62.70 TO 298+24.21
 SB "SR 99" Sta 296+74.95 TO 299+16.63

DRAINAGE DETAILS
NO SCALE

DD-5

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS: 1, a, c, 4, i, 8, k, 14, d, f, h, j, 17, b, d, l, n
 DAVID LIU
 CHUONG NGUYEN
 REVISOR: CHUONG NGUYEN
 DATE: 9/04/12
 REVISIONS: 1, a, c, 4, i, 8, k, 14, d, f, h, j, 17, b, d, l, n
 STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS: 1, a, c, 4, i, 8, k, 14, d, f, h, j, 17, b, d, l, n
 DAVID LIU
 CHUONG NGUYEN
 REVISOR: CHUONG NGUYEN
 DATE: 9/04/12
 REVISIONS: 1, a, c, 4, i, 8, k, 14, d, f, h, j, 17, b, d, l, n

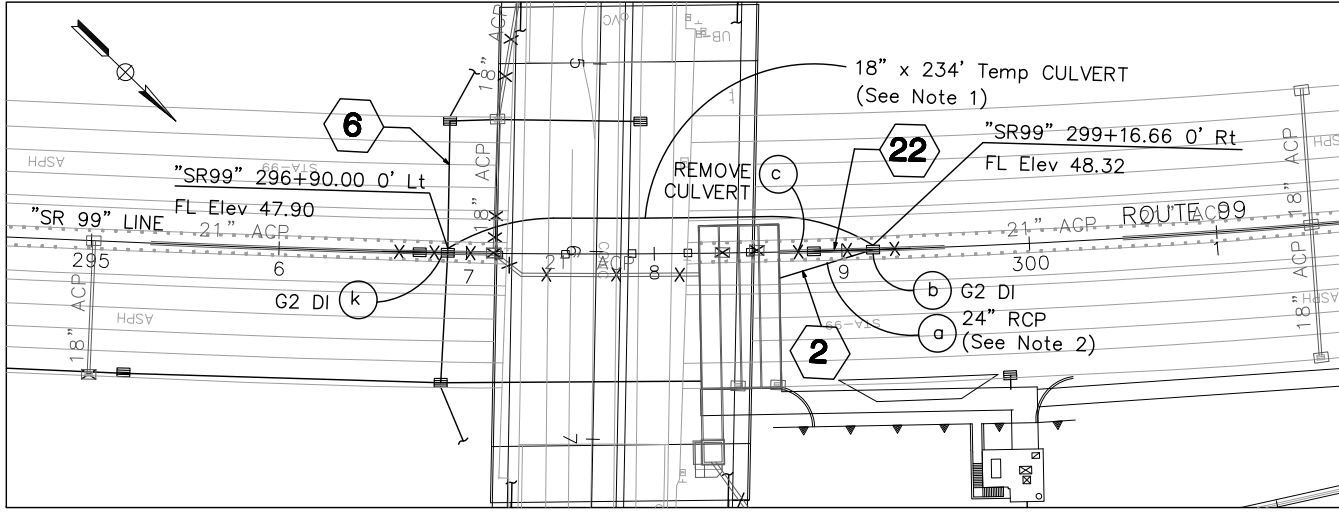
LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 REVISOR BY
 DATE REVISOR
 DAVID LIU
 CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	89	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

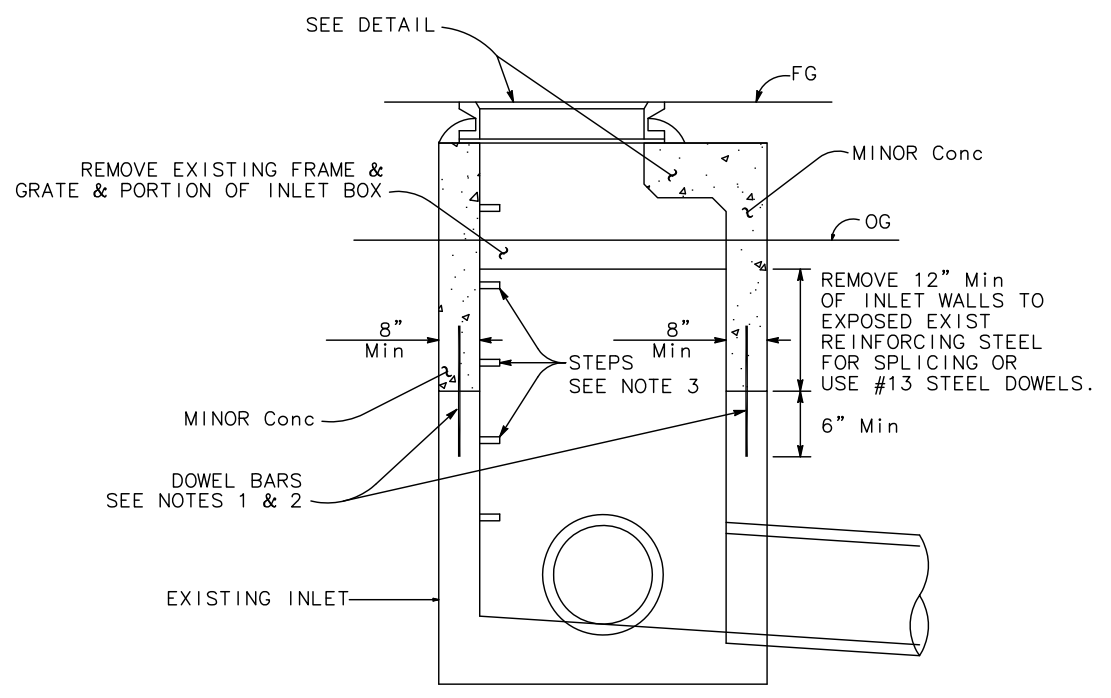
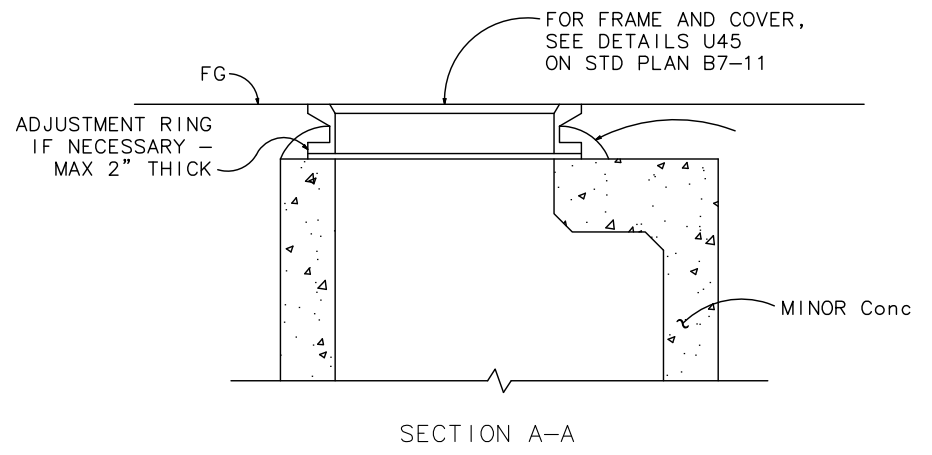
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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TEMPORARY CULVERT FOR PUMPING PLANT CONSTRUCTION

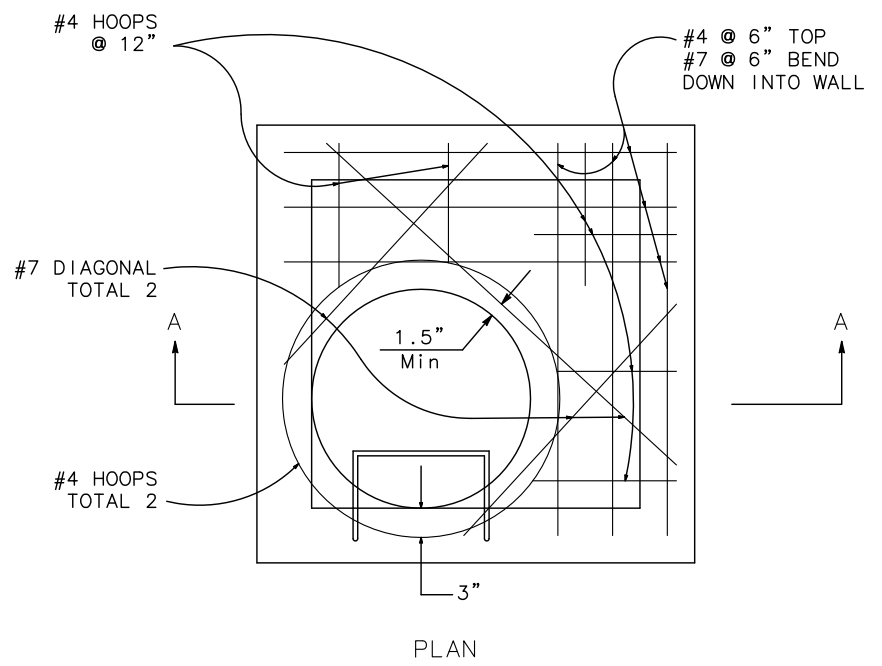
NOTES:

1. PROVIDE TEMPORARY CULVERT BEFORE REMOVING DRAINAGE ITEM **22** (c)
2. CONSTRUCT DRAINAGE ITEM **2** (a) AT THE LAST PHASE OF PUMPING PLANT CONSTRUCTION.



NOTES:

1. USE EXISTING REINFORCING STEEL WHEN JOINING NEW CONCRETE TO EXISTING
2. ALL DOWELS AT JOINTS BETWEEN EXISTING AND NEW CONCRETE SHALL BE EMBEDDED 6\"/>
- 3. PLACE STEP AS REQUIRED.
- 4. FOR DETAILS NOT SHOWN, SEE Std PLAN D73 - TYPE G2 INLET.




MODIFY INLET TO MANHOLE

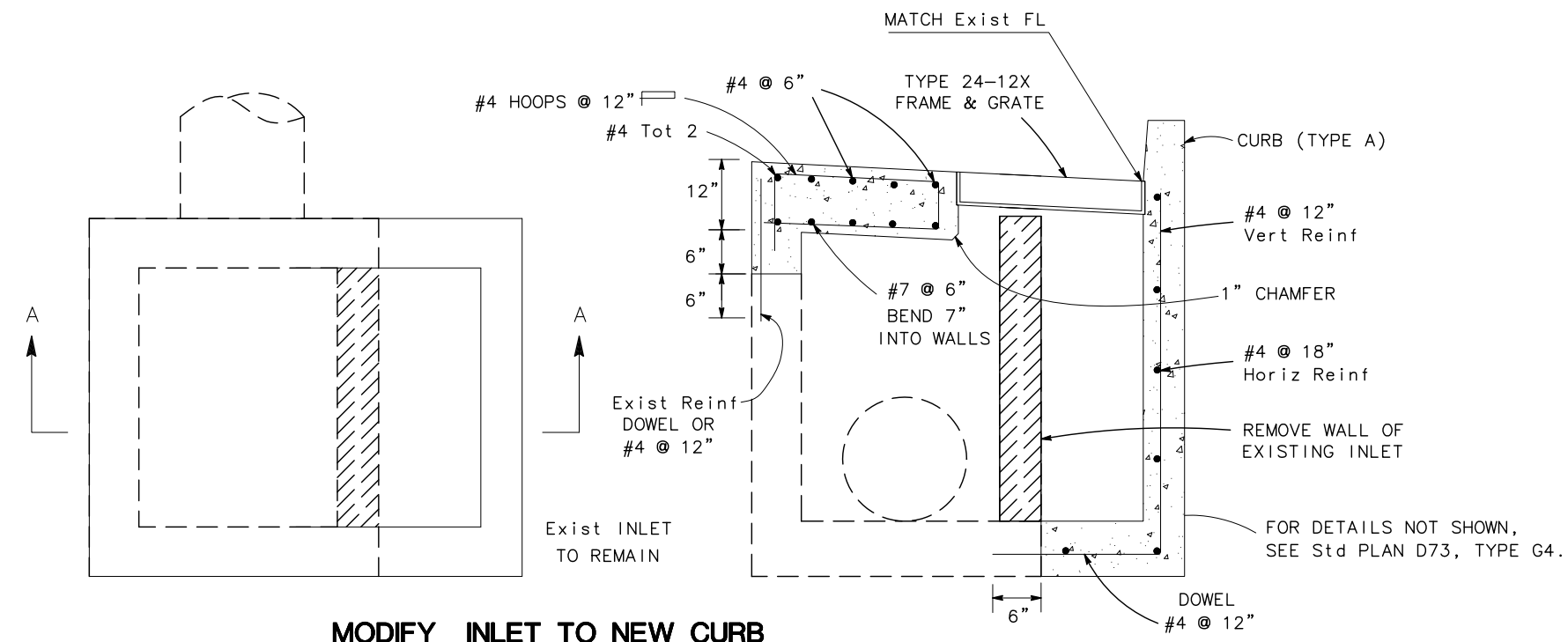
10 (c) (d) **12** (e) **13** (e)

DRAINAGE DETAILS
NO SCALE

DD-6

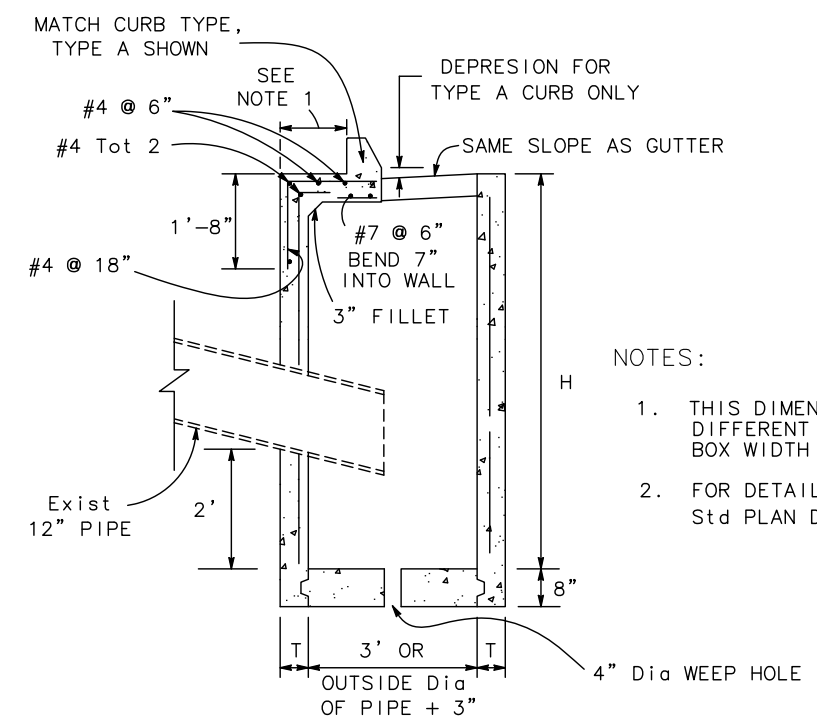
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	90	423

 REGISTERED CIVIL ENGINEER		05-18-12 DATE
9/04/12 PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



MODIFY INLET TO NEW CURB

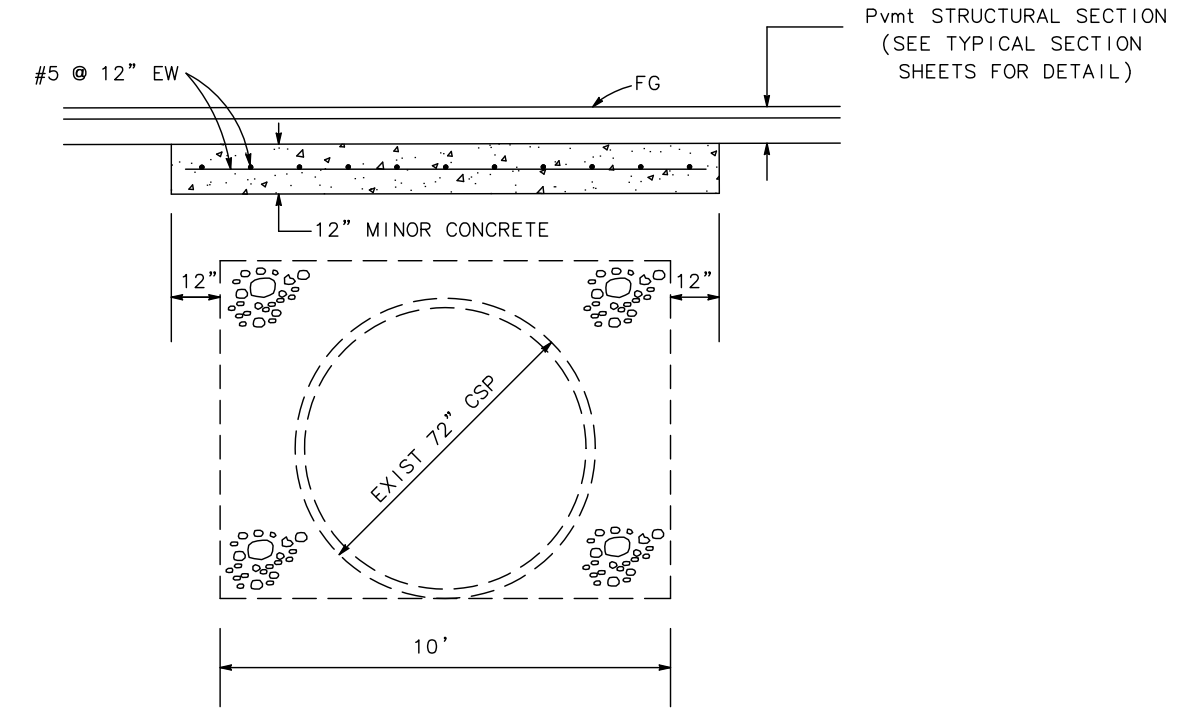
33 d



TYPE G4 Mod INLET

32 a 33 a

- NOTES:
1. THIS DIMENSION WILL VARY WITH DIFFERENT GRATES CURB TYPES, BOX WIDTH AND WALL THICKNESS.
 2. FOR DETAILS NOT SHOWN, SEE Std PLAN D73, TYPE G4 INLET.



UNDERDRAIN PROTECTION SLAB

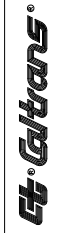
"S" Sta 11+52.00 TO 12+24.00

12 h

DRAINAGE DETAILS
NO SCALE

DD-7

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT SUPERVISOR: KEITH G. MEYER
 REVISIONS: DAVID LIU, CHUONG NGUYEN, REVISOR, DATE, REVISIONS



DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	92	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT		
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)										CY	SQYD
D-5	2	a										52			S										9.6	24" RCP w/ Conc COLLAR	"SR99" LINE 298+66.03 22.04' Rt	D-5	2	a					
		b													S		326								9.5	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 299+16.66 0' Rt			b					
		c										30			S										4.0	24" RCP	"SR99" LINE 299+16.66 0' Rt			c					
		d													S		326								5.0	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 298+85.10 0' Rt			d					
D-5	2	e										20			S										3.0	24" RCP w/ Conc COLLAR	"SR99" LINE 298+85.10 0' Rt			e					
D-2	3	a													S		326								4.2	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 283+32.87 81.27' Rt	D-2	3	a					
		b										298			S										2.2	24" RCP	"SR99" LINE 283+32.87 81.27' Rt			b					
		c													S		326								4.2	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 286+35.00 32.00' Rt			c					
D-2		d										258			S										2.2	24" RCP	"KR1" LINE 286+35.00 32.00' Rt	D-2		d					
D-5		e													S		326								5.3	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 288+94.07 32.00' Rt	D-5		e					
		f										78			S										3.3	24" RCP	"KR1" LINE 288+94.07 32.00' Rt			f					
		g													S		326								4.8	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 289+75.00 32.00' Rt			g					
		h										58			S										2.8	24" RCP	"KR1" LINE 289+75.00 32.00' Rt			h					
		i													S		326								4.8	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 290+35.85 32.00' Rt			i					
		j										120			S										2.8	24" RCP	"KR1" LINE 290+35.85 32.00' Rt			j					
		k													S		326								5.1	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 291+56.56 43.66' Rt			k					
		l										268			S										2.9	24" RCP	"KR1" LINE 291+56.56 43.66' Rt			l					
		m													S		326								4.8	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 294+30.00 44.00' Rt			m					
		n										280			S										2.8	24" RCP	"KR1" LINE 294+30.00 44.00' Rt			n					
		o													S		326								4.3	TYPE G2 DI w/ 1.25" SGD	"KR1" LINE 297+11.98 44.00' Rt			o					
		p										60			S										2.3	24" RCP	"KR1" LINE 297+11.98 44.00' Rt			p					
		q													S		239								4.6	TYPE G2 DI w/ 1.25" SGD	"K" LINE 7+95.90 45.00' Rt			q					
		r										116			S										2.6	24" RCP	"K" LINE 7+95.90 45.00' Rt			r					
D-5	3	s													S		239								4.1	TYPE G2 DI w/ 1.25" SGD	"K" LINE 7+66.85 75.00' Lt	D-5	3	s					
SHEET TOTAL											1638							3738	2	10	23.07	12.68													

DRAINAGE QUANTITIES
DQ-2

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	94	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT		
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)										CY	SQYD
D-5	6	e														S											4.0	24" RCP	"SR99" LINE 293+00.00 72.03' Rt	D-5	6	e			
		f														S		326			1						6.0	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 295+20.00 71.69' Rt			f			
		g														S		326			1					4.0	24" RCP	"SR99" LINE 295+20.00 71.69' Rt			g				
		h														S		326			1					8.7	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 296+87.40 71.61' Rt			h				
		i														S										5.7	36" RCP w/ Conc COLLAR	"SR99" LINE 296+87.40 71.61' Rt			i				
		j											68			S										3.9	24" JACKED RCP	"SR99" LINE 296+87.40 71.61' Rt			j				
		k														S		326			1					5.7	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 296+90.00 0' Lt			k				
		l														S										2.6	24" JACKED RCP	"SR99" LINE 296+90.00 0' Lt			l				
		m														S		326			1					4.6	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 296+90.02 70.22' Lt			m				
		n														S										2.2	24" RCP w/ Conc COLLAR	"SR99" LINE 296+90.02 70.22' Lt			n				
		o														S										2.2	24" RCP w/ Conc COLLAR	"SR99" LINE 297+15.90 71.36' Lt			o				
		p														S		326			1					4.4	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 297+93.00 72.84' Lt			p				
		q														S										2.1	24" RCP w/ Conc COLLAR	"SR99" LINE 296+75.00 0' Lt			q				
		r														S		326			1					4.1	TYPE G2 DI w/ 1.25" SGD	"SR99" LINE 296+75.00 0' Lt			r				
D-5	6	s														S										2.1	24" RCP	"SR99" LINE 296+90.00 0' Lt	D-5	6	s				
D-5	7	a														S		326			1					12.1	TYPE G2 DI w/ 1.25" SGD	"SR99" ILINE 299+86.77 69.70' Rt	D-5	7	a				
D-5	7	b														S										9.5	24" RCP	"SR99" ILINE 299+86.77 69.70' Rt	D-5	7	b				
D-5	8	a														S		326			1					4.3	TYPE G2 DI w/ 1.25" SGD	"KR2" LINE 298+90.00 44.00' Rt	D-5	8	a				
D-5		b														S										2.3	24" RCP	"KR2" LINE 298+90.00 44.00' Rt	D-5		b				
D-10		c														S		326			1					4.4	TYPE G2 DI w/ 1.25" SGD	"KR2" LINE 300+30.00 44.00' Rt	D-10		c				
		d														S										2.4	24" RCP	"KR2" LINE 300+30.00 44.00' Rt			d				
D-10	8	e														S		326			1					5.9	TYPE G2 DI w/ 1.25" SGD	"KR2" LINE 302+00.00 44.00' Rt	D-10	8	e				
SHEET TOTAL											913	138	136					3260			10			21.33	11.82										

DRAINAGE QUANTITIES

DQ-4

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	95	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	LF	CY	LF	EA	EA	EA	EA	EA	EA	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	
												18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)										
D-10	8	f															S											3.6	24" RCP	"KR2" LINE 302+00.00 44.00' Rt	D-10	8	f		
		g															S	326			1		1.90	0.95				5.3	TYPE G2 DI w/ 1.25" SGD	"KR2" LINE 303+21.09 45.81' Rt			g		
		h															S											3.3	24" RCP	"KR2" LINE 303+21.09 45.81' Rt			h		
		i															S												4.0	TYPE G2 DI w/ 1.25" SGD	"KR2" LINE 303+76.75 48.98' Rt			i	
		j															S												2.0	18" RCP	"KR2" LINE 300+30.00 50.40' Rt			j	
		k															S	4.0	236			1							3.5	TYPE GCP DI	"KR2" LINE 300+30.00 50.40' Rt			k	
		l															S												3.6	24" RCP	"KR2" LINE 302+00.00 44.00' Rt			l	
		m															S																	m	
D-10	8	n															S																	n	
		a															S																		a
D-3	9	a															S	6.4	236			1							0.09	TYPE GCP DI w/ Conc COLLAR	"SA" LINE 50+28.88 67.42' Lt	D-3	9	a	
		b															S																		b
D-4	10	a															S																		a
		b															S																		b
		c															S																		c
		d															S																		d
		e															S																		e
		f															S																		f
D-4	10	g															S																		g
		a															S																		a
D-6	11	a															S																		a
		b															S																		b
D-6	11	c															S																		c
SHEET TOTAL																																			

DRAINAGE QUANTITIES

DQ-5

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	96	423

REGISTERED CIVIL ENGINEER *Chuong Nguyen*
 DATE 05-18-12
 PLANS APPROVAL DATE 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

Caltrans

CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER

CHECKED BY: CHUONG NGUYEN

DESIGNED BY: TINH TRUONG

REVISIONS: REVISOR, DATE, REVISION

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)									
D-6	11	d										58			S										5.5	24" RCP	"K" LINE 11+50.00 77.38' Lt	D-6	11	d			
		e															326			1		2.49			7.8	TYPE G4 DI	"K" LINE 11+69.98 15.99' Lt			e			
		f										28			S										5.9	24" RCP	"K" LINE 11+69.98 15.99' Lt			f			
		g															326			1		2.52			7.9	TYPE G4 DI	"K" LINE 11+99.98 16.66' Lt			g			
D-6		h										208			S										5.8	24" RCP	"K" LINE 11+99.98 16.66' Lt	D-6		h			
D-7		i															326			1		3.77			8.9	TYPE G4 DI	"K" LINE 14+07.43 20.60' Lt	D-7		i			
		j										108			S										6.9	24" RCP	"K" LINE 14+07.43 20.60' Lt			j			
		k															239		1		3.72	0.95			8.7	TYPE G2 DI w/ 1.25" SGD	"K" LINE 14+19.04 129.95' Lt			k			
D-7		l										116			S										6.7	24" RCP	"K" LINE 14+19.04 129.95' Lt	D-7		l			
D-6		m															8.9	236		1					8.4	TYPE GCP DI	"K" LINE 15+22.35 104.46' Lt	D-6		m			
		n										294			S										6.4	24" RCP	"K" LINE 15+22.35 104.46' Lt			n			
		o																440			1	4.35			10.5	TYPE G2 MANHOLE	"K" LINE 17+93.01 96.64' Lt			o			
		p										38			S										8.5	24" RCP	"K" LINE 17+93.01 96.64' Lt			p			
		q												1													24" Conc FES	"K" LINE 17+85.67 135.04' Lt			q		
		r																							8.0	16.0	RSP	"K" LINE 17+85.67 135.04' Lt			r		
		s										78			S										5.2	24" RCP	"K" LINE 11+70.00 77.38' Lt			s			
		t															239		1		1.59	0.95			4.1	TYPE G2 DI w/ 1.25" SGD	"K" LINE 12+50.00 79.16' Lt			t			
		u										194			S										6.9	24" RCP	"K" LINE 14+07.43 20.60' Lt			u			
D-6	11	v															326			1	3.77				8.9	TYPE G4 DI	"K" LINE 15+99.99 22.90' Lt	D-6	11	v			
		a																239		1	1.57	0.95			4.0	TYPE G2 DI w/ 1.25" SGD	"S" LINE 11+57.60 42.00' Rt	D-7	12	a			
		b										72			S										2.5	18" RCP	"S" LINE 11+57.60 42.00' Rt			b			
		c															239		1		1.59	1.35			4.1	TYPE G2 DI w/ 1.25" SGD	"S" LINE 12+32.06 42.00' Rt			c			
D-7	12	d										16			S										2.9	18" RCP w/ Conc COLLAR	"S" LINE 12+32.06 42.00' Rt	D-7	12	d			
SHEET TOTAL											88	1122		1		8.9	2936	4	4	1	1	25.37	4.29	8.0	16.0								

DRAINAGE QUANTITIES

DQ-6

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	97	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE: 05-18-12
 9/04/12
 PLANS APPROVAL DATE
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	LF	CY	LF	EA	EA	EA	EA	EA	EA	RCP			CONC FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS B) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT
												18"	24"	36"					JACKED 24" RCP (CLASS IV)	24"	TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)									
D-7	12	e									1						440								6.0	MODIFY INLET TO MANHOLE	"S" LINE 12+32.06 22.28' Rt	D-7	12	e			
		f																									ADJUST MANHOLE TO GRADE	"S" LINE 12+20.00 26.32' Rt			f		
		g									1																ADJUST MANHOLE TO GRADE	"S" LINE 11+57.77 25.95' Rt			g		
D-7	12	h																									UNDERDRAIN PROTECTION	STA "S" 11+52.59 TO "S" 12+23.57	D-7	12	h		
D-7	13	a															239	1		1.57	0.95					4.0	TYPE G2 DI w/ 1.25" SGD	"S" LINE 16+92.06 55.36' Rt	D-6	13	a		
		b										78		S											2.0	18" RCP	"S" LINE 16+92.06 55.36' Rt			b			
		c												S			239	1		1.62	1.35				4.2	TYPE G2 DI w/ 1.25" SGD	"S" LINE 17+71.09 52.38' Rt			c			
		d										18		S											2.3	18" RCP w/ Conc COLLAR	"S" LINE 17+71.09 52.38' Rt			d			
		e															440						1		5.0	MODIFY INLET TO MANHOLE	"S" LINE 17+70.37 30.53' Rt			e			
		f			78.5																					3.3	REMOVE CULVERT	"S" LINE 17+70.37 30.53' Rt			f		
D-7	13	g																								4.6	REMOVE INLET	"S" LINE 16+91.42 30.18' Rt	D-6	13	g		
D-7	14	a										44		S												9.9	18" RCP w/ Conc COLLAR	"K" LINE 19+31.07 112.54' Lt	D-7	14	a		
		b												S			440									11.4	TYPE G2 MANHOLE	"K" LINE 19+84.79 105.90' Lt			b		
		c										172		S												7.8	18" RCP	"K" LINE 19+84.79 105.90' Lt			c		
		d												S		6.4	236									5.9	TYPE GCP DI	"K" LINE 21+50.00 105.20' Lt			d		
D-7		e										276		S												3.3	18" RCP	"K" LINE 21+50.00 105.20' Lt	D-7		e		
D-8		f												S		3.1	236										2.6	TYPE GCP DI	"K" LINE 24+27.37 102.60' Lt	D-8		f	
		g										170		S												1.1	18" RCP	"K" LINE 24+27.37 102.60' Lt			g		
		h												S		3.1	236										2.6	TYPE GCP DI	"K" LINE 26+00.00 91.85' Lt			h	
		i										144		S												1.1	18" RCP	"K" LINE 26+00.00 91.85' Lt			i		
		j												S		3.0	236										2.5	TYPE GCP DI	"K" LINE 27+45.40 74.90' Lt			j	
		k										72		S												1.0	18" RCP	"K" LINE 27+45.40 74.90' Lt			k		
D-8	14	l										106		S													0.09	18" RCP	"K" LINE 24+27.37 100.11' Lt	D-8	14	l	
SHEET TOTAL					78.5		1		2	2	1080				15.6	2742	2	4	3	7.85	38.51												

DRAINAGE QUANTITIES

DQ-7

LAST REVISION: 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT														
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)										CY	SQYD	LF	LF										
D-7	14	m						1																																							
D-7		n			358.5																																										
D-8		o					1																																								
		p					1																																								
		q			94.1																																										
		r					1																																								
D-8	14	s			75.7																																										
D-10	15	a																										8.0	16.0																		
		b										1																																			
		c											14			S														2.0																	
		d															440								1	1.77																					
		e																													2.8																
		f															440								1	1.72																					
		g											54																		2.6																
		h															326							1	1.67	0.95																					
		i																													2.4																
		j															326																														
		k																																													
D-10	15	l															326																														
D-10	16	a																326																													
		b			63.7																																										
D-10	16	c					1																																								
SHEET TOTAL						592.0	2	2	1			564		1			2184				4	2	9.72	3.94	8.0	16.0																					

DRAINAGE QUANTITIES

DQ-8

LAST REVISION DATE PLOTTED => \$DATE 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	99	423

Chung Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 T INH TRUONG
 CHUONG NGUYEN
 KEITH G. MEYER
 CALCULATED-DRAWN BY
 CHECKED BY
 REVISOR BY
 DATE REVISED

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	LF	CY	LF	EA	EA	EA	EA	EA	RCP	JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(N) FRAME, GRATE & COVER		MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT			
																					(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)	CY	SQYD												
D-10	17	a																																		
		b									188			S																						
		c																																		
		d									10			S																						
		e																																		
		f									180			S																						
		g																																		
		h										82		S																						
		i																																		
		j																																		
		k																																		
		l																																		
		m																																		
		n																																		
D-10	17	o									24			S																						
D-5	18	a																																		
		b																																		
		c																																		
		d																																		
		e	70.6	4.6																																
		f																																		
		g																																		
		h	69.9	4.6																																
D-5	18	i																																		
SHEET TOTAL			140.5	9.2	177.2						24	440			24.6	2036		2	4	1		8.46	7.38													

DRAINAGE QUANTITIES

DQ-9

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	100	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)									
D-5	18	j					1																			4.3		REMOVE INLET	"KR1" LINE 289+50.03 18.95' Rt	D-5	18	j	
		k			50.9																					2.8		REMOVE CULVERT	"KR1" LINE 289+50.03 18.95' Rt			k	
		l					1																			3.5		REMOVE INLET	"KR1" LINE 290+00.87 19.37' Rt			l	
		m	296.7	19.4																						2.0		ABANDON CULVERT	"SR99" LINE 289+50.11 0.00' Rt			m	
		n	68.0	4.5																						2.2		ABANDON CULVERT	"SR99" LINE 292+49.84 70.88' Rt			n	
		o					1																			3.6		REMOVE INLET	"SR99" LINE 292+49.84 70.88' Rt			o	
D-5	18	p					1																			3.5		REMOVE INLET	"SR99" LINE 288+69.07 0.21' Lt	D-5	18	p	
D-5	19	a	68.5	4.5																						2.3		ABANDON CULVERT	"SR99" LINE 295+01.54 71.24' Rt	D-5	19	a	
D-5	19	b					1																			3.7		REMOVE INLET	"SR99" LINE 295+01.54 71.24' Rt	D-5	19	b	
D-5	20	a					1																					REMOVE FES	"KR1" LINE 296+00.03 45.54' Rt	D-5	20	a	
D-5	20	b			136.8																					1.5		REMOVE CULVERT	"KR1" LINE 296+00.03 45.54' Rt	D-5	20	b	
D-5	21	a					1																			3.5		REMOVE INLET	"K" LINE 4+19.39 39.36' Lt	D-5	21	a	
		b			24.3																					2.0		REMOVE CULVERT	"K" LINE 4+19.39 39.36' Lt			b	
		c						1																		3.9		REMOVE MH	"K" LINE 3+95.13 38.92' Lt			c	
		d			81.1																					2.4		REMOVE CULVERT	"K" LINE 3+95.13 38.92' Lt			d	
		e					1																			4.7		REMOVE INLET	"K" LINE 4+21.69 37.82' Rt			e	
		f			109.5																					3.2		REMOVE CULVERT	"K" LINE 4+21.69 37.82' Rt			f	
		g	50.2	3.3																						3.5		ABANDON CULVERT	"SR99" 297+16.36 71.00' Lt			g	
		h			19.7																					3.0		REMOVE CULVERT	"SR99" 297+15.34 19.88' Lt			h	
		i					1																			4.7		REMOVE INLET	"SR99" 297+14.44 0.21' Lt			i	
		j			60.0																					3.2		REMOVE CULVERT	"SR99" 296+90.00 0.00' Lt			j	
D-5	21	k			113.6																					3.2		REMOVE CULVERT	"SR99" 297+14.44 0.21' Lt	D-5	21	k	
SHEET TOTAL			483.4	31.7	595.9	1	8	1																									

DRAINAGE QUANTITIES
DQ-10

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	101	423

Chuong Nguyen
REGISTERED CIVIL ENGINEER

DATE: 05-18-12

PLANS APPROVAL DATE: 9/04/12

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
1038 LEIGH AVENUE, SUITE 100
SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CALIFORNIA 95358

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE	RSP (BACKING No. 2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)										
D-5	22	a					1																			REMOVE INLET	"SR99" LINE 298+36.32 0.28' Lt	D-5	22	a		
		b					1																			REMOVE INLET	"SR99" LINE 298+54.65 1.20' Lt			b		
D-5	22	c			70.0																					REMOVE CULVERT	"SR99" LINE 298+66.66 0.00' Lt	D-5	22	c		
D-6	23	a						1																		REMOVE MANHOLE	"K" LINE 10+24.97 55.60' Lt	D-6	23	a		
		b			41.0																					REMOVE CULVERT	"K" LINE 10+24.97 55.60' Lt			b		
		c						1																		REMOVE MANHOLE	"K" LINE 10+57.59 55.17' Lt			c		
		d			17.6																					REMOVE CULVERT	"K" LINE 10+57.59 55.17' Lt			d		
		e					1																			REMOVE INLET	"K" LINE 10+75.43 59.44' Lt			e		
D-6	23	f			45.9																					REMOVE CULVERT	"K" LINE 10+75.43 59.44' Lt			f		
		g						1																		REMOVE INLET	"K" LINE 11+21.56 60.87' Lt	D-6	23	g		
D-6	24	a						1																		REMOVE INLET	"K" LINE 12+78.74 6.24' Lt	D-6	24	a		
		b			33.0																					REMOVE CULVERT	"K" LINE 12+78.74 6.24' Lt			b		
		c						1																		REMOVE INLET	"K" LINE 13+12.70 6.94' Lt			c		
		d			64.9																					REMOVE CULVERT	"K" LINE 13+12.70 6.94' Lt			d		
		e						1																		REMOVE INLET	"K" LINE 13+77.39 8.43' Lt			e		
		f			99.6																					REMOVE CULVERT	"K" LINE 13+77.39 8.43' Lt			f		
		g						1																		REMOVE INLET	"K" LINE 14+76.64 10.77' Lt			g		
		h			82.1																					REMOVE CULVERT	"K" LINE 14+76.64 10.77' Lt			h		
		i			94.5																					REMOVE CULVERT	"K" LINE 13+77.39 8.43' Lt			i		
		j						1																		REMOVE MANHOLE	"K" LINE 14+08.03 97.02' Lt			j		
		k			96.0																					REMOVE CULVERT	"K" LINE 14+08.03 97.02' Lt			k		
D-6		l						1																		REMOVE FES	"K" LINE 14+92.39 120.63' Lt	D-6		l		
D-7	24	m							1																	REMOVE INLET	"K" LINE 15+58.49 13.62' Lt	D-7	24	m		
D-7	25	a						1																		REMOVE INLET	"K" LINE 16+39.74 79.82' Lt	D-7	25	a		
		b			153.0																					REMOVE CULVERT	"K" LINE 16+39.74 79.82' Lt			b		
D-7	25	c						1																		REMOVE FES	"K" LINE 17+64.59 85.41' Lt	D-7	25	c		
SHEET TOTAL						797.6	2	10	3																							

DRAINAGE QUANTITIES

DQ-11

LAST REVISION: 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

x
x
x
x
 STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 TINH TRUONG
 CHECKED BY
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	18"	24"	36"	JACKED 24" RCP (CLASS IV)	24"	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT												
																				TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)																					
D-8	26	a			67.5	1																																								
D-8	26	b																																												
D-8	26	c					1																																							
D-10	27	a	71.9	4.7																																										
D-10	27	b			24.9																																									
D-4	28	a							1																																					
D-4	29	a							1																																					
D-4	30	a							1																																					
D-3	31	a							1																																					
D-6	32	a																			239	1				2.55	1.09																			
		b			1.0																																									
D-6	32	c					1																																							
D-6	33	a																			239	1				3.49	1.09																			
		b			4.0																																									
		c					1																																							
D-6	33	d								1																																				
SHEET TOTAL			71.9	4.7	97.4	2	2		4	1															478	2			6.04	2.18																

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	102	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-18-12 DATE	
		9/04/12 PLANS APPROVAL DATE		REGISTERED PROFESSIONAL ENGINEER No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

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LAST REVISION
 06-08-12 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	103	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER

05-18-12
 DATE

9/04/12
 PLANS APPROVAL DATE

CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT	ABANDON CULVERT	SAND BACKFILL	REMOVE CULVERT	REMOVE FES	REMOVE INLET	REMOVE MANHOLE	ADJUST MANHOLE TO GRADE	MODIFY INLET	RCP			JACKED 24" RCP (CLASS IV)	Conc FES	PIPE JOINT CLASSIFICATION (N)	36" GCP INLET	Misc IRON AND STEEL (F)	(N) FRAME, GRATE & COVER				MINOR CONCRETE		RSP (BACKING No.2, METHOD B) (F)	RSP FABRIC (CLASS 8) (N)	HEIGHT OF INLET (N)	MAXIMUM COVER (N)	DESCRIPTION	STATION	DRAINAGE PLAN SHEET No.	DRAINAGE SYSTEM No.	DRAINAGE UNIT
											18"	24"	36"						TYPE 24-12X	TYPE 24-12	TYPE 36R	MANHOLE COVER (TYPE U45)	(MINOR STRUCTURE) (F)	(Misc CONSTRUCTION)									
D-2	34	a	69.0	4.5																								ABANDON CULVERT	"SR99" LINE 285+99.10 69.00' Lt				
		b			16.9																		2.3					REMOVE CULVERT	"SR99" LINE 285+99.10 69.00' Lt				
D-2	34	c					1																	3.8				REMOVE INLET	"SR99" LINE 285+98.65 83.74' Lt				
SHEET TOTAL			69.0	4.5	16.9		1																										
GRAND TOTAL			764.8	50.**	2355.5	7	27	5	7	5	1276	8411	138	422	4	70.9	29414	18	48	14	14	153.61	109.6*	24.0	48.0								

(*) - QUANTITY INCLUDED IN MINOR CONCRETE (MISCELLANEOUS CONSTRUCTION) TABLE IN SUMMARY OF QUANTITIES SHEET.
 (**) - QUANTITY INCLUDED IN UTILITIES TABLE IN SUMMARY OF QUANTITIES SHEET.

DRAINAGE QUANTITIES

DQ-13

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEN NGAI
 CALCULATED/DESIGNED BY
 CHECKED BY
 JOHN BEEBE
 JOHN BEEBE
 REVISED BY
 DATE REVISED

GENERAL NOTES:

- ALL EXISTING UTILITIES ARE INTENDED TO SHOW APPROXIMATE LOCATIONS ONLY. CONTRACTOR SHALL POTHOLE AND VERIFY THE ACTUAL LOCATIONS PRIOR TO STARTING CONSTRUCTION.
- PRIOR TO COMMENCEMENT OF WORK, CONTRACTOR SHALL CONTACT THE UNDERGROUND SERVICE ALERT AT 811.
- CONTRACTOR TO NOTIFY PG&E TRANSMISSION DIVISION FOR ANY WORK BEING PERFORMED ADJACENT TO GAS MAINS, AT LEAST 24 HR. IN ADVANCE OF PROPOSED WORK.
- EXISTING STORM DRAIN FACILITIES ARE SHOWN ON DRAINAGE
- FOR COMPLETE RIGHT OF WAY AND ACCURATE ACCESS DATA, SEE RIGHT OF WAY RECORD MAPS AT DISTRICT OFFICE.
- PG&E GAS PIPE ON BRIDGE TO BE CUT OFF AND ABANDONED BY OWNER BY 12/21/12.
- CHARTER OVERHEAD FIBER OPTIC CABLE TO BE RELOCATED TO THE UNDERGROUND BY OWNER BY 12/21/12.
- MID ELECTRIC FACILITIES TO BE RELOCATED BY OWNER BY 12/21/12.
- AT&T FACILITIES ON THE BRIDGE WILL BE RELOCATED AFTER CONDUITS ARE ACCEPTED BY AT&T. ALL OFF BRIDGE FACILITIES WILL BE RELOCATED BY OWNER BY 12/21/12.
- UTILITY OWNERSHIP:
 - ELECTRIC - MODESTO IRRIGATION DISTRICT (MID)
 - GAS DISTRIBUTION - PACIFIC GAS & ELECTRIC (PG&E)
 - GAS TRANSMISSION - PACIFIC GAS & ELECTRIC (PG&E)
 - TELECOMMUNICATION - AMERICAN TELEPHONE & TELEGRAPH (AT&T) CHARTER
 - WATER - CITY OF MODESTO (COM)

LEGEND:

- CPC - C-PLASTIC CONDUIT
- GT - GAS TRANSMISSION
- HDPE - HIGH DENSITY POLYETHYLENE EXTRUSION
- MH - MAN HOLE
- PL - PLASTIC
- PH - POTHOLE
- PM - PAD MOUNT
- SAI - SERVING AREA INTERFACE
- STL - STEEL
- SW - SWITCH
- (R) - RELOCATE UTILITY (BY OWNER)
- (A) - ADJUST TO FINAL GRADE (BY OWNER)
- (AB) - ABANDON
- [A] - PROJECT TO ADJUST UTILITY TO GRADE
- [R] - PROJECT TO RELOCATE UTILITY
- [M] - UTILITY MAN HOLE
- [] - UTILITY BOX
- - POTHOLE LOCATION
- (---) - TRENCH SECTION
- (---) - TRENCH OCCUPANT
- (---) - TRENCH FOOTAGE
- ③ - 3" SCH 40 PVC CONDUIT
- ④ - 4" SCH 40 PVC CONDUIT

MID CONSTRUCTION NOTES:

- NOTIFY MID INSPECTOR AT LEAST 48 HOURS PRIOR TO TRENCHING. DO NOT BACKFILL UNTIL CONDUIT SYSTEM HAS BEEN INSPECTED. 209-526-7457.
- VERIFY LOCATION OF EXISTING MID AND ALL OTHER UNDERGROUND FACILITIES BY CONTACTING UNDERGROUND SERVICE ALERT (USA) TWO (2) DAYS PRIOR TO TRENCHING.
- CONTRACTOR IS RESPONSIBLE FOR COST OF REPAIRS, REPLACEMENT, RELOCATION MADE NECESSARY BY DAMAGE TO MID FACILITIES.
- THE INSPECTOR OR DESIGN ENGINEER MUST AUTHORIZE ANY CHANGES.

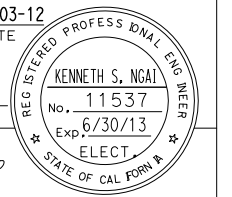
MID CONSTRUCTION NOTES CONTINUED:

- CONTRACTOR TO PROVIDE "AS BUILT" DRAWINGS WITH TRENCH SECTION FOOTAGES TO MID INSPECTOR AND THE DESIGN ENGINEER UPON COMPLETION OF THE PROJECT.
- MINIMUM PARALLEL CLEARANCE IS 12" FROM ALL DRY UTILITIES (C.P.U.C., GENERAL ORDER 128), UNLESS THERE IS PRIOR AGREEMENT. A MINIMUM OF 3' SEPARATION WITH 1' UNDISTURBED EARTH, IS PREFERRED FROM WET UTILITIES.
- MINIMUM PERPENDICULAR (CROSSING) CLEARANCE IS 6" FROM ANY OTHER UTILITY (C.P.U.C., GENERAL ORDER 128).
- MINIMUM TRENCH COVER SHALL BE AS FOLLOWS:
 - SECONDARY SERVICE IN DIRT (0 TO 750 VOLTS) 18" TO FINAL GRADE
 - SECONDARY SERVICE IN THE STREET 36" TO FINAL GRADE
 - PRIMARY IN DIRT OR UNDER THE SIDE WALK 36" TO FINAL GRADE
 - PRIMARY IN THE STREET OR DRIVEWAY 36" TO FINAL GRADE
- MANAGEMENT OF ALL TRENCHES SHALL BE PER THE CAL/OSHA AND PERMITTING JURISDICTIONAL AGENCY.
- ALL CONDUITS MUST BE NEW POLYVINYL CHLORIDE (PVC) DUCT DESIGNED FOR DIRECT BURIAL INSTALLATION AND SHALL COMPLY WITH ONE OF THE FOLLOWING STANDARDS: CONDUIT MARKED SCHEDULE 40 PVC ASTM TC-2, OR SCHEDULE 40U.L. 90°C.
- MID RESERVES THE RIGHT TO REJECT ANY OF THE ABOVE CONDUITS WITH SIGNS OF DAMAGE, OR WERE IMPROPERLY INSTALLED.
- SOLVENT CEMENTED JOINTS SHALL BE MADE ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS, USING PRIMER AND CEMENTS MEETING THE REQUIREMENTS OF ASTM D2564 OF PVC DUCT.
- ALL CONDUIT SECTIONS MUST BE RODDED, CLEANED, MANDRELED AND A PULLING TAPE PER GE-03-125.0, WITH SEQUENTIAL FOOTAGE MARKINGS INSTALLED AND ATTACHED TO AN END CAP PRIOR TO MID PULLING CABLE, MID TO SUPPLY THE MANDREL. PRIOR TO INSPECTION ALL WORK AT THE TERMINATING BOX SHALL BE COMPLETE.
- MINIMUM CONDUIT BEND RADIUS:

SIZE:	HORIZONTAL	VERTICAL
2"	24"	24"
3"	36"	36"
4"	48"	48"
5"	60"	60"
6"	72"	60"
- ONLY STANDARD FACTORY SWEEPS ON ALL BENDS WITH A RADIUS OF LESS THAN 80' ARE ACCEPTABLE.
- END BELLS ARE TO BE USED TO TERMINATED IN AN ECLOSURE EQUIPPED WITH TERMINATORS. ALL 6", 5", AND 4" PRIMARY CONDUITS MUST TERMINATE IN THE VAULT WALL TERMINATORS. ALL 4", 3", AND 2" SECONDARY CONDUITS ENTERING THE SIDE OR END OF A BOX MUST BE CUT 1" FLUSH TO THE INSIDE WALL AND SEALED. CONDUITS ENTERING IN THE BOTTOM OF A BOX OR PEDETAL MUST BE CUT OFF 2" FROM THE BASE. MAIN CONDUITS MUST BE PLACED IN LOWER TERMINATORS OR KNOCKOUTS. ALL STUBBED CONDUITS MUST BE CAPPED WITH PULL-TAB PLUG.
- ALL TRENCH BACKFILL MATERIAL SHALL BE, APPROVED EXCAVATED NATIVE, OR AS DIRECTED BY THE PERMITTING JURISDICTIONAL AGENCY. BACKFILL SHALL BE FREE OF SHARP EDGED ROCKS, PAVING MATERIAL, CINDERS, VEGETATION, CORROSIVE MATERIAL, OR OTHER UNSATISFACTORY MATERIAL. IF SATISFACTORY MATERIAL FOR USE AS STRUCTURE BACKFILL CANNOT BE OBTAINED FROM EXCAVATION, SUITABLE IMPORT MATERIAL, APPROVED BY THE ENGINEER, SHALL BE FURNISHED BY THE CONTRACTOR AT THEIR EXPENSE. BEDDING AND SHADING MATERIAL SHALL BE SALT FREE FILL SAND. ON AREAS TO BE PAVED, THE FINISHED SUBGRADE SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 95% FOR A DEPTH OF 6".
- AFTER FINAL COMPACTION, ALL NEW AND INTERCEPTED CONDUITS MUST BE MANDRELED WITH A MID SUPPLIED MANDREL IN THE PRESENTS OF THE MID INSPECTOR.
- MID SERVICE GUIDE 2009 DOCUMENT GE-04-195.0 SECTION 4 GENERAL UNDERGROUND PROVIDES DIMENSIONS AND SPECIFICATIONS FOR PRIMARY ELECTRIC EQUIPMENT AND SPLICE VAULTS.
- MID SERVICE GUIDE 2009 DOCUMENT GE-05-185.0 SECTION 5 GENERAL UNDERGROUND SERVICE EXTENSIONS PROVIDES DIMENSIONS AND SPECIFICATIONS FOR 7' X 6' TRANSFORMER PAD.

MODESTO IRRIGATION DISTRICT CONTACT:
SAM RONVEAUX 209-529-7423 SAMR@MID.ORG

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	104	423
			05-03-12		
			REGISTERED CIVIL ENGINEER		
			9/04/12		
			PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



APPROVED FOR UTILITY WORK ONLY

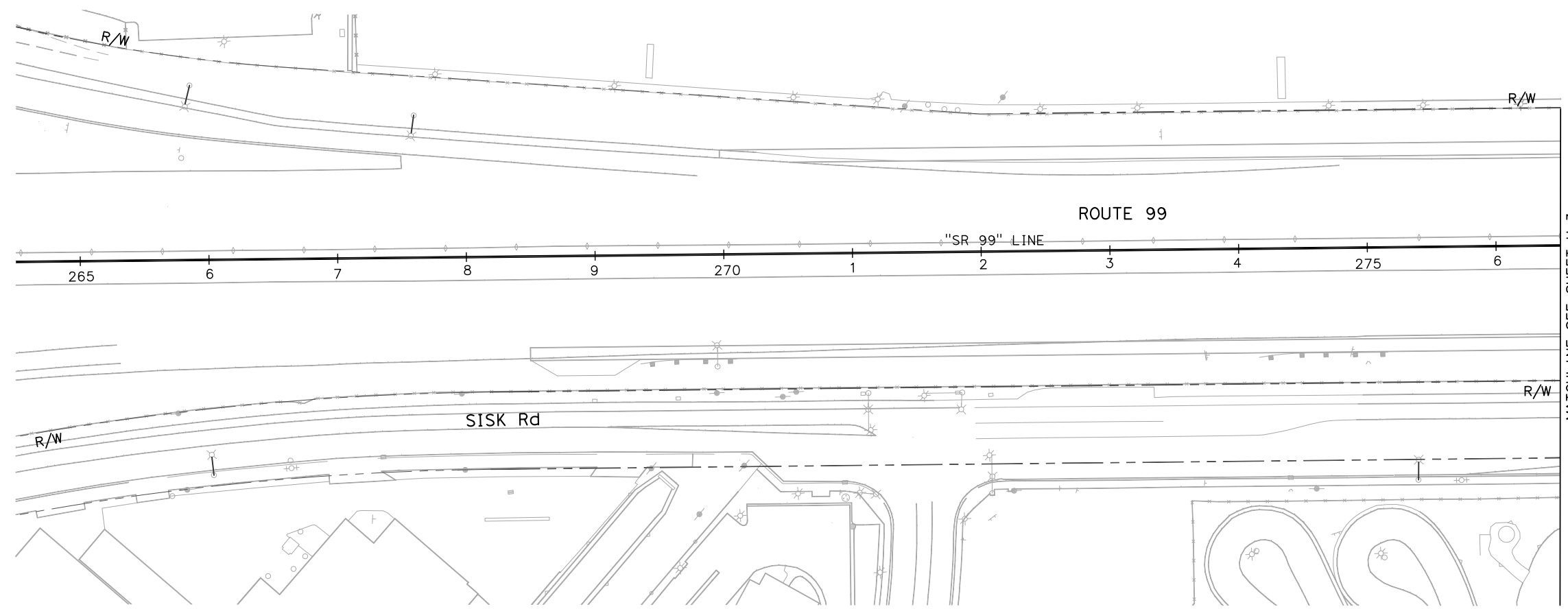
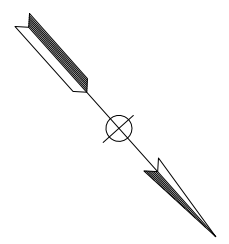
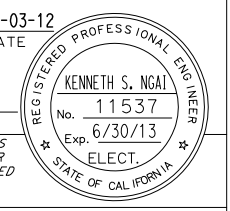
UTILITY PLANS

SCALE: 1" = 50'

U-1



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	105	423
		<i>KS Ngai</i>	05-03-12		
		REGISTERED CIVIL ENGINEER	DATE		
		9/04/12			
		PLANS APPROVAL DATE			
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
Caltrans	KEN NGAI	CHECKED BY	JOHN BEEBE
			JOHN BEEBE
			DATE
			REVISED

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

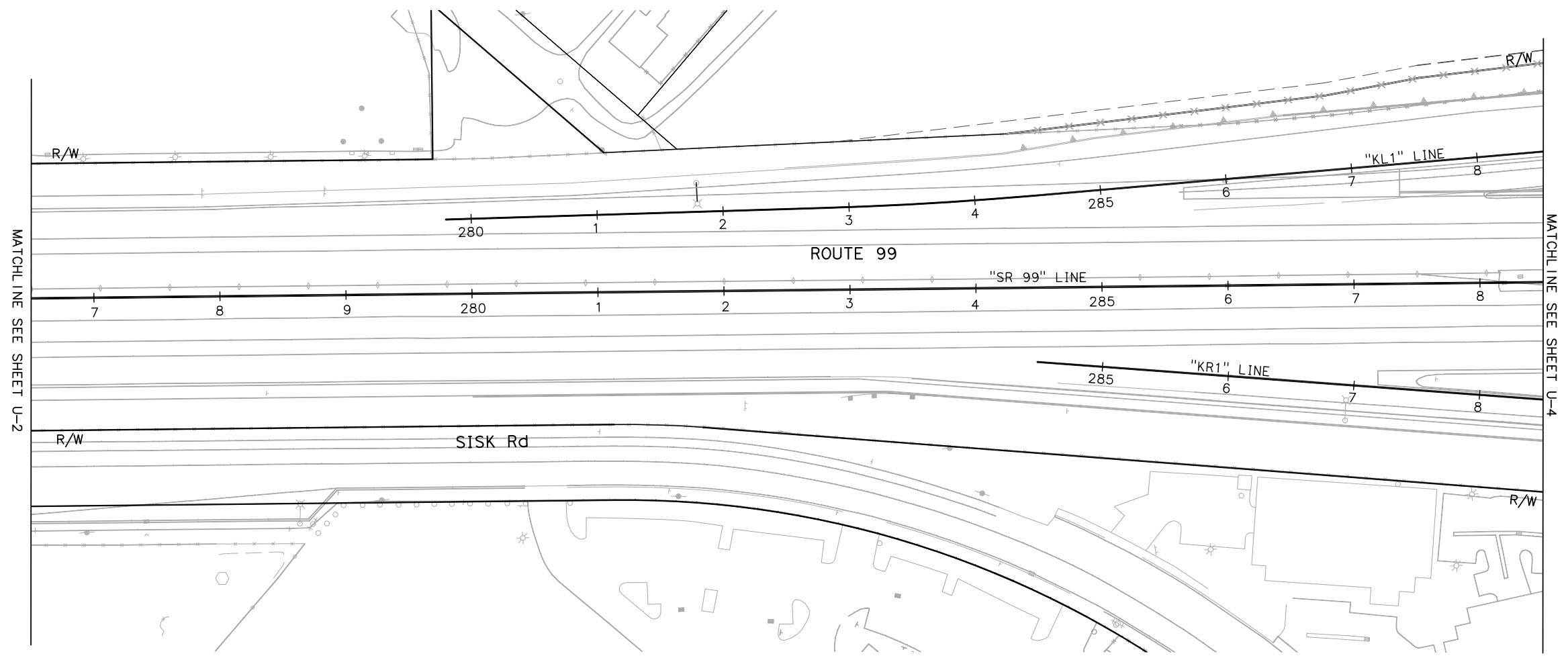
APPROVED FOR UTILITY WORK ONLY

UTILITY PLANS
SCALE: 1" = 50'

U-2

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	106	423
		<i>KS Ngai</i> REGISTERED CIVIL ENGINEER DATE 05-03-12			
		9/04/12 PLANS APPROVAL DATE			
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	JOHN BEEBE	REVISOR BY	
	KEN NGAI	CHECKED BY	JOHN BEEBE	DATE REVISED	



FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

UTILITY PLANS

SCALE: 1" = 50'

U-3

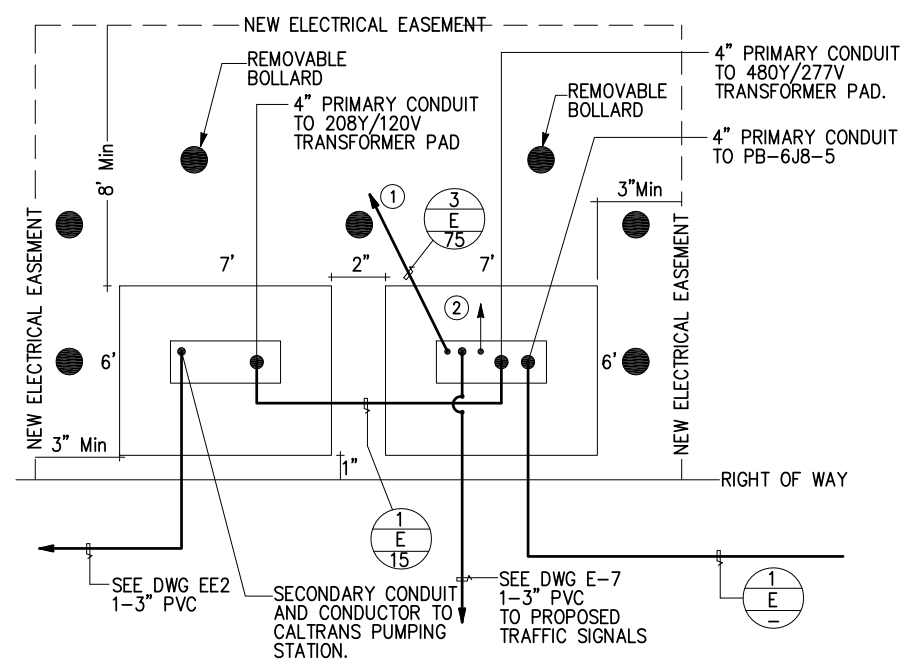
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: JOHN BEEBE
 CHECKED BY: JOHN BEEBE
 REVISIONS: REVISED BY: DATE REVISED:

NOTES:

- SECONDARY CONDUIT TO Exist METER 254698. SERVICE MAY BE ABANDONED PRIOR TO CONSTRUCTION START.
- FUTURE CONDUIT TO SERVE PROPOSED GAS STATION.
- LOCATE AND EXPOSE EXISTING CITY OF MODESTO WATER PIPE. TRENCH FROM EXISTING PIPE TO END OF NEW PIPE. PROTECT TRENCH UNTIL CITY OF MODESTO TIES-IN NEW PIPE TO EXISTING SYSTEM.

UTILITY RELOCATION NOTES:

- AT&T TO RELOCATE THEIR CABLES FROM THE EXISTING BRIDGE TO THE NEW BRIDGE. THEN ABANDON CONDUIT ON EXISTING BRIDGE.
- PG&E TO CUT OFF GAS PIPE AND ABANDON THE PIPE ON THE EXISTING BRIDGE.
- MID TO RELOCATE OVERHEAD FACILITIES.
- PROJECT TO REPLACE EXISTING 8" WITH 16" PVC IN 24" STEEL CASING. COM WILL TIE-IN TO THEIR EXISTING SYSTEM.



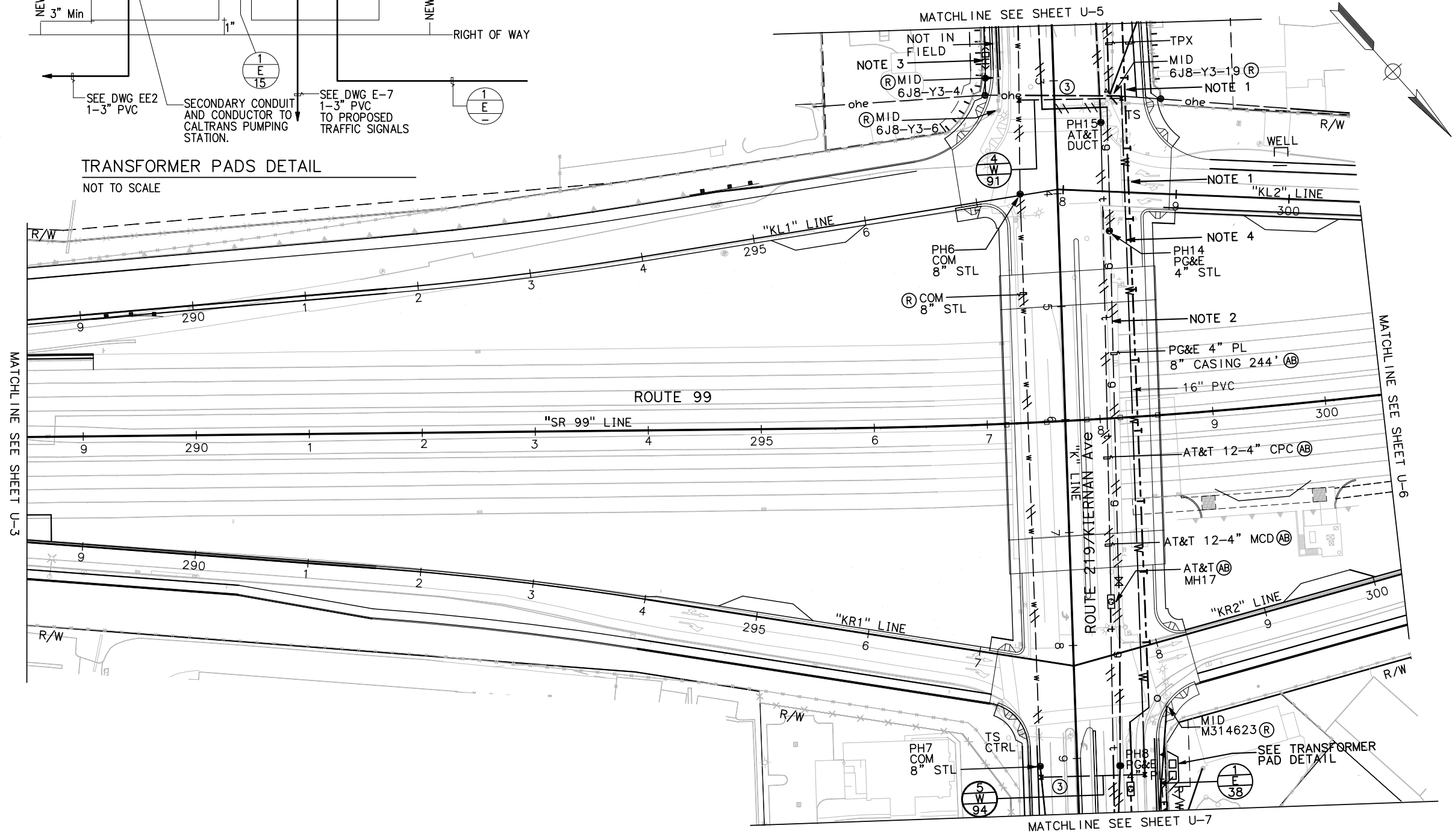
POSITIVE LOCATION INFORMATION

POTHOLE #	UTILITY	COVER	ELEVATION
PH6	COM 8" STEEL WATER	6.42	67.26
PH7	COM 8" STEEL WATER	4.25	70.98
PH8	PG&E 4" PLASTIC GAS	4.00	71.26
PH14	PG&E 4" STEEL	6.75	67.62
PH15	AT&T DUCT TELEPHONE	8.00	64.88

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	107	423

REGISTERED CIVIL ENGINEER: *W.S. Ng* DATE: 05-03-12
 PLANS APPROVAL DATE: 9/04/12
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER: KENNETH S. NGAI No. 11537 Exp. 6/30/13 ELECT



UTILITY PLANS
 SCALE: 1" = 50'
U-4

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

UTILITY RELOCATION NOTES:

1. AT&T TO RELOCATE THEIR FACILITIES SALIDA Blvd AND ADJUST MANHOLES TO FINAL GRADE.
2. MID TO RELOCATE OVERHEAD FACILITIES ON BROADWAY AVENUE.

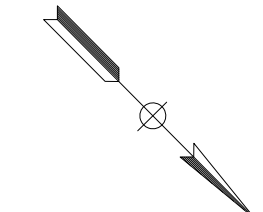
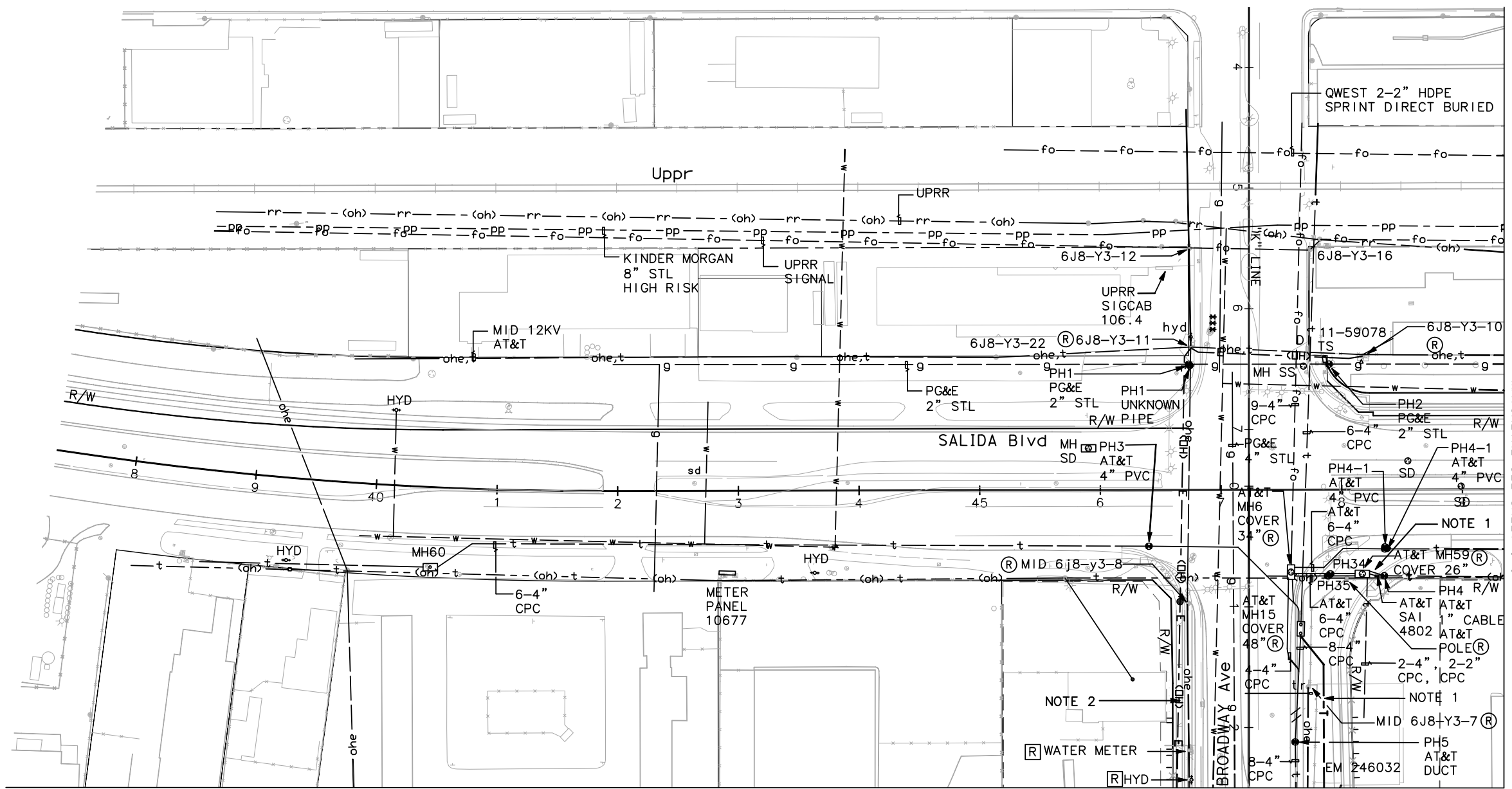
POSITIVE LOCATION INFORMATION

POTHOLE #	UTILITY	COVER	ELEVATION
PH1	PG&E 2" STEEL GAS	3.00	69.45
PH2	PG&E 2" STEEL GAS	4.25	68.50
PH3	AT&T 4" PVC TELEPHONE	3.50	69.60
PH4	AT&T 4" PVC TELEPHONE	2.75	69.15
PH4-1	AT&T 4" PVC TELEPHONE	3.92	69.62
PH5	AT&T DUC TELEPHONE	5.42	67.22
PH34	AT&T CONDUIT	5.17	68.18
PH35	AT&T CONDUIT	5.17	68.03

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	108	423

W.S. Ng
 REGISTERED CIVIL ENGINEER DATE 05-03-12
 9/04/12
 PLANS APPROVAL DATE
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 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR KEN NGAI
 CALCULATED/DESIGNED BY CHECKED BY
 JOHN BEEBE JOHN BEEBE
 REVISED BY DATE REVISED
 JOHN BEEBE



UTILITY PLANS
SCALE: 1" = 50'

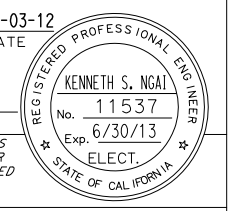
U-5

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

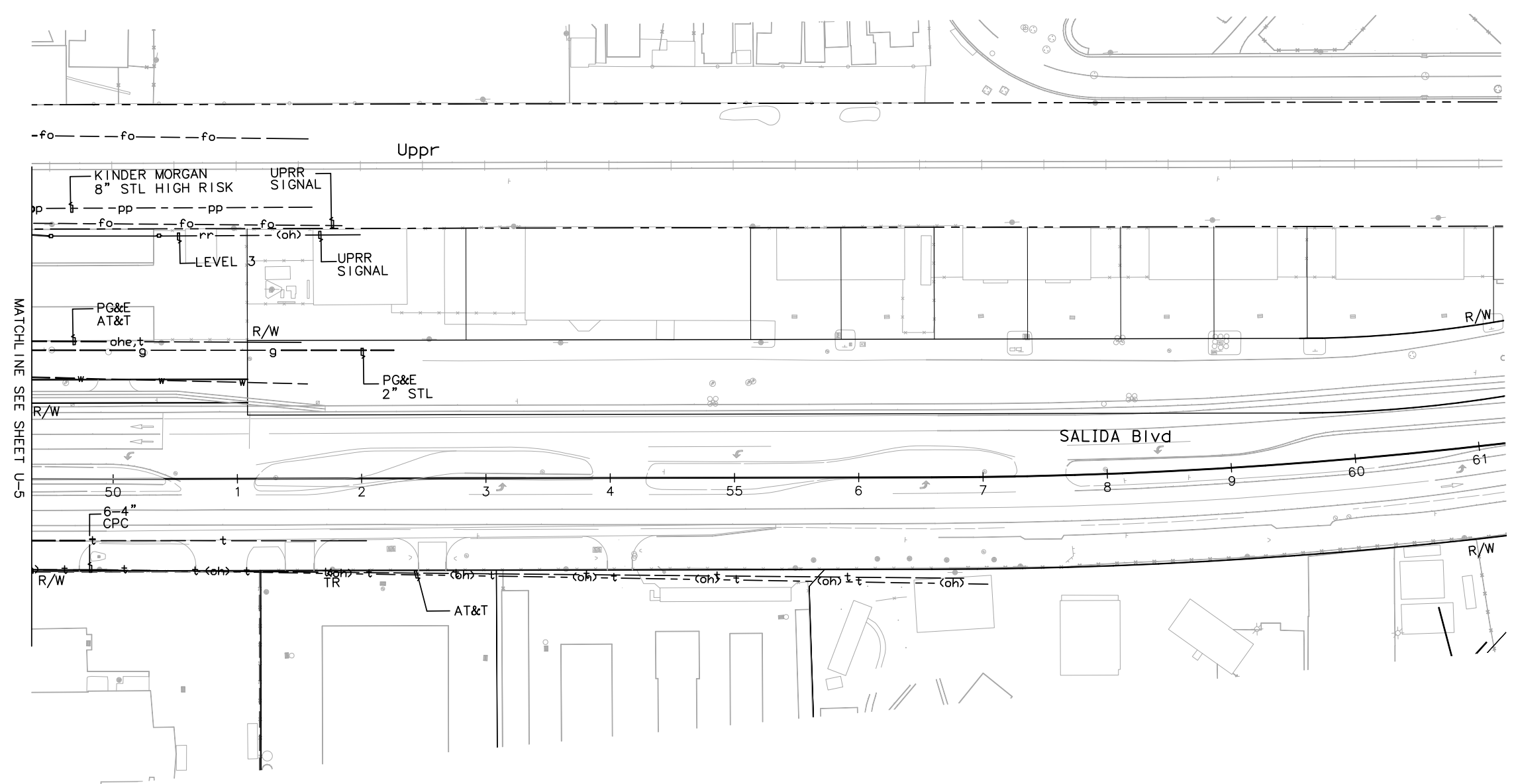
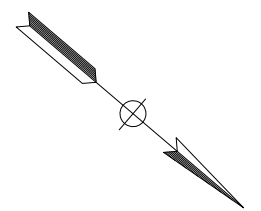
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LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	109	423
			05-03-12		
REGISTERED CIVIL ENGINEER			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
	KEN NGAI	JOHN BEEBE	JOHN BEEBE
		CHECKED BY	DATE REVISED
		JOHN BEEBE	



MATCHLINE SEE SHEET U-5

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

UTILITY PLANS

SCALE: 1" = 50'

U-6

LAST REVISION DATE PLOTTED => \$DATE
05-01-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 REVISIONS: JOHN BEEBE (DESIGNED BY), JOHN BEEBE (CHECKED BY), JOHN BEEBE (REVISOR), JOHN BEEBE (DATE REVISOR)
 REVISIONS: JOHN BEEBE (DESIGNED BY), JOHN BEEBE (CHECKED BY), JOHN BEEBE (REVISOR), JOHN BEEBE (DATE REVISOR)

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	110	423

REGISTERED CIVIL ENGINEER: *W.S. Ng* DATE: 05-03-12
 PLANS APPROVAL DATE: _____
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 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTES:

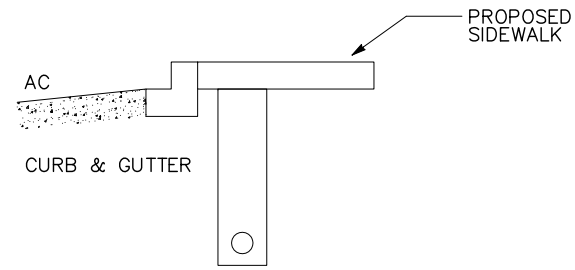
- LOCATE AND EXPOSE (BY HAND) 1-4" MID PRIMARY CONDUIT PROTECT OPEN TRENCH UNTIL MID CUTS OVER THEIR FACILITIES. MID WILL PLACE 1-4" CONDUIT & CONDUCTORS DURING CUT OVER.

UTILITY RELOCATION NOTES:

- CHARTER TO UNDERGROUND THEIR OVERHEAD FACILITY ON KIERNAN AVENUE.
- MID TO UNDERGROUND THEIR FACILITIES ON KIERNAN AVENUE AFTER CONDUIT PLACED BY CONTRACT.
- AT&T TO RELOCATE THEIR CONDUIT STRUCTURE.

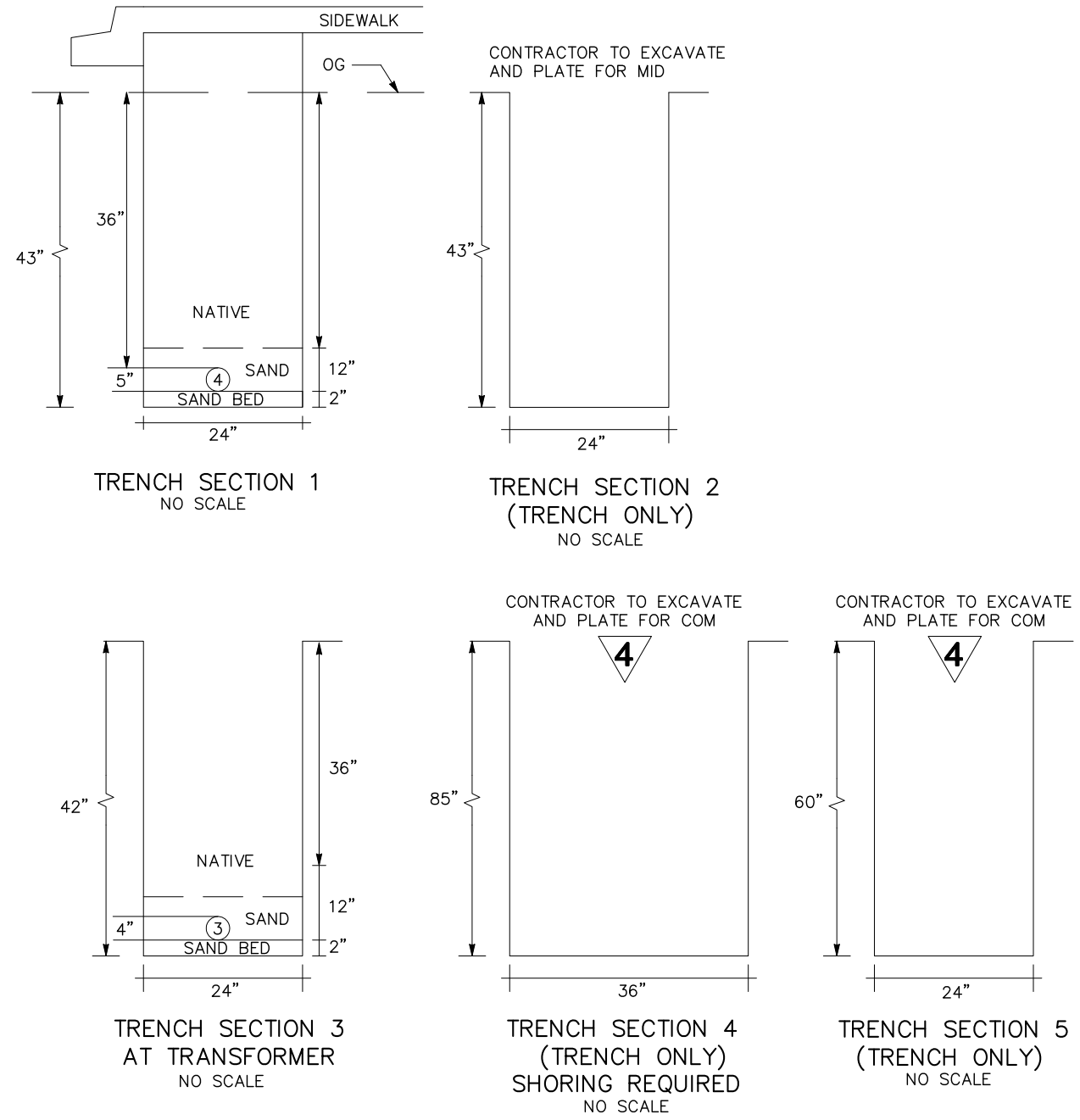
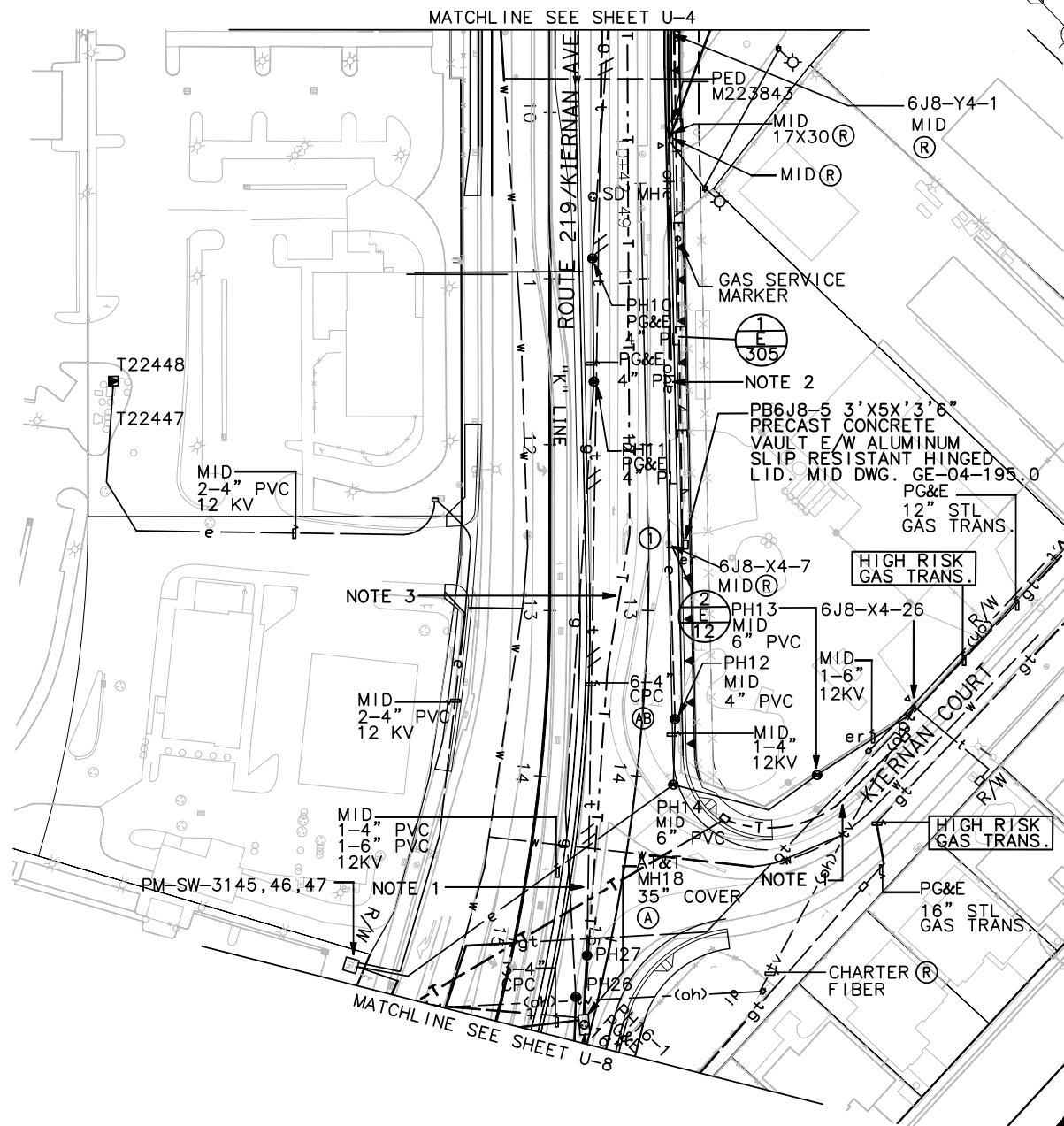
POSITIVE LOCATION INFORMATION

POTHOLE #	UTILITY	COVER	ELEVATION
PH10	PG&E 4" PLASTIC GAS	4.59	70.25
PH11	PG&E 4" PLASTIC GAS	4.33	70.54
PH12	MID 4" PVC ELECTRICAL	4.00	65.42
PH13	MID 6" PVC ELECTRICAL	4.09	68.89
PH14	MID 6" PVC ELECTRICAL	6.75	68.46
PH26	4" PL GAS	5.58	70.20
PH27	AT&T CONDUIT	6.75	68.90



TYPICAL UTILITY TRENCH SECTION

K 9+40 TO 12+40
 NO SCALE



4 REVISED PER ADDENDUM No. 4 DATED OCTOBER 16, 2012

APPROVED FOR UTILITY WORK ONLY

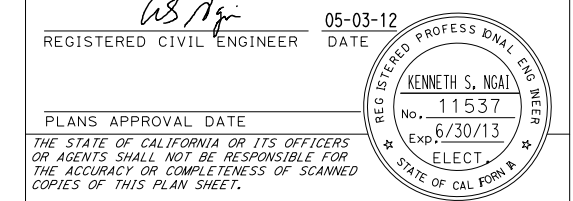
UTILITY PLANS

SCALE: 1" = 50'

U-7

LAST REVISION | DATE PLOTTED => \$DATE | 05-01-12 | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	111	423
			05-03-12 REGISTERED CIVIL ENGINEER DATE		
PLANS APPROVAL DATE THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

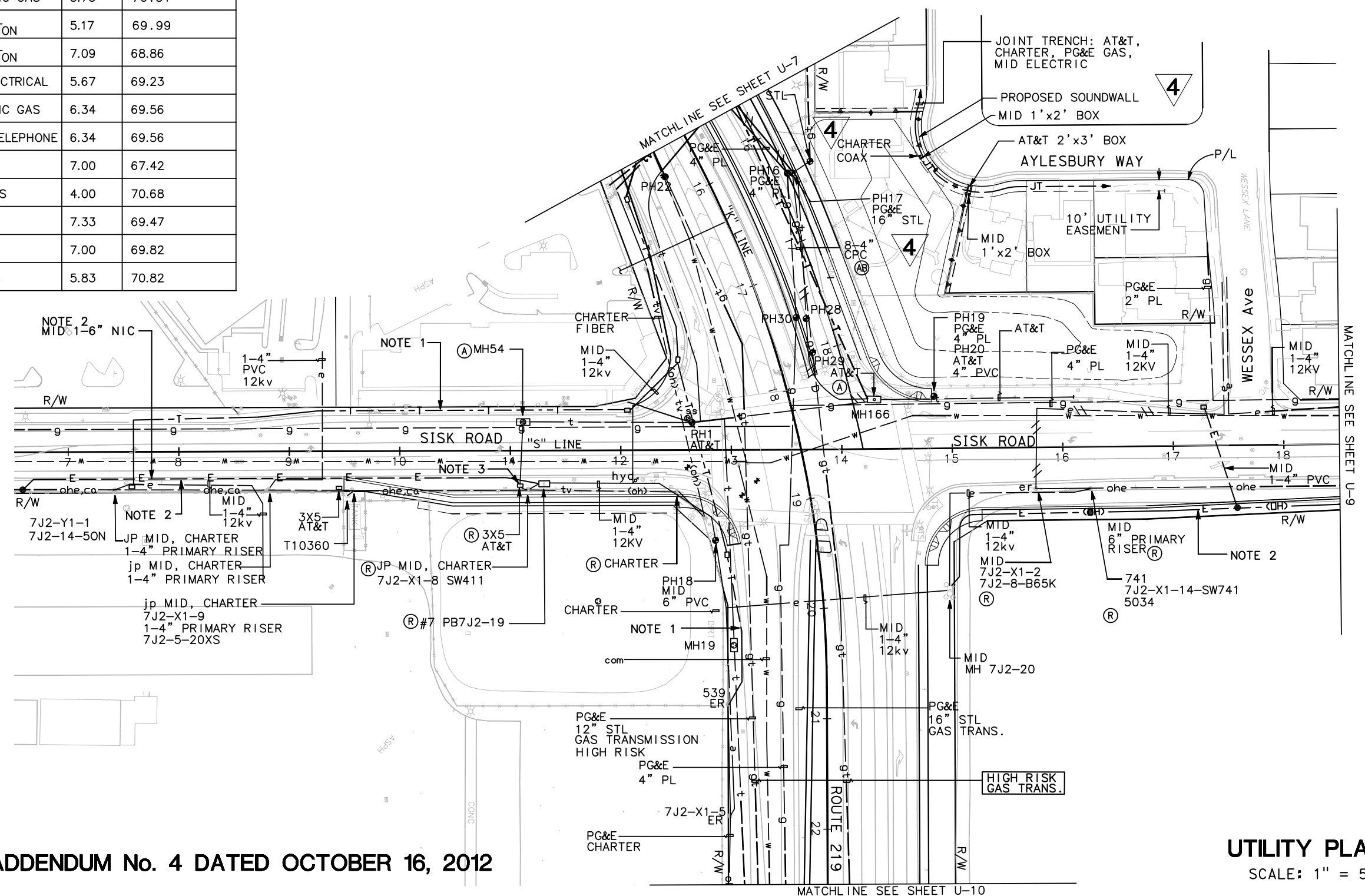


UTILITY RELOCATION NOTES:

1. CHARTER TO UNDERGROUND THEIR OVERHEAD FACILITIES ON KIERNAN AVENUE.
2. MID TO RELOCATE THEIR OVERHEAD AND UNDERGROUND FACILITIES ON SISK ROAD. MID TO CONVERT BOX 7J2-19 TO FULL TRAFFIC.
3. AT&T TO RELOCATE THEIR OVERHEAD AND UNDERGROUND FACILITIES ON SISK ROAD.

POSITIVE LOCATION INFORMATION

POTHOLE #	UTILITY	COVER	ELEVATION
PH16	PG&E 4" PLASTIC GAS	5.75	70.31
PH16-1	PG&E 16" STEEL GAS TRANSMISSION	5.17	69.99
PH17	PG&E 16" STEEL GAS TRANSMISSION	7.09	68.86
PH18	MID 6" PVC ELECTRICAL	5.67	69.23
PH19	PG&E 4" PLASTIC GAS	6.34	69.56
PH20	AT&T 4" PVC TELEPHONE	6.34	69.56
PH1	AT&T DUCT	7.00	67.42
PH22	12" PLASTIC GAS	4.00	70.68
PH28	4" AT&T DUCT	7.33	69.47
PH29	16" STEEL GAS	7.00	69.82
PH30	4" PLASTIC GAS	5.83	70.82



4 REVISED PER ADDENDUM No. 4 DATED OCTOBER 16, 2012

UTILITY PLANS

SCALE: 1" = 50'

U-8

APPROVED FOR UTILITY WORK ONLY

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 CALCULATED, DESIGNED BY
 CHECKED BY
 REVISIONS: JOHN BEEBE, JOHN BEEBE, JOHN BEEBE, JOHN BEEBE
 REVISOR: JOHN BEEBE, JOHN BEEBE, JOHN BEEBE, JOHN BEEBE
 DATE: [REDACTED], [REDACTED], [REDACTED], [REDACTED]

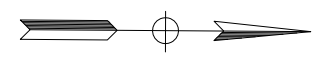
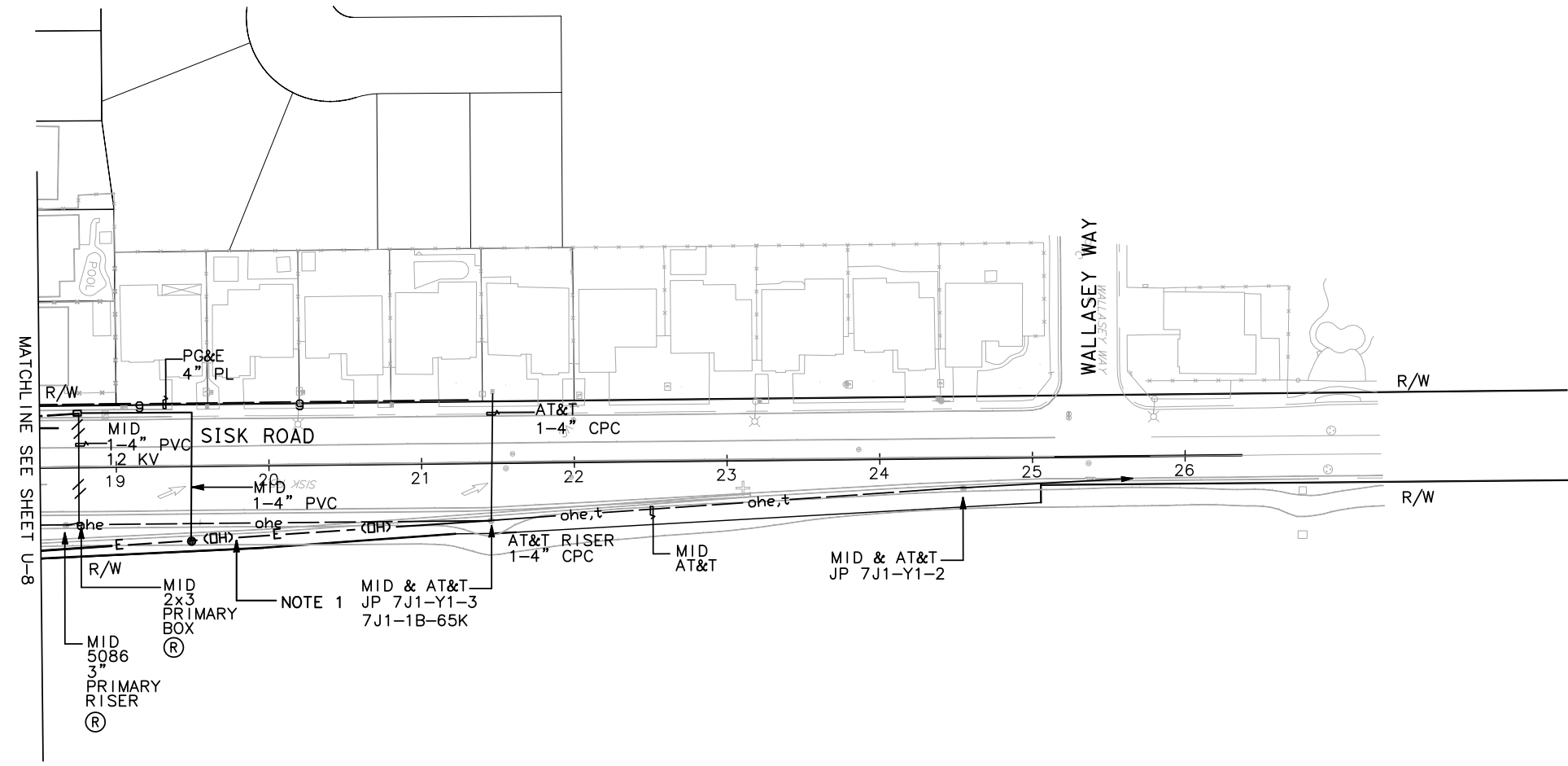
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	112	423

<i>KS Ngai</i>		05-03-12
REGISTERED CIVIL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

UTILITY RELOCATION NOTES:

- MID TO RELOCATE THEIR FACILITIES ON SISK ROAD.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	JOHN BEEBE	REVISOR	JOHN BEEBE
Caltrans	KEN NGAI	CHECKED BY	JOHN BEEBE	DATE	

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

UTILITY PLANS

SCALE: 1" = 50'

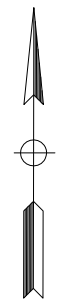
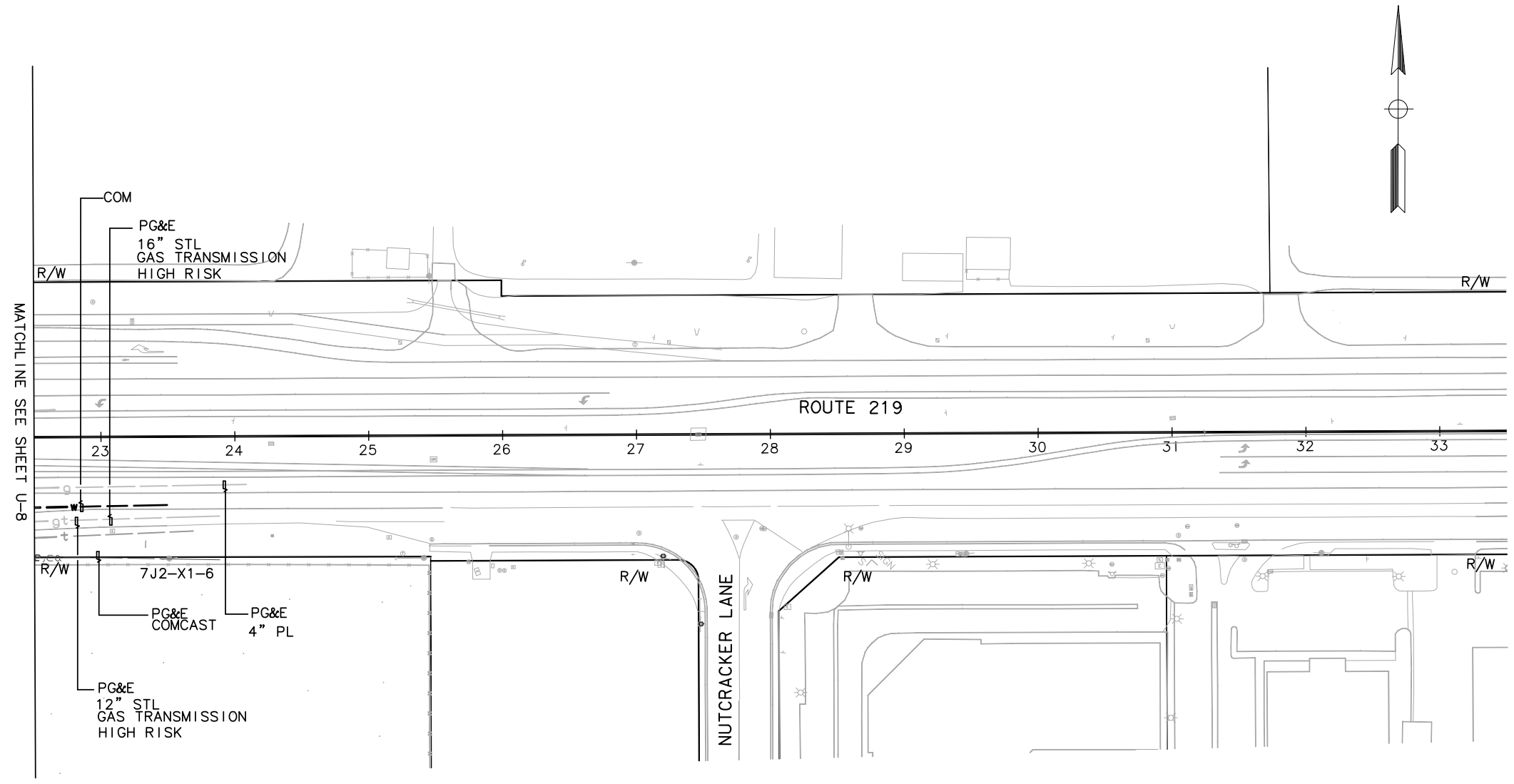
U-9

LAST REVISION | DATE PLOTTED => \$DATE
 05-01-12 | TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: JOHN BEEBE
 CHECKED BY: JOHN BEEBE
 REVISED BY: [] DATE REVISED: []

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	113	423

KS Ngai
 REGISTERED CIVIL ENGINEER DATE: 05-03-12
 PLANS APPROVAL DATE: 9/04/12
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

UTILITY PLANS

SCALE: 1" = 50'

U-10

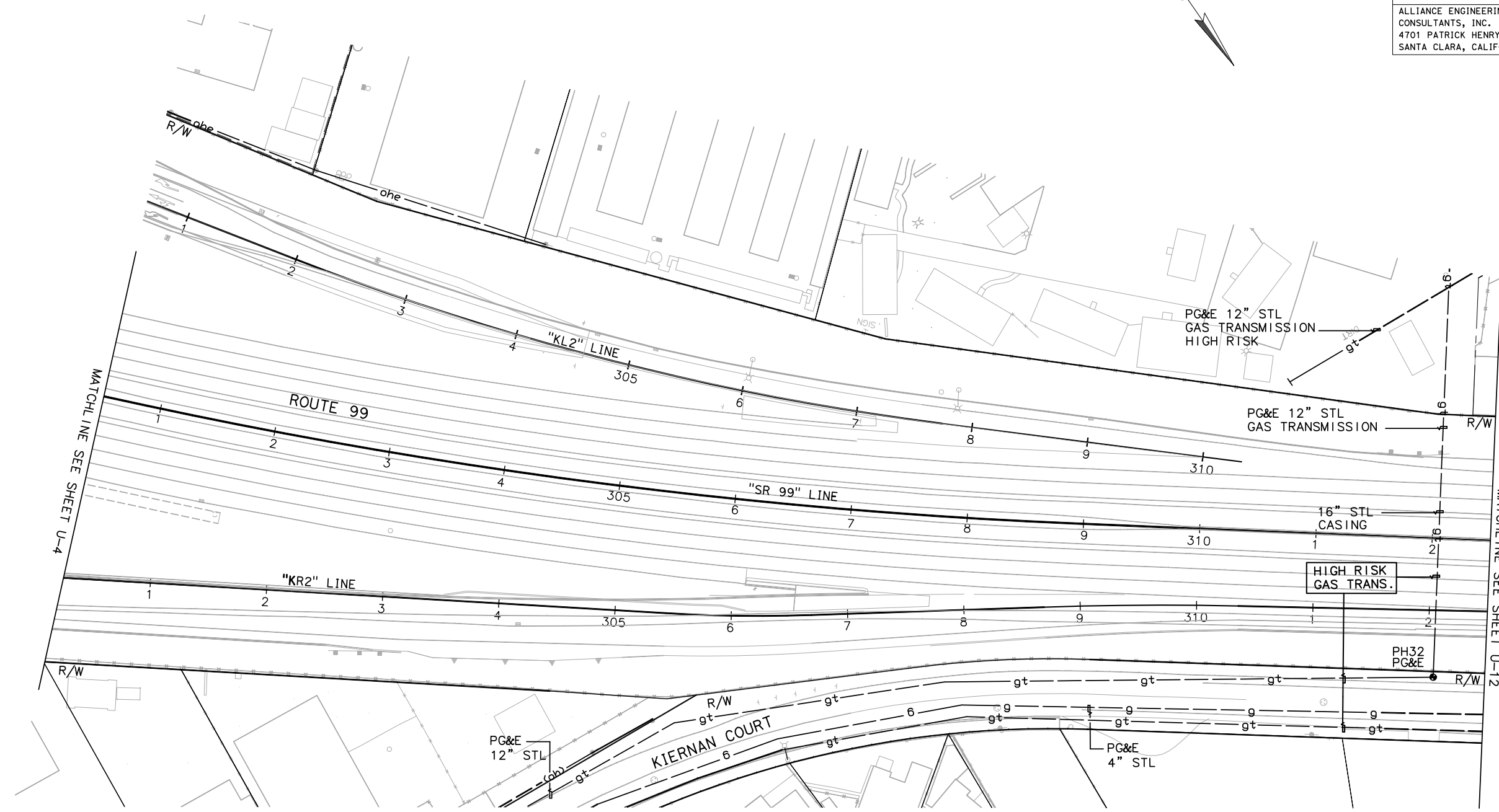
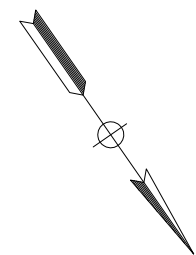
LAST REVISION | DATE PLOTTED => \$DATE
 05-01-12 | TIME PLOTTED => \$TIME

POSITIVE LOCATION INFORMATION

POTHOLE #	UTILITY	COVER	ELEVATION
PH32	12" STEEL GAS	5.66	70.98

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	114	423

KS Ngai
 REGISTERED CIVIL ENGINEER DATE 05-03-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: JOHN BEEBE
 CHECKED BY: JOHN BEEBE
 REVISED BY: JOHN BEEBE
 DATE REVISED:

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

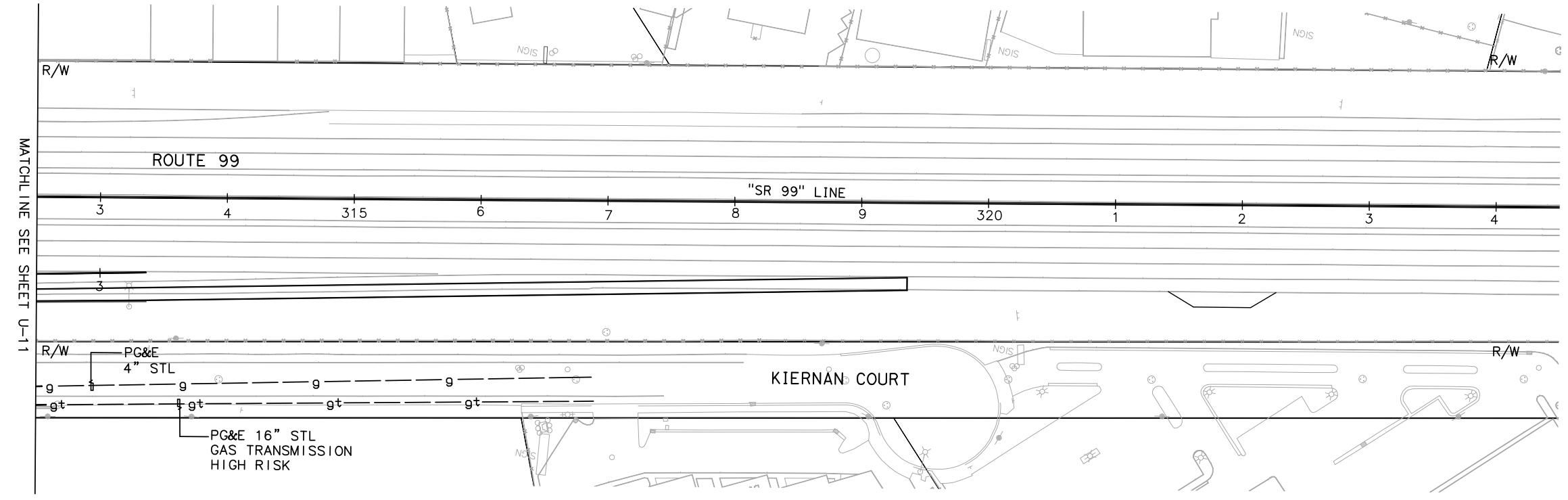
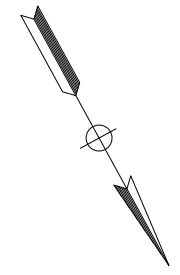
UTILITY PLANS

SCALE: 1" = 50'

U-11

LAST REVISION | DATE PLOTTED => \$DATE
 05-01-12 | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	115	423
		<i>WS Ngai</i> REGISTERED CIVIL ENGINEER DATE 05-03-12			
		9/04/12 PLANS APPROVAL DATE		THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
ALLIANCE ENGINEERING CONSULTANTS, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358			



MATCHLINE SEE SHEET U-11

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
	KEN NGAI	JOHN BEEBE	JOHN BEEBE
		CHECKED BY	DATE REVISED

FOR NOTES, ABBREVIATION AND LEGEND SEE SHEET U-1.

APPROVED FOR UTILITY WORK ONLY

UTILITY PLANS
SCALE: 1" = 50'

U-12

LAST REVISION DATE PLOTTED => \$DATE
05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	116	423

<i>Chung Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

STATIONARY MOUNTED CONSTRUCTION AREA SIGNS

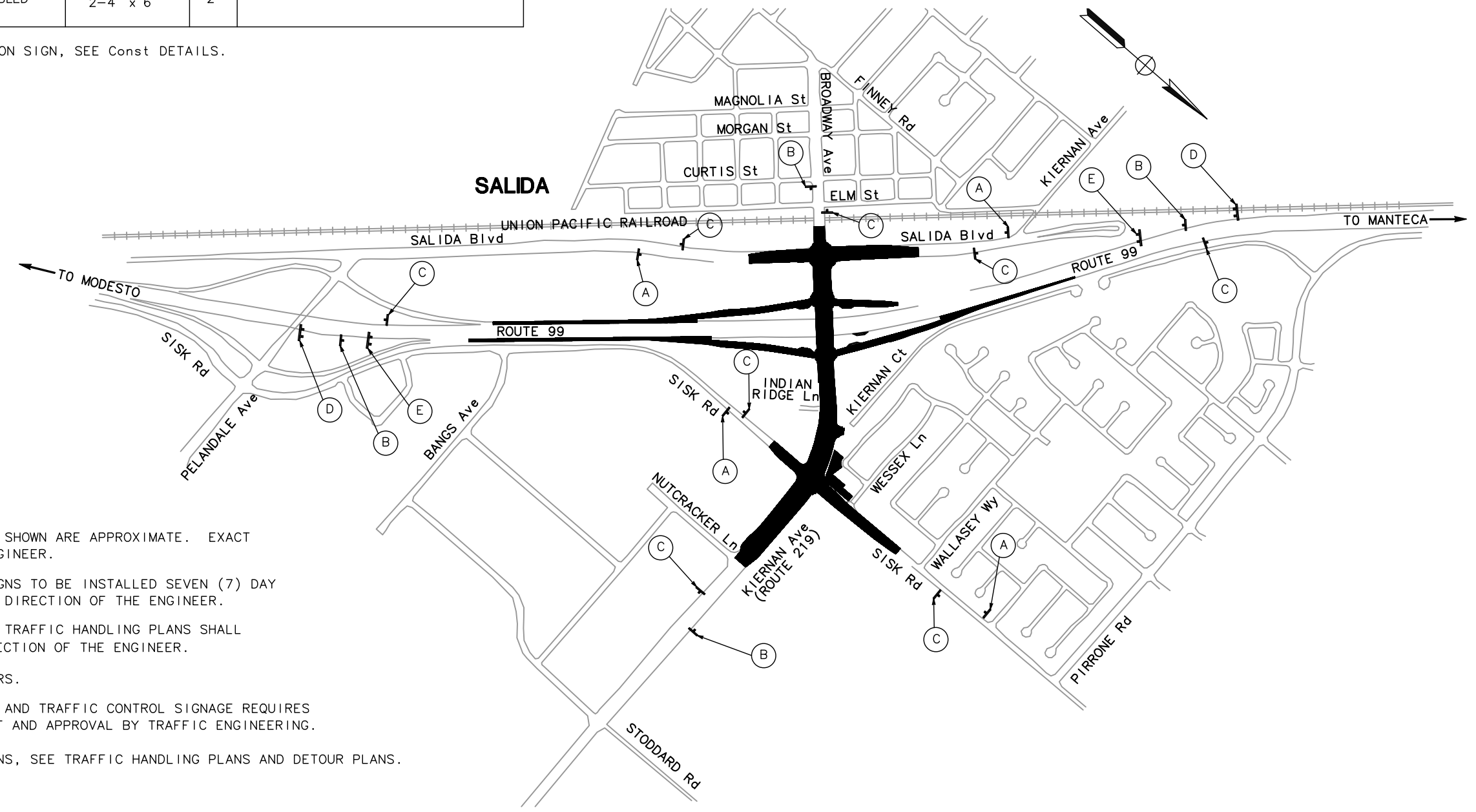
SIGN	SIGN CODE	PANEL SIZE	SIGN MESSAGE	No. OF POST AND SIZE	EACH	REMARKS
(A)	W20-1	36" x 36"	ROAD WORK AHEAD	1-4" x 6"	4	
(B)	W20-1	48" x 48"	ROAD WORK AHEAD	1-6" x 6"	4	MIN 2640' IN ADVANCE OF BEGIN CONSTRUCTION
(C)	G20-2	48" x 24"	END ROAD WORK	1-4" x 6"	8	MIN 1000' AFTER END CONSTRUCTION
(D)	CPFIS*	SEE DETAIL	CONSTRUCTION PROJECT FUNDING IDENTIFICATION	2-6" x 8"	2	
(E)	C40A (CA)	108" x 42"	TRAFFIC FINES DOUBLED IN WORK ZONE	2-4" x 6"	2	

LEGEND

(A) CONSTRUCTION AREA SIGN LETTER

CONSTRUCTION AREA

* CONSTRUCTION PROJECT FUNDING IDENTIFICATION SIGN, SEE Const DETAILS.



NOTES:

1. LOCATION OF CONSTRUCTION AREA SIGNS SHOWN ARE APPROXIMATE. EXACT LOCATION TO BE DETERMINED BY THE ENGINEER.
2. OVERLAYS FOR ADVANCE INFORMATION SIGNS TO BE INSTALLED SEVEN (7) DAY PRIOR TO ROAD CLOSURE AND UNDER THE DIRECTION OF THE ENGINEER.
3. ALL EXISTING SIGNS IN CONFLICT WITH TRAFFIC HANDLING PLANS SHALL BE COVERED OR REMOVED UNDER THE DIRECTION OF THE ENGINEER.
4. SEE DETOUR PLANS FOR PROPOSED DETOURS.
5. ANY LOCAL STREET CLOSURES, DETOURS, AND TRAFFIC CONTROL SIGNAGE REQUIRES A CITY OR COUNTY ENCROACHMENT PERMIT AND APPROVAL BY TRAFFIC ENGINEERING.
6. FOR ADDITIONAL CONSTRUCTION AREA SIGNS, SEE TRAFFIC HANDLING PLANS AND DETOUR PLANS.

REVISED PER ADDENDUM No. 5 DATED OCTOBER 18, 2012

CONSTRUCTION AREA SIGNS
NO SCALE **CS-1**

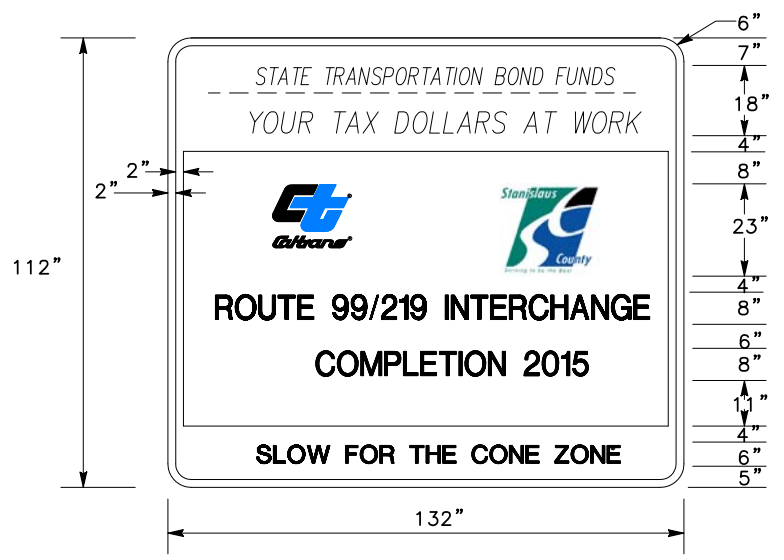
APPROVED FOR CONSTRUCTION AREA SIGN WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 CHECKED BY: CHUONG NGUYEN
 REVISOR: DAVID LIU
 DATE REVISOR: CHUONG NGUYEN

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION: 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	117	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



PROP 1-B BOND FUNDS - GENERAL SIGN SPECIFICATION

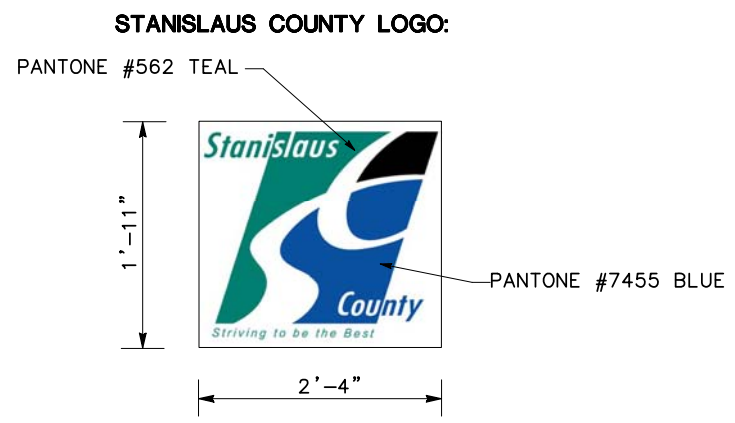
DESIGN NOTES:

1. MAIN TEXT (EXAMPLES: EXTENDING HOV LANES, HIGHWAY CONSTRUCTION) USE 6" SERIES D TEXT MINIMUM.
2. COMPLETION DATE TEXT (EXAMPLES: COMPLETION 2010, COMPLETION FALL 2009, YEAR OF COMPLETION 2010) USE 8" SERIES D TEXT MINIMUM FOR ALL TEXT. OPTIONALLY, IF "COMPLETION" IS USED ON A SEPARATE LINE AND TO THE LEFT OR RIGHT OF A LOGO, IT MAY BE USED AS SMALL TEXT, 4" SERIES D MINIMUM.
3. ALL FUNDING AGENCY PARTICIPANT LOGOS ARE TO BE DISPLAYED.
4. INDIVIDUAL LOGO HEIGHT VARIABLE UP TO 23" MAXIMUM.
5. CALTRANS LOGO SHALL BE DISPLAYED AS THE UPPERMOST AND LEFTMOST LOGO AMONGST A GROUP OR GROUPS OF LOGOS.
6. CONSTRUCT CALTRANS LOGO PER CALTRANS STANDARD PLAN T7 DETAIL A-2.
7. OVERALL SIGN SIZE SHALL NOT EXCEED 11' x 11' (132" x 132").

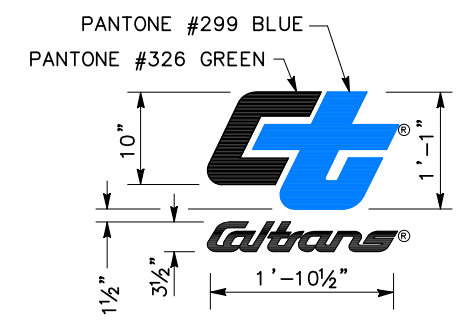
FABRICATION NOTES:

1. VECTOR FILES FOR LOGOS TO BE SUPPLIED TO THE SIGN MANUFACTURER UPON REQUEST.
2. COMPLY WITH 2006 STANDARD PLANS S93 AND S94 FOR SIGN PANEL.
3. COMPLY WITH ASTM TYPE II RETROREFLECTIVE SHEETING (OR HIGHER GRADE) FOR ALL COLORS EXCEPT BLACK.
4. LEGEND AND ARTWORK MAY BE TRANSPARENT FILM, INK, OR APPLIED DECALS.
5. OVERLAY ENTIRE SIGN FACE WITH PREMIUM GRADE GRAFFITI FILM.
6. COMPLY WITH RETROREFLECTIVE SHEETING MANUFACTURER'S RECOMMENDATIONS FOR TRANSPARENT FILM, INK, APPLIED DECALS, AND GRAFFITI FILM.

COLORS: BORDER - PURPLE (PANTONE #520)
 LEGEND - BLACK, WHITE, AND ORANGE
 BACKGROUND - PURPLE (PANTONE #520) AND WHITE
 CT LOGO - GREEN (PANTONE #326) AND BLUE (PANTONE #299)



DETAIL A



DETAIL B

CONSTRUCTION PROJECT FUNDING IDENTIFICATION SIGN
NO SCALE

APPROVED FOR CONSTRUCTION AREA SIGN WORK ONLY

CS-2






STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	REVISOR	DATE
Caltrans	REVISOR	DATE
	REVISOR	DATE
CONSULTANT SUPERVISOR	DESIGNED BY	CHECKED BY
KEITH G. MEYER	ANDY CHAN	CHUONG NGUYEN

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: ANDY DO
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

NOTES:

1. WORK NOT NECESSARY FOR TRAFFIC HANDLING MAY BE COMPLETED AT ANY TIME AND IS NOT SHOWN.
2. FOR DETAILS ON TRAFFIC HANDLING AT STAGE CONSTRUCTION, SEE TRAFFIC HANDLING PLANS.
3. ACCESS TO BUSINESS AND PEDESTRIAN CIRCULATION SHALL BE MAINTAINED AT ALL TIMES.

LEGEND

-  FIRST ORDER OF WORK
-  CONSTRUCT TEMPORARY PAVEMENT
-  CONSTRUCT THIS STAGE
-  CONSTRUCTED AT PREVIOUS STAGES
-  (3) TRAFFIC DIRECTION AND NUMBER OF LANES

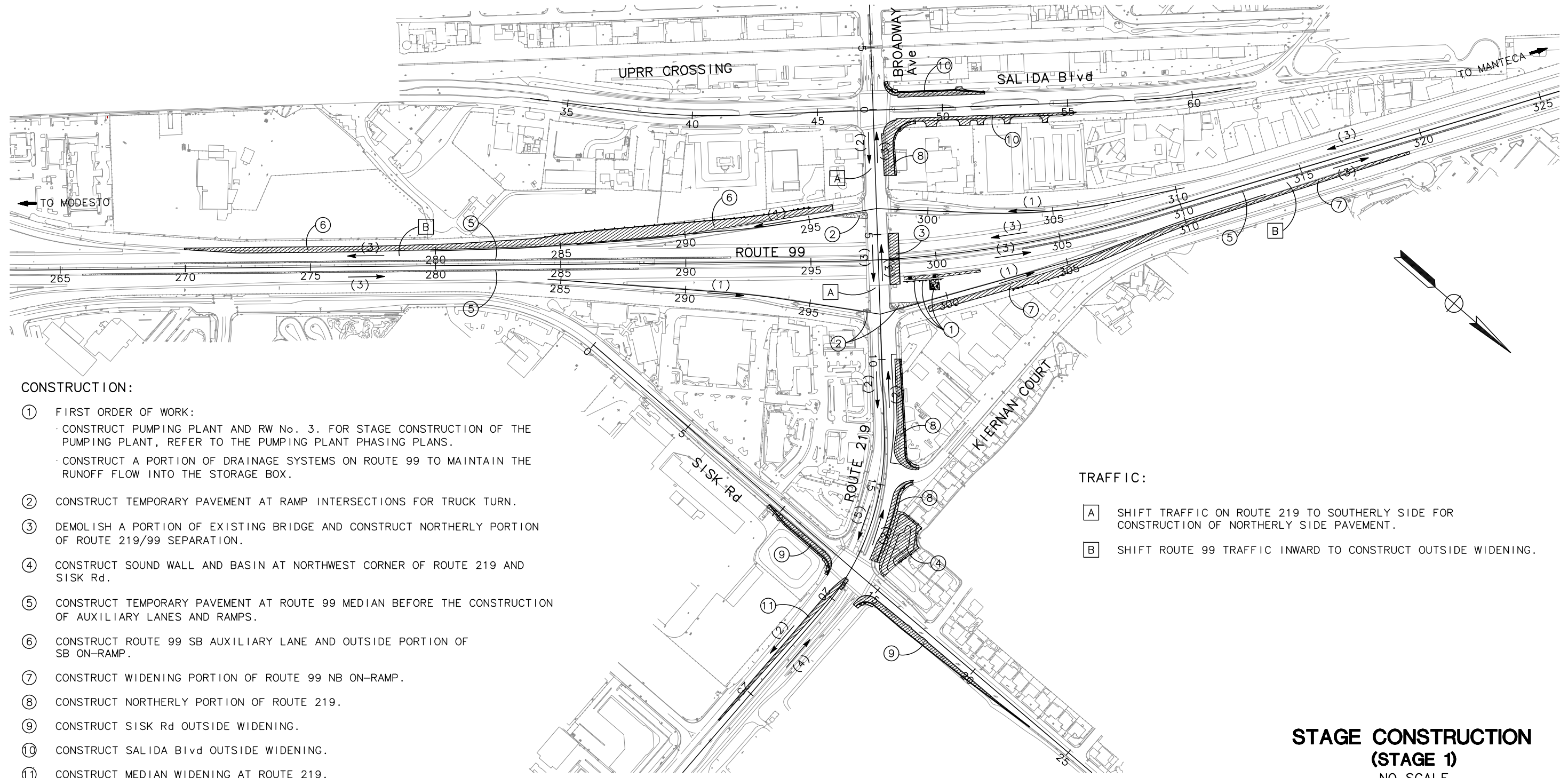
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	118	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



CONSTRUCTION:

- 1 FIRST ORDER OF WORK:
 - CONSTRUCT PUMPING PLANT AND RW No. 3. FOR STAGE CONSTRUCTION OF THE PUMPING PLANT, REFER TO THE PUMPING PLANT PHASING PLANS.
 - CONSTRUCT A PORTION OF DRAINAGE SYSTEMS ON ROUTE 99 TO MAINTAIN THE RUNOFF FLOW INTO THE STORAGE BOX.
- 2 CONSTRUCT TEMPORARY PAVEMENT AT RAMP INTERSECTIONS FOR TRUCK TURN.
- 3 DEMOLISH A PORTION OF EXISTING BRIDGE AND CONSTRUCT NORTHERLY PORTION OF ROUTE 219/99 SEPARATION.
- 4 CONSTRUCT SOUND WALL AND BASIN AT NORTHWEST CORNER OF ROUTE 219 AND SISK Rd.
- 5 CONSTRUCT TEMPORARY PAVEMENT AT ROUTE 99 MEDIAN BEFORE THE CONSTRUCTION OF AUXILIARY LANES AND RAMPS.
- 6 CONSTRUCT ROUTE 99 SB AUXILIARY LANE AND OUTSIDE PORTION OF SB ON-RAMP.
- 7 CONSTRUCT WIDENING PORTION OF ROUTE 99 NB ON-RAMP.
- 8 CONSTRUCT NORTHERLY PORTION OF ROUTE 219.
- 9 CONSTRUCT SISK Rd OUTSIDE WIDENING.
- 10 CONSTRUCT SALIDA Blvd OUTSIDE WIDENING.
- 11 CONSTRUCT MEDIAN WIDENING AT ROUTE 219.

TRAFFIC:

- A** SHIFT TRAFFIC ON ROUTE 219 TO SOUTHERLY SIDE FOR CONSTRUCTION OF NORTHERLY SIDE PAVEMENT.
- B** SHIFT ROUTE 99 TRAFFIC INWARD TO CONSTRUCT OUTSIDE WIDENING.

APPROVED FOR STAGE CONSTRUCTION WORK ONLY

STAGE CONSTRUCTION (STAGE 1)
 NO SCALE
SC-1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 ANDY DO
 CHUONG NGUYEN
 REVISED BY: DATE REVISOR
 DATE REVISOR

TRAFFIC:

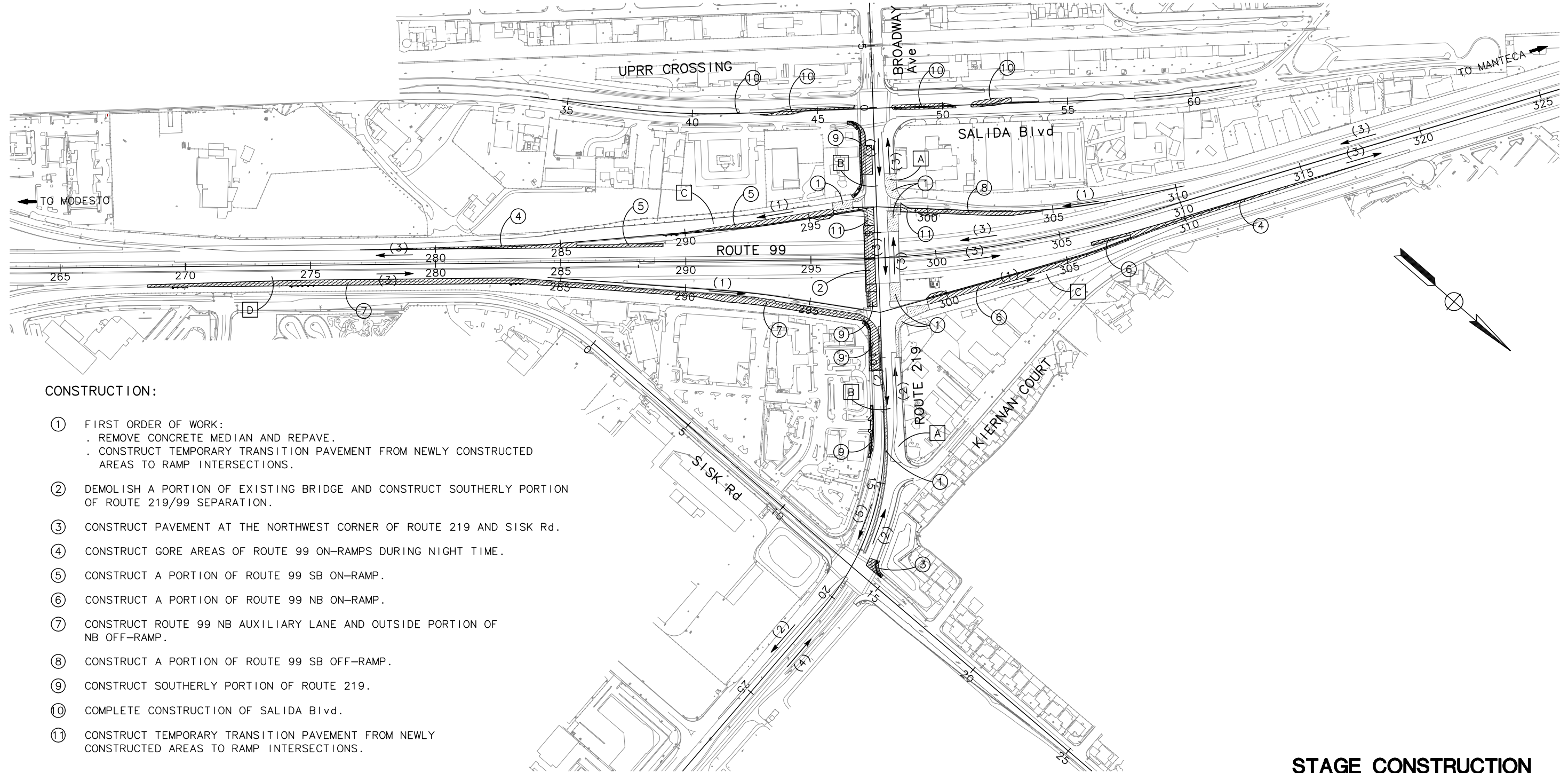
- A** SHIFT WB TRAFFIC ON ROUTE 219 TO NEWLY CONSTRUCTED PAVEMENT.
- B** SHIFT EB TRAFFIC ON ROUTE 219 TO THE MIDDLE PORTION OF ROADWAY.
- C** SHIFT TRAFFIC ON ROUTE 99 ON-RAMP TO NEWLY CONSTRUCTED PAVEMENT.
- D** SHIFT ROUTE 99 NB TRAFFIC INWARD TO CONSTRUCT OUTSIDE WIDENING.
- E** THE CONSTRUCTION OF INTERSECTIONS SHALL BE SEQUENCED TO MAINTAIN PEDESTRIAN CIRCULATION AT ALL TIMES.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	119	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--



CONSTRUCTION:

- ① FIRST ORDER OF WORK:
 . REMOVE CONCRETE MEDIAN AND REPAVE.
 . CONSTRUCT TEMPORARY TRANSITION PAVEMENT FROM NEWLY CONSTRUCTED AREAS TO RAMP INTERSECTIONS.
- ② DEMOLISH A PORTION OF EXISTING BRIDGE AND CONSTRUCT SOUTHERLY PORTION OF ROUTE 219/99 SEPARATION.
- ③ CONSTRUCT PAVEMENT AT THE NORTHWEST CORNER OF ROUTE 219 AND SISK Rd.
- ④ CONSTRUCT GORE AREAS OF ROUTE 99 ON-RAMPS DURING NIGHT TIME.
- ⑤ CONSTRUCT A PORTION OF ROUTE 99 SB ON-RAMP.
- ⑥ CONSTRUCT A PORTION OF ROUTE 99 NB ON-RAMP.
- ⑦ CONSTRUCT ROUTE 99 NB AUXILIARY LANE AND OUTSIDE PORTION OF NB OFF-RAMP.
- ⑧ CONSTRUCT A PORTION OF ROUTE 99 SB OFF-RAMP.
- ⑨ CONSTRUCT SOUTHERLY PORTION OF ROUTE 219.
- ⑩ COMPLETE CONSTRUCTION OF SALIDA Blvd.
- ⑪ CONSTRUCT TEMPORARY TRANSITION PAVEMENT FROM NEWLY CONSTRUCTED AREAS TO RAMP INTERSECTIONS.

APPROVED FOR STAGE CONSTRUCTION WORK ONLY

STAGE CONSTRUCTION
(STAGE 2)
 NO SCALE
SC-2

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CHECKED BY: CHUONG NGUYEN
 DESIGNED BY: ANDY DO
 REVISED BY: CHUONG NGUYEN
 DATE REVISED: []

TRAFFIC:

- A WB TRAFFIC ON ROUTE 219 TO REMAIN THE SAME AS IN PREVIOUS STAGE 2.
- B SHIFT EB TRAFFIC ON ROUTE 219 TO NEW SOUTHERLY CONSTRUCTED PAVEMENT.
- C FINAL TRAFFIC ON ROUTE 99 AND ROUTE 99 ON-RAMPS.
- D SHIFT TRAFFIC ON ROUTE 99 OFF-RAMP TO NEWLY CONSTRUCTED PAVEMENT.
- E THE CONSTRUCTION OF INTERSECTIONS SHALL BE SEQUENCED TO MAINTAIN PEDESTRIAN CIRCULATION AT ALL TIMES.

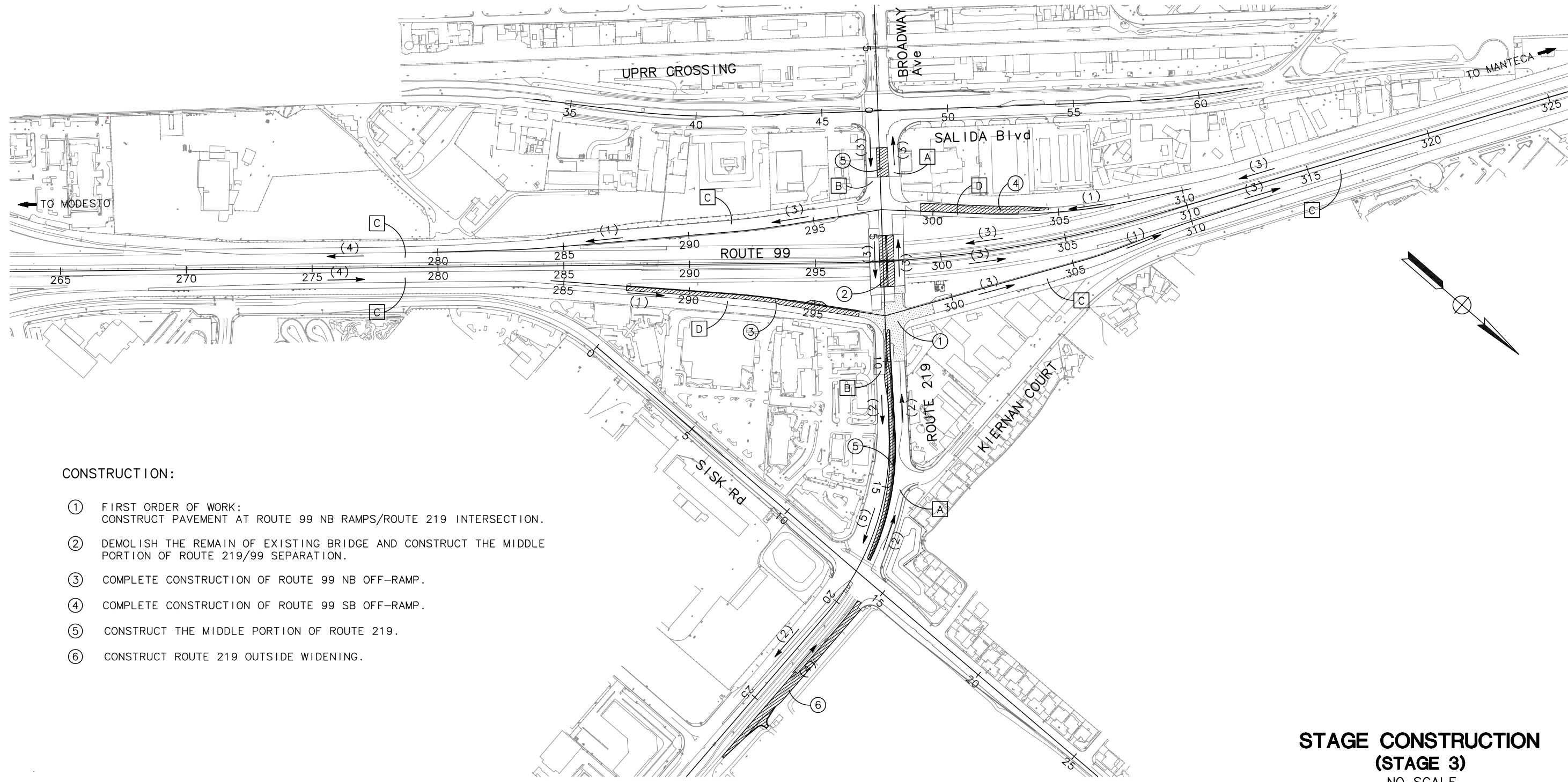
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	120	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
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 STATE OF CALIFORNIA

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 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



CONSTRUCTION:

- ① FIRST ORDER OF WORK: CONSTRUCT PAVEMENT AT ROUTE 99 NB RAMPS/ROUTE 219 INTERSECTION.
- ② DEMOLISH THE REMAIN OF EXISTING BRIDGE AND CONSTRUCT THE MIDDLE PORTION OF ROUTE 219/99 SEPARATION.
- ③ COMPLETE CONSTRUCTION OF ROUTE 99 NB OFF-RAMP.
- ④ COMPLETE CONSTRUCTION OF ROUTE 99 SB OFF-RAMP.
- ⑤ CONSTRUCT THE MIDDLE PORTION OF ROUTE 219.
- ⑥ CONSTRUCT ROUTE 219 OUTSIDE WIDENING.

APPROVED FOR STAGE CONSTRUCTION WORK ONLY

STAGE CONSTRUCTION (STAGE 3)
 NO SCALE
SC-3

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 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 ANDY DO
 REVISOR: CHUONG NGUYEN
 DATE REVISOR: CHUONG NGUYEN

CONSTRUCTION:

- ① FIRST ORDER OF WORK: CONSTRUCT PAVEMENT AT ROUTE 99 SB RAMPS/ROUTE 219 INTERSECTION.
- ② COMPLETE CONSTRUCTION OF SIDEWALK ON ROUTE 219/99 SEPARATION.
- ③ COMPLETE CONSTRUCTION OF SIDEWALK ON ROUTE 219.
- ④ CONSTRUCT THRIE BEAM BARRIER AND RUMBLE STRIP AT ROUTE 99 MEDIAN.
- ⑤ PLACE FINAL LIFT OF PAVEMENT ON RAMPS AND 0.15' THICK RHMA-G ON ROUTE 219.

TRAFFIC:

- A SIDEWALK SHALL BE AVAILABLE FOR PEDESTRIAN AT ALL TIMES.
- B FINAL TRAFFIC ON ROUTE 219 AND ROUTE 99 OFF-RAMPS.

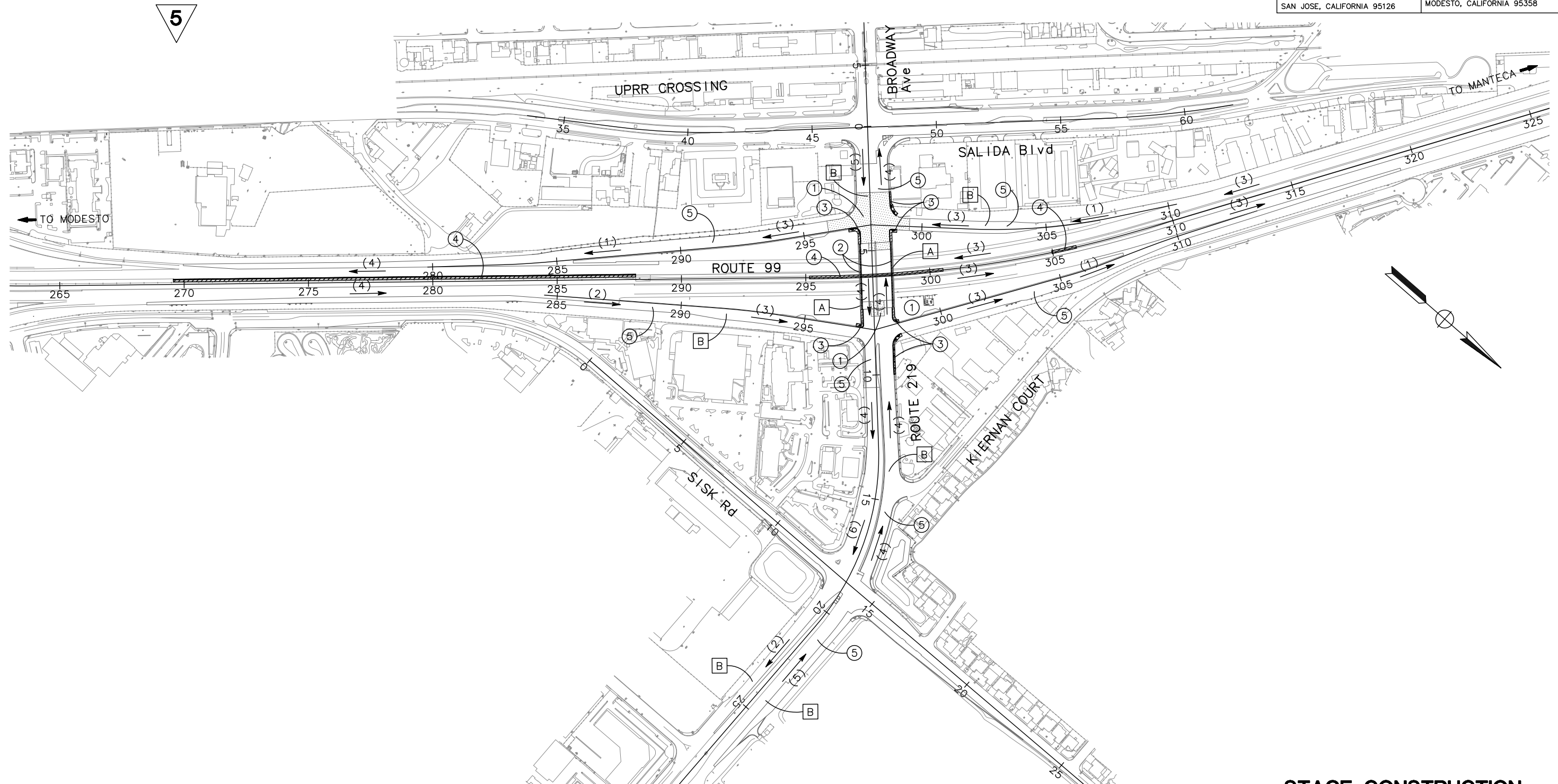
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	121	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
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REVISED PER ADDENDUM No. 5 DATED OCTOBER 18, 2012

APPROVED FOR STAGE CONSTRUCTION WORK ONLY

STAGE CONSTRUCTION (STAGE 4)
 NO SCALE
SC-4

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
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Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-18-12
 DATE

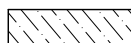
9/04/12
 PLANS APPROVAL DATE

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
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LEGEND

-  FIRST ORDER OF WORK THIS STAGE
-  CONSTRUCT THIS STAGE
-  CONSTRUCT TEMPORARY PAVEMENT
-  CONSTRUCTED AT PREVIOUS STAGES
-  TEMPORARY RAILING (TYPE K)
-  TEMPORARY RAILING (TYPE K) FROM PREVIOUS STAGES
-  TEMPORARY CRASH CUSHION MODULES (ARRAY AS NOTED)
-  TEMPORARY CRASH CUSHION MODULES (ARRAY AS NOTED) FROM PREVIOUS STAGES
-  TEMPORARY CRASH CUSHION ABSORB 350 (TYPE TL2)
-  CHANNELIZER (SURFACE MOUNTED)
-  CHANNELIZER (SURFACE MOUNTED) FROM PREVIOUS STAGES
-  ONE POST SIGN LOCATION
-  ONE POST SIGN LOCATION FROM PREVIOUS STAGES
-  TYPE III BARRICADE
-  TYPE II OR TYPE III BARRICADE WITH SIGN
-  TYPE P MARKER
-  TEMPORARY PAVEMENT DELINEATION (PAINT) DETAIL No.
-  CONSTRUCTION SIGN No.
-  FINAL PAVEMENT DELINEATION (SEE PD PLANS)
-  DIRECTION OF TRAVEL
-  LIMIT OF PAVEMENT DELINEATION
-  TYPE I 24'-0" ARROW
-  TYPE II (B) ARROW
-  TYPE III (R OR L) ARROW
-  TYPE VI (R OR L) ARROW

NOTES

1. TRAFFIC HANDLING PLANS DO NOT SHOW ALL WORK REQUIRED TO BE COMPLETED. WORK NOT NECESSARY FOR TRAFFIC HANDLING MAY BE COMPLETE AT ANY TIME AND IS NOT SHOWN.
2. ADDITIONAL CONSTRUCTION AREA SIGNS ARE SHOWN ON CONSTRUCTION AREA SIGNS PLANS AND DETOUR PLANS.
3. TEMPORARY RAILING (TYPE K) FLARE SHALL BE AT 10:1 OR FLATTER.
4. SURFACE MOUNTED CHANNELIZERS SHALL BE SPACED AT 25' INTERVAL, UNLESS OTHERWISE NOTED.
5. ALL STATIONING OFFSETS TO TEMPORARY RAILINGS (TYPE K) CORRESPOND TO CENTERLINE OF RAILING.
6. CONTRACTOR SHALL REMOVE ANY EXISTING CONFLICTING PAVEMENT DELINEATION.
7. REFER TO PAVEMENT DELINEATION PLANS FOR FINAL ADDITIONAL PAVEMENT DELINEATION AS NOTED  .
8. THE LOCATIONS FOR CONSTRUCTION ACCESSES WILL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.
9. SIGN LOCATIONS SHOWN ARE APPROXIMATE ONLY. EXACT LOCATIONS TO BE DETERMINED BY THE ENGINEER.
10. PROVIDE TEMPORARY FENCE (TYPE CL-6) AT TEMPORARY CONSTRUCTION EASEMENT LINE DURING CONSTRUCTION UNLESS OTHERWISE NOTED. SEE LAYOUT PLANS FOR THE LOCATION OF TEMPORARY CONSTRUCTION EASEMENT.
11. TEMPORARY FENCE SHALL BE REMOVED AFTER THE CONSTRUCTION.
12. CONTRACTOR SHALL REMOVE OR COVER EXISTING SIGNS WHICH CONFLICT WITH TRAFFIC HANDLING PLANS.
13. ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES.
14. ACCESS TO BUSINESSES FOR PEDESTRIAN SHALL PROVIDE AT ALL TIMES.

ABBREVIATION

- APS APPROACH SLAB
- BL BIKE LANE
- CW CROSS WALK
- LL 12' WHITE LIMIT LINE
- TT TRANSITION TAPER

TRAFFIC HANDLING PLAN
NOTES AND LEGEND
 NO SCALE
TH-1

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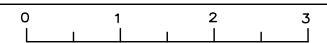
REVISED BY
 DATE REVISED

ANDY DO
 CHUONG NGUYEN

CALCULATED-
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 CHECKED BY

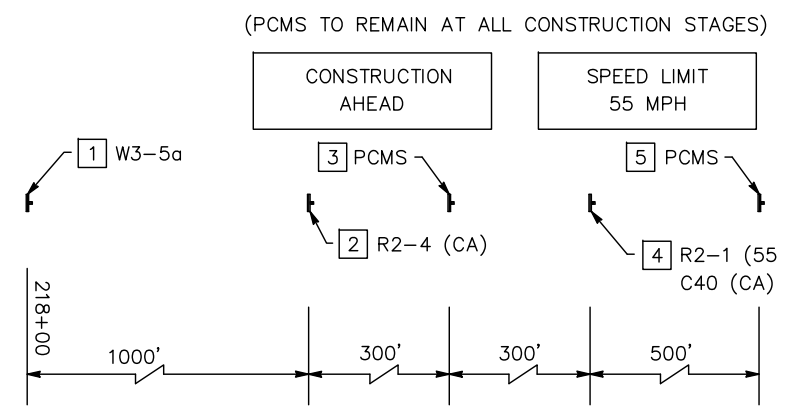
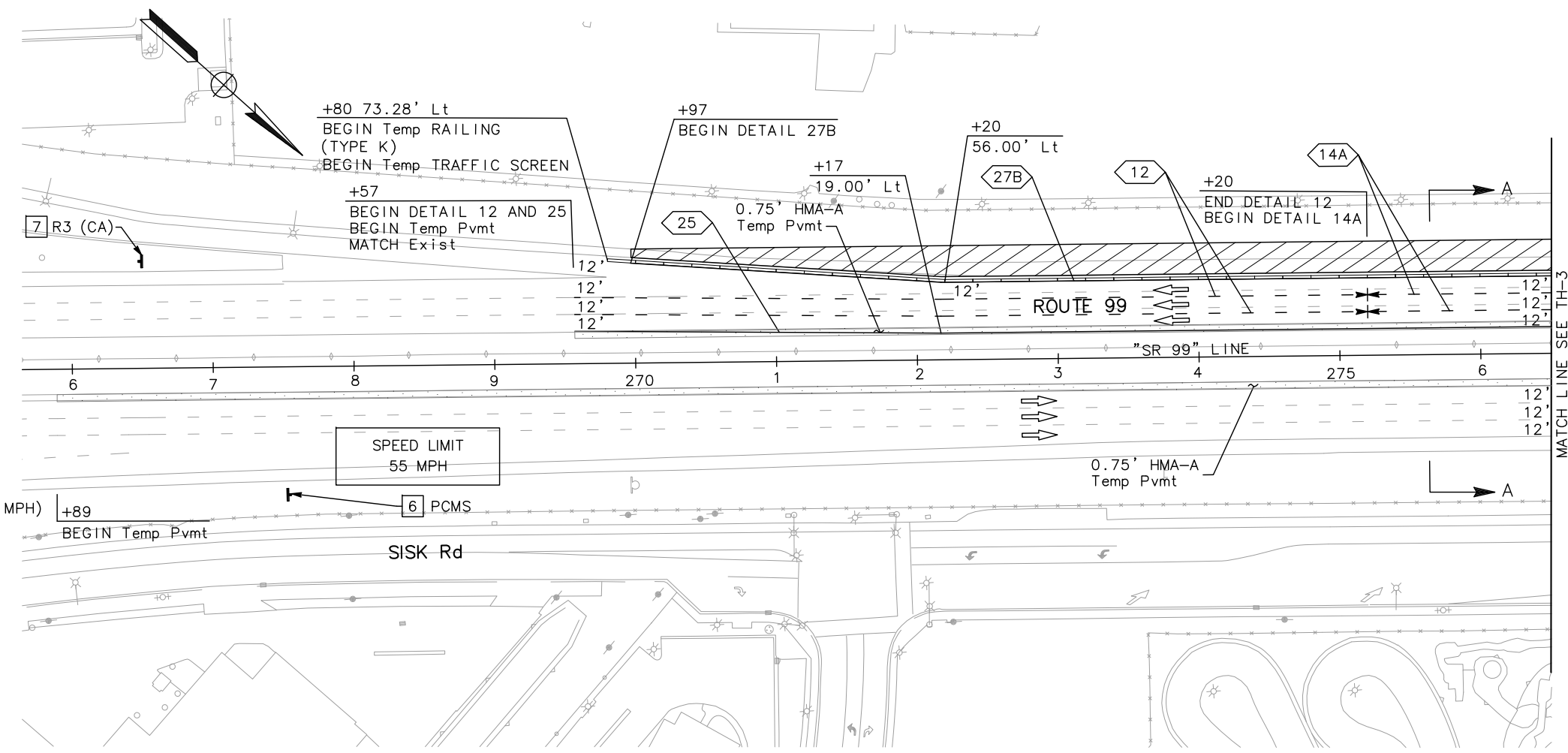
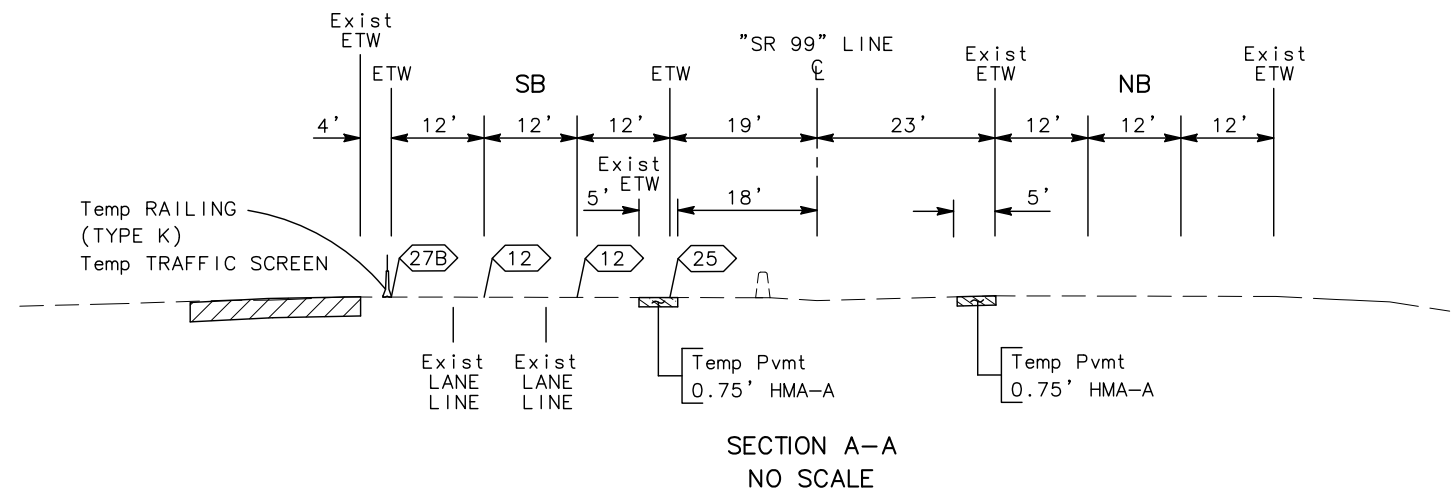
CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

DEPARTMENT OF TRANSPORTATION



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	123	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

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CONSULTANT SUPERVISOR: KEITH G. MEYER

DESIGNED BY: ANDY DO

CHECKED BY: CHUONG NGUYEN

REVISIONS:

NO.	DESCRIPTION	DATE

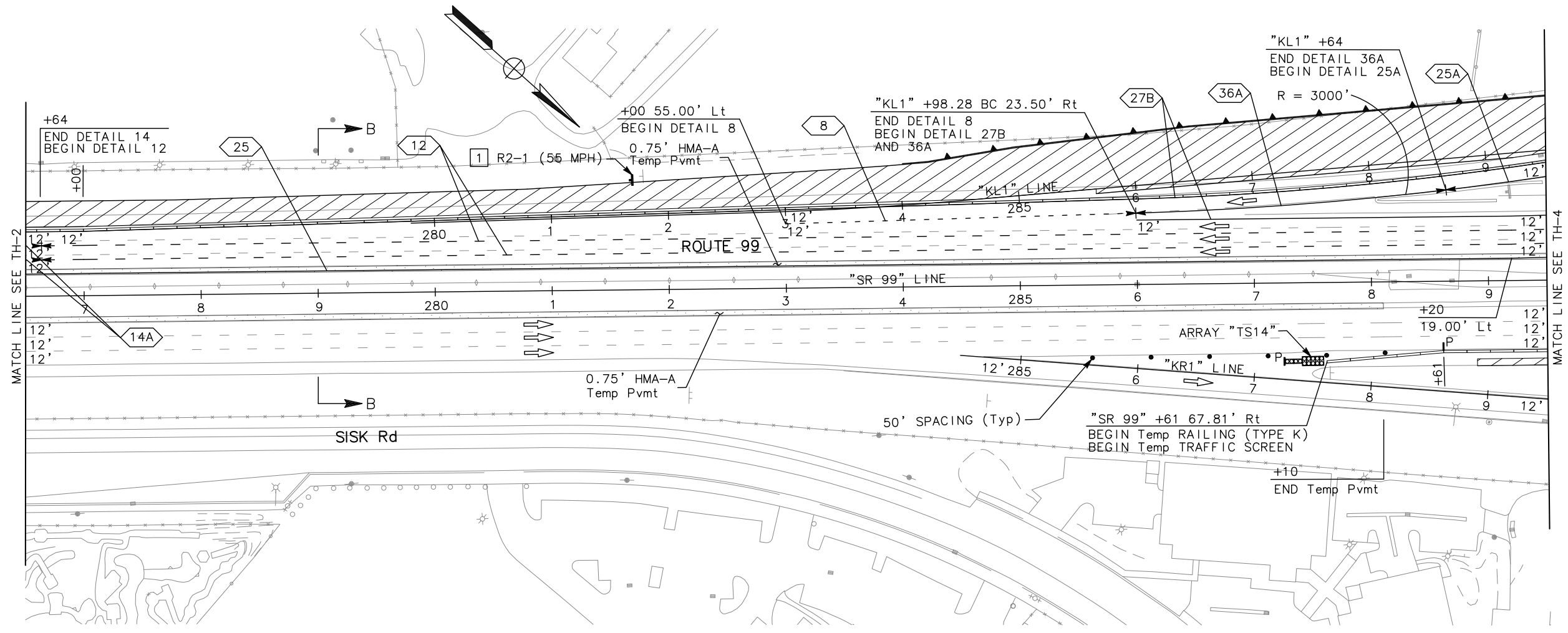
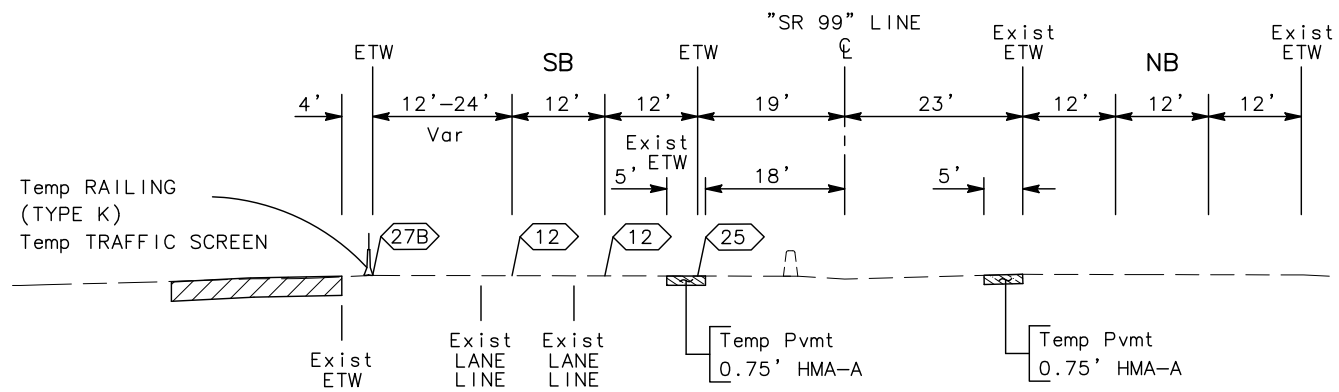
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TRAFFIC HANDLING PLAN
(STAGE 1)
SCALE: 1" = 50'
TH-2

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	124	423

REGISTERED CIVIL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
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CHECKED BY: CHUONG NGUYEN

REVISOR: REVISED BY

DATE: DATE REVISED

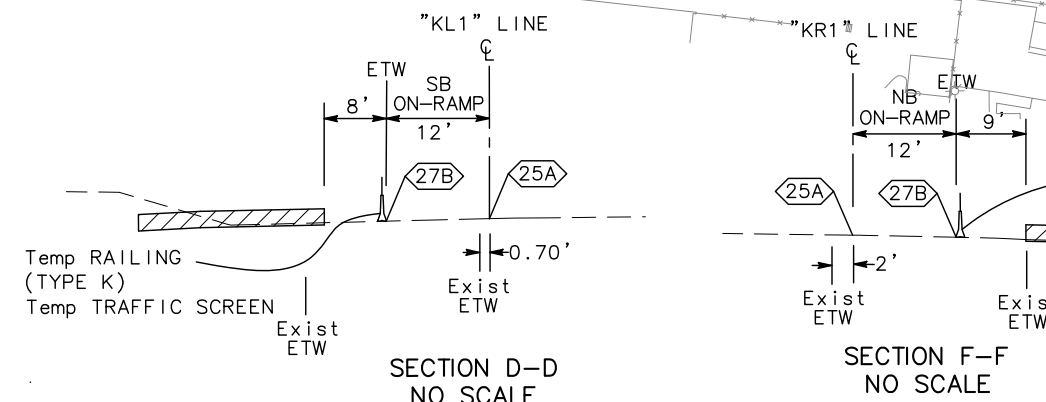
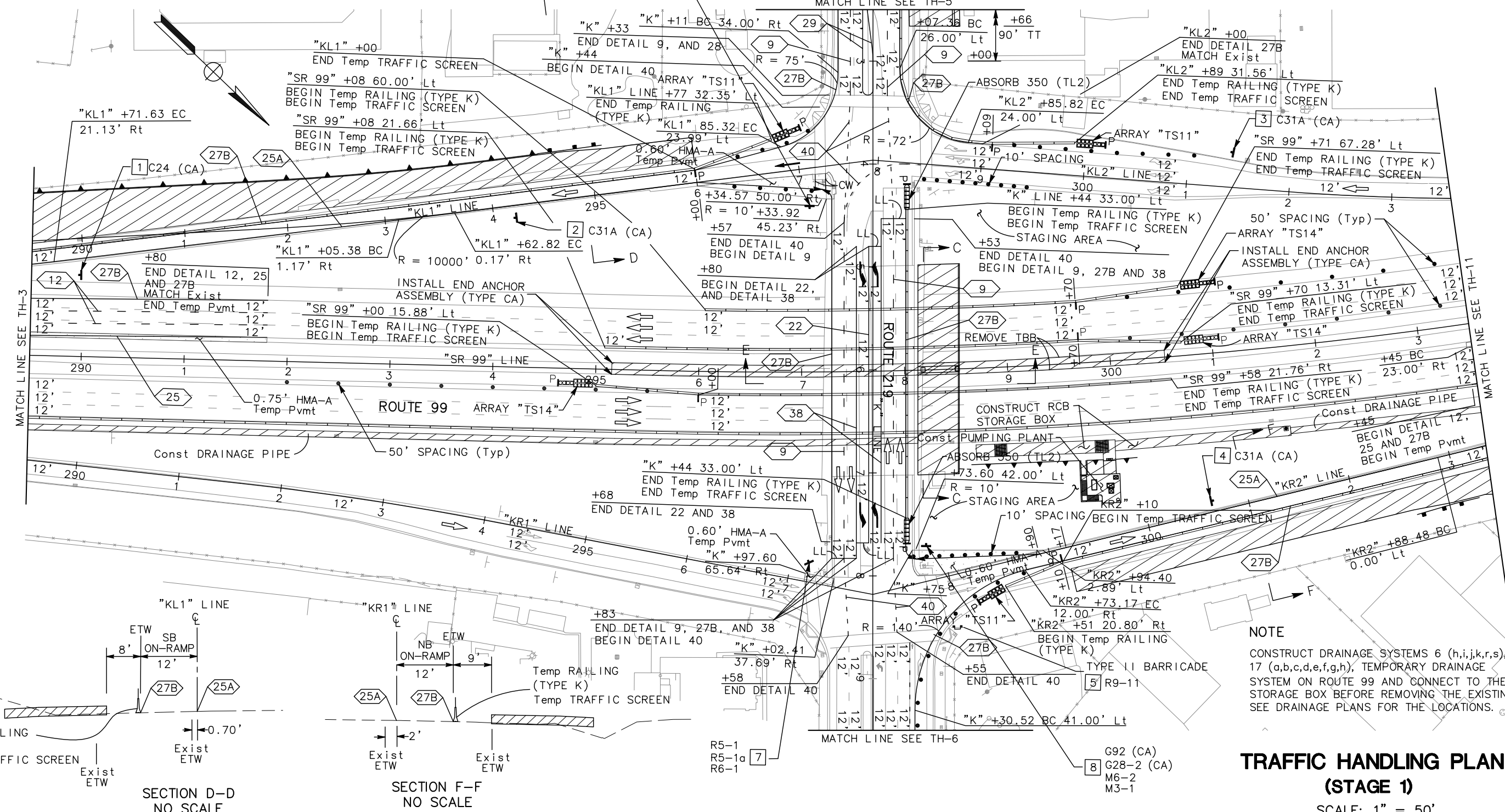
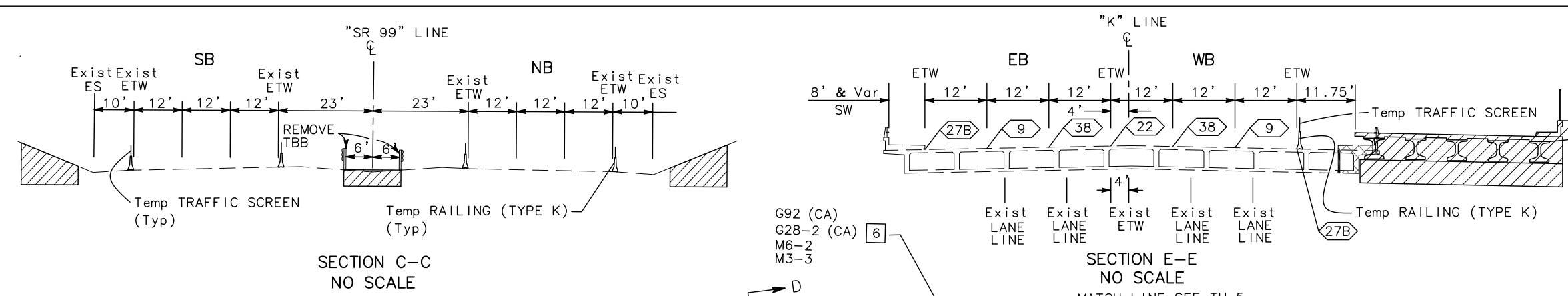
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TRAFFIC HANDLING PLAN
(STAGE 1)
 SCALE: 1" = 50'
TH-3

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	125	423

REGISTERED CIVIL ENGINEER	DATE 05-18-12
9/04/12	
PLANS APPROVAL DATE	
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NOTE
 CONSTRUCT DRAINAGE SYSTEMS 6 (h,i,j,k,r,s), 17 (a,b,c,d,e,f,g,h), TEMPORARY DRAINAGE SYSTEM ON ROUTE 99 AND CONNECT TO THE STORAGE BOX BEFORE REMOVING THE EXISTING. SEE DRAINAGE PLANS FOR THE LOCATIONS.

TRAFFIC HANDLING PLAN (STAGE 1)
 SCALE: 1" = 50'
TH-4

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 ANDY DO
 CHUONG NGUYEN
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 G92 (CA)
 G28-2 (CA)
 M6-2
 M3-3

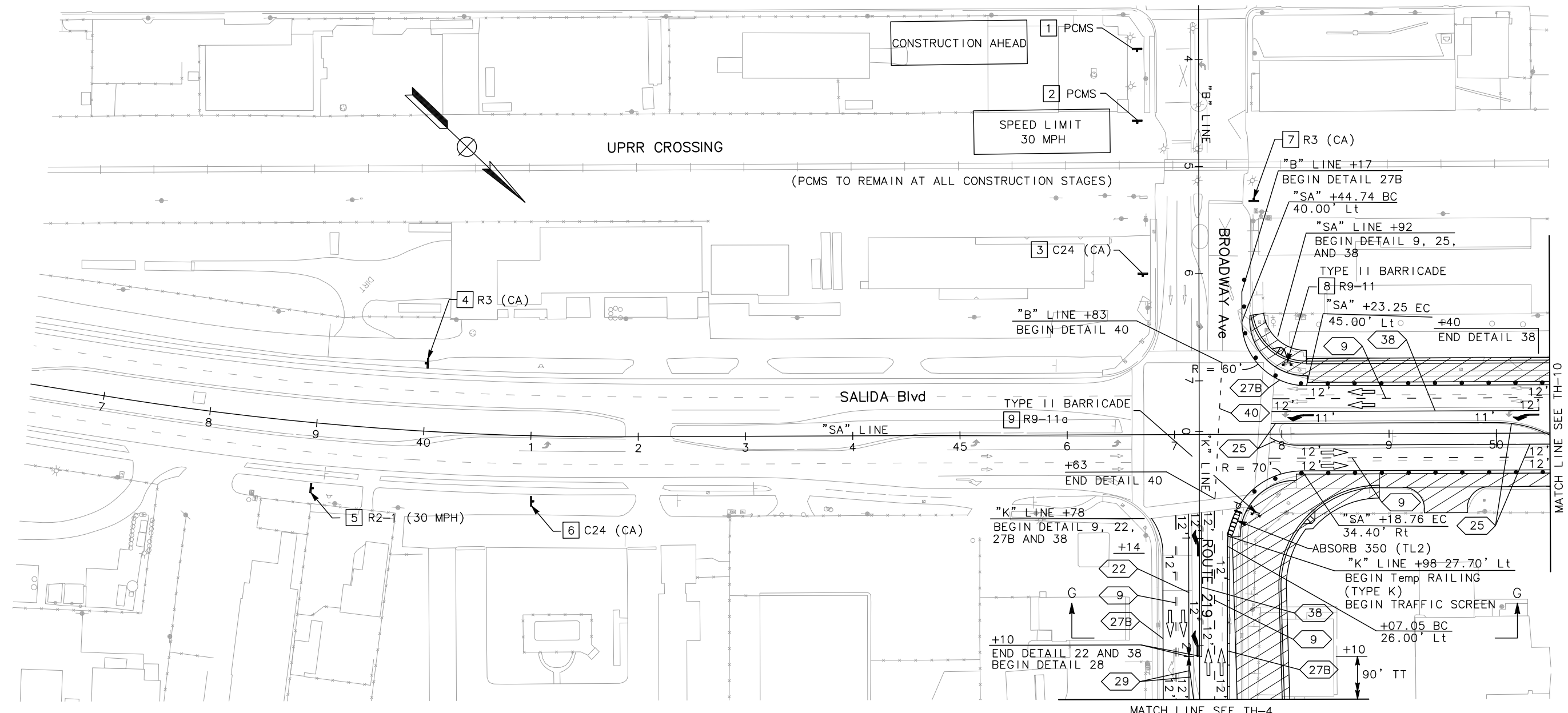
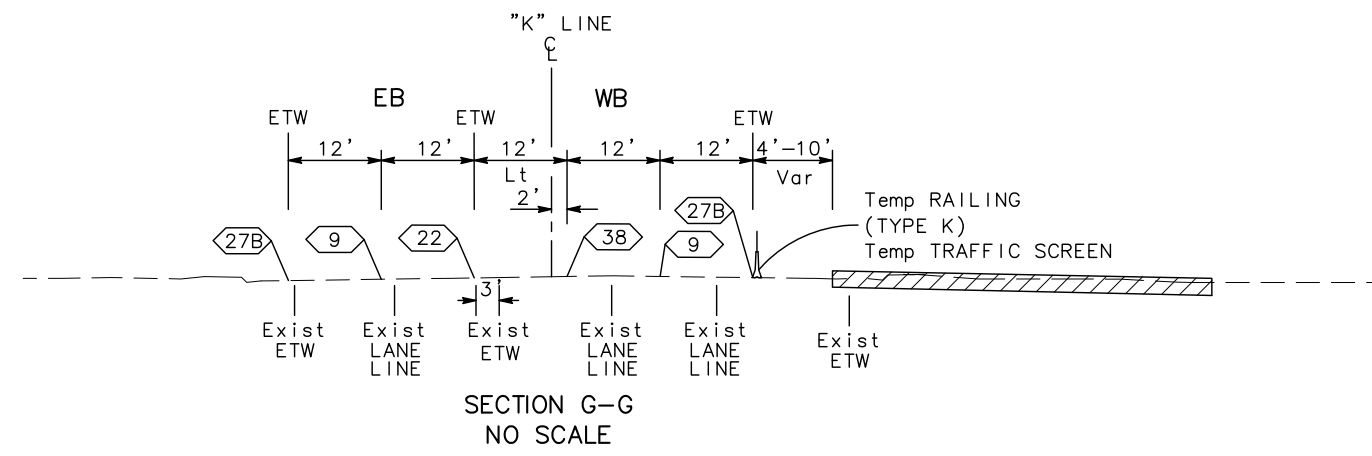
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 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 DESIGNED BY: ANDY DO
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: REVISED BY: DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	126	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



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**TRAFFIC HANDLING PLAN
 (STAGE 1)**
 SCALE: 1" = 50'
TH-5

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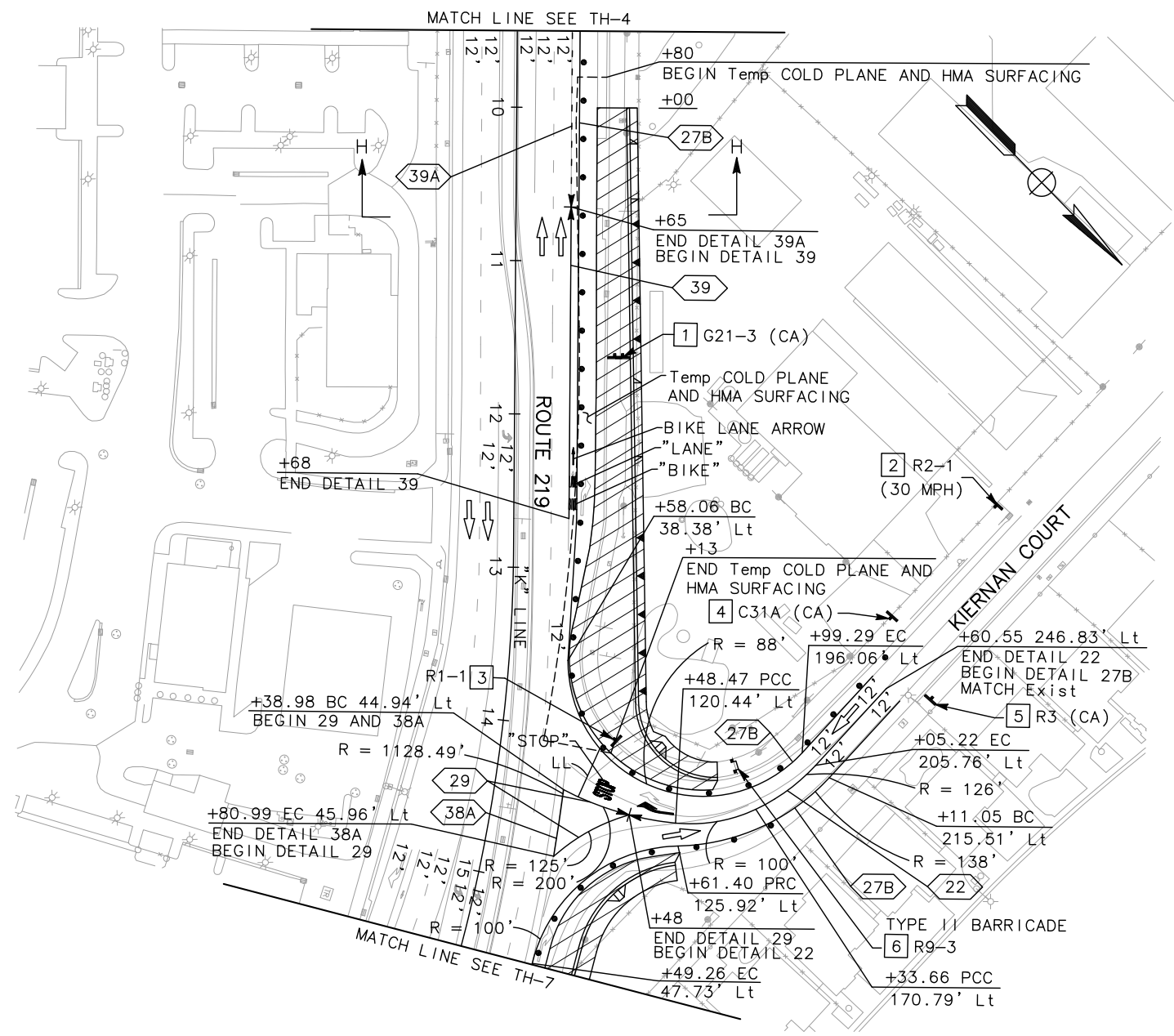
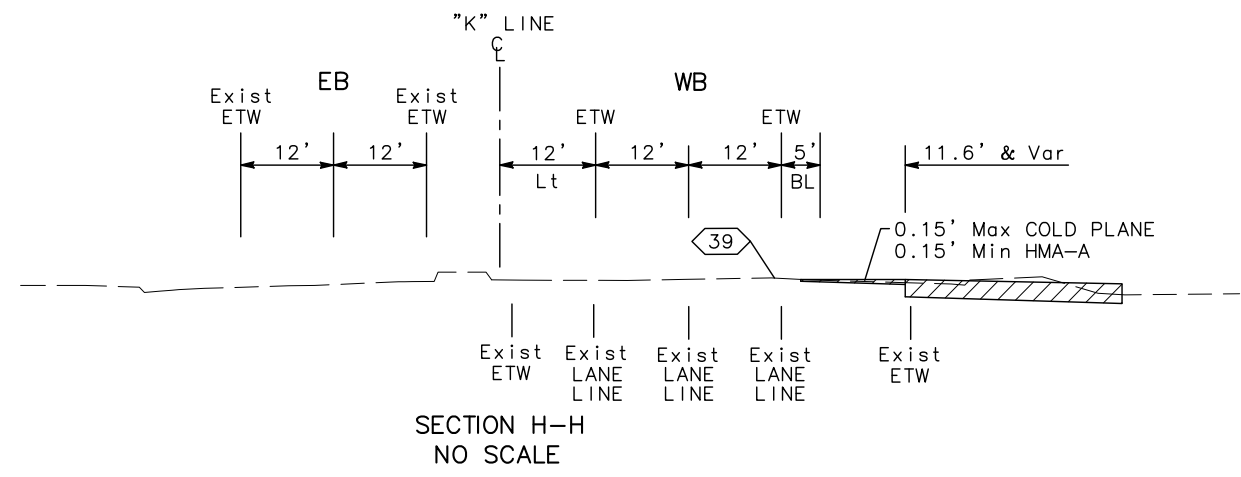
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	127	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
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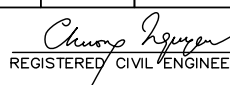
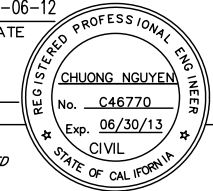
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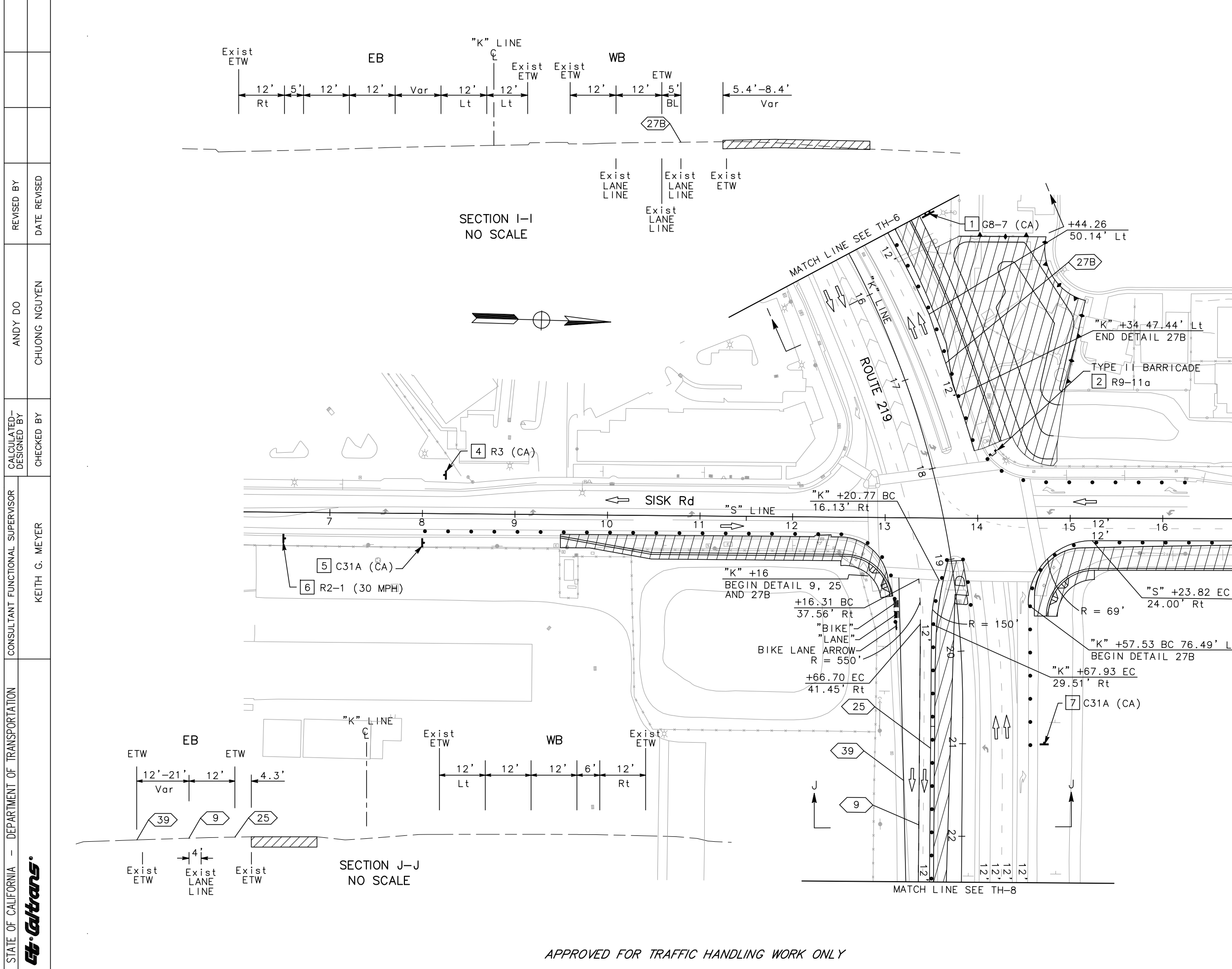
STANISLAUS COUNTY
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TRAFFIC HANDLING PLAN
(STAGE 1)
 SCALE: 1" = 50'
TH-6

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	128	423
 REGISTERED CIVIL ENGINEER			05-06-12 DATE		
9/04/12 PLANS APPROVAL DATE					
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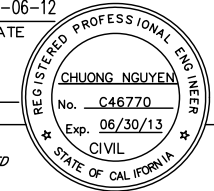


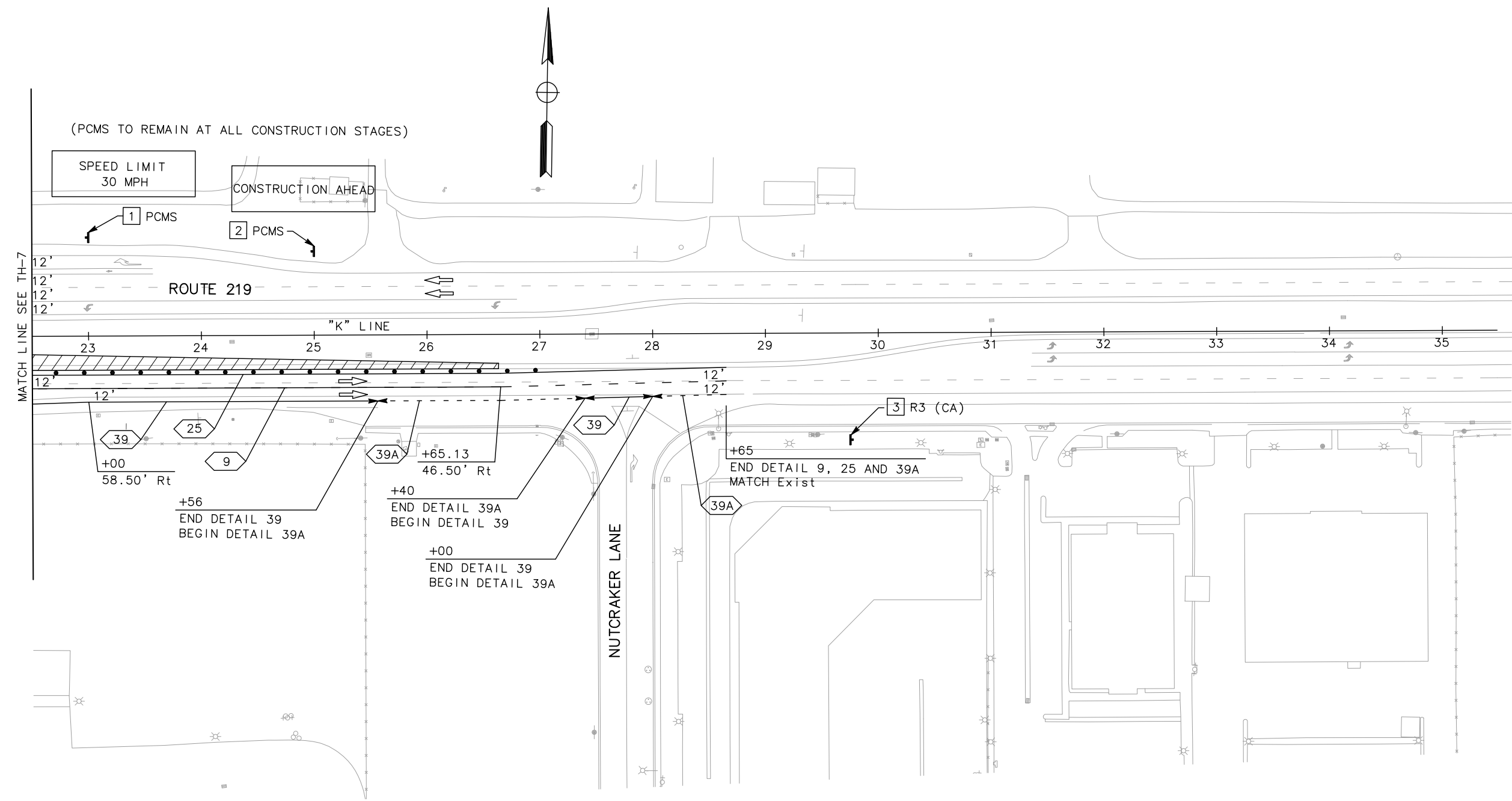
**TRAFFIC HANDLING PLAN
(STAGE 1)**
SCALE: 1" = 50'
TH-7

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 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	129	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



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**TRAFFIC HANDLING PLAN
(STAGE 1)**
 SCALE: 1" = 50'
TH-8

LAST REVISION: 05-01-12
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 TIME PLOTTED => \$TIME

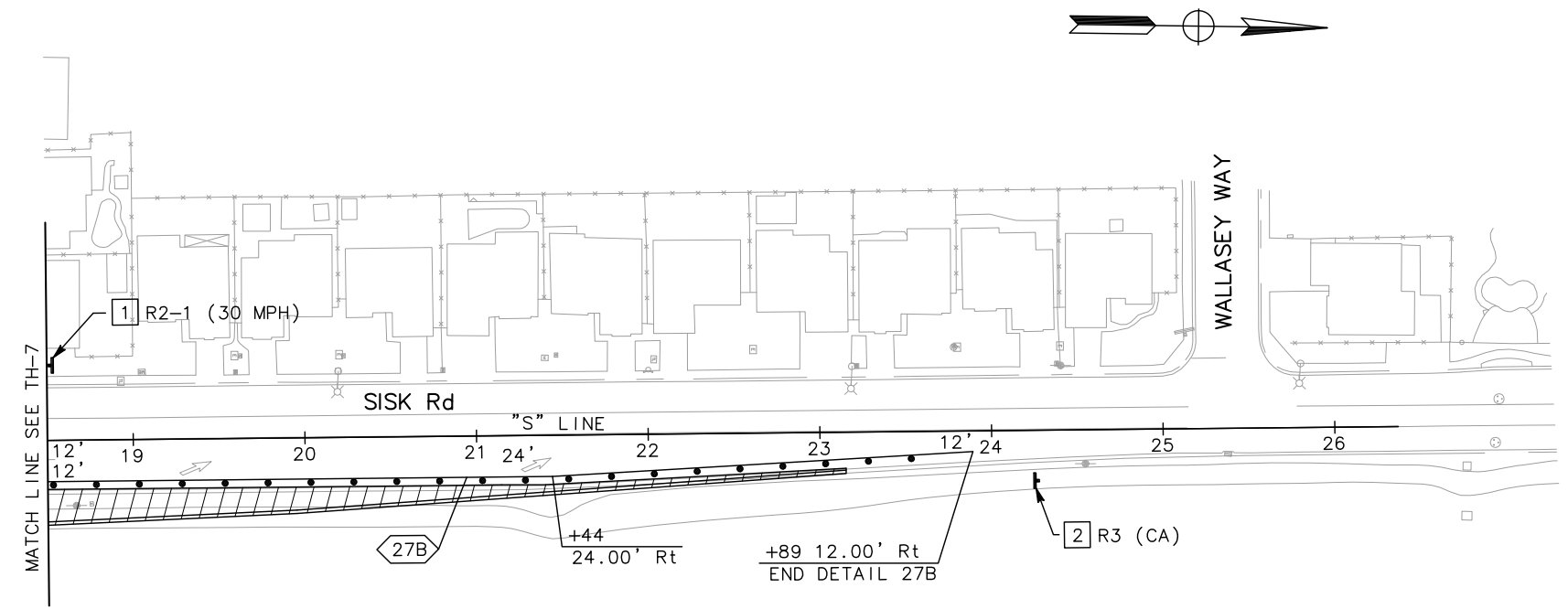
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 REVISOR: CHUONG NGUYEN
 DATE REVISION: 9/04/12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	130	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
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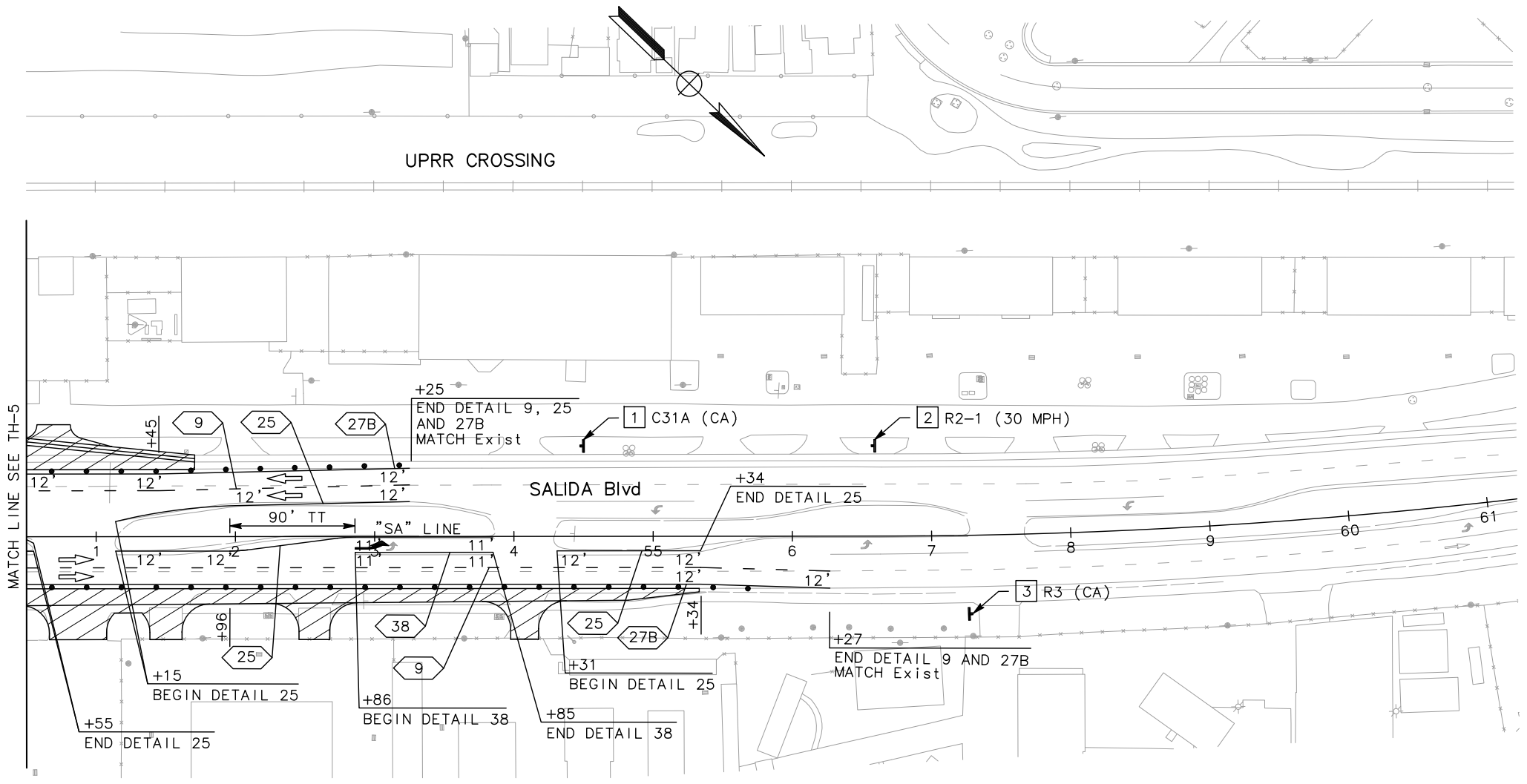
**TRAFFIC HANDLING PLAN
 (STAGE 1)**
 SCALE: 1" = 50'
TH-9

LAST REVISION: 05-01-12 | DATE PLOTTED => \$DATE | TIME PLOTTED => \$TIME

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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	131	423

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TRAFFIC HANDLING PLAN
(STAGE 1)
 SCALE: 1" = 50'
TH-10

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 TIME PLOTTED => \$TIME

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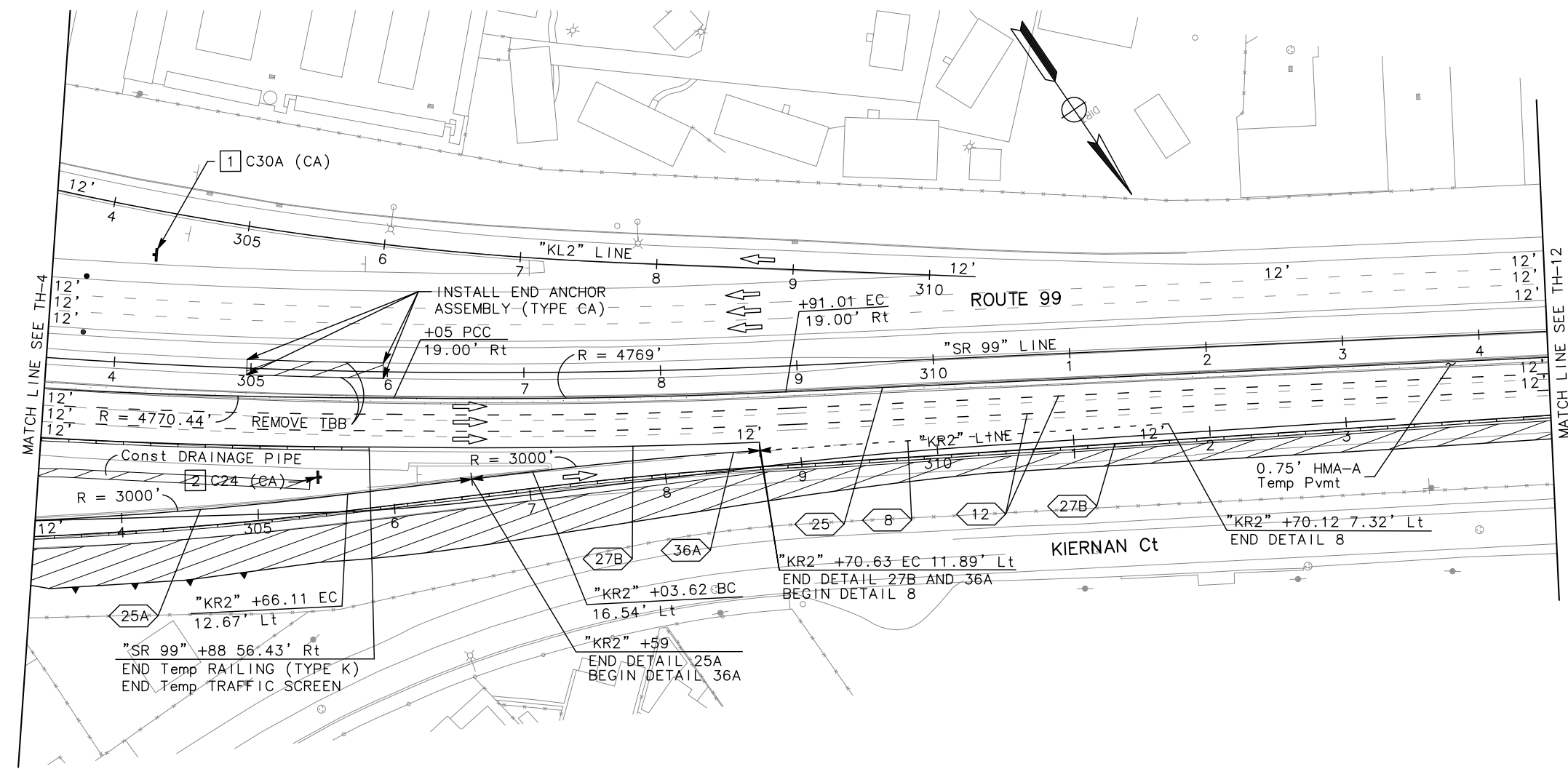
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	132	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



APPROVED FOR TRAFFIC HANDLING WORK ONLY

TRAFFIC HANDLING PLAN
(STAGE 1)
 SCALE: 1" = 50'
TH-11

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 06-08-12

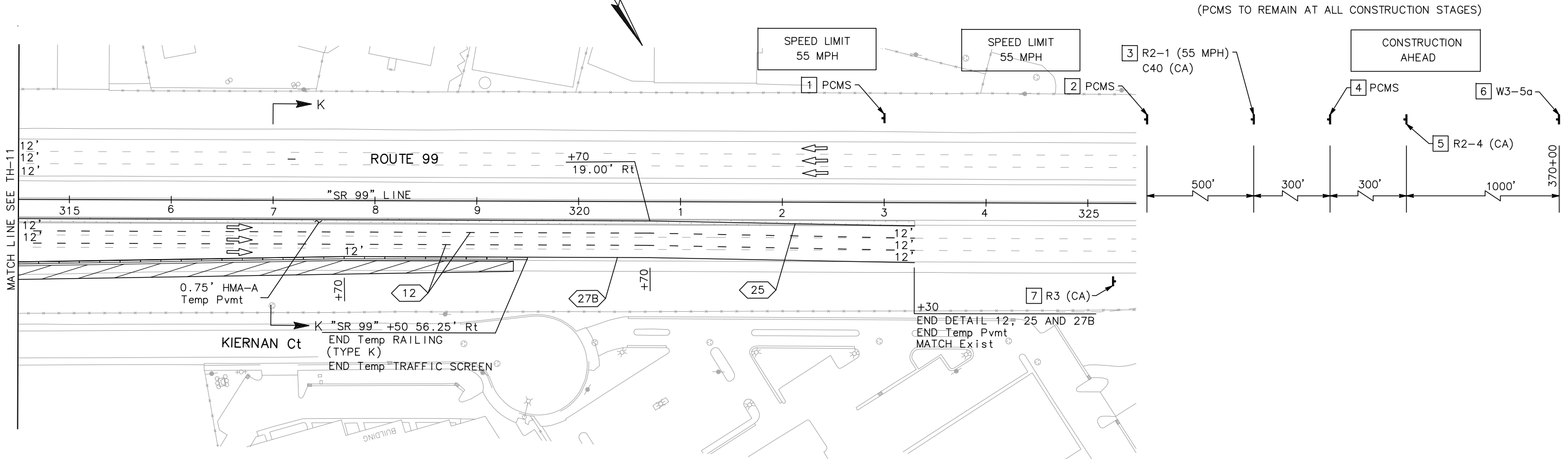
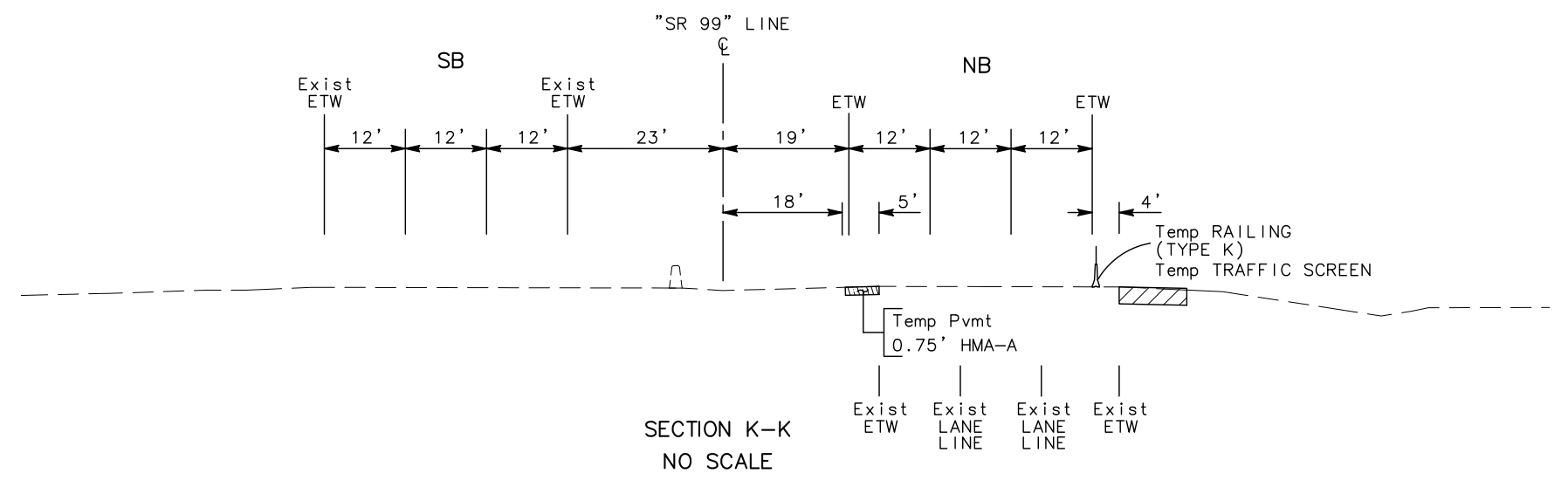
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO: CHUONG NGUYEN
 REVISIONS: REVISED BY, DATE REVISED, CALCULATED-DESIGNED BY, CHECKED BY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	133	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL ENGINEER
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
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 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
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TRAFFIC HANDLING PLAN (STAGE 1)
 SCALE: 1" = 50'
TH-12

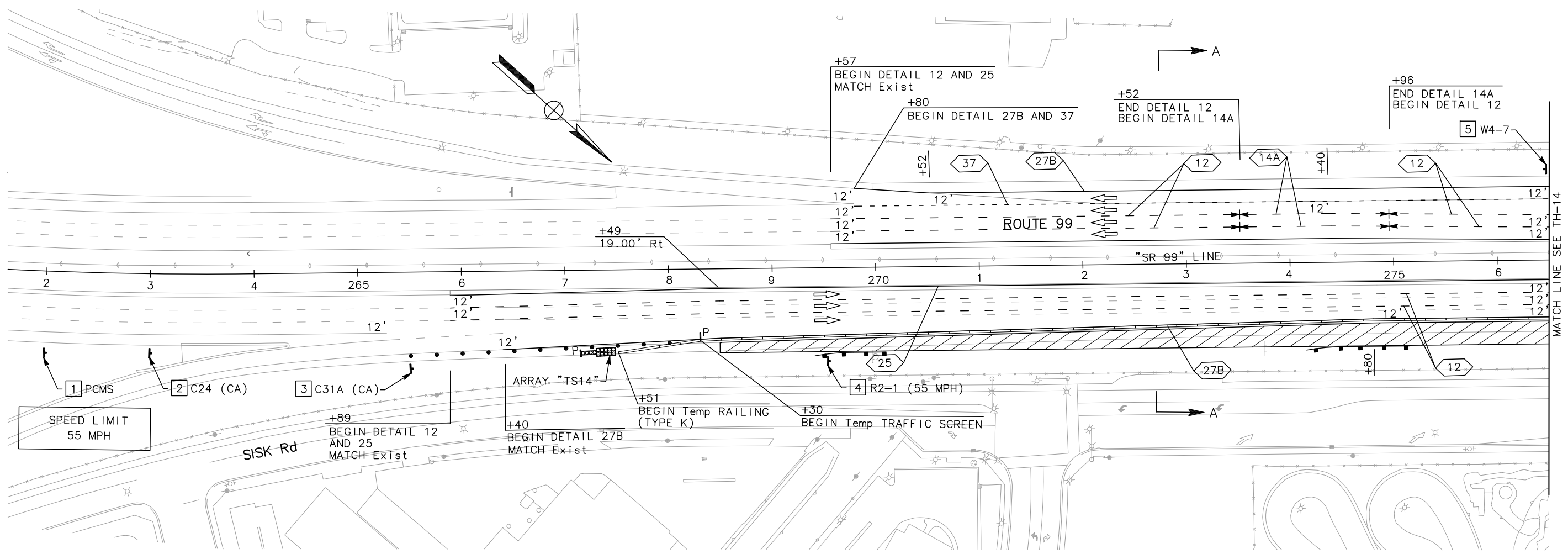
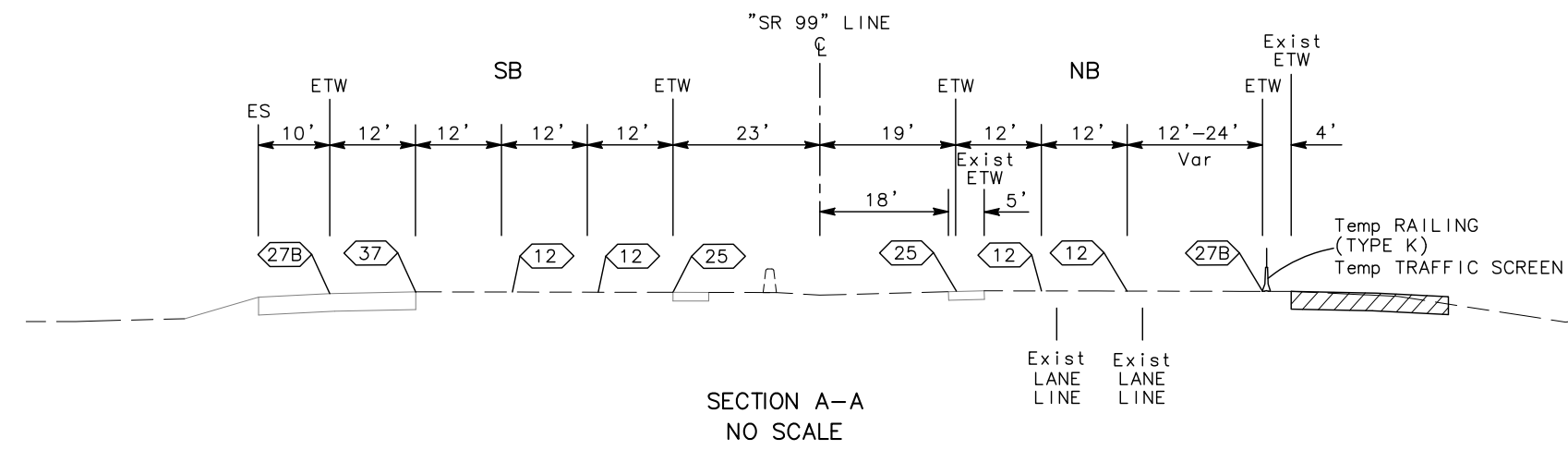
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CHECKED BY: CHUONG NGUYEN
 DESIGNED BY: ANDY DO
 REVISIONS: REVISED BY: DATE REVISIONS: DATE

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	134	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



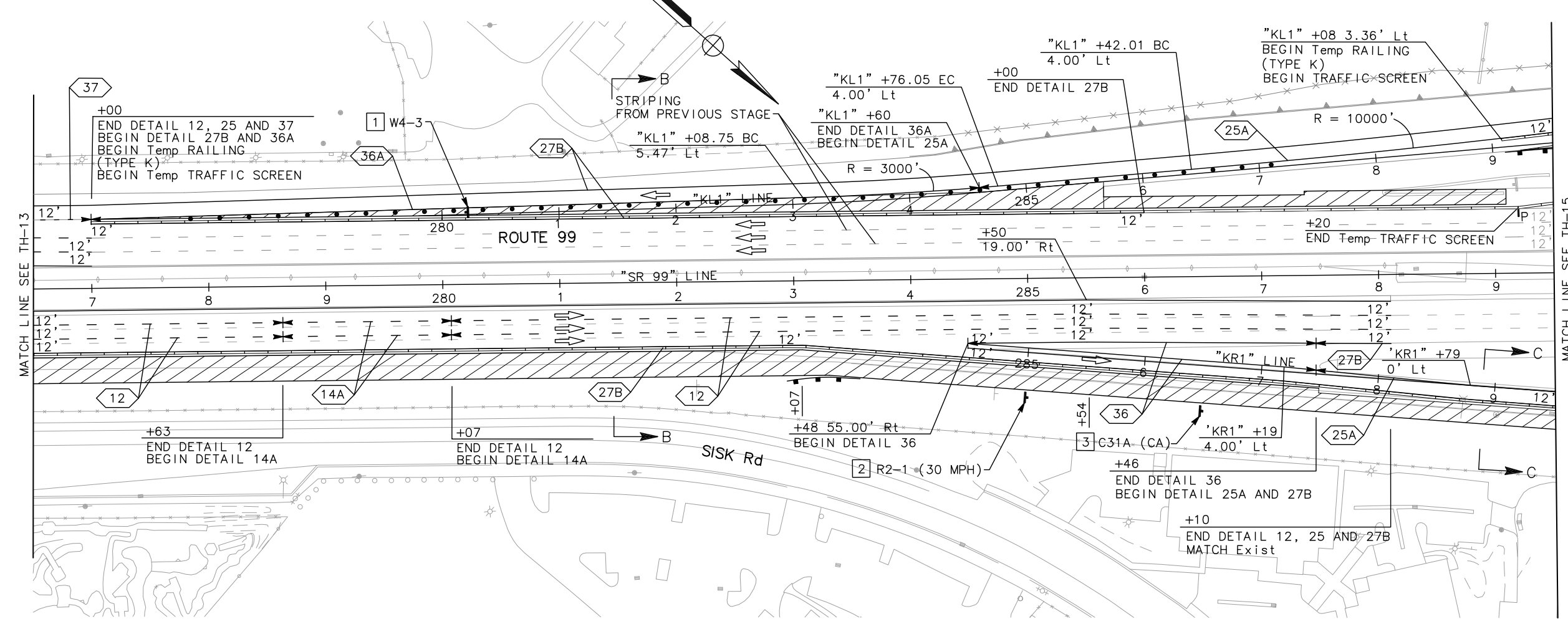
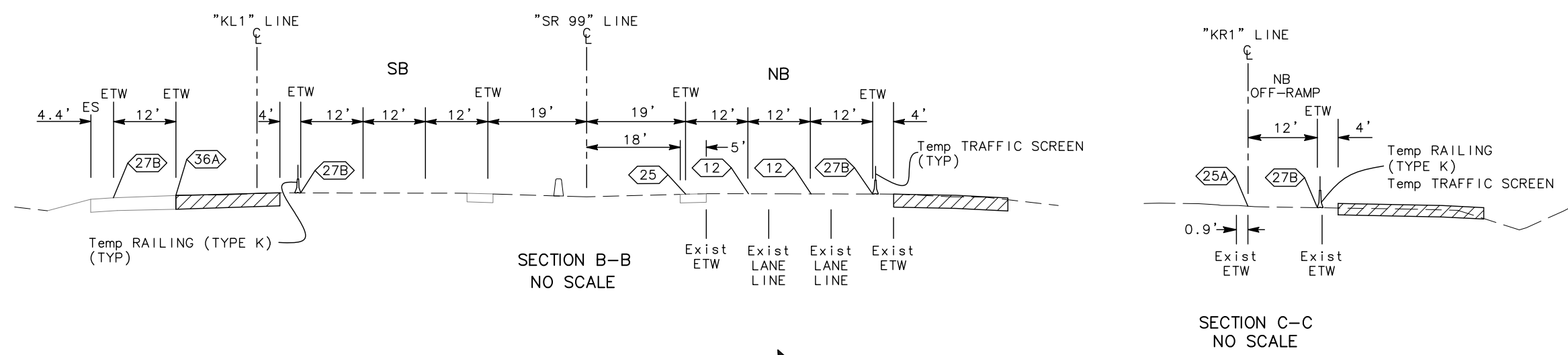
**TRAFFIC HANDLING PLAN
 (STAGE 2)**
 SCALE: 1" = 50'
TH-13

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DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	135	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE
9/04/12 PLANS APPROVAL DATE		
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

Caltrans

CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER

ANDY DO

CHUONG NGUYEN

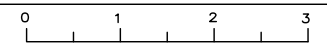
REVISOR: REVISED BY DATE

CALCULATED/DESIGNED BY CHECKED BY

BORDER LAST REVISED 7/2/2010

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UNIT 1457 PROJECT NUMBER & PHASE 100000100

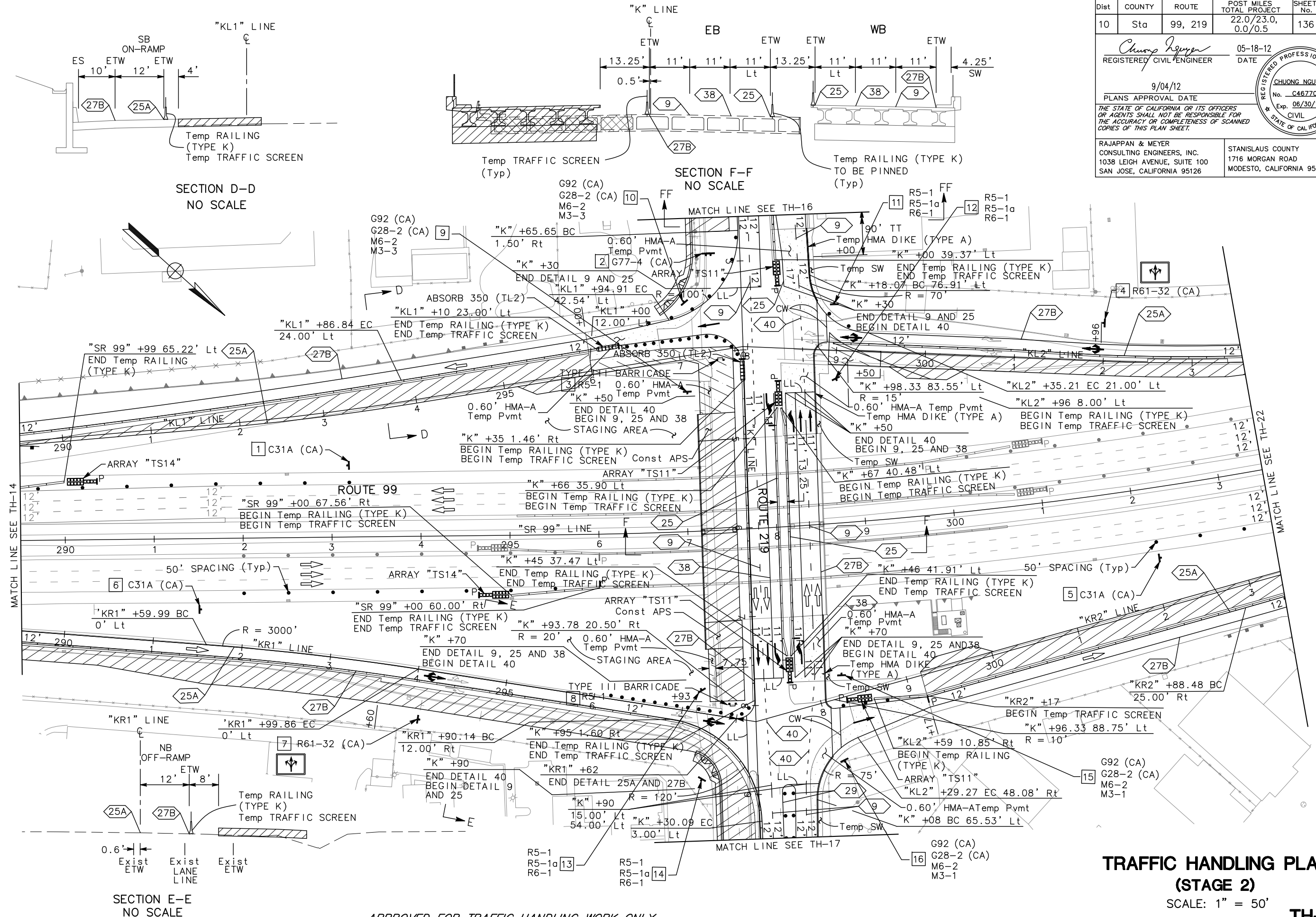
TRAFFIC HANDLING PLAN
(STAGE 2)
SCALE: 1" = 50'
TH-14

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
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 CHUONG NGUYEN
 REVISOR: REVISED BY
 DATE: DATE REVISOR
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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	136	423

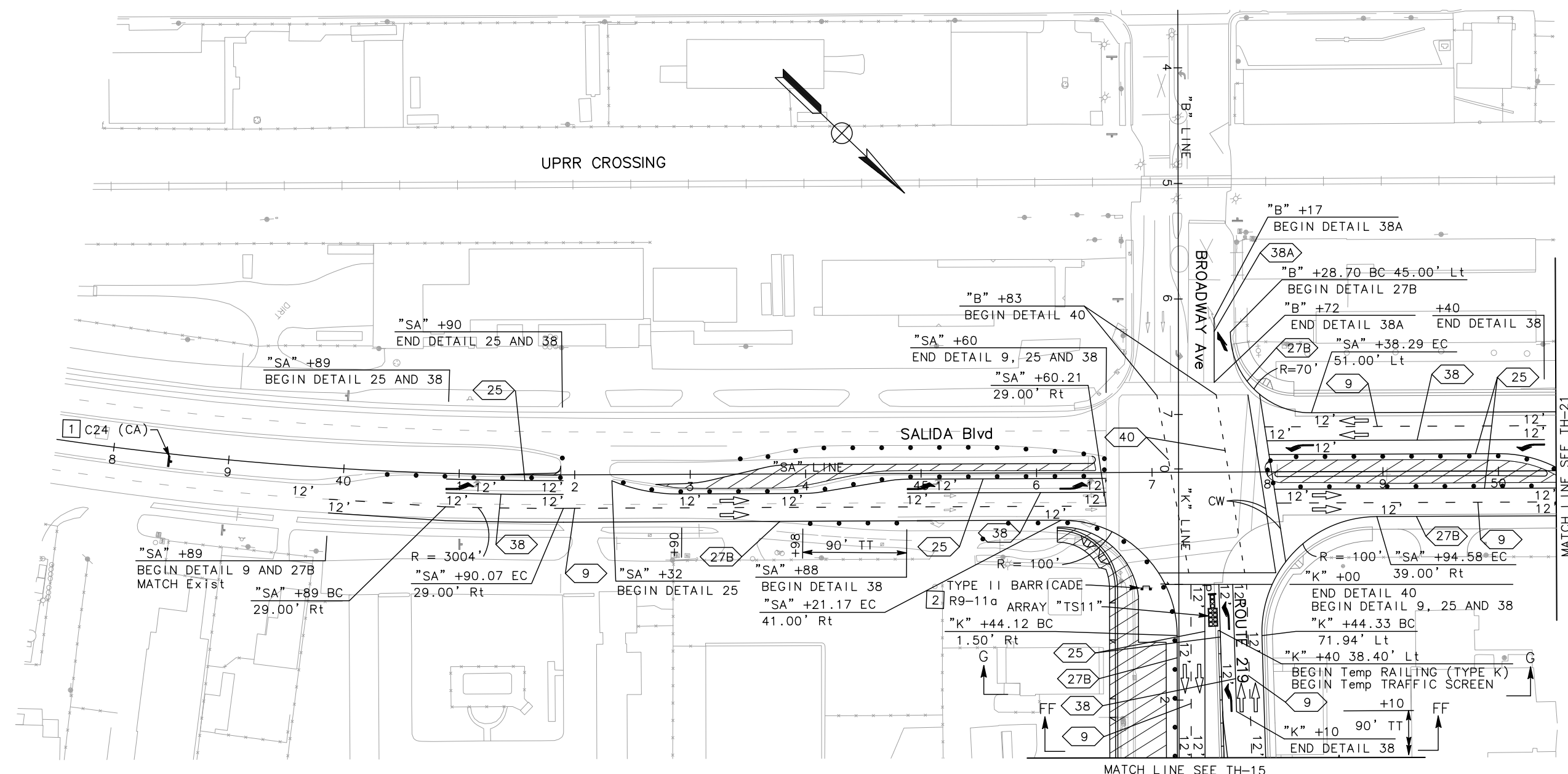
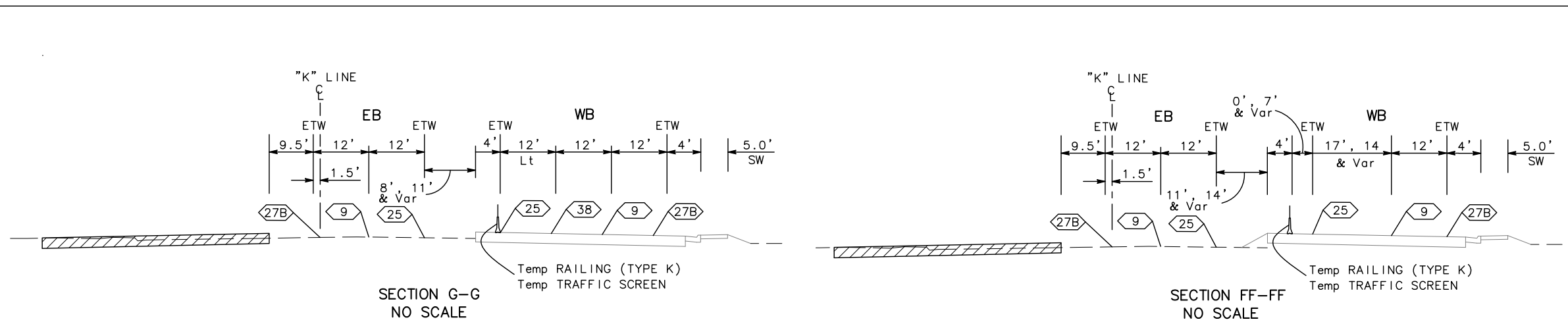
REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
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 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



TRAFFIC HANDLING PLAN
 (STAGE 2)
 SCALE: 1" = 50'
TH-15

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
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 CHECKED BY: CHUONG NGUYEN
 REVISED BY: ANDY DO
 DATE REVISED: CHUONG NGUYEN



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	137	423

9/04/12
 REGISTERED CIVIL ENGINEER DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

**TRAFFIC HANDLING PLAN
 (STAGE 2)**
 SCALE: 1" = 50'
TH-16

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DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

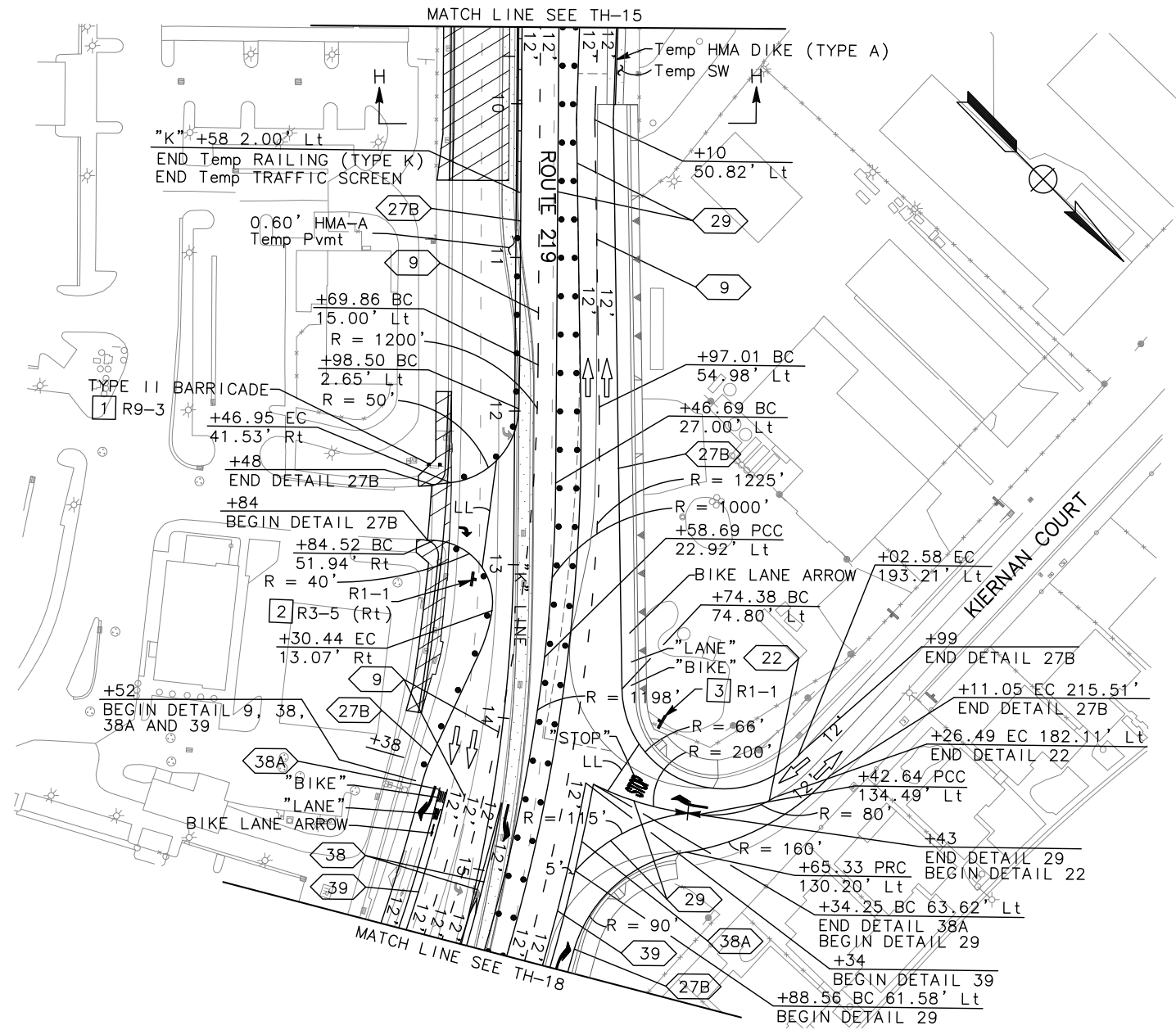
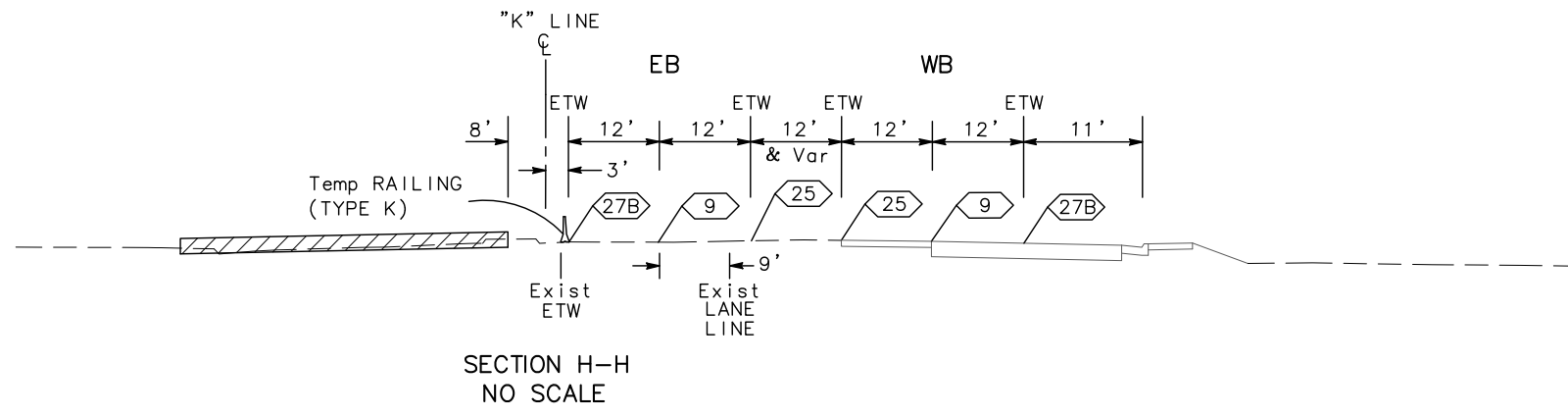
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Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHECKED BY:
 ANDY DO: CHUONG NGUYEN
 REVISED BY: DATE REVISED:

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	138	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA


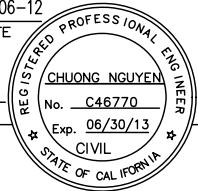
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

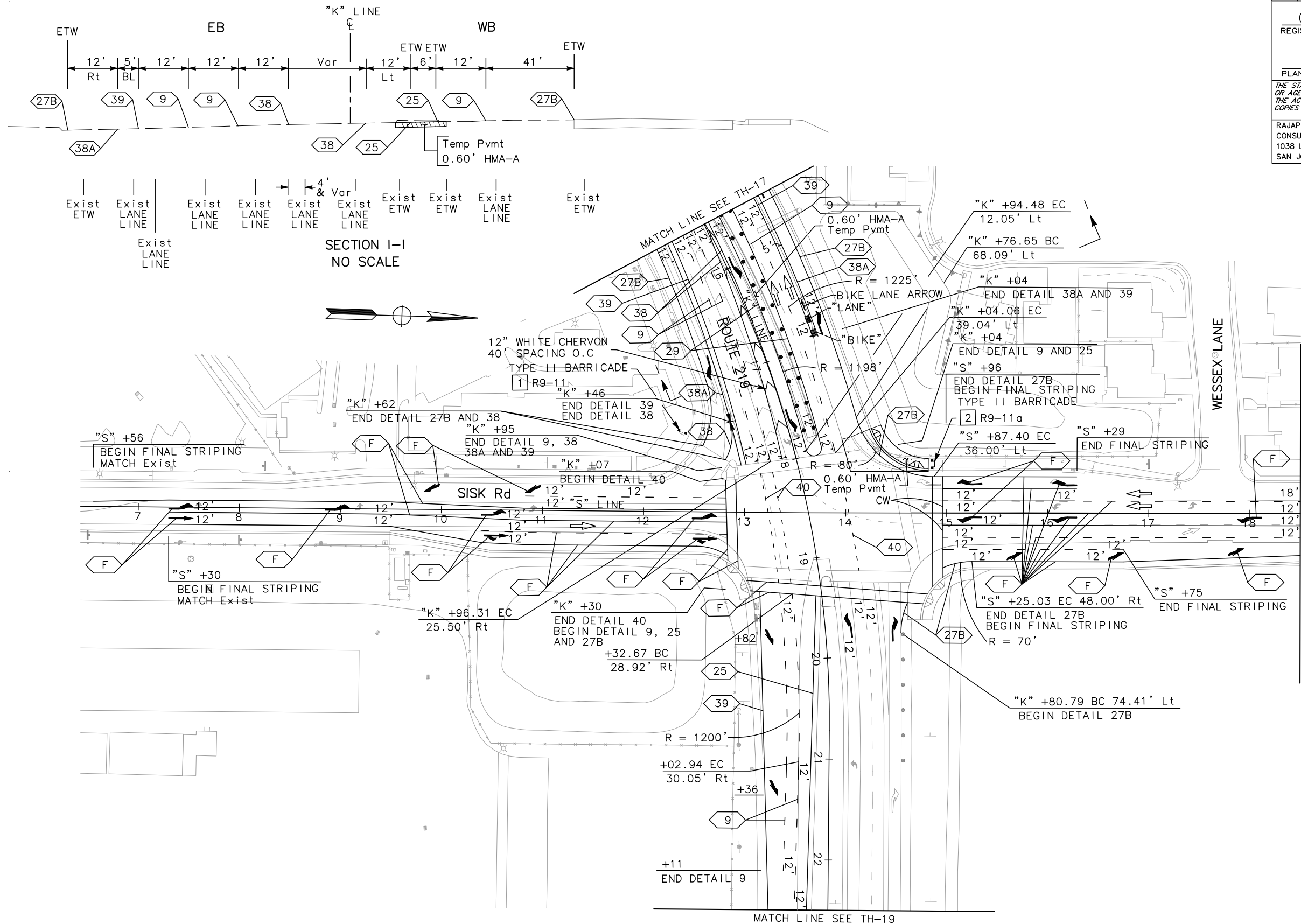
STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



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TRAFFIC HANDLING PLAN
(STAGE 2)
 SCALE: 1" = 50'
TH-17

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	139	423
 REGISTERED CIVIL ENGINEER		05-06-12 DATE			
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



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**TRAFFIC HANDLING PLAN
(STAGE 2)**
 SCALE: 1" = 50'
TH-18

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

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 CALCULATED/DESIGNED BY: CHUONG NGUYEN
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED: 9/04/12

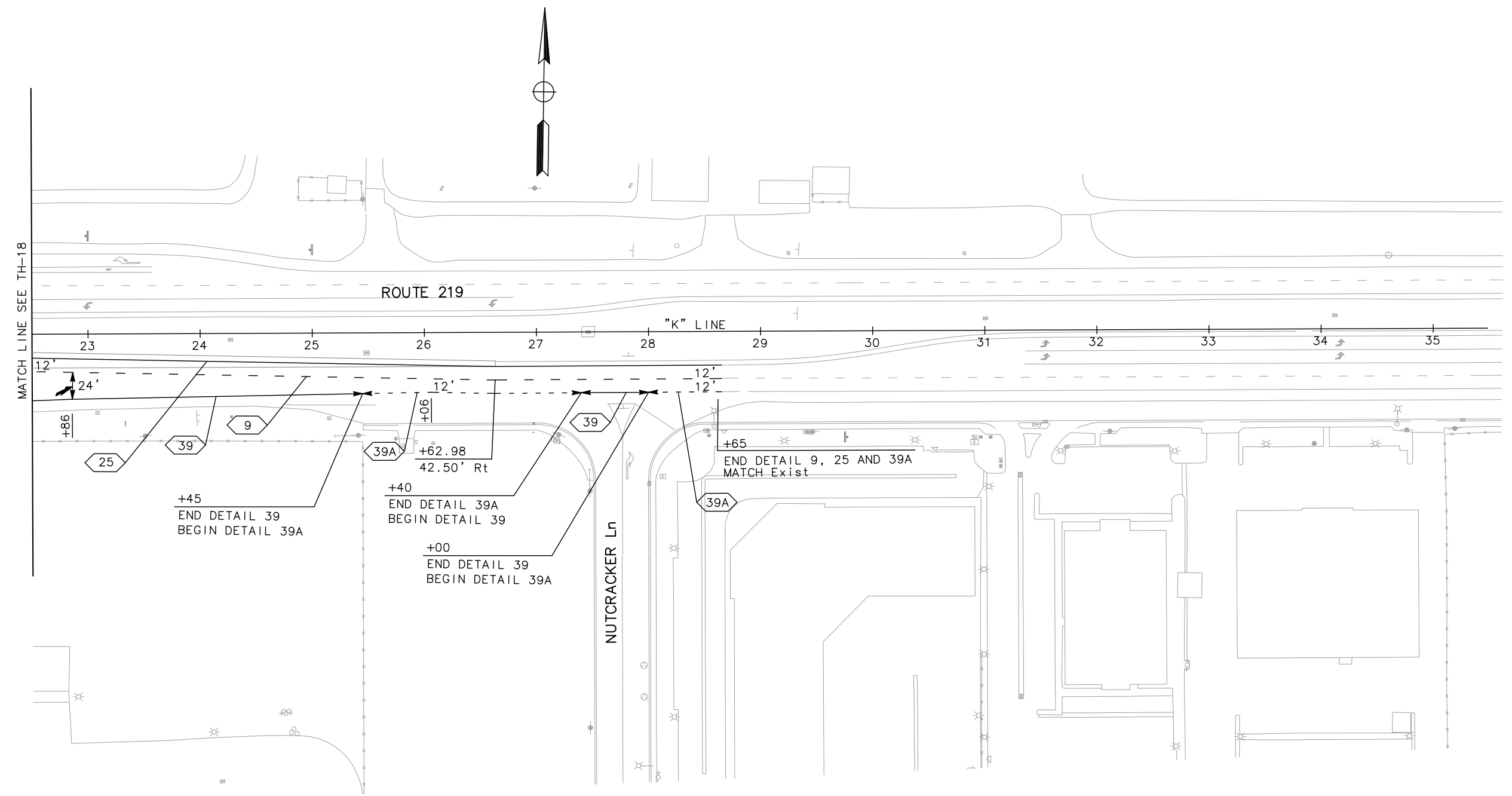
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 REVISOR: ANDY DO
 DESIGNED BY: CHUONG NGUYEN
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	140	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
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**TRAFFIC HANDLING PLAN
 (STAGE 2)**
 SCALE: 1" = 50'
TH-19

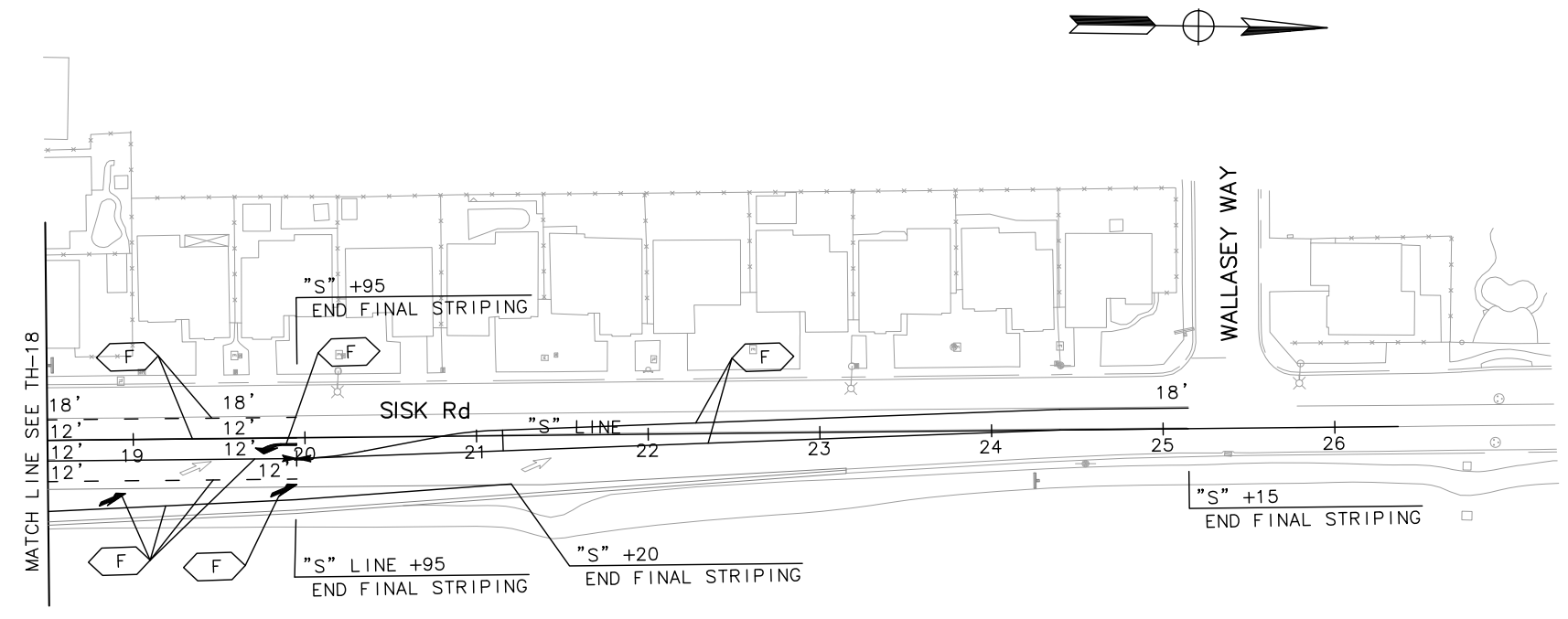
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO
 CHUONG NGUYEN
 REVISOR: CHUONG NGUYEN
 DATE REVISION: 9/04/12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	141	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
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 SAN JOSE, CALIFORNIA 95126
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 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



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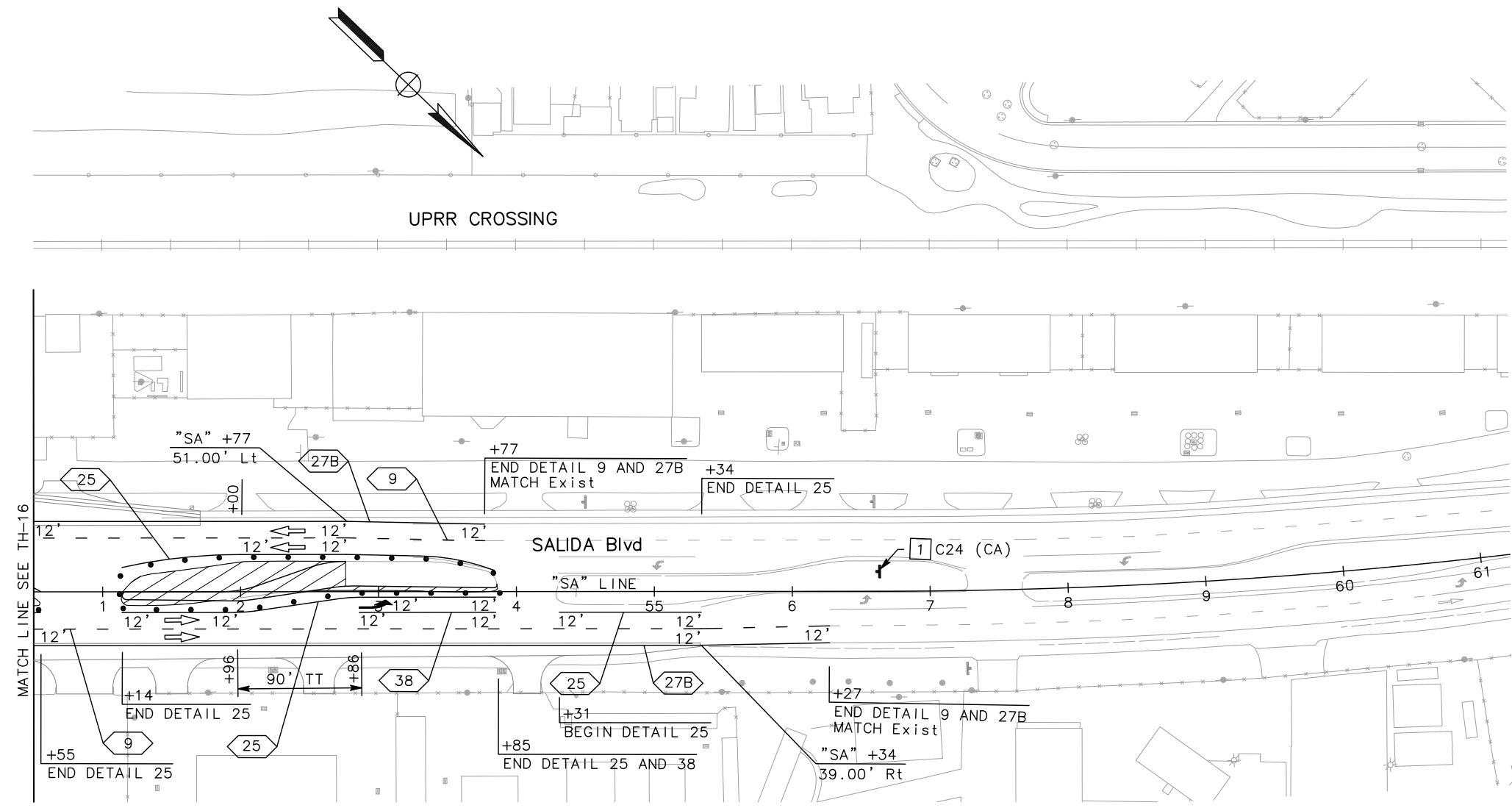
TRAFFIC HANDLING PLAN
(STAGE 2)
 SCALE: 1" = 50'
TH-20

LAST REVISION: 05-01-12 | DATE PLOTTED => \$DATE | TIME PLOTTED => \$TIME

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 ANDY DO
 CHUONG NGUYEN
 REVISOR: REVISED BY: DATE
 DESIGNED BY: CHECKED BY:

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	142	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



TRAFFIC HANDLING PLAN
(STAGE 2)
 SCALE: 1" = 50'
TH-21

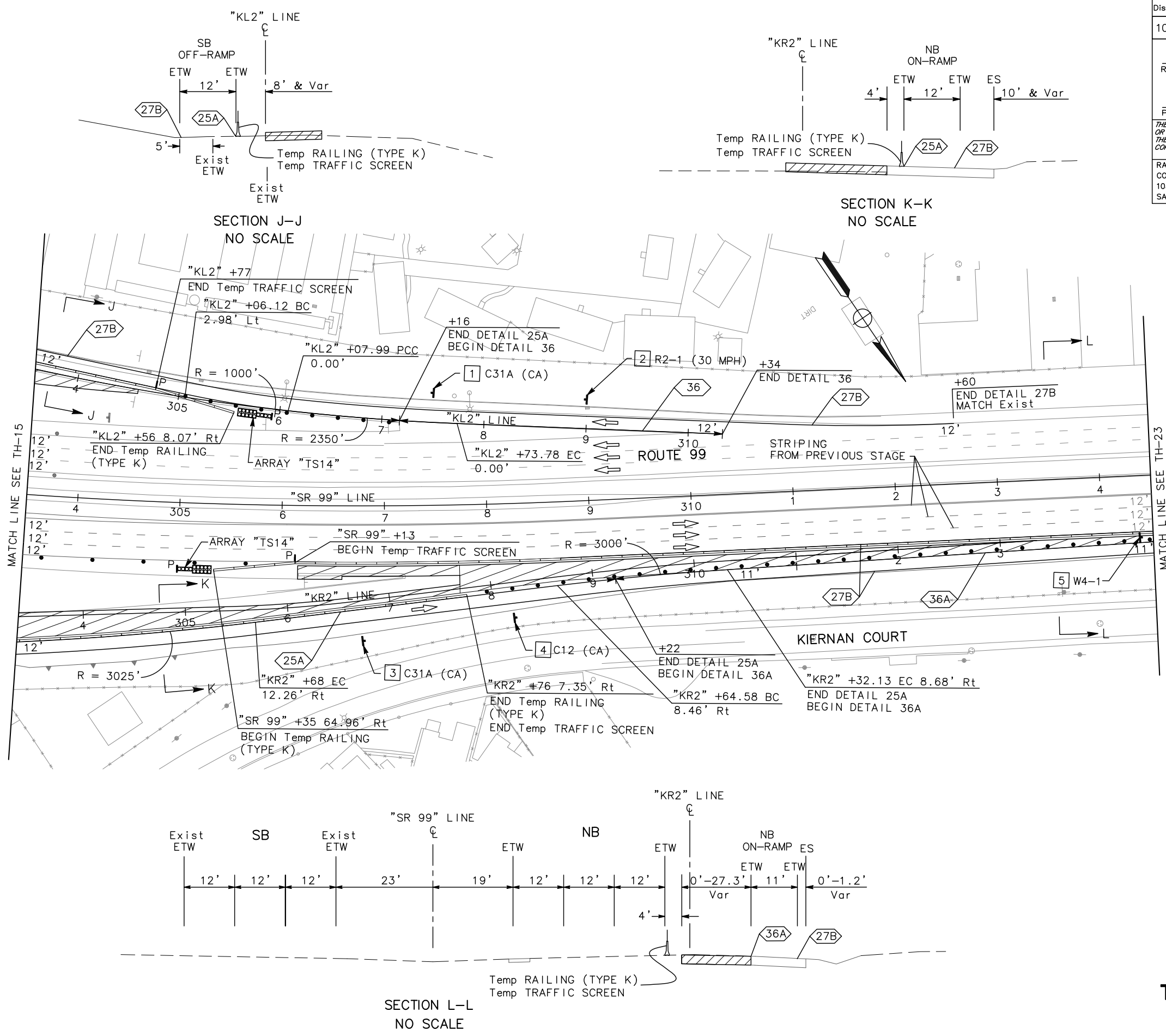
APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION: 05-01-12 | DATE PLOTTED => \$DATE | TIME PLOTTED => \$TIME

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Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 ANDY DO
 REVISOR: CHUONG NGUYEN
 DATE REVISOR: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	143	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen* DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



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TRAFFIC HANDLING PLAN
(STAGE 2)
 SCALE: 1" = 50'
TH-22

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
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Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE

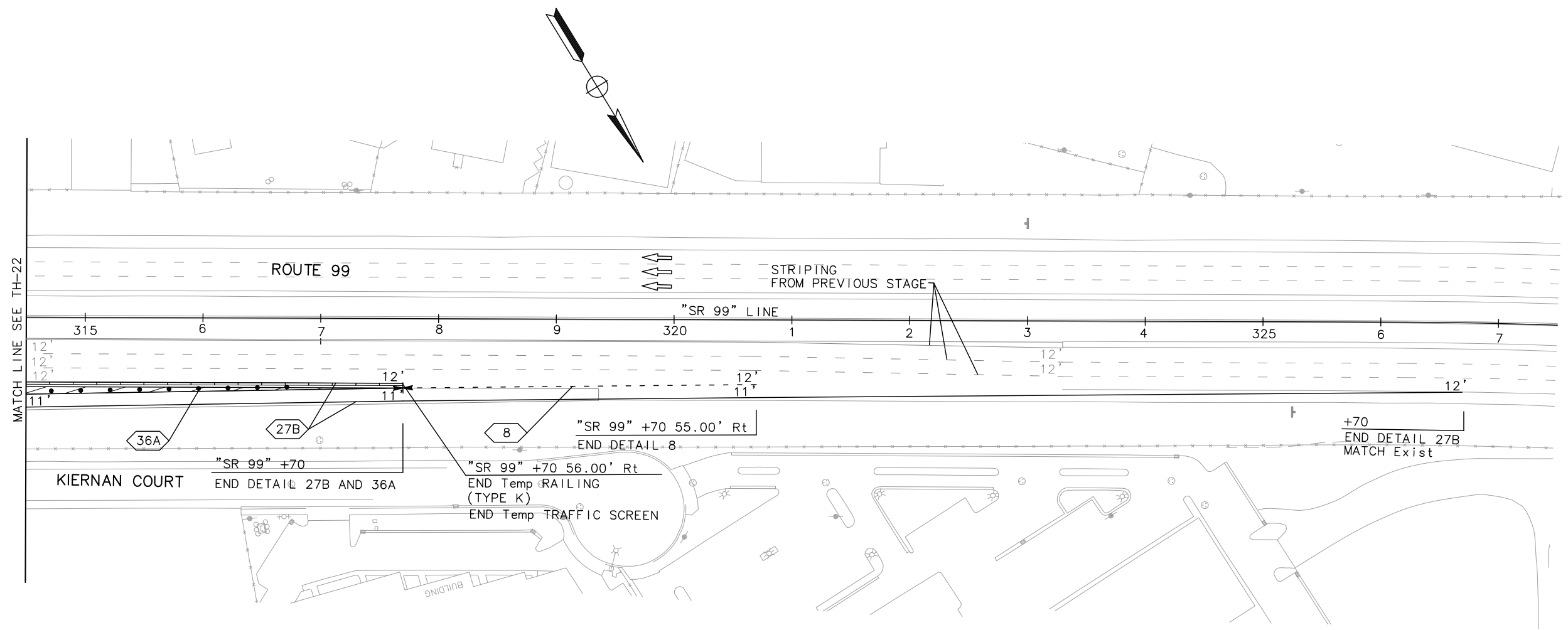
9/04/12
 PLANS APPROVAL DATE

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STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED- DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED



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**TRAFFIC HANDLING PLAN
(STAGE 2)**
SCALE: 1" = 50'
TH-23

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	145	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE

9/04/12
 PLANS APPROVAL DATE

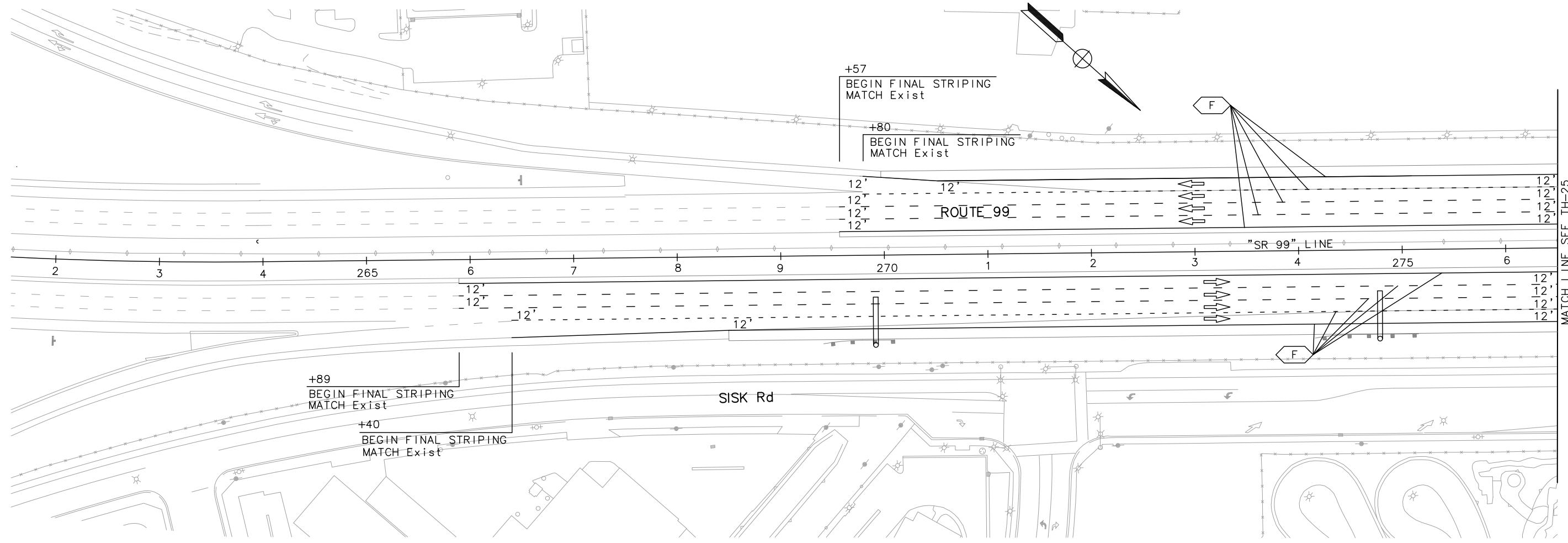
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STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED



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TRAFFIC HANDLING PLAN
(STAGE 3)
 SCALE: 1" = 50'
TH-24

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	146	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE

9/04/12
 PLANS APPROVAL DATE

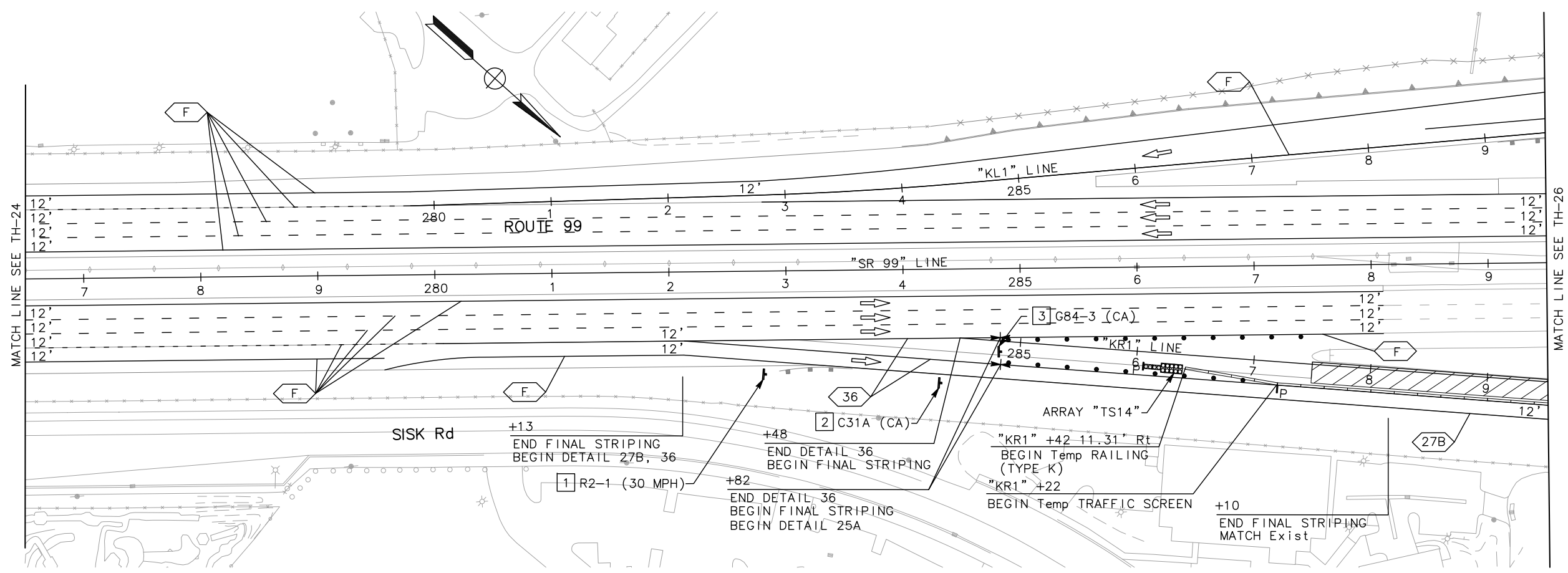
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 MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED



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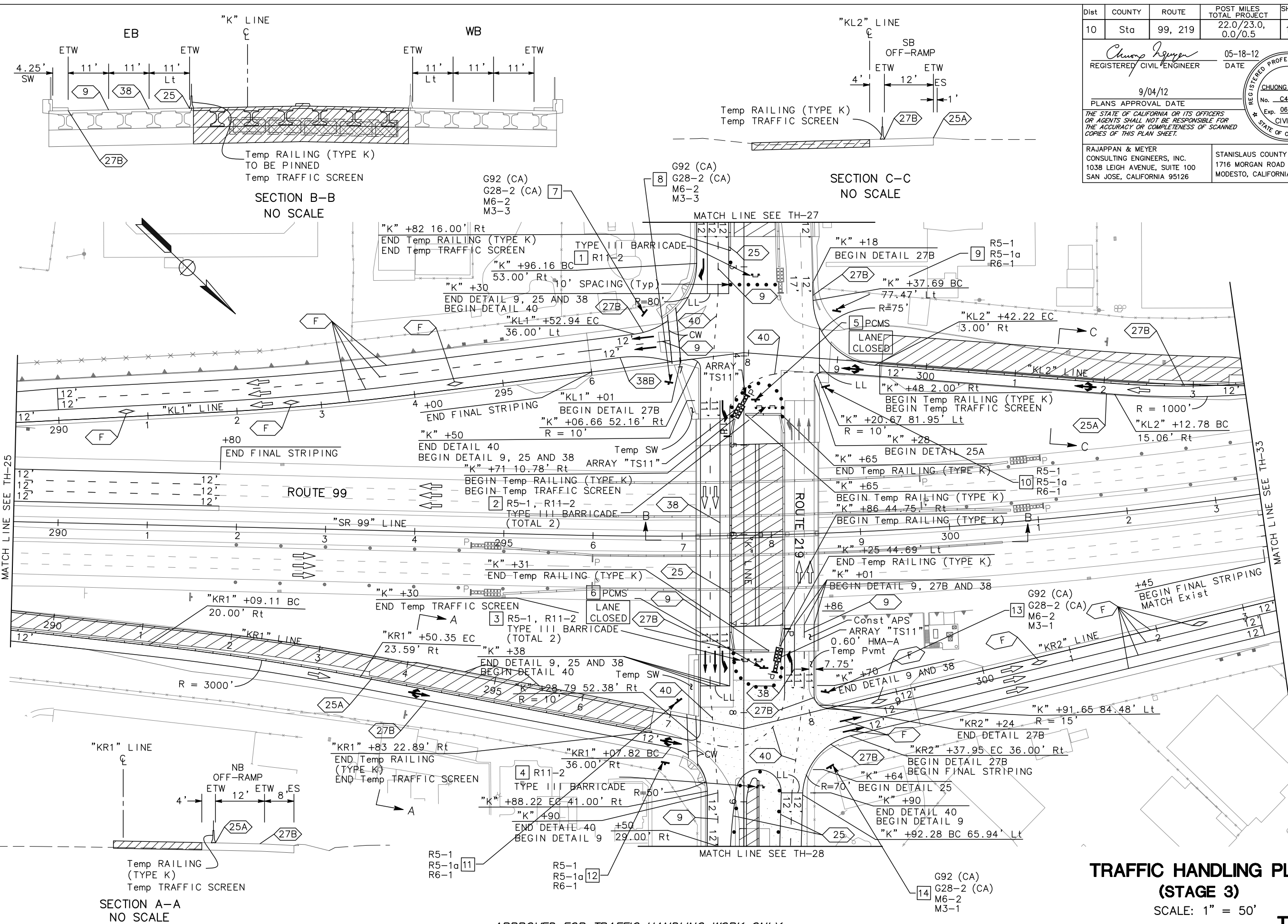
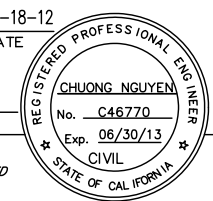
TRAFFIC HANDLING PLAN
(STAGE 3)
 SCALE: 1" = 50'
TH-25

LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	147	423

REGISTERED CIVIL ENGINEER	DATE
<i>Chuong Nguyen</i>	05-18-12
PLANS APPROVAL DATE	
9/04/12	

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**TRAFFIC HANDLING PLAN
(STAGE 3)
SCALE: 1" = 50'
TH-26**


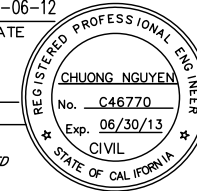
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 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO: CHUONG NGUYEN
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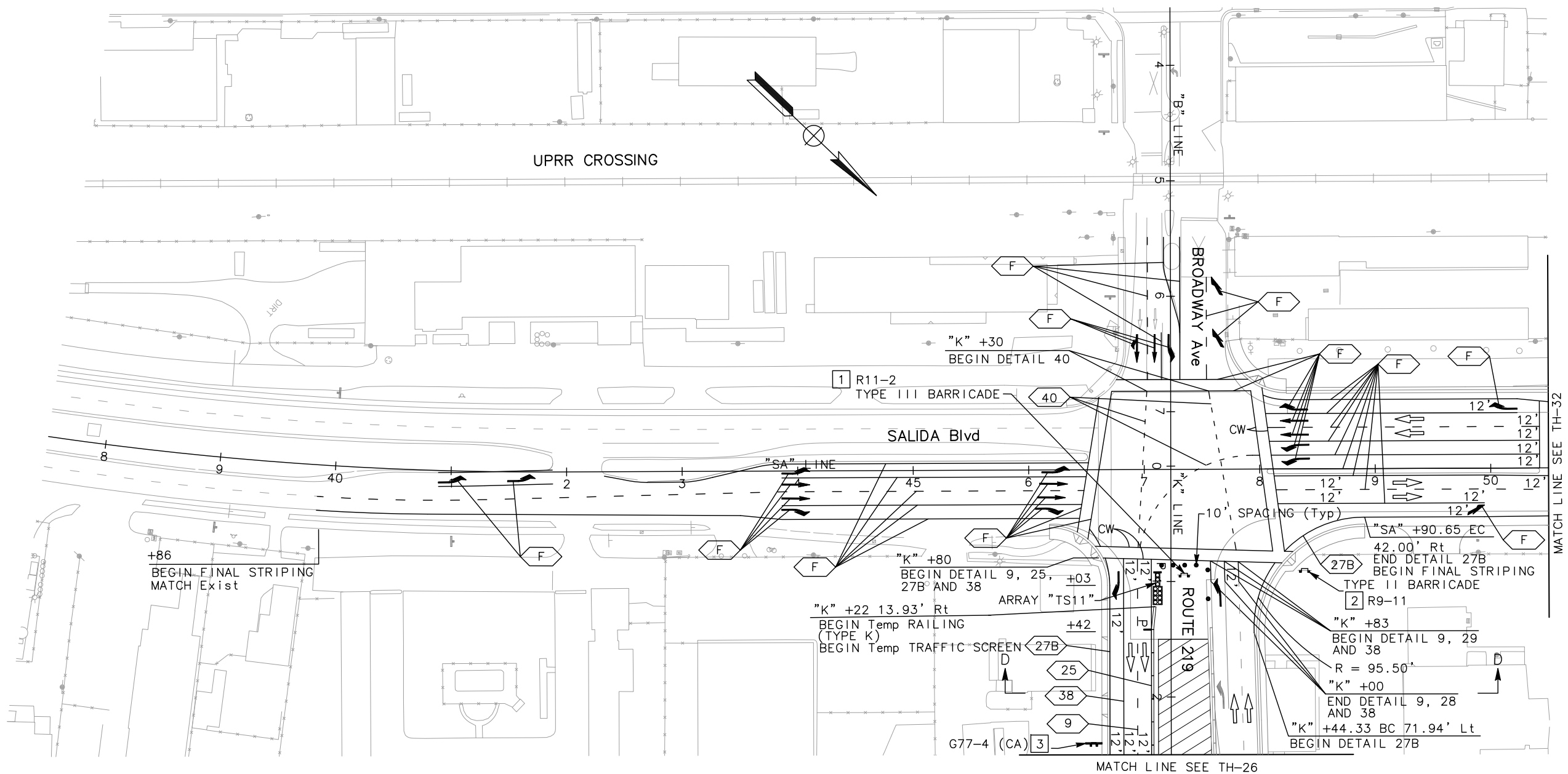
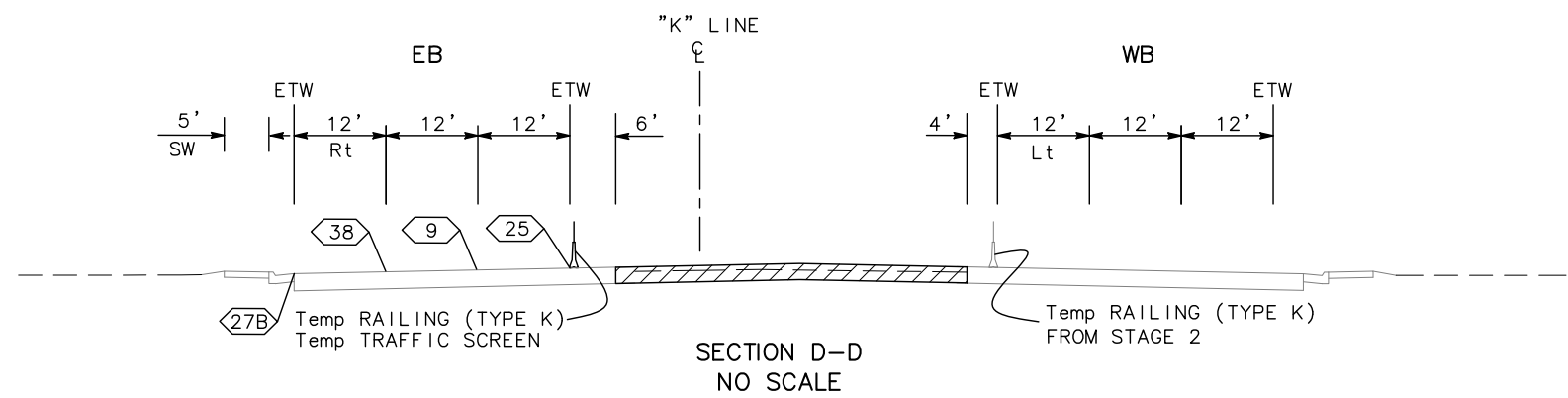
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DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	148	423


 REGISTERED CIVIL ENGINEER DATE 05-06-12	
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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APPROVED FOR TRAFFIC HANDLING WORK ONLY

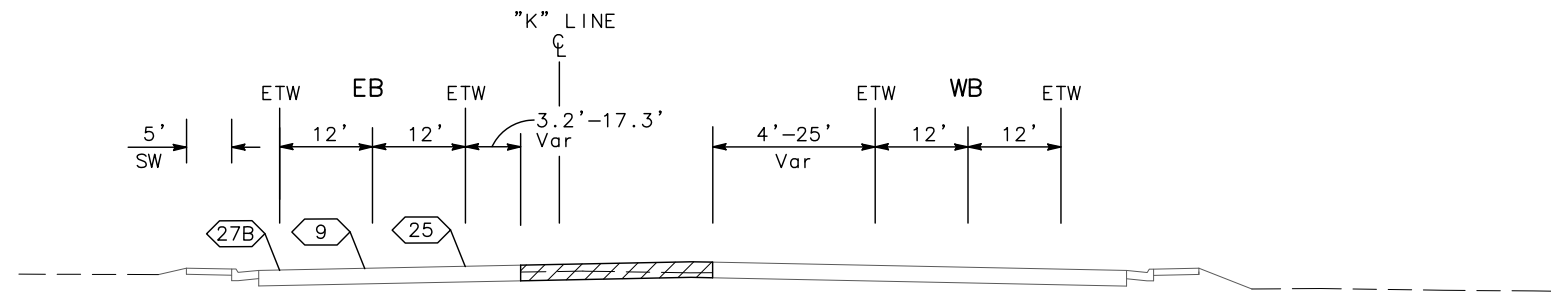
**TRAFFIC HANDLING PLAN
(STAGE 3)**
SCALE: 1" = 50'
TH-27

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO
 CHUONG NGUYEN
 REVISED BY: [blank]
 DATE REVISED: [blank]

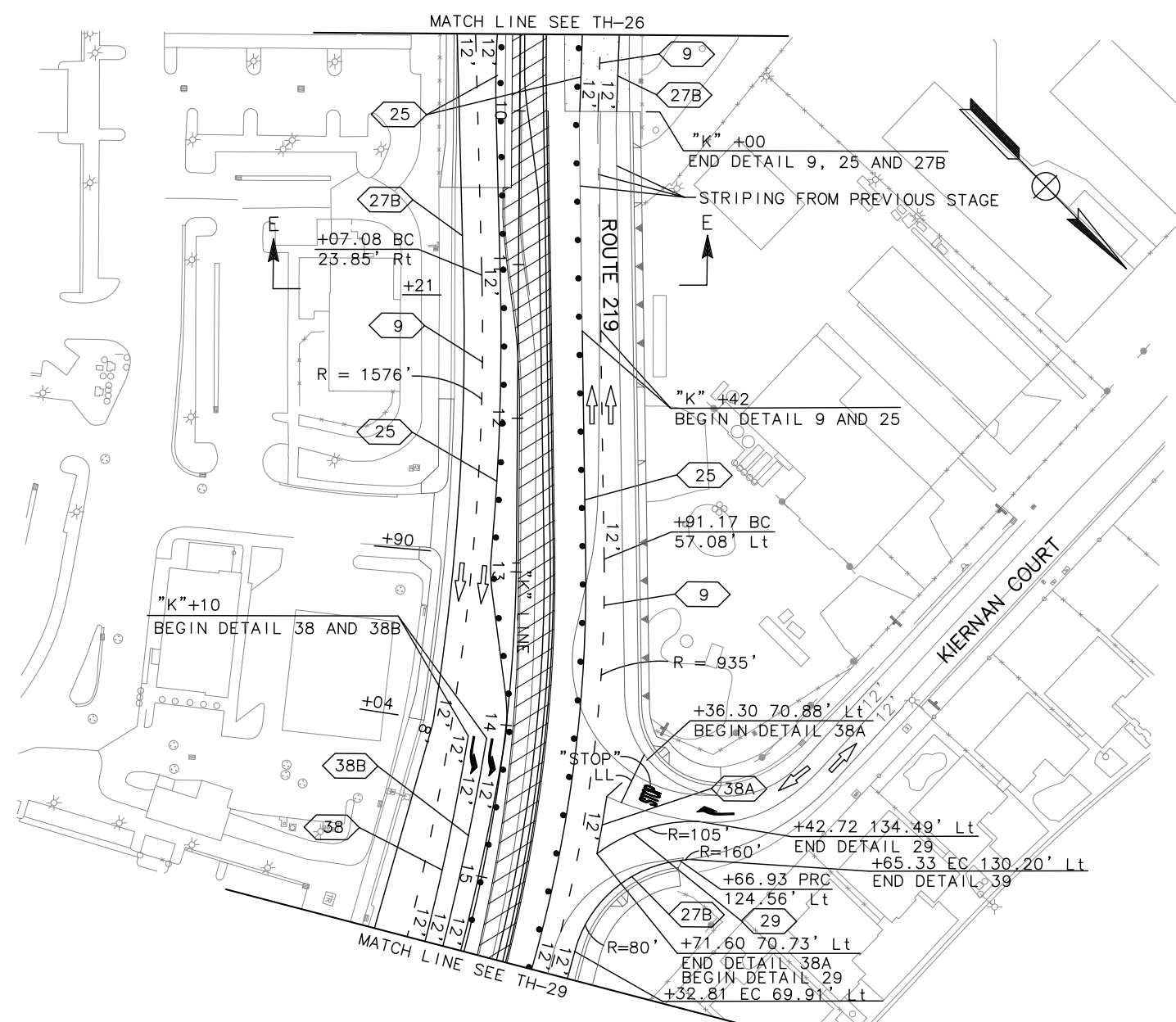
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 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	149	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-18-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



SECTION E-E
NO SCALE



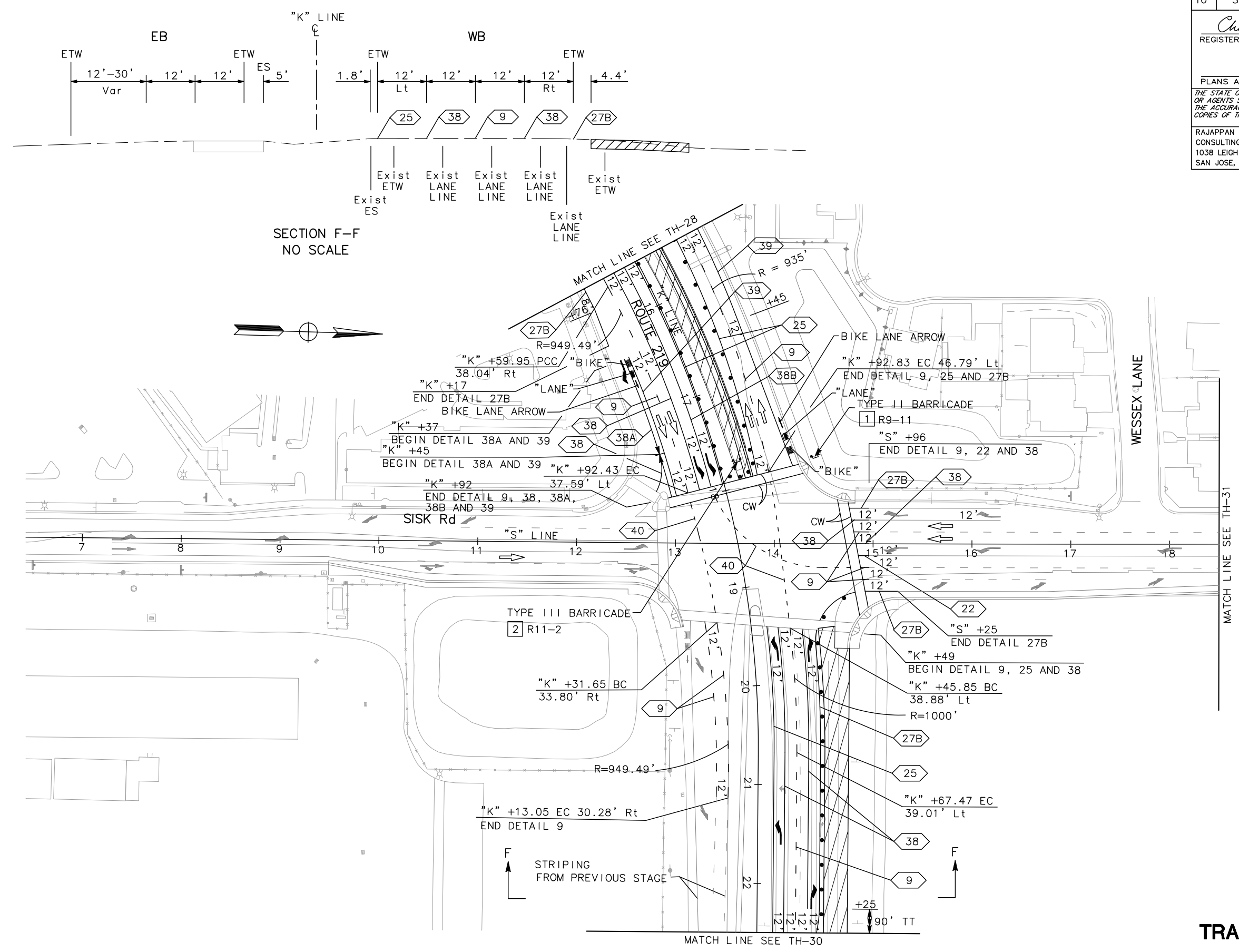
APPROVED FOR TRAFFIC HANDLING WORK ONLY

TRAFFIC HANDLING PLAN
(STAGE 3)
SCALE: 1" = 50'
TH-28

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	150	423

<i>Chung Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE
9/04/12 PLANS APPROVAL DATE		
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358	



APPROVED FOR TRAFFIC HANDLING WORK ONLY

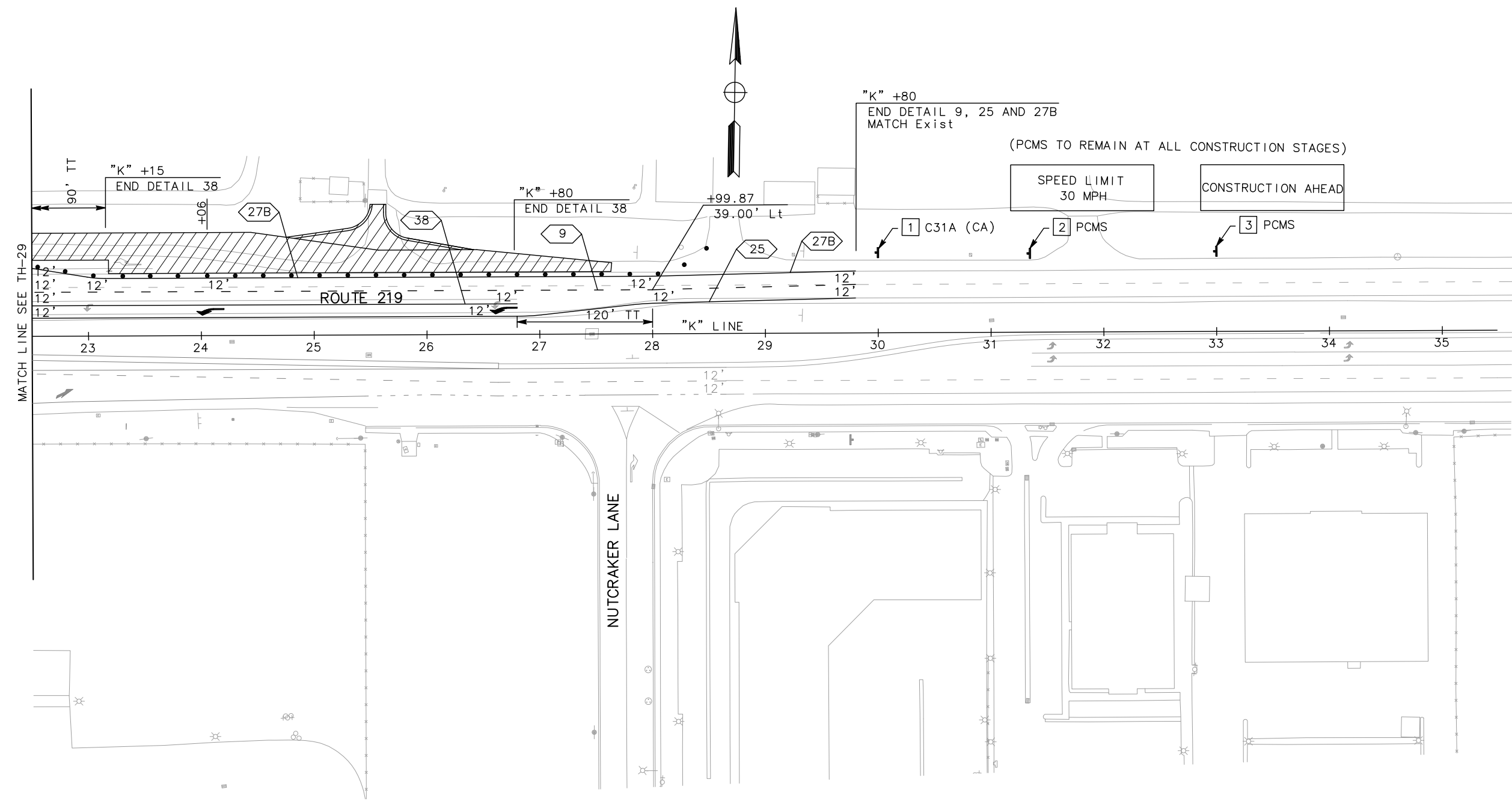
**TRAFFIC HANDLING PLAN
(STAGE 3)**
SCALE: 1" = 50'
TH-29

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO
 CHUONG NGUYEN
 REVISIONS: REVISED BY, DATE REVISED
 CALCULATED/DESIGNED BY, CHECKED BY

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHUONG NGUYEN
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: ANDY DO
 DATE REVISED: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	151	423
			REGISTERED CIVIL ENGINEER	DATE	
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



**TRAFFIC HANDLING PLAN
 (STAGE 3)**
 SCALE: 1" = 50'
TH-30

APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12 05-01-12

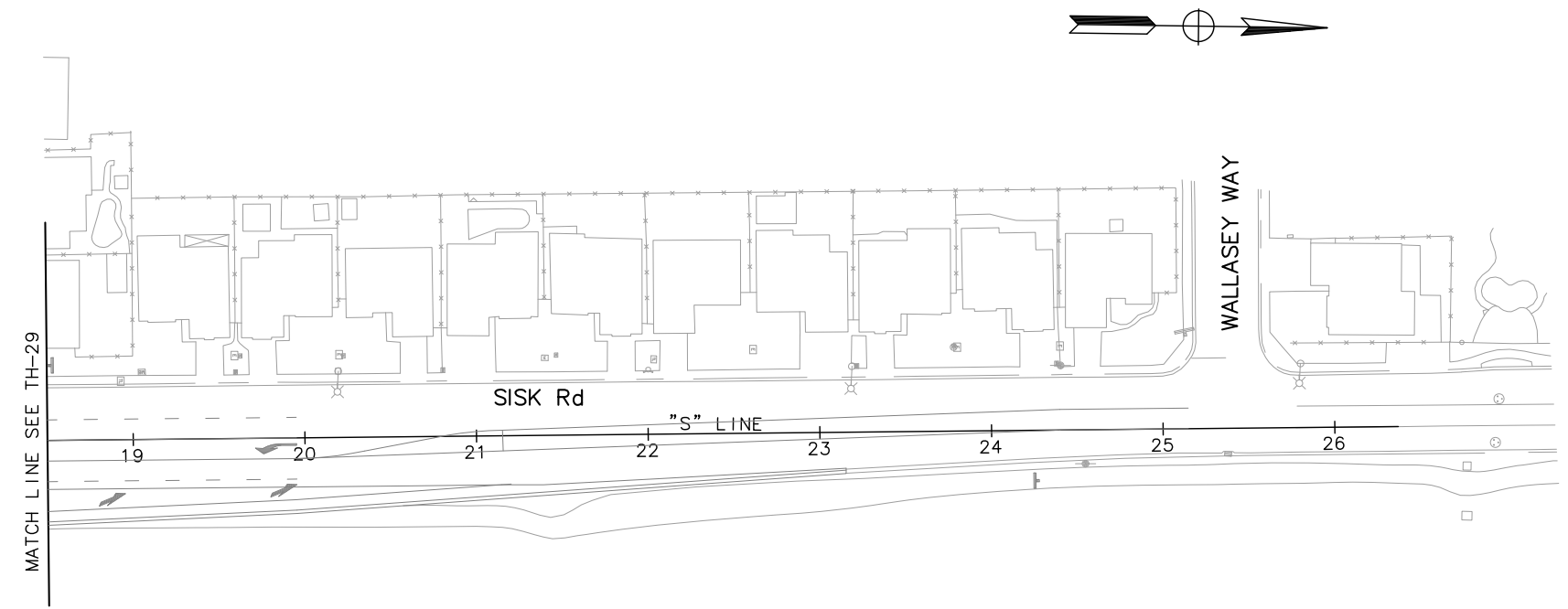
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY DO
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	152	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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TRAFFIC HANDLING PLAN
(STAGE 3)
 SCALE: 1" = 50'
TH-31

LAST REVISION
05-01-12
DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO
 CHUONG NGUYEN
 REVISIONS: REVISED BY, DATE, REVISIONS

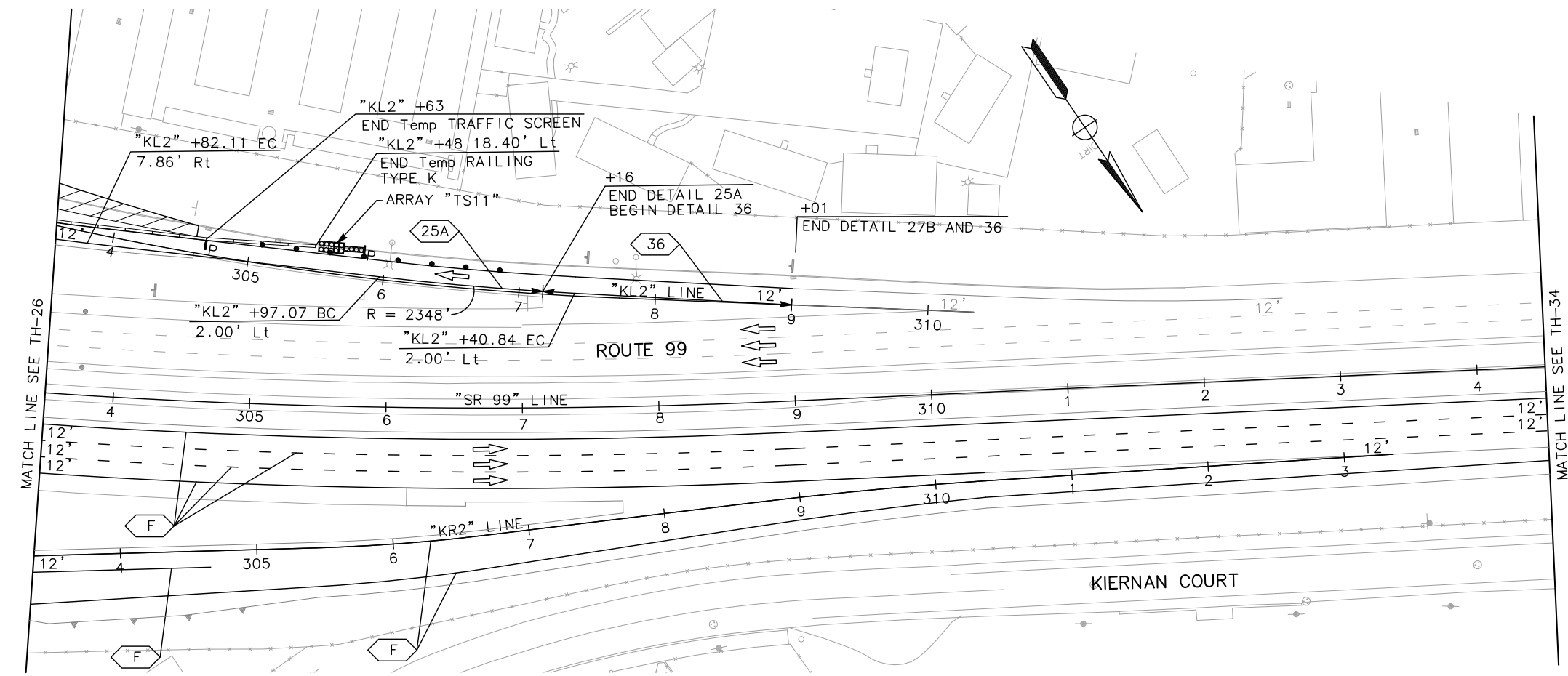
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	154	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



APPROVED FOR TRAFFIC HANDLING WORK ONLY

TRAFFIC HANDLING PLAN
(STAGE 3)
 SCALE: 1" = 50'
TH-33

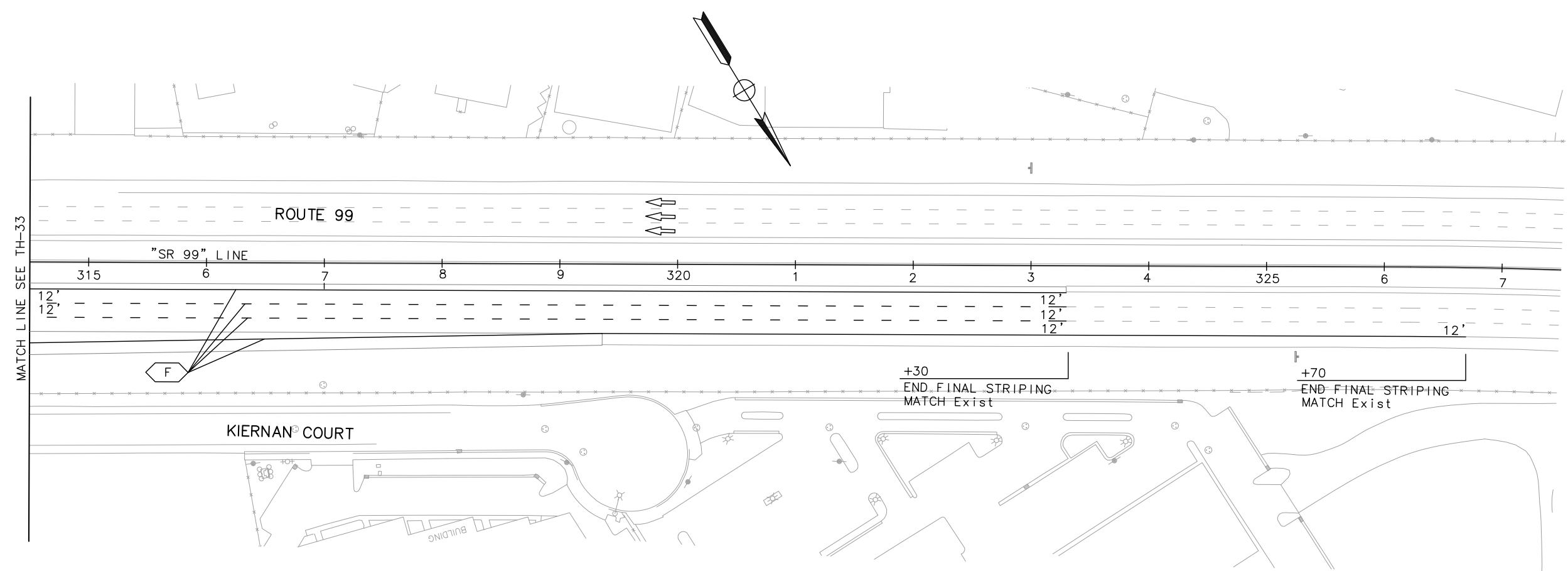
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: ANDY DO
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	155	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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TRAFFIC HANDLING PLAN
(STAGE 3)
 SCALE: 1" = 50'
TH-34

LAST REVISION: 05-01-12 | DATE PLOTTED => \$DATE | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	156	423

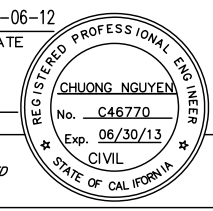
Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE

9/04/12
 PLANS APPROVAL DATE

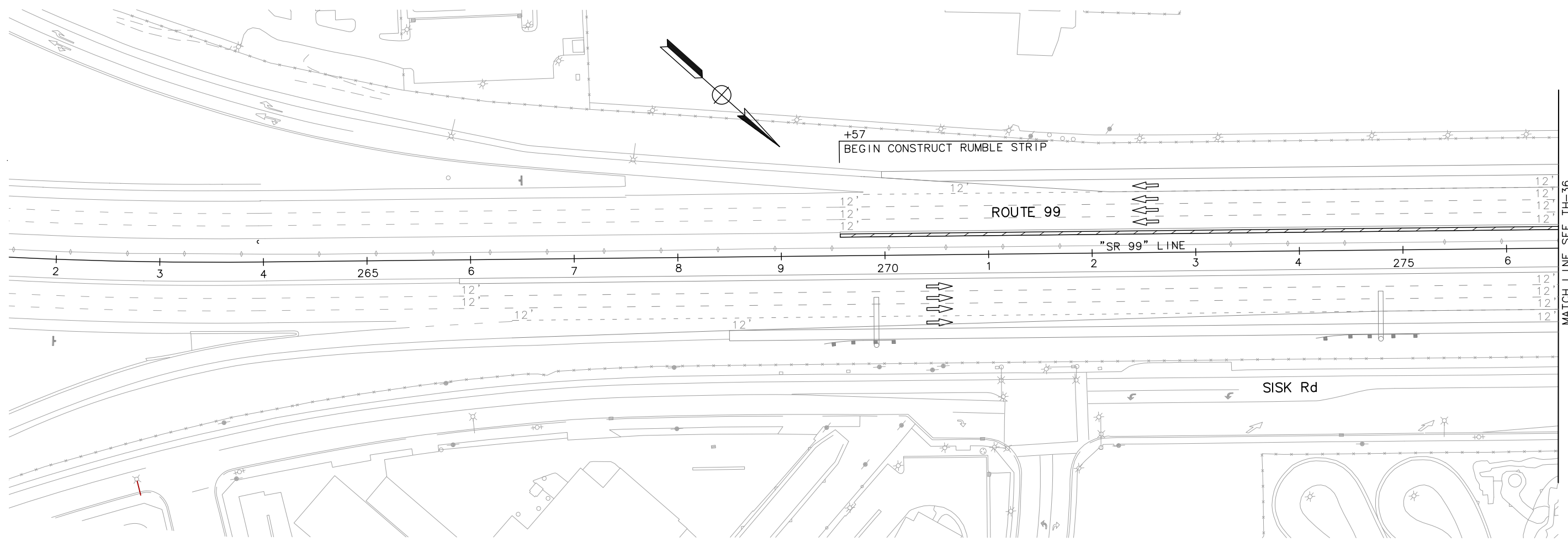
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 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED

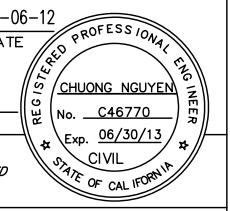


TRAFFIC HANDLING PLAN
(STAGE 4)
 SCALE: 1" = 50'
TH-35

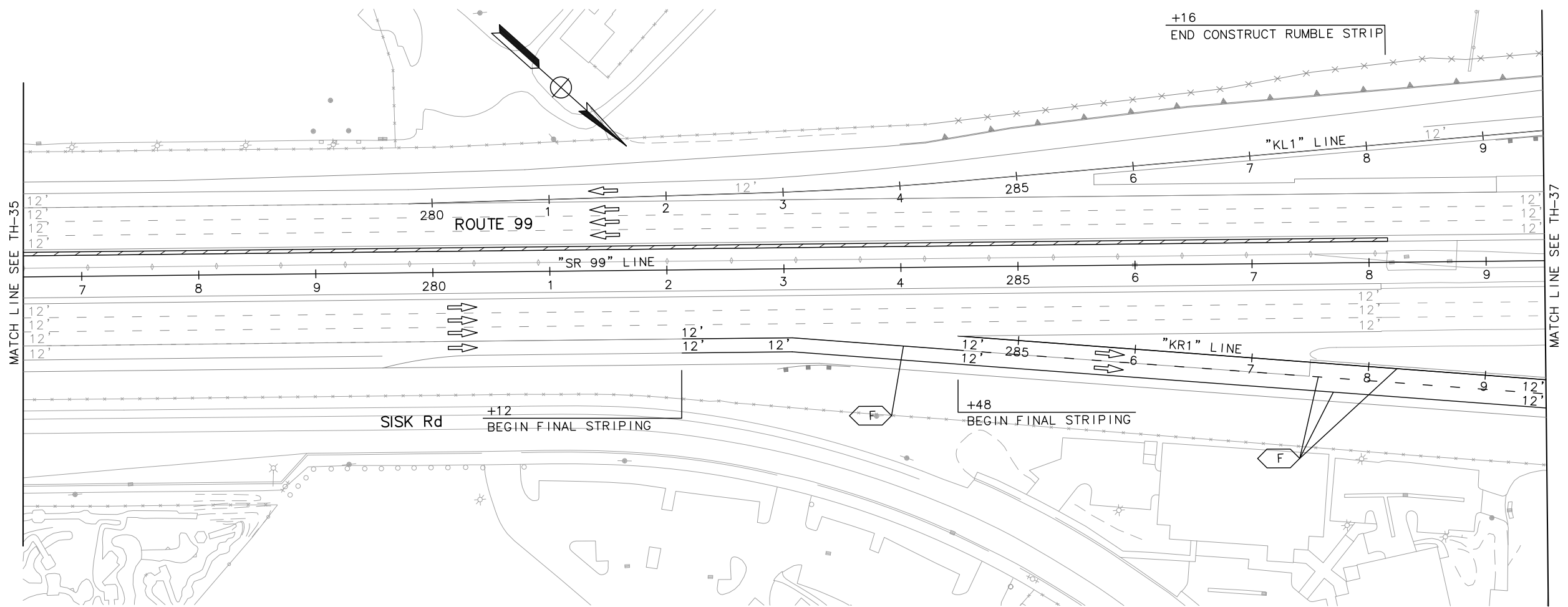
APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	157	423
			05-06-12		
			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED



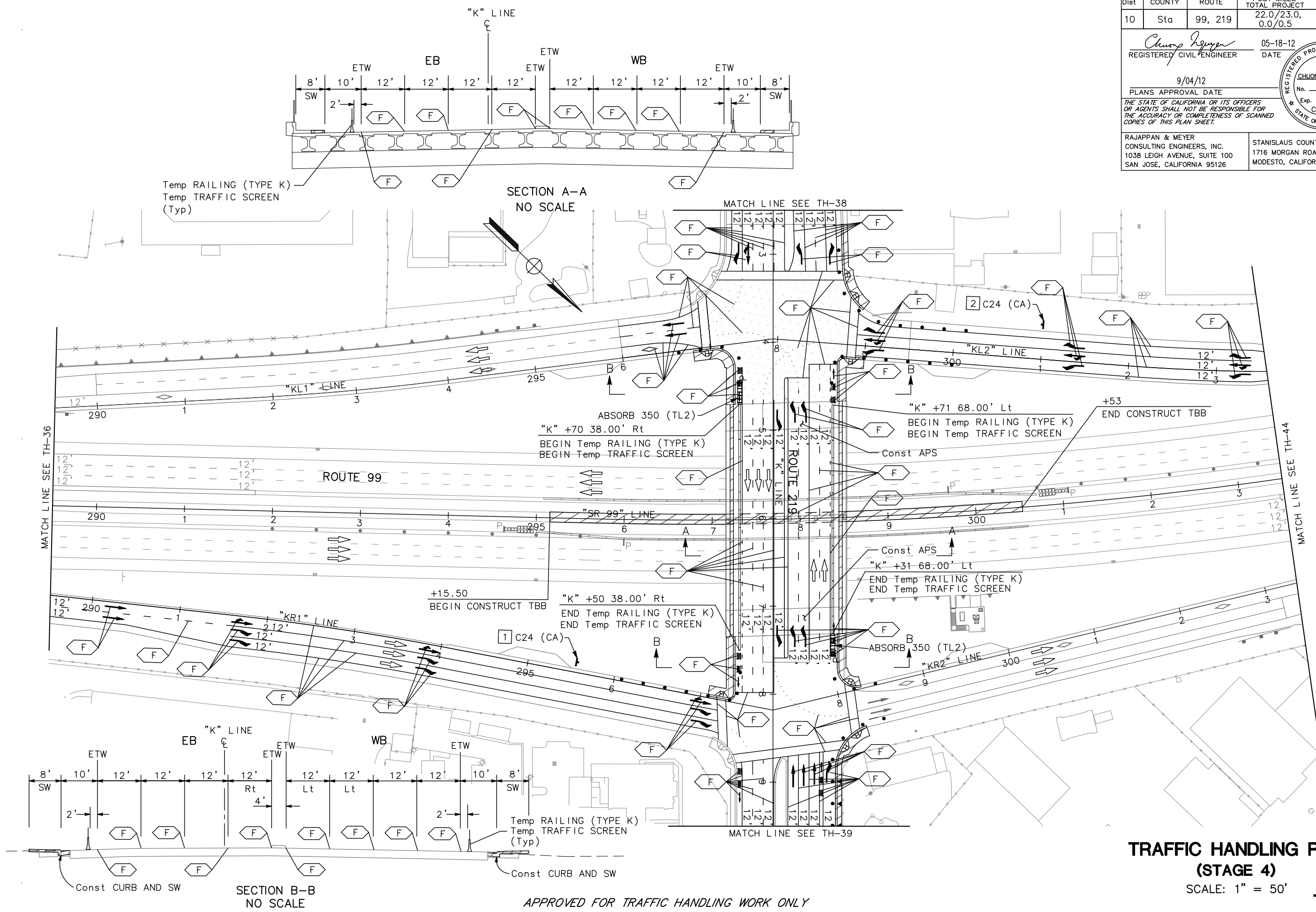
**TRAFFIC HANDLING PLAN
(STAGE 4)**
SCALE: 1" = 50'
TH-36

APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
05-01-12 05-01-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 ANDY DO
 CHUONG NGUYEN
 REVISOR: REVISED BY DATE
 DESIGNED BY: CHECKED BY
 DESIGNED BY: CHECKED BY
 DESIGNED BY: CHECKED BY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	158	423
REGISTERED CIVIL ENGINEER <i>Chuong Nguyen</i> 9/04/12 PLANS APPROVAL DATE			05-18-12 DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



TRAFFIC HANDLING PLAN
(STAGE 4)
 SCALE: 1" = 50'
TH-37

APPROVED FOR TRAFFIC HANDLING WORK ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

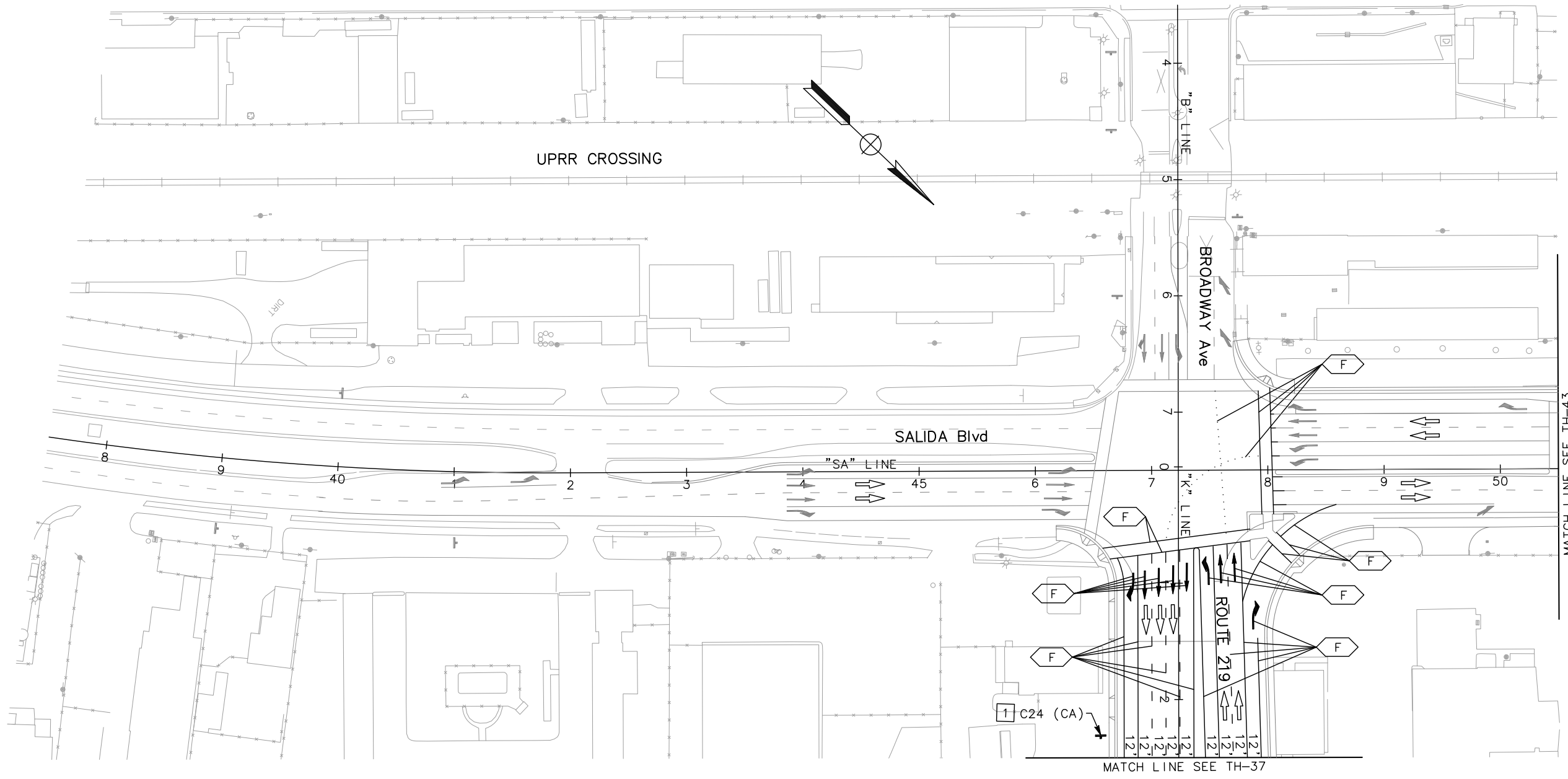
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY DO
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	159	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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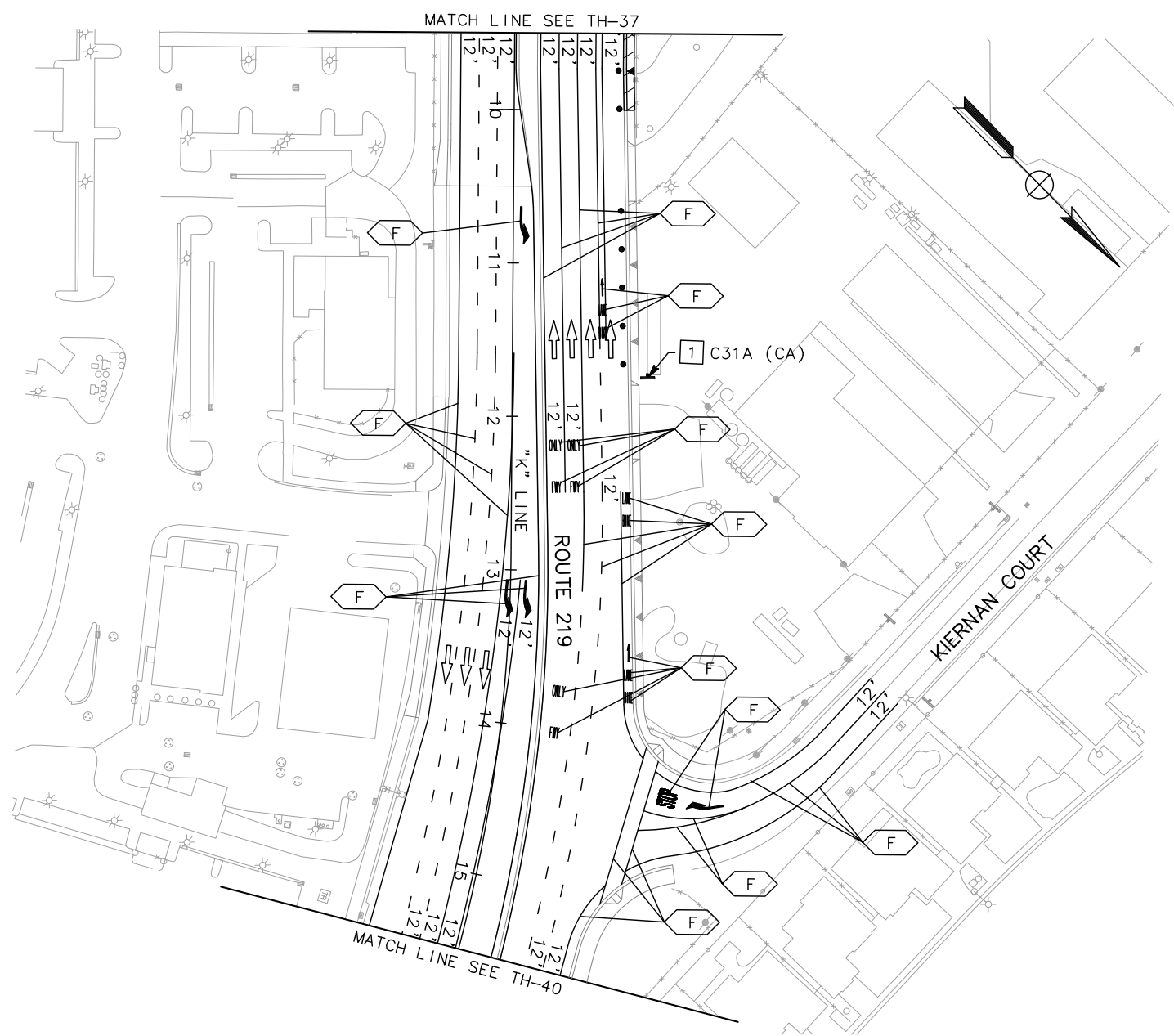
**TRAFFIC HANDLING PLAN
 (STAGE 4)**
 SCALE: 1" = 50'
TH-38

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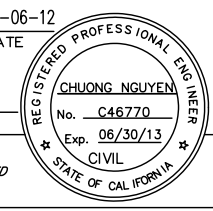
DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION
 05-01-12

x
x
x
x
x

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED




Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	160	423
			05-06-12		
REGISTERED CIVIL ENGINEER			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

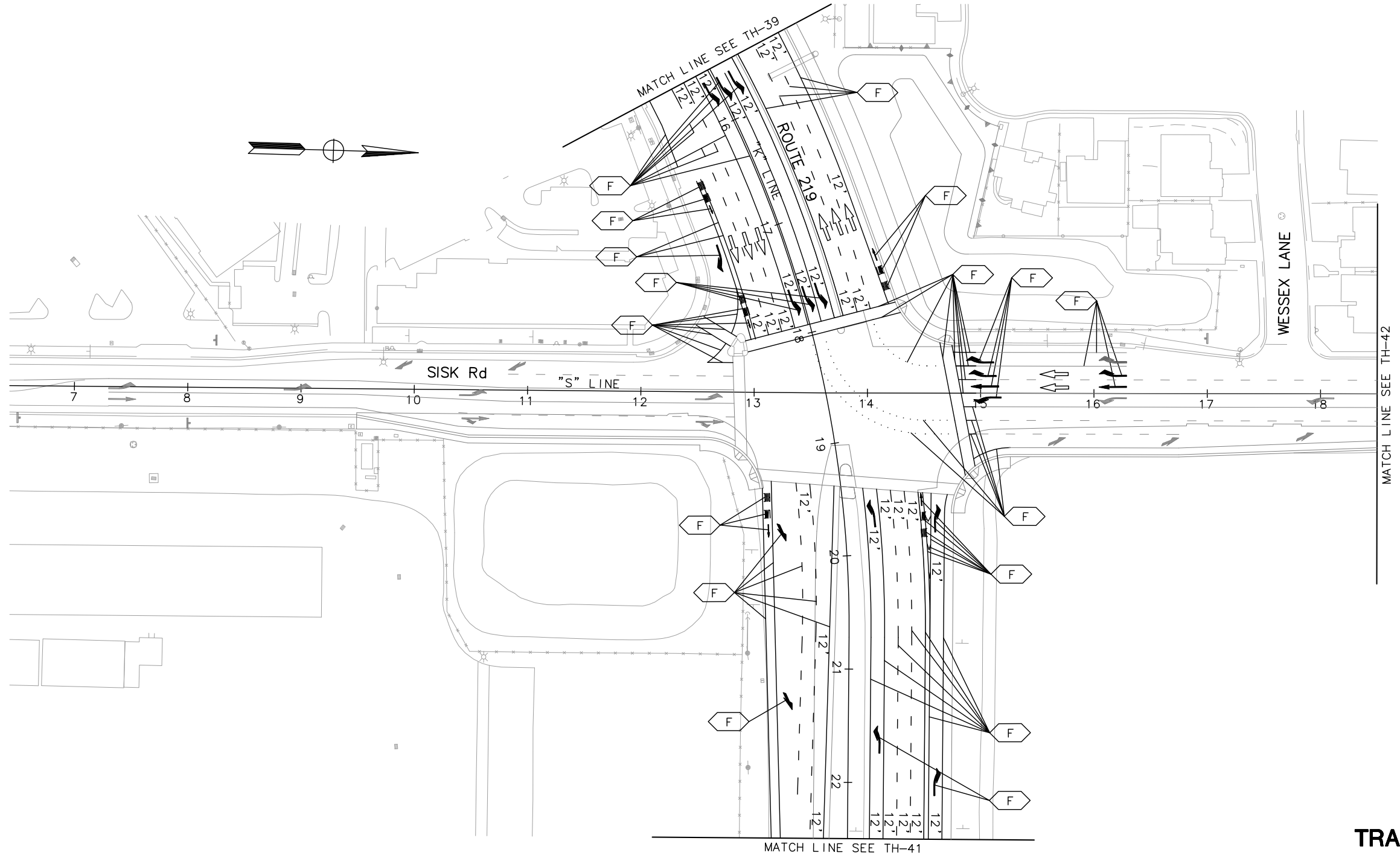
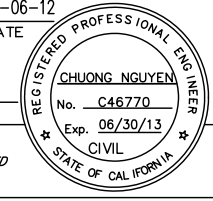


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TRAFFIC HANDLING PLAN
(STAGE 4)
 SCALE: 1" = 50'
TH-39


LAST REVISION 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	161	423
			05-06-12		
 REGISTERED CIVIL ENGINEER			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



**TRAFFIC HANDLING PLAN
(STAGE 4)**
SCALE: 1" = 50'
TH-40

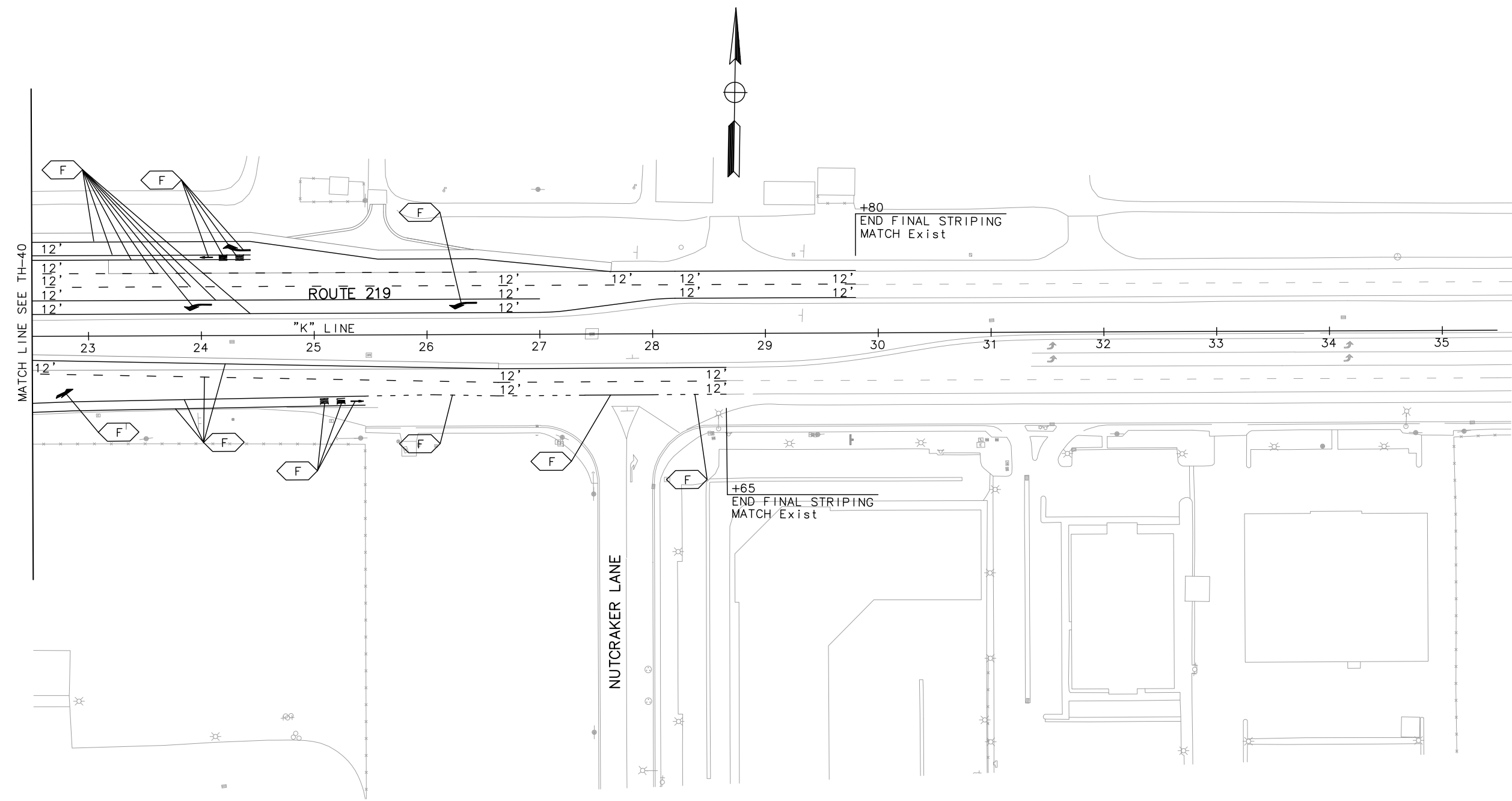
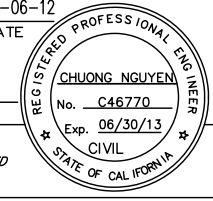
APPROVED FOR TRAFFIC HANDLING WORK ONLY

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED

LAST REVISION 05-01-12
DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	162	423
			05-06-12		
			DATE		
			9/04/12		
			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



TRAFFIC HANDLING PLAN
(STAGE 4)
 SCALE: 1" = 50'
TH-41

APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

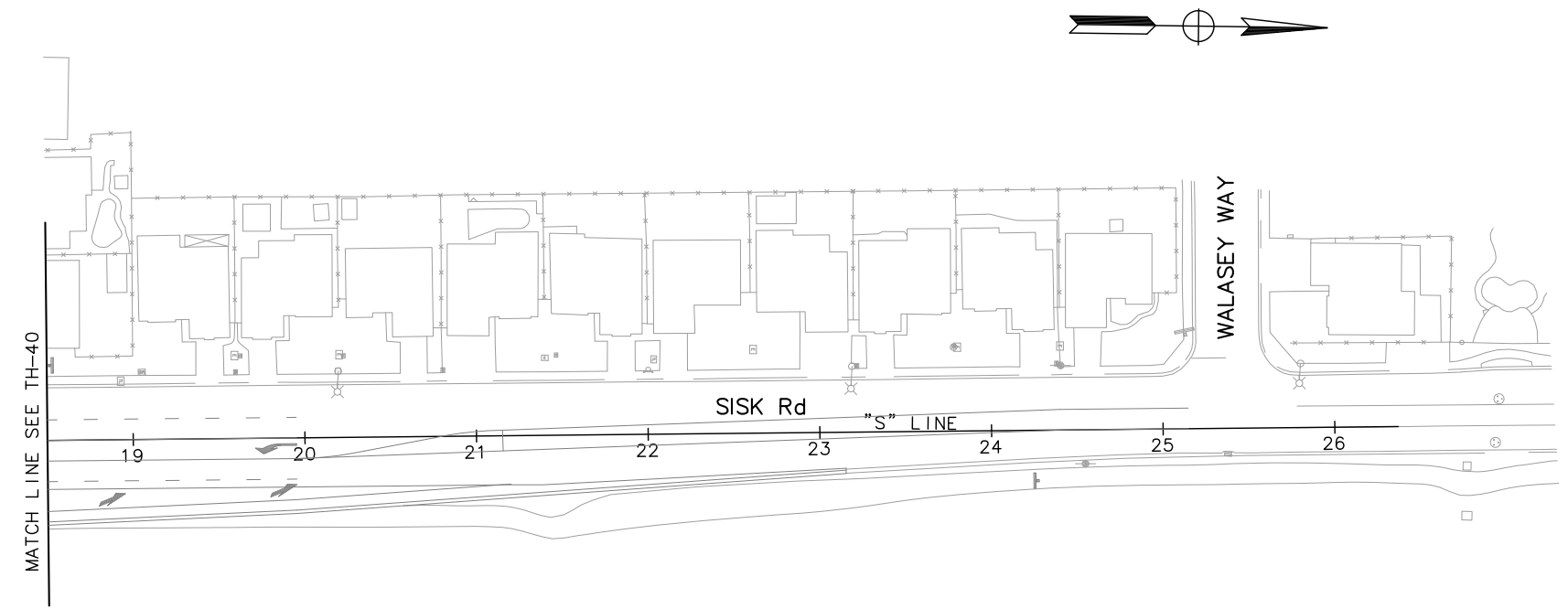
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY DO
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	163	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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APPROVED FOR TRAFFIC HANDLING WORK ONLY

**TRAFFIC HANDLING PLAN
 (STAGE 4)**
 SCALE: 1" = 50'
TH-42

LAST REVISION 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans

REVISOR BY
 DATE REVISED

ANDY DO
 CHUONG NGUYEN

CALCULATED-
 DESIGNED BY
 CHECKED BY

CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	164	423

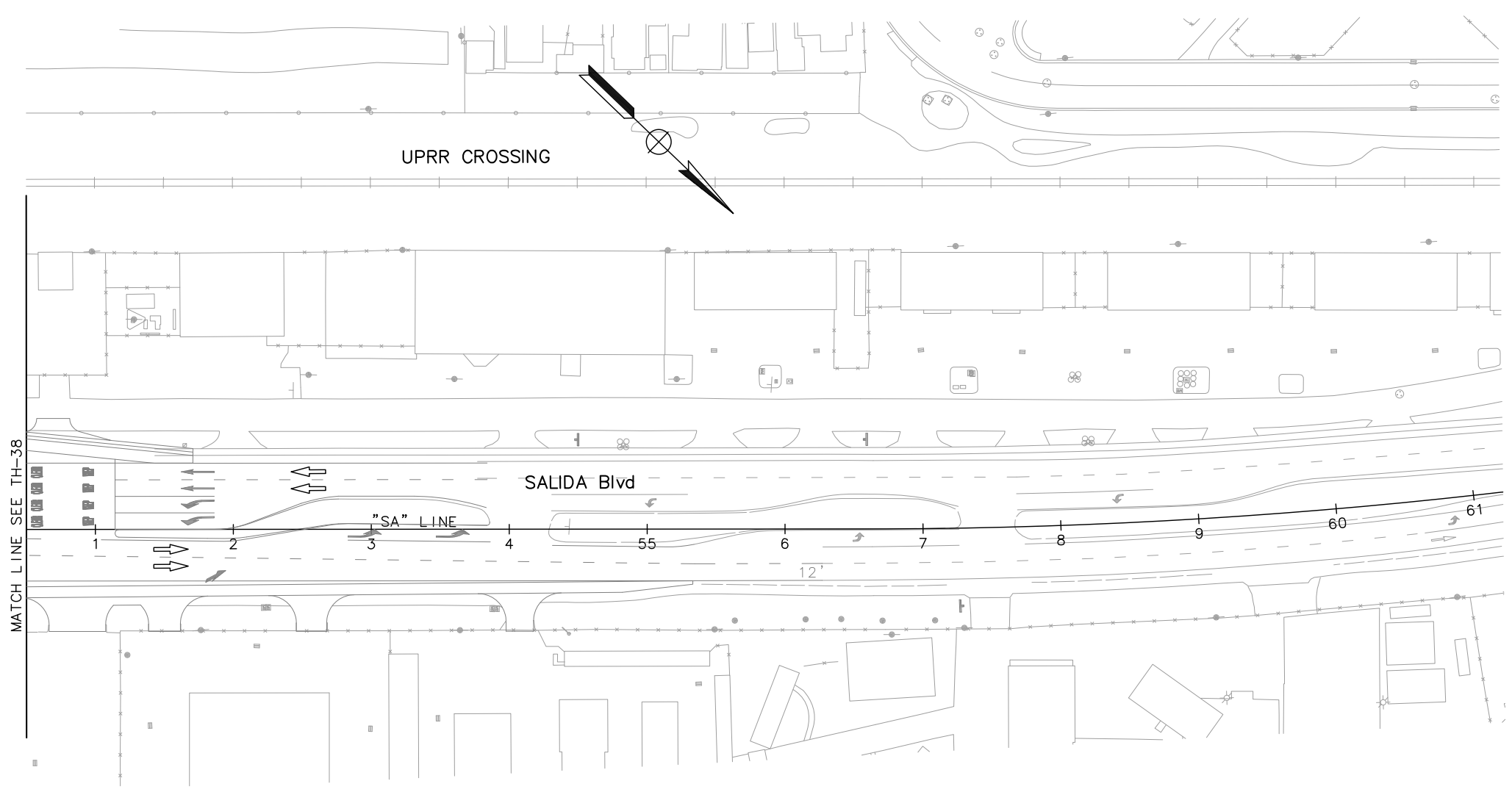
Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE

9/04/12
 PLANS APPROVAL DATE

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 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

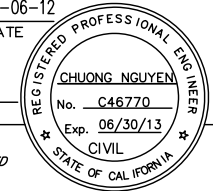
STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358




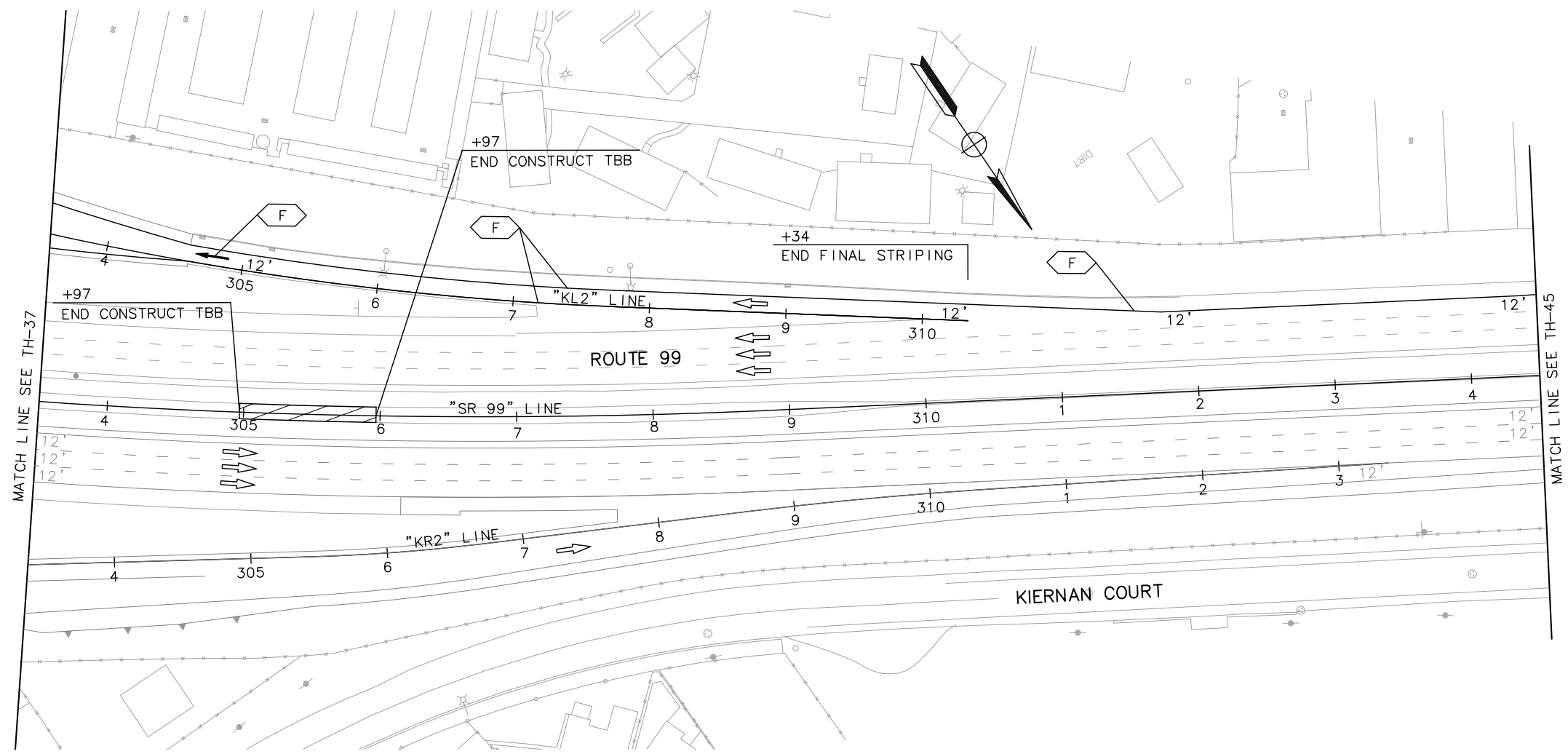
APPROVED FOR TRAFFIC HANDLING WORK ONLY

**TRAFFIC HANDLING PLAN
 (STAGE 4)**
 SCALE: 1" = 50'
TH-43

LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	165	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	ANDY DO	REVISED BY
	KEITH G. MEYER	CHECKED BY	CHUONG NGUYEN	DATE REVISED



TRAFFIC HANDLING PLAN
(STAGE 4)
 SCALE: 1" = 50'
TH-44

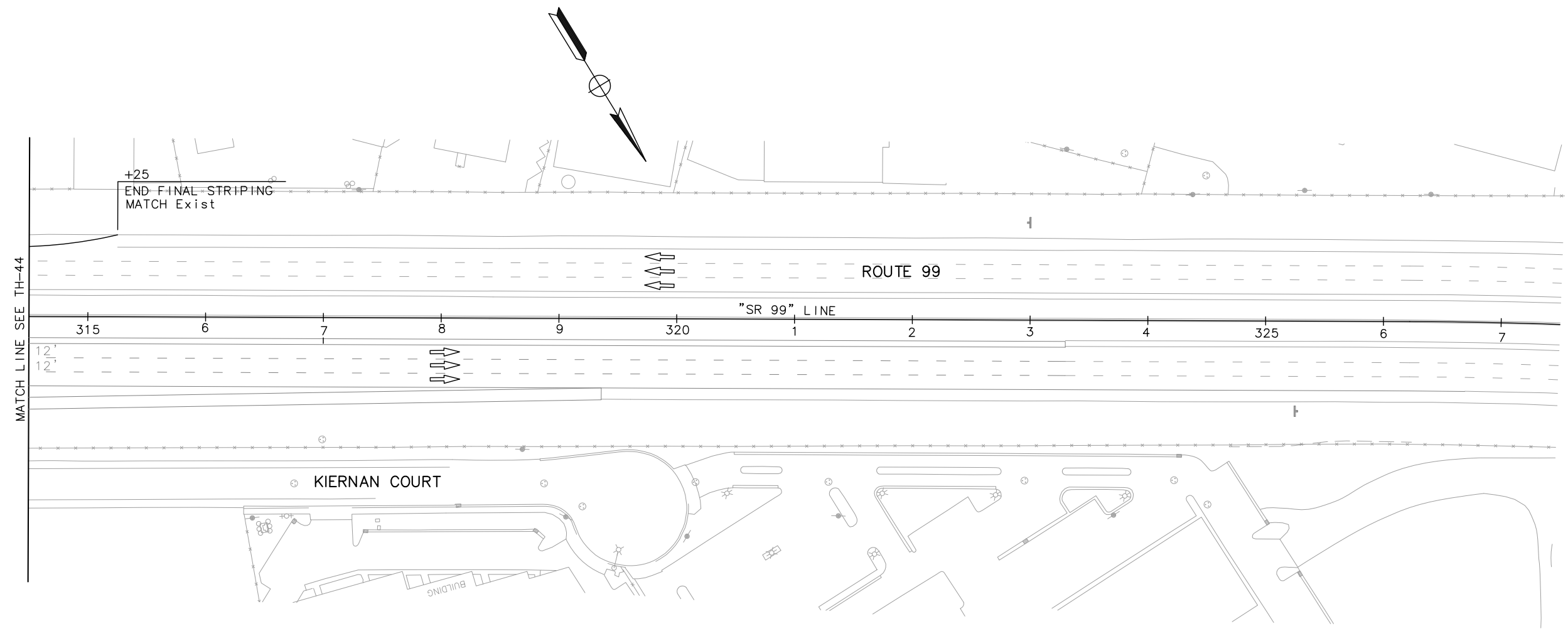
APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY DO
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	166	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126
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 MODESTO, CALIFORNIA 95358



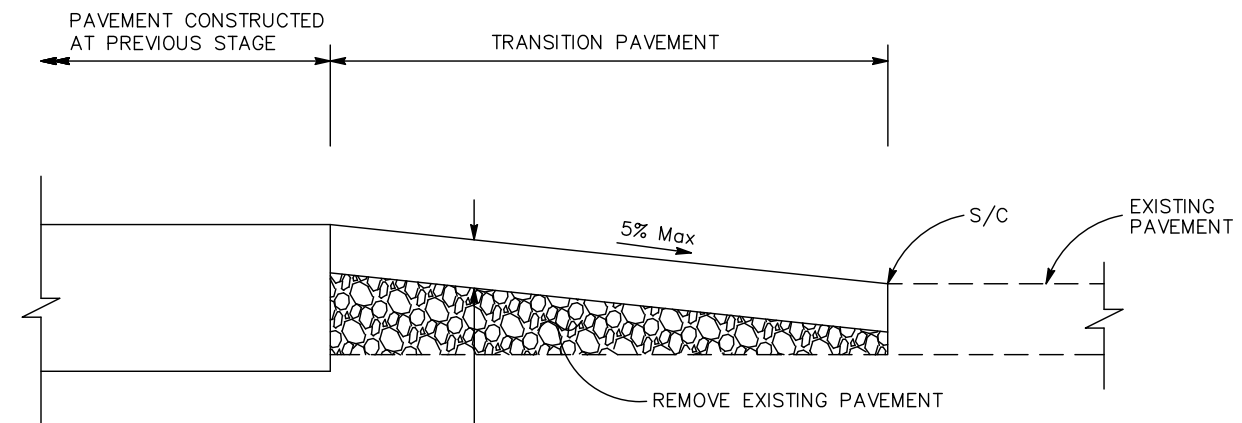
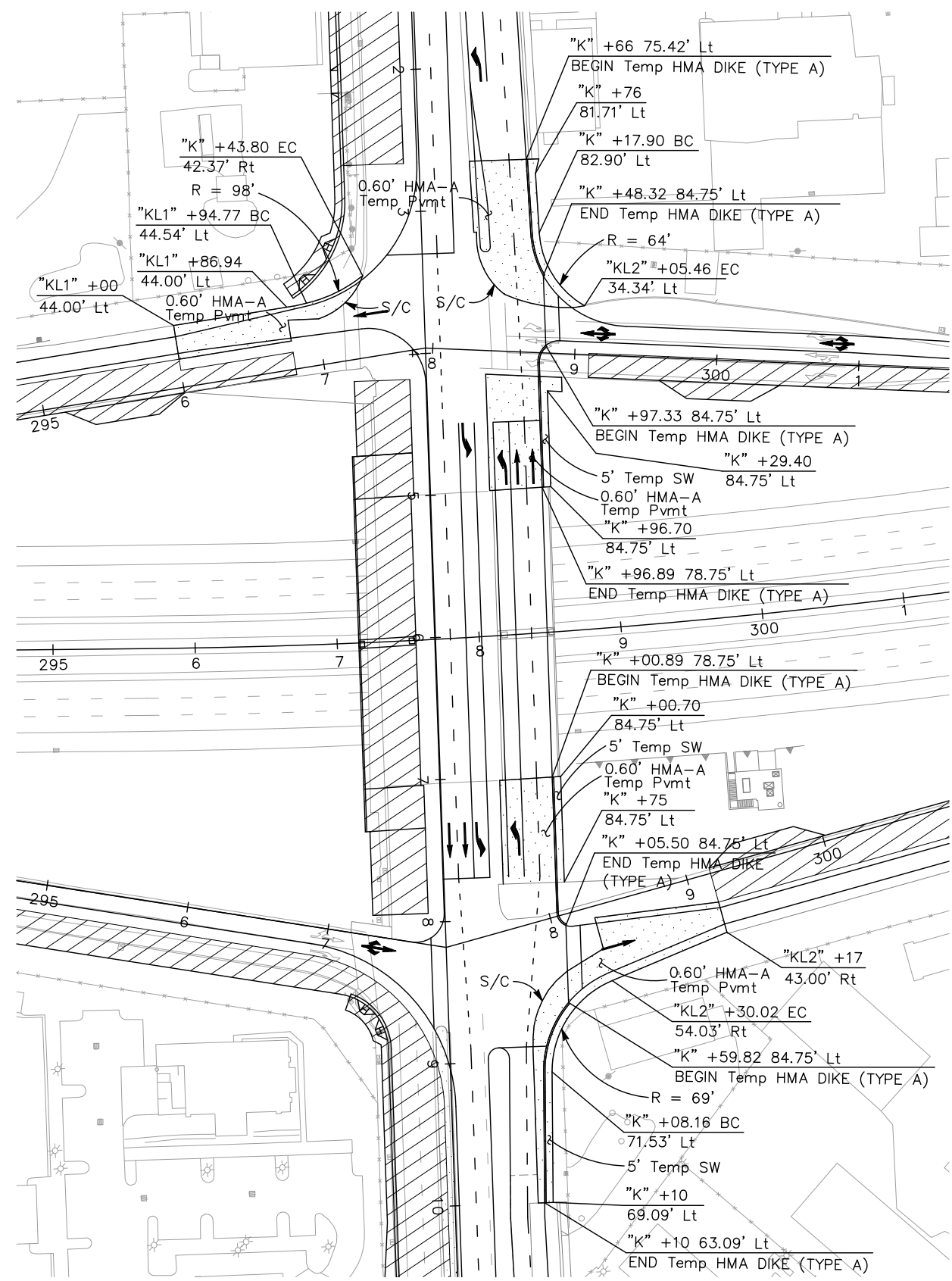
TRAFFIC HANDLING PLAN
(STAGE 4)
 SCALE: 1" = 50'
TH-45

APPROVED FOR TRAFFIC HANDLING WORK ONLY

LAST REVISION 05-01-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	167	423

<i>Chuong Nguyen</i>		05-06-12
REGISTERED CIVIL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		
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NO SCALE
TEMPORARY PAVEMENT TRANSITION

STAGE 2
TEMPORARY PAVEMENT

TRAFFIC HANDLING DETAILS

SCALE: 1" = 50'

THD-1

APPROVED FOR TRAFFIC HANDLING WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CHECKED BY: ANDY DO
 CHUONG NGUYEN
 REVISOR: REVISED BY: DATE REVISOR: REVISED BY: DATE

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: ANDY DO
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

NOTE :

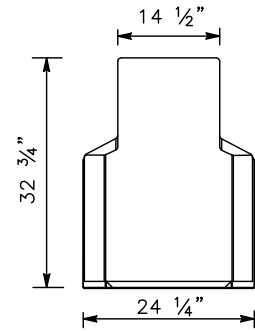
TEMPORARY CRASH CUSHION ABSORB 350 SHALL BE MANUFACTURED BY BARRIER SYSTEMS INC. AND SHALL BE INSTALLED IN CONFORMANCE WITH THE MANUFACTURERS INSTALLATION INSTRUCTIONS.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	168	423

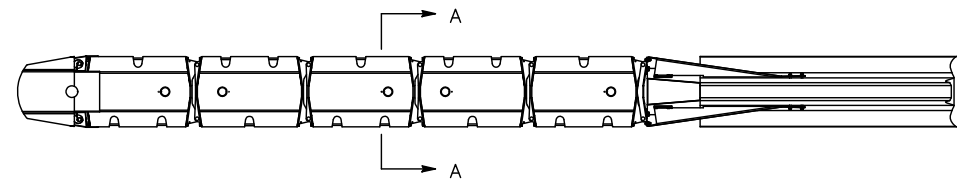
Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

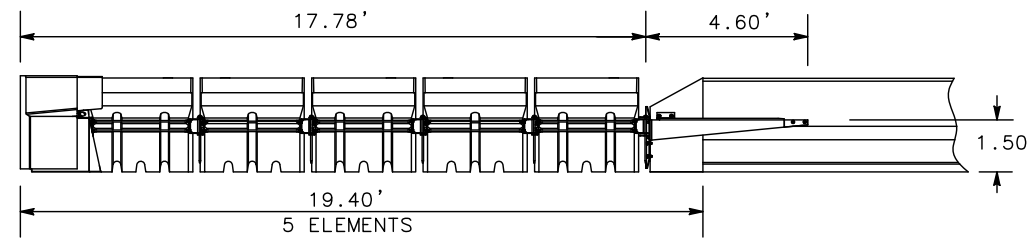
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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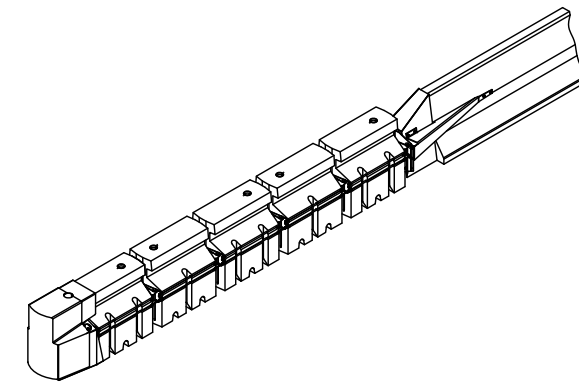
SECTION A-A



PLAN



ELEVATION



TEMPORARY CRASH CUSHION ABSORB 350 TYPE TL2

TRAFFIC HANDLING DETAILS

NO SCALE

THD-2

APPROVED FOR TRAFFIC HANDLING WORK ONLY



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	169	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

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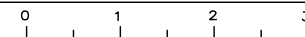
STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

TRAFFIC STRIPE AND PAVEMENT MARKER (TRAFFIC HANDLING PLAN)

STAGE	STATION	Loc	PAINT TRAFFIC STRIPE (2-COAT)																	PAVEMENT MARKER (RETROREFLECTIVE)					
			DETAIL 8	DETAIL 9	DETAIL 12	DETAIL 14A	DETAIL 22	DETAIL 25	DETAIL 25A	DETAIL 27B	DETAIL 29	DETAIL 36	DETAIL 36A	DETAIL 37	DETAIL 38	DETAIL 38A	DETAIL 38B	DETAIL 39	DETAIL 39A	DETAIL 40	TYPE C	TYPE D	TYPE G	TYPE H	
			LF																	EA					
1	"SR 99" 269+57 TO 291+80	SB			4,158	288		2,223		1,564											6		88	47	
1	"SR 99" 303+45 TO 323+30	NB			3,979		1,987		1,526													84	42		
1	"KL1" 279+80 TO 297+72		300						852	1,805					265							12	37		
1	"KR2" 297+24 TO 313+36		300						887	1,650					212							10	38		
1	"B" 6+17 TO "K" 28+65	EB		1,524			421	942	592	252					303			695	249	294		82	48	21	
1	"B" 6+17 TO "K" 18+07	WB		589			201	297	2,012	102					449	44		203	200	307		28	33	7	
1	"SA" 47+63 TO 56+27	NB		839				642	808						99							23	14		
1	"SA" 47+63 TO 53+25	SB		533				480	624						249							23	11		
1	"S" 14+55 TO 23+20	NB							974																
2	"SR 99" 269+57 TO 286+00	SB			1,198	288		743		1,900					280	720						52	87	16	
2	"SR 99" 310+34 TO 312+60	SB								226															
2	"SR 99" 265+89 TO 288+10	NB			4,154	288		2,221		1,873												6	102	47	
2	"SR 99" 308+70 TO 326+70	NB	300							2,233					434								19		
2	"KL1" 279+80 TO 297+69								1,320	1,780					480								21	56	
2	"KR1" 284+48 TO 297+84								1,027	1,350					596								26	44	
2	"KL2" 279+95 TO 310+34								838	1,166					316								14	36	
2	"KR2" 297+24 TO 313+36								1,116	1,544					414								18	48	
2	"B" 6+63 TO "K" 28+65	EB		2,989				1,477		1,544					1,046	327		999	260	618			123	32	
2	"K" 8+90 TO 18+01	MED								1,865													158		
2	"B" 5+84 TO "K" 21+36	WB		1,486			59	547	1,953	149					430	274		273				505	20	51	12
2	"SA" 39+89 TO 56+27	NB		1,492				1,156	1,559						373								49	25	
2	"SA" 47+93 TO 53+84	SB		579				528	587						240								24	12	
2	"S" 14+96 TO 15+25	NB							30																
3	"B" 6+82 TO "K" 21+13	EB		1,810				563	1,530						982	148	378	937				1056		115	13
3	"K" 8+64 TO 18+04	MED						1,095																24	
3	"B" 6+82 TO "K" 29+80	WB		2,024				1,828	1,734	203					1,134	38		377			520		18	91	39
3	"SR 99" 282+13 TO 284+48	NB					44		234						470								4	21	
3	"KR1" 284+48 TO 297+84								1,275	1,278					34									1	54
3	"KL2" 279+95 TO 310+34								838	1,035					183									9	36
3	"S" 6+50 TO 18+50	NB		15	11				33														2	1	
3	"S" 6+50 TO 18+50	SB		15					153						164									9	
	SUBTOTAL		900	13,895	13,500	864	725	16,729	8,153	35,297	2,571	1,599	2,085	720	5,469	831	378	3,484	709	3,300	64	312	1102	711	
	TOTAL																								2,189*

(*) QUANTITY INCLUDED IN PAVEMENT DELINEATION QUANTITIES TABLE.

TRAFFIC HANDLING QUANTITIES THQ-1



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	170	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

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RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

REMOVE PAVEMENT DELINEATION

STAGE	LOCATION	REMOVE THERMOPLASTIC TRAFFIC STRIPE	REMOVE YELLOW THERMOPLASTIC TRAFFIC STRIPE (HAZARDOUS)	REMOVE PAINTED TRAFFIC STRIPE	REMOVE YELLOW PAINTED TRAFFIC STRIPE (HAZARDOUS)	REMOVE THERMOPLASTIC PAVEMENT MARKING	REMOVE YELLOW THERMOPLASTIC PAVEMENT MARKING (HAZARDOUS)	REMOVE PAINTED PAVEMENT MARKING	PAVEMENT MARKER	REMARK
		LF	LF	LF	LF	SQFT	SQFT	SQFT	EA	
STAGE 1	"SR 99" 265+89 TO 291+80			5,905	4,444				190	REMOVE FROM EXISTING
	"KL1" 279+80 TO 297+20			2,097	932			188	46	REMOVE FROM EXISTING
	"SR 99" 298+15 TO 323+30			5,644	2,056				131	REMOVE FROM EXISTING
	"KR2" 297+89 TO 313+36			1,886	1,013			62	50	REMOVE FROM EXISTING
	"K" 0+76 TO 28+65	7,215	1,012			1,881	102		175	REMOVE FROM EXISTING
	"SA" 47+90 TO 56+27	3,090	1,115			387			69	REMOVE FROM EXISTING
STAGE 2	"SR 99" 265+89 TO 288+10			6,314	2,221				142	REMOVE FROM EXISTING
	"KR1" 284+48 TO 297+43			2,151	968			388	81	REMOVE FROM EXISTING
	"KL2" 298+37 TO 310+34			2,120	876			456	72	REMOVE FROM EXISTING
	"B" 6+17 TO "K" 21+36	5,624	2,129			1,423			172	REMOVE FROM EXISTING
	"SA" 38+89 TO 53+83	1,901	593			392			51	REMOVE FROM EXISTING
	"S" 6+56 TO 25+15	1,558	1,926			800			116	REMOVE FROM EXISTING
	"SR 99" 269+57 TO 279+80			2,470	743				49	REMOVE FROM STAGE 1
	"KL1" 279+80 TO 297+49			2,370	862			31	50	REMOVE FROM STAGE 1
	"KR2" 297+24 TO 313+36			2,101	768				44	REMOVE FROM STAGE 1
	"B" 6+83 TO "K" 28+65			7,187	2,760			418	160	REMOVE FROM STAGE 1
"SA" 47+91 TO 56+27			3,153	1,121			126	71	REMOVE FROM STAGE 1	
"S" 14+55 TO 23+20			905						REMOVE FROM STAGE 1	
STAGE 3	"B" 5+49 TO "K" 29+80	3,201	967			1,236			79	REMOVE FROM EXISTING
	"SR 99" 265+89 TO 323+30			22,017	6,432				500	REMOVE FROM STAGE 2
	"KL1" 279+80 TO 297+72			2,260	1,336				78	REMOVE FROM STAGE 2
	"KL2" 297+95 TO 310+34			1,216	837			199	45	REMOVE FROM STAGE 2
	"KR1" 284+48 TO 297+84			1,647	1,028			182	58	REMOVE FROM STAGE 2
	"KR2" 297+24 TO 313+36			2,392	1,116				67	REMOVE FROM STAGE 2
	"B" 5+84 TO "K" 21+36			9,009	2,708			1,522	226	REMOVE FROM STAGE 2
"SA" 39+89 TO 56+27			4,889	1,684			294	109	REMOVE FROM STAGE 2	
STAGE 4	"SR 99" 282+13 TO 284+48			705					21	REMOVE FROM STAGE 3
	"KR1" 284+48 TO 297+84			1,346	1,285			170	54	REMOVE FROM STAGE 3
	"KL2" 297+95 TO 310+34			1,712	837			169	51	REMOVE FROM STAGE 3
	"B" 6+82 TO "K" 29+80			15,456	6,183			2,509	387	REMOVE FROM STAGE 3
TOTAL		22,589	7,844	106,952	42,210	6,119	102	12,833	3,344	

TRAFFIC HANDLING QUANTITIES THQ-2

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 ANDY CHAN
 CHUONG NGUYEN
 REVISOR
 KEITH G. MEYER
 CHECKED BY
 REVISED BY
 DATE REVISED

LAST REVISION DATE PLOTTED => \$DATE 05-01-12 TIME PLOTTED => \$TIME

**PAINT PAVEMENT MARKING
(TRAFFIC HANDLING PLAN)**

PLAN SHEET No.	TYPE I ARROW	TYPE II ARROW	TYPE III ARROW	TYPE IV ARROW	TYPE VI ARROW	12" LIMIT LINE	12" CROSSWALK	12" SOLID WHITE CHERVON	"STOP"	"BIKE"	"LANE"	BIKE LANE ARROW
	SQFT											
TH-4	31		168			72	31					
TH-5			168									
TH-6			42			35			22			
TH-7										5	6	3.5
TH-10			42									
TH-15	186	236	168			211	180					
TH-16			294		42		497					
TH-17			168	15		107		20	22	5	6	3.5
TH-18	31	90	630		294		209	128		5	6	3.5
TH-19					42							
TH-20			42		84							
TH-21			42									
TH-26	186	236	210			184	223					
TH-27			84				599					
TH-28			126			35			22			
TH-29			294				504			10	12	7
TH-30			84									
SUBTOTAL	434	562	2,562	15	462	644	2,242	148	66	25	30	17.5
TOTAL	7208											

TEMPORARY PAVEMENT

LOCATION	SIDE	HOT MIXED ASPHALT (HMA-A)	CLASS 2 AGGREGATE BASE	COLD PLANE AC PAVEMENT	REMARK
		TON	CY	SQYD	
"SR 99" 269+57 TO 291+80	Lt	649			STAGE 1
"SR 99" 265+89 TO 288+10	Rt	648			STAGE 1
"SR 99" 303+45 TO 323+30	Rt	580			STAGE 1
"KL1" 296+00 TO 297+30	Rt	85			STAGE 1
"K" 7+98 TO 8+02	Lt	11			STAGE 1
"KR2" 297+71 TO 298+90	Lt	86			STAGE 1
"K" 9+80 TO 14+94	Lt			31	STAGE 1
"KL1" 296+00 TO 297+37	Rt	146	138		STAGE 2
"K" 2+66 TO 3+70	Lt	219	208		STAGE 2
"K" 4+16 TO 4+98	Lt	165	157		STAGE 2
"K" 7+02 TO 7+75	Lt	148	141		STAGE 2
"K" 8+01 TO 10+00	Lt	275	262		STAGE 2
"K" 8+73 TO 17+83	Lt/Rt	406			STAGE 2
"K" 3+96 TO 4+71	Rt	225	214		STAGE 2
"K" 7+35 TO 7+94	Rt	28			STAGE 2
"KL2" 298+91 TO 299+50	Rt	58	55		STAGE 2
"K" 7+31 TO 7+90	Lt	20			STAGE 3
TEMPORARY SIDEWALK	Rt	33			STAGE 3
TOTAL		3,782*	1,175*	31*	

(*) QUANTITY INCLUDED IN SUMMARY OF QUANTITIES TABLES.

TEMPORARY HMA DIKE

LOCATION	Loc	HMA		REMARK
		LF	TON	
"K" 2+66 TO 3+48	Lt	84	2.4	STAGE 2
"K" 3+97 TO 4+97	Lt	100	2.9	STAGE 2
"K" 7+00 TO 8+05	Lt	105	3.0	STAGE 2
"K" 8+60 TO 10+00	Lt	145	4.1	STAGE 2
TOTAL		434*	12.4*	

(*) QUANTITY INCLUDED IN SUMMARY OF QUANTITIES TABLES.

THRIE BEAM BARRIER & END ANCHOR ASSEMBLY

LOCATION	REMOVE SINGLE THRIE BEAM BARRIER	TBB	END ANCHOR ASSEMBLY (TYPE CA)	REMARK
	LF		EA	
"SR 99" 295+15.50 TO 300+53.00	1,166		4	STAGE 1
"SR 99" 304+97.00 TO 305+97.00	200		4	STAGE 1
"SR 99" 295+15.50 TO 300+53.00		1,166		STAGE 4
"SR 99" 304+97.00 TO 305+97.00		200		STAGE 4
TOTAL	1,366	1,366	8*	

(*) QUANTITY INCLUDED IN SUMMARY OF QUANTITIES TABLES.

TEMPORARY CRASH CUSHION MODULE

PLAN SHEET No.	STAGE	TEMPORARY CRASH CUSHION ABSORB 350 (TYPE TL2) **	TEMPORARY CRASH CUSHION MODULE	ARRAY TYPE (N)	OBJECT MARKER (TYPE P) (N)
		EA	EA		EA
TH-3	1		14	TS14	P
TH-4	1	2	61	TS11/TS14	P
TH-5	1	1			P
TH-13	2		14	TS14	P
TH-15	2	2	72	TS11/TS14	P
TH-16	2		11	TS11	P
TH-22	2		28	TS14	P
TH-25	3		14	TS14	P
TH-26	3		22	TS11	P
TH-27	3		11	TS11	P
TH-33	3		11	TS11	P
TH-37	4	2			P
TOTAL		7	258		

(N) - NOT A SEPARATE PAY ITEM, FOR INFORMATION ONLY.

(**) - ABSORB 350 TYPE TL2 IS MANUFACTURED BY BARRIER SYSTEM, INC.

**TRAFFIC HANDLING QUANTITIES
THQ-3**

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	171	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE

9/04/12
PLANS APPROVAL DATE

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 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	172	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-18-12
 DATE

9/04/12
 PLANS APPROVAL DATE

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 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

TEMPORARY RAILING

STAGE	STATION	LOCATION	TEMPORARY RAILING (TYPE K)	TEMPORARY TRAFFIC SCREEN
			LF	LF
1	"SR 99" 269+80 TO 279+80	Lt	1,000	1,000
1	"SR 99" 295+08 TO 300+70	Lt	560	560
1	"SR 99" 296+08 TO 300+71	Lt	460	460
1	"SR 99" 295+00 TO 300+58	Rt	560	560
1	"SR 99" 287+61 TO 305+88	Rt	1,840	1,840
1	"SR 99" 313+30 TO 319+50	Rt	620	620
1	"KL1" 279+80 TO 296+77	Lt / Rt	1,700	1,700
1	"KR2" 298+51 TO 313+30	Lt / Rt	1,480	1,480
1	"K" 0+98 TO "KL2" 299+89	Lt	420	420
1	"K" 4+44 TO 7+44	Lt	300	300
2	"SR 99" 277+00 TO 289+99	Lt	1,300	1,300
2	"SR 99" 267+51 TO 284+50	Rt	1,700	1,620
2	"SR 99" 295+00 TO 296+00	Rt	100	100
2	"SR 99" 305+35 TO 317+70	Rt	1,240	1,160
2	"KR1" 284+50 TO "K" 10+58	Rt	1,540	1,540
2	"KR2" 298+59 TO 307+76	Lt / Rt	920	860
2	"KL2" 298+96 TO 305+56	Lt / Rt	660	580
2	"KL1" 289+08 TO 296+10	Lt	700	700
2	"K" 1+40 TO 3+00	Lt	160	160
2	"K" 4+66 TO 7+46	Lt	280	280
2	"K" 4+67 TO 7+46	Lt	280	280
2	"K" 4+35 TO 7+95	Rt	360	360
3	"KR1" 286+42 TO 296+83	Rt	1,040	960
3	"KL2" 299+48 TO 305+48	Lt / Rt	600	520
3	"K" 1+22 TO 2+82	Rt	160	160
3	"K" 4+65 TO 4+65	Lt	40	40
3	"K" 4+71 TO 7+31	Rt	260	220
3	"K" 6+86 TO 7+25	Lt	20	20
4	"K" 4+71 TO 7+31	Lt	260	260
4	"K" 4+70 TO 7+50	Rt	280	280
1-4	MISCELLANEOUS CONSTRUCTION		700	
TOTAL			21,540	20,340

CHANNELIZER (SURFACE MOUNTED)

PLAN SHEET No.	STAGE	CHANNEL IZER (SURFACE MOUNTED)
		EA
TH-3	1	6
TH-4	1	65
TH-5	1	27
TH-6	1	36
TH-7	1	85
TH-8	1	18
TH-9	1	21
TH-10	1	32
TH-11	1	2
TH-13	2	11
TH-14	2	38
TH-15	2	63
TH-16	2	80
TH-17	2	63
TH-18	2	20
TH-21	2	26
TH-22	2	41
TH-23	2	9
TH-25	3	20
TH-26	3	35
TH-27	3	6
TH-28	3	49
TH-29	3	37
TH-30	3	25
TH-33	3	8
TH-37	4	37
TH-39	4	7
TOTAL		867

SHOULDER RUMBLE STRIP (HMA, GROUND-IN INDENTATIONS)

LOCATION	Loc	HMA	REMARK
		Sta	
"SR 99" 269+57 TO 288+16	Lt	19	STAGE 4

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT SUPERVISOR
 ANDY CHAN
 CHUONG NGUYEN
 KEITH G. MEYER
 REVISIONS BY
 DATE REVISIONS
 REVISIONS BY
 DATE REVISIONS
 REVISIONS BY
 DATE REVISIONS



LAST REVISION DATE PLOTTED => \$DATE
 06-08-12 TIME PLOTTED => \$TIME

TRAFFIC HANDLING QUANTITIES THQ-4

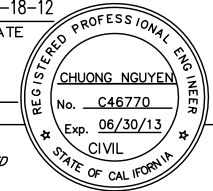
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: ANDY CHAN
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: ANDY CHAN
 DATE REVISED: CHUONG NGUYEN

CONSTRUCTION AREA SIGNS (TRAFFIC HANDLING PLAN)

PLAN SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (IN)	NUMBER OF POST & SIZE (IN)		SIGN MESSAGE	REMARK
				4 x 6	6 x 6		
TH-2	1	W3-5a	48" x 48"	1		55 MPH SPEED ZONE AHEAD	
	2	R2-4 (CA)	48" x 60"		1	55 MPH ZONE AHEAD	
	3	PCMS				CONSTRUCTION AHEAD	
	4	R2-1	48" x 48"	1		SPEED LIMIT (55 MPH)	
		C40 (CA)	144" x 60"			TRAFFIC LINES DOUBLED IN CONSTRUCTION ZONES	
	5	PCMS				SPEED LIMIT (55 MPH)	
	6	PCMS				SPEED LIMIT (55 MPH)	
TH-3	1	R3	48" x 60"		1	END SPEED LIMIT	
TH-4	1	R2-1	48" x 48"	1		SPEED LIMIT (55 MPH)	
	1	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
	2	C31A (CA)	48" x 48"	1		NO SHOULDER	
	3	C31A (CA)	48" x 48"	1		NO SHOULDER	
	4	C31A (CA)	48" x 48"	1		NO SHOULDER	
	5	R9-11	48" x 36"			SIDEWALK CLOSED AHEAD, CROSS HERE	ON TYPE II BARRICADE
	6	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
		M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY	
		M3-3	36" x 18"			SOUTH	
	7	R5-1	36" x 36"		1	DO NOT ENTER	
		R5-1a	36" x 24"			WRONG WAY	
		R6-1	36" x 12"			ONE WAY	
	8	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
M6-2		21" x 15"			DIRECTIONAL ARROW AUXILIARY		
M3-1		36" x 18"			NORTH		
TH-5	1	PCMS				CONSTRUCTION AHEAD	
	2	PCMS				SPEED LIMIT (30 MPH)	
	3	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
	4	R3 (CA)	48" x 60"	1		END SPEED LIMIT	
	5	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	6	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
	7	R3 (CA)	48" x 60"	1		END SPEED LIMIT	
	8	R9-11	48" x 36"			SIDEWALK CLOSED AHEAD, CROSS HERE	ON TYPE II BARRICADE
	9	R9-11a	48" x 24"			SIDEWALK CLOSED, CROSS HERE	ON TYPE II BARRICADE
TH-6	1	G21-3 (CA)			2	FREEWAY SOUTH LEFT LANE NORTH RIGHT LANE	
	2	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	3	R1-1	30" x 30"	1		STOP	
	4	C31A (CA)	48" x 48"	1		NO SHOULDER	
	5	R3 (CA)	48" x 60"	1		END SPEED LIMIT	

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	173	423

05-18-12
 REGISTERED CIVIL ENGINEER DATE
 9/04/12
 PLANS APPROVAL DATE



THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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TRAFFIC HANDLING QUANTITIES THQ-5

LAST REVISION: 06-08-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

CONSTRUCTION AREA SIGNS (TRAFFIC HANDLING PLAN)

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	174	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-18-12
 DATE

9/04/12
 PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OR COMPLETENESS OF SCANNED
 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

PLAN SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (IN)	NUMBER OF POST & SIZE (IN)		SIGN MESSAGE	REMARK
				4 x 6	6 x 6		
TH-7	1	G8-7 (CA)	48" x 12"	2		↑ 99 FREEWAY KIERNAN CT →	
	2	R9-11a	48" x 24"			SIDEWALK CLOSED, CROSS HERE	ON TYPE II BARRICADE
	3	C31A (CA)	48" x 48"	1		NO SHOULDER	
	4	R3 (CA)	48" x 60"	1		END SPEED LIMIT	
	5	C31A (CA)	48" x 48"	1		NO SHOULDER	
	6	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	7	C31A (CA)	48" x 48"	1		NO SHOULDER	
TH-8	1	PCMS				SPEED LIMIT (30 MPH)	
	2	PCMS				CONSTRUCTION AHEAD	
	3	R3 (CA)	48" x 60"	1		END SPEED LIMIT	
TH-9	1	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	2	R3 (CA)	48" x 60"	1		END SPEED LIMIT	
TH-10	1	C31A (CA)	48" x 48"	1		NO SHOULDER	
	2	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	3	R3 (CA)	48" x 60"	1		END SPEED LIMIT	
TH-11	1	C30A (CA)	48" x 48"	1		SHOULDER CLOSED	
	2	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
TH-12	1	PCMS				SPEED LIMIT (55 MPH)	
	2	PCMS				SPEED LIMIT (55 MPH)	
	3	R2-1	48" x 48"	1		SPEED LIMIT (55 MPH)	
		C40 (CA)	144" x 60"			TRAFFIC FINES DOUBLED IN CONSTRUCTION ZONES	
	4	PCMS				CONSTRUCTION AHEAD	
	5	R2-4 (CA)	48" x 60"	1		55 MPH ZONE AHEAD	
	6	W3-5a	48" x 48"	1		55 MPH SPEED ZONE AHEAD	
7	R3 (CA)	48" x 60"	1		END SPEED LIMIT		
TH-13	1	PCMS				SPEED LIMIT (55 MPH)	
	2	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
	3	C31A (CA)	48" x 48"	1		NO SHOULDER	
	4	R2-1	48" x 48"	1		SPEED LIMIT (55 MPH)	
	5	W4-7	48" x 48"	1		THRU TRAFFIC MERGE LEFT	
TH-14	1	W4-3	48" x 48"	1		ADDED LANE (SYMBOL)	
	2	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	3	C31A (CA)	48" x 48"	1		NO SHOULDER	
TH-15	1	C31A (CA)	48" x 48"	1		NO SHOULDER	
	2	G77-4 (CA)	54" x 54"	2		↑ NORTH 99 SOUTH →	
	3	R5-1	36" x 36"	1		DO NOT ENTER	ON TYPE III BARRICADE
	4	R61-32 (CA)	36" x 30"	1		INTERSECTION LANE CONTROL	
	5	C31A (CA)	48" x 48"	1		NO SHOULDER	
	6	C31A (CA)	48" x 48"	1		NO SHOULDER	
	7	R61-32 (CA)	36" x 30"	1		INTERSECTION LANE CONTROL	
	8	R5-1	36" x 36"	1		DO NOT ENTER	ON TYPE III BARRICADE
	9	G92 (CA)	48" x 30"			FREEWAY ENTRANCE	
G28-2 (CA)		24" x 24"	1		ROUTE MARKER 99 CALIFORNIA		
M6-2		21" x 15"			DIRECTIONAL ARROW AUXILIARY		
M3-3		36" x 18"			SOUTH		

TRAFFIC HANDLING QUANTITIES

THQ-6

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans

REVISED BY
 DATE REVISED

ANDY CHAN
 CHUONG NGUYEN

CALCULATED-
 DESIGNED BY
 CHECKED BY

CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

DEPARTMENT OF TRANSPORTATION

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

**CONSTRUCTION AREA SIGNS
(TRAFFIC HANDLING PLAN)**

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	175	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OR COMPLETENESS OF SCANNED
 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

PLAN SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (IN)	NUMBER OF POST & SIZE (IN)		SIGN MESSAGE	REMARK
				4 x 6	6 x 6		
TH15	10	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
		M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY	
		M3-3	36" x 18"			SOUTH	
	11	R5-1	36" x 36"		1	DO NOT ENTER	
		R5-1a	36" x 24"			WRONG WAY	
		R6-1	36" x 12"			ONE WAY	
	12	R5-1	36" x 36"		1	DO NOT ENTER	
		R5-1a	36" x 24"			WRONG WAY	
		R6-1	36" x 12"			ONE WAY	
	13	R5-1	36" x 36"		1	DO NOT ENTER	
		R5-1a	36" x 24"			WRONG WAY	
	14	R6-1	36" x 12"		1	ONE WAY	
		R5-1	36" x 36"			DO NOT ENTER	
		R5-1a	36" x 24"			WRONG WAY	
	15	R6-1	36" x 12"		1	ONE WAY	
		G92 (CA)	48" x 30"			FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
		M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY	
	16	M3-1	36" x 18"		1	NORTH	
		G92 (CA)	48" x 30"			FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
		M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY	
	TH-16	1	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD
2		R9-11a	48" x 24"			SIDEWALK CLOSED, CROSS HERE	ON TYPE II BARRICADE
TH-17	1	R9-3	18" x 18"			NO PEDESTRIAN CROSSING	ON TYPE II BARRICADE
	2	R3-5 (R)	30" x 36"	1		(RIGHT) TURN ONLY	
		R1-1	30" x 30"		STOP		
3	R1-1	30" x 30"	1		STOP		
TH-18	1	R9-11	48" x 36"			SIDEWALK CLOSED AHEAD, CROSS HERE	ON TYPE II BARRICADE
	2	R9-11a	48" x 24"			SIDEWALK CLOSED, CROSS HERE	ON TYPE II BARRICADE
TH-21	1	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
TH-22	1	C31A (CA)	48" x 48"	1		NO SHOULDER	
	2	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	3	C31A (CA)	48" x 48"	1		NO SHOULDER	
	4	C12 (CA)	48" x 48"	1		NARROW LANE	
	5	W4-1	48" x 48"	1		MERGING TRAFFIC	
TH-25	1	R2-1	48" x 48"	1		SPEED LIMIT (30 MPH)	
	2	C31A (CA)	48" x 48"	1		NO SHOULDER	
	3	G84-3 (CA)	48" x 60"		1	EXIT 233 WITH ARROW	

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT SUPERVISOR
 REVISIONS: 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.

**TRAFFIC HANDLING QUANTITIES
THQ-7**

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 06-08-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

**CONSTRUCTION AREA SIGNS
 (TRAFFIC HANDLING PLAN)**

PLAN SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (IN)	NUMBER OF POST & SIZE (IN)		SIGN MESSAGE	REMARK
				4 x 6	6 x 6		
TH-26	1	R11-2	48" x 30"			ROAD CLOSED	ON TYPE III BARRICADE
	2	R5-2	36" x 36"			DO NOT ENTER	ON TYPE III BARRICADE
		R11-2	48" x 30"			ROAD CLOSED	ON TYPE III BARRICADE
	3	R5-2	36" x 36"			DO NOT ENTER	ON TYPE III BARRICADE
		R11-2	48" x 30"			ROAD CLOSED	ON TYPE III BARRICADE
	4	R11-2	48" x 30"			ROAD CLOSED	ON TYPE III BARRICADE
	5	PCMS				LANE CLOSED	
	6	PCMS				LANE CLOSED	
	7	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
		M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY	
		M3-3	36" x 18"			SOUTH	
	8	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE	
		G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA	
		M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY	
		M3-3	36" x 18"			SOUTH	
	9	R5-1	36" x 36"		1	DO NOT ENTER	
		R5-1a	36" x 24"			WRONG WAY	
		R6-1	36" x 12"			ONE WAY	
	10	R5-1	36" x 36"		1	DO NOT ENTER	
R5-1a		36" x 24"		WRONG WAY			
R6-1		36" x 12"		ONE WAY			
11	R5-1	36" x 36"		1	DO NOT ENTER		
	R5-1a	36" x 24"			WRONG WAY		
	R6-1	36" x 12"			ONE WAY		
12	R5-1	36" x 36"		1	DO NOT ENTER		
	R5-1a	36" x 24"			WRONG WAY		
	R6-1	36" x 12"			ONE WAY		
13	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE		
	G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA		
	M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY		
	M3-1	36" x 18"			NORTH		
14	G92 (CA)	48" x 30"		1	FREEWAY ENTRANCE		
	G28-2 (CA)	24" x 24"			ROUTE MARKER 99 CALIFORNIA		
	M6-2	21" x 15"			DIRECTIONAL ARROW AUXILIARY		
	M3-1	36" x 18"			NORTH		

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	176	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

**TRAFFIC HANDLING QUANTITIES
 THQ-8**

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	177	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

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 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OR COMPLETENESS OF SCANNED
 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

**CONSTRUCTION AREA SIGNS
(TRAFFIC HANDLING PLAN)**

PLAN SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (IN)	NUMBER OF POST & SIZE (IN)		SIGN MESSAGE	REMARK
				4 x 6	6 x 6		
TH-27	1	R11-2	48" x 30"			ROAD CLOSED	ON TYPE III BARRICADE
	2	R9-11	48" x 36"			SIDEWALK CLOSED AHEAD, CROSS HERE	ON TYPE II BARRICADE
	3	G77-4 (CA)	54" x 54"	2		↑ NORTH Ⓣ SOUTH →	
TH-29	1	R9-11	48" x 36"			SIDEWALK CLOSED AHEAD, CROSS HERE	ON TYPE II BARRICADE
	2	R11-2	48" x 30"			ROAD CLOSED	ON TYPE III BARRICADE
TH-30	1	C31A (CA)	48" x 48"	1		NO SHOULDER	
	2	PCMS				SPEED LIMIT (30 MPH)	
	3	PCMS				CONSTRUCTION AHEAD	
TH-37	1	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
	2	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
TH-38	1	C24 (CA)	48" x 48"	1		SHOULDER WORK AHEAD	
TH-39	1	C31A (CA)	48" x 48"	1		NO SHOULDER	

**TRAFFIC HANDLING QUANTITIES
THQ-9**

LAST REVISION
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	178	423

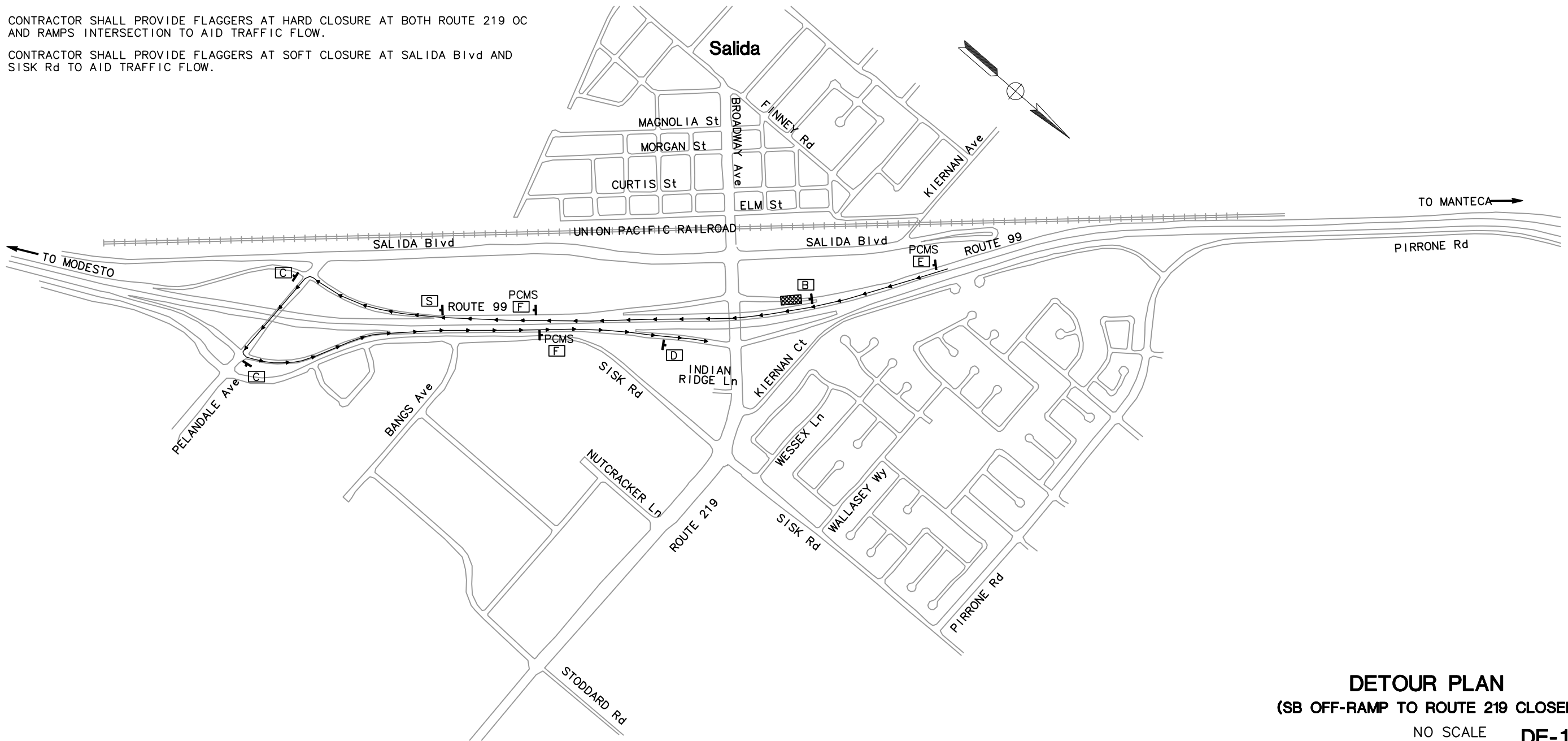
<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTES:

- LOCATION OF DETOUR SIGNS SHOWN ARE APPROXIMATE. EXACT LOCATION TO BE DETERMINED BY ENGINEER.
- OVERLAYS FOR ADVANCE INFORMATION SIGNS TO BE INSTALLED AT LEAST SEVEN (7) DAYS PRIOR TO ROAD CLOSURE AND UNDER THE DIRECTION OF THE ENGINEER.
- ALL EXISTING SIGNS IN CONFLICT WITH TRAFFIC HANDLING PLANS SHALL BE COVERED OR REMOVED UNDER THE DIRECTION OF THE ENGINEER.
- SEE CONSTRUCTION AREA SIGN PLAN AND TRAFFIC HANDLING PLANS FOR MORE CONSTRUCTION AREA SIGNS.
- CONTRACTOR SHALL FOLLOW CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR TRAFFIC CONTROL.
- CONTRACTOR SHALL PROVIDE FLAGGERS AT HARD CLOSURE AT BOTH ROUTE 219 OC AND RAMPS INTERSECTION TO AID TRAFFIC FLOW.
- CONTRACTOR SHALL PROVIDE FLAGGERS AT SOFT CLOSURE AT SALIDA Blvd AND SISK Rd TO AID TRAFFIC FLOW.

LEGEND

- [A] DETOUR AREA SIGN LETTER
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- [X] CLOSED FACILITY
- DIRECTION OF DETOUR TRAFFIC



DETOUR PLAN
(SB OFF-RAMP TO ROUTE 219 CLOSED)
NO SCALE **DE-1**

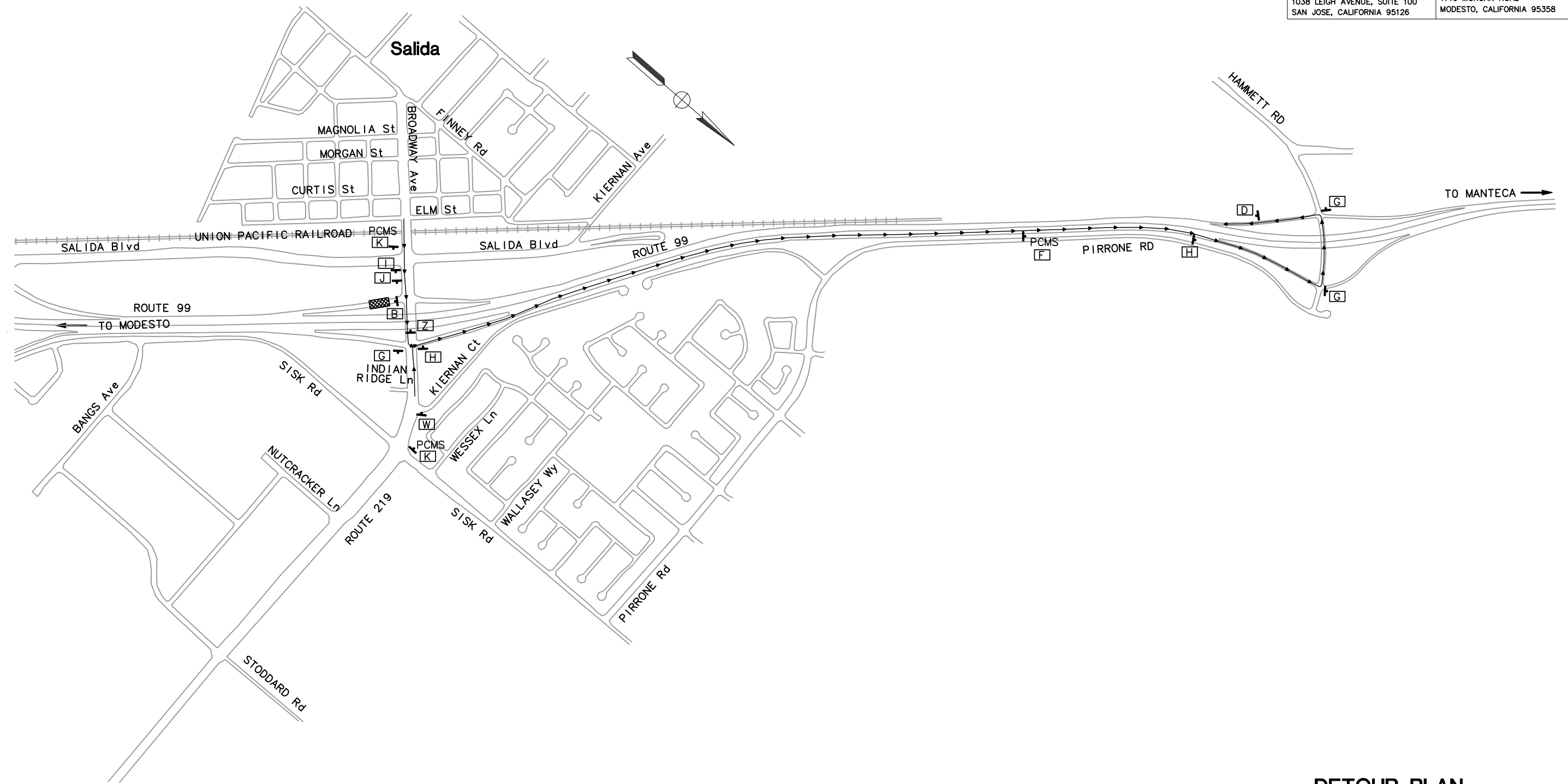
APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED—DESIGNED BY: [blank] CHECKED BY: [blank]
 DAVID LIU CHUONG NGUYEN
 REVISED BY: [blank] DATE REVISED: [blank]

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

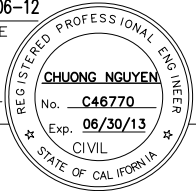
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	179	423
			REGISTERED CIVIL ENGINEER CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA		
			REGISTERED PROFESSIONAL ENGINEER CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA		
			PLANS APPROVAL DATE: 9/04/12 DATE: 05-06-12		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



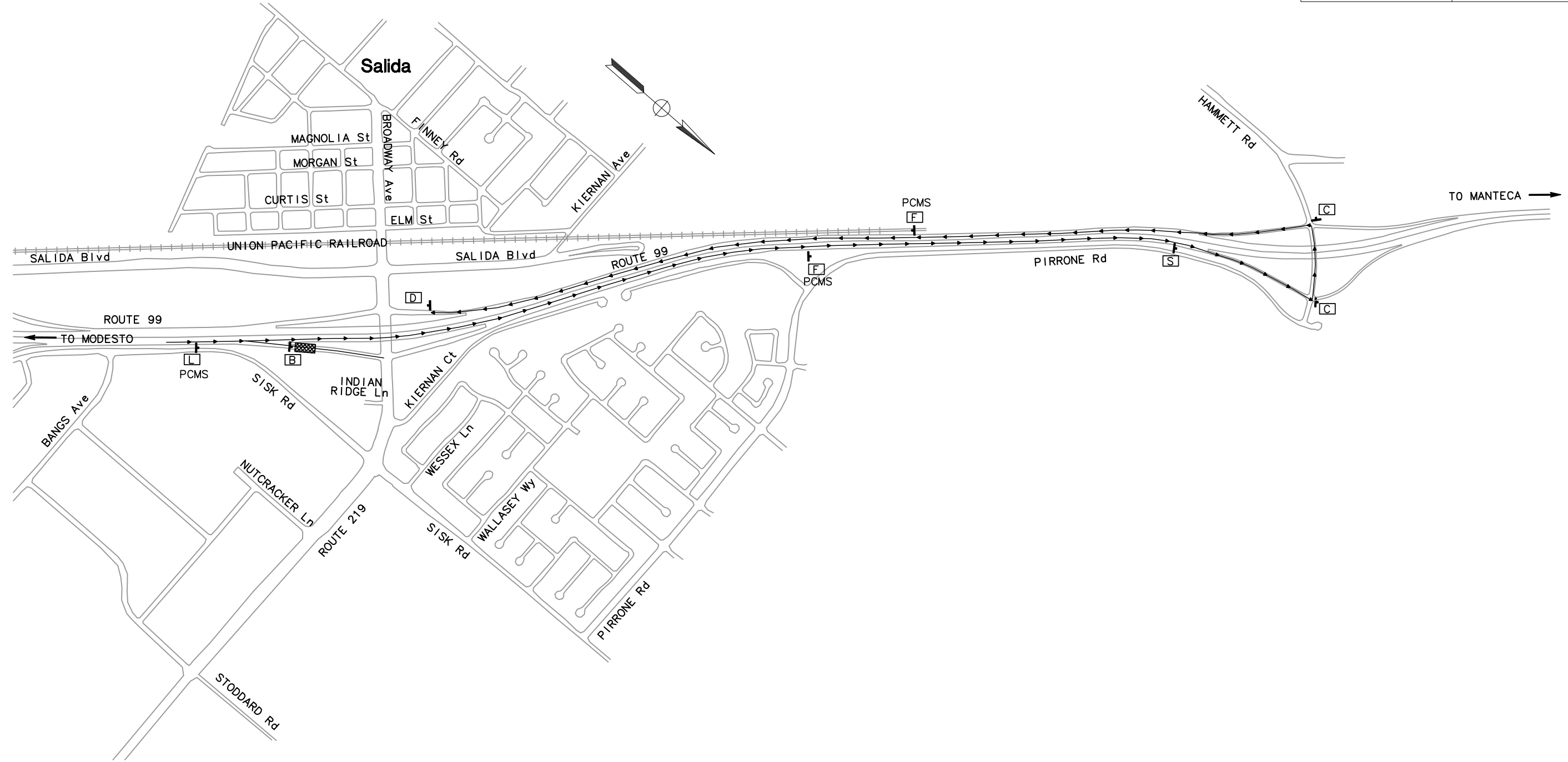
DETOUR PLAN
 (SB ON-RAMP FROM ROUTE 219 CLOSED)
 NO SCALE **DE-2**

APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

LAST REVISION: 05-01-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	180	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
Caltrans	KEITH G. MEYER	CHECKED BY	DAVID LIU
			CHUONG NGUYEN
			DATE REVISED



DETOUR PLAN
 (NB OFF-RAMP TO ROUTE 219 CLOSED)

NO SCALE **DE-3**

APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 05-01-12

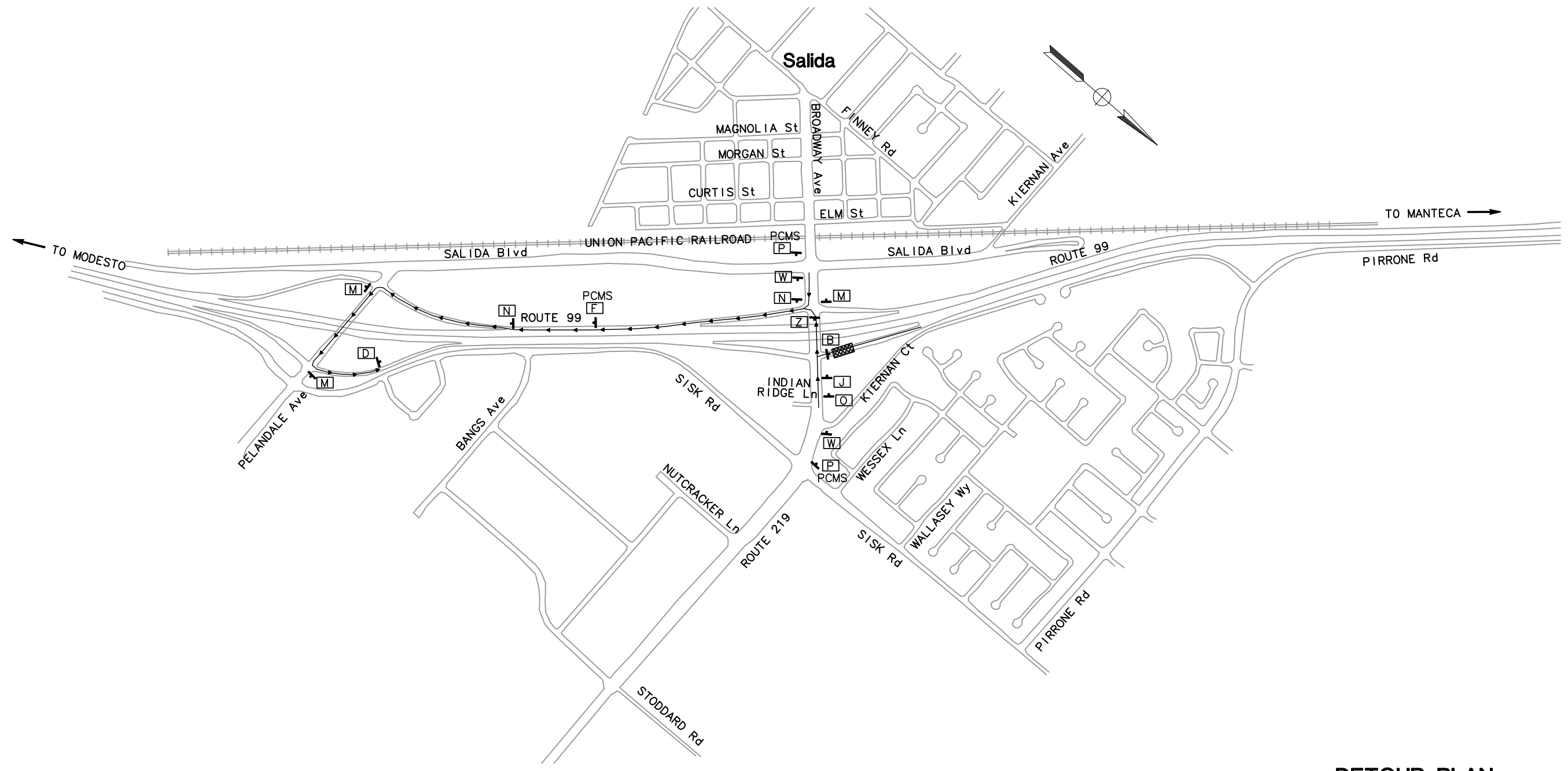
STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	181	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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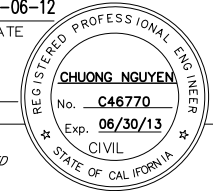


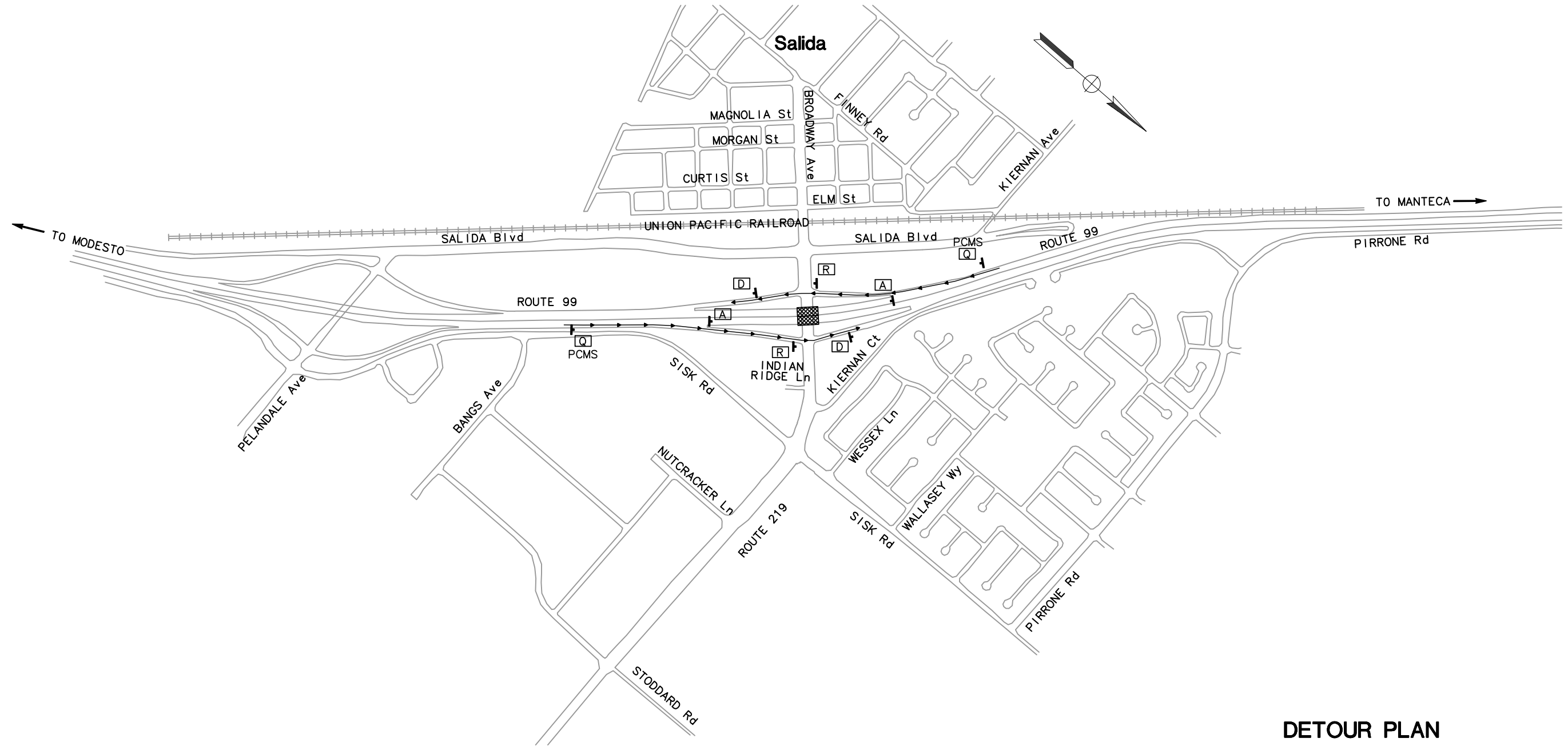
DETOUR PLAN
 (NB ON-RAMP FROM ROUTE 219 CLOSED)
 NO SCALE **DE-4**

APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 REVISIONS: DAVID LIU, CHUONG NGUYEN, REVISOR, DATE

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	182	423
		 REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		




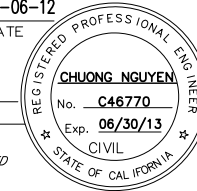
DETOUR PLAN
 (BRIDGE REMOVAL/PRECAST-PRESTRESSED GIRDER AND DECK FORM PLACEMENT/DECK FORM REMOVAL FOR ROUTE 99 CLOSURE.)

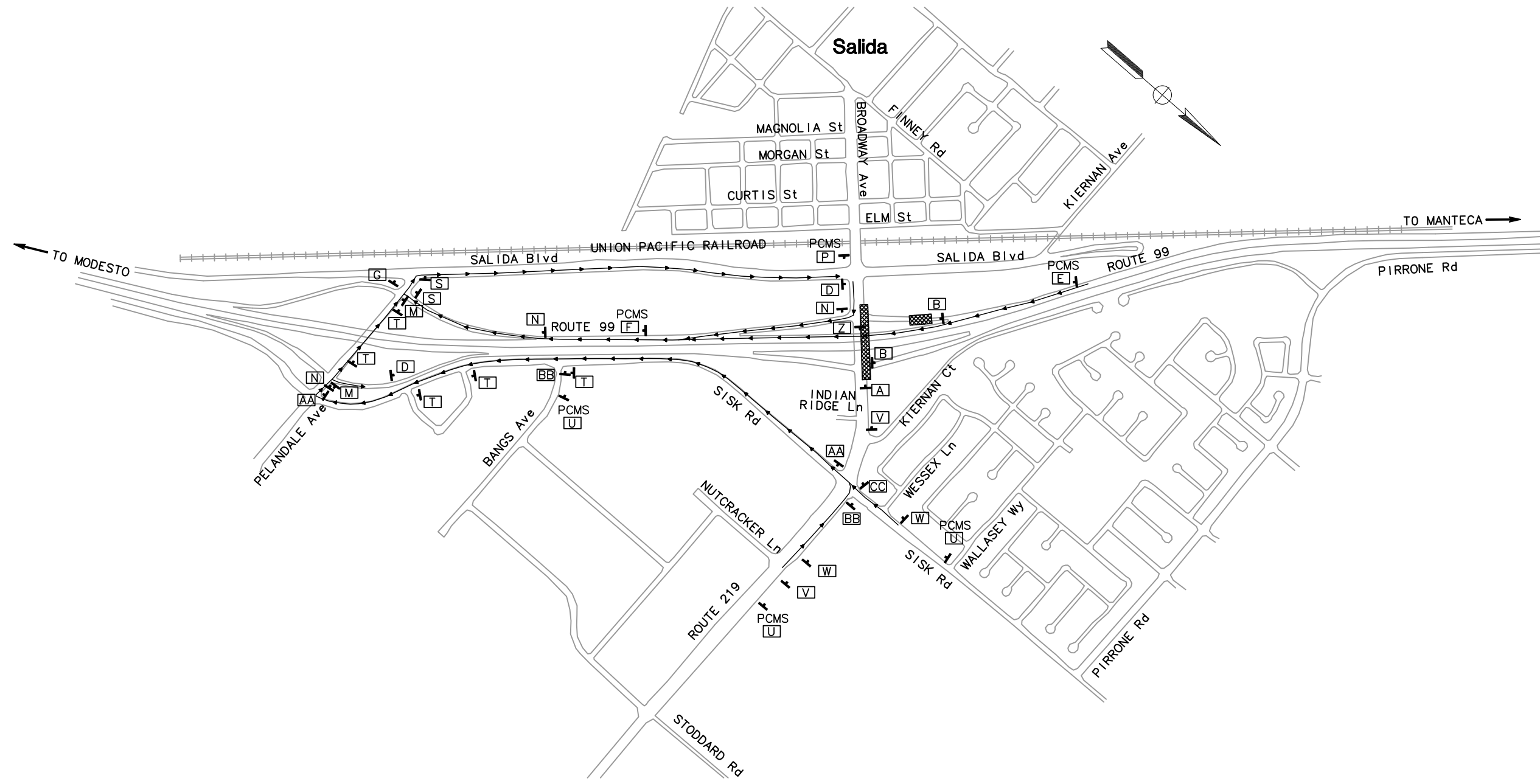
NO SCALE **DE-5**

APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: []
 DATE REVISED: []

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	183	423
		 REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



DETOUR PLAN
 (WB ROUTE 219 CLOSED
 FROM SB OFF-RAMP TO NB ON-RAMP)

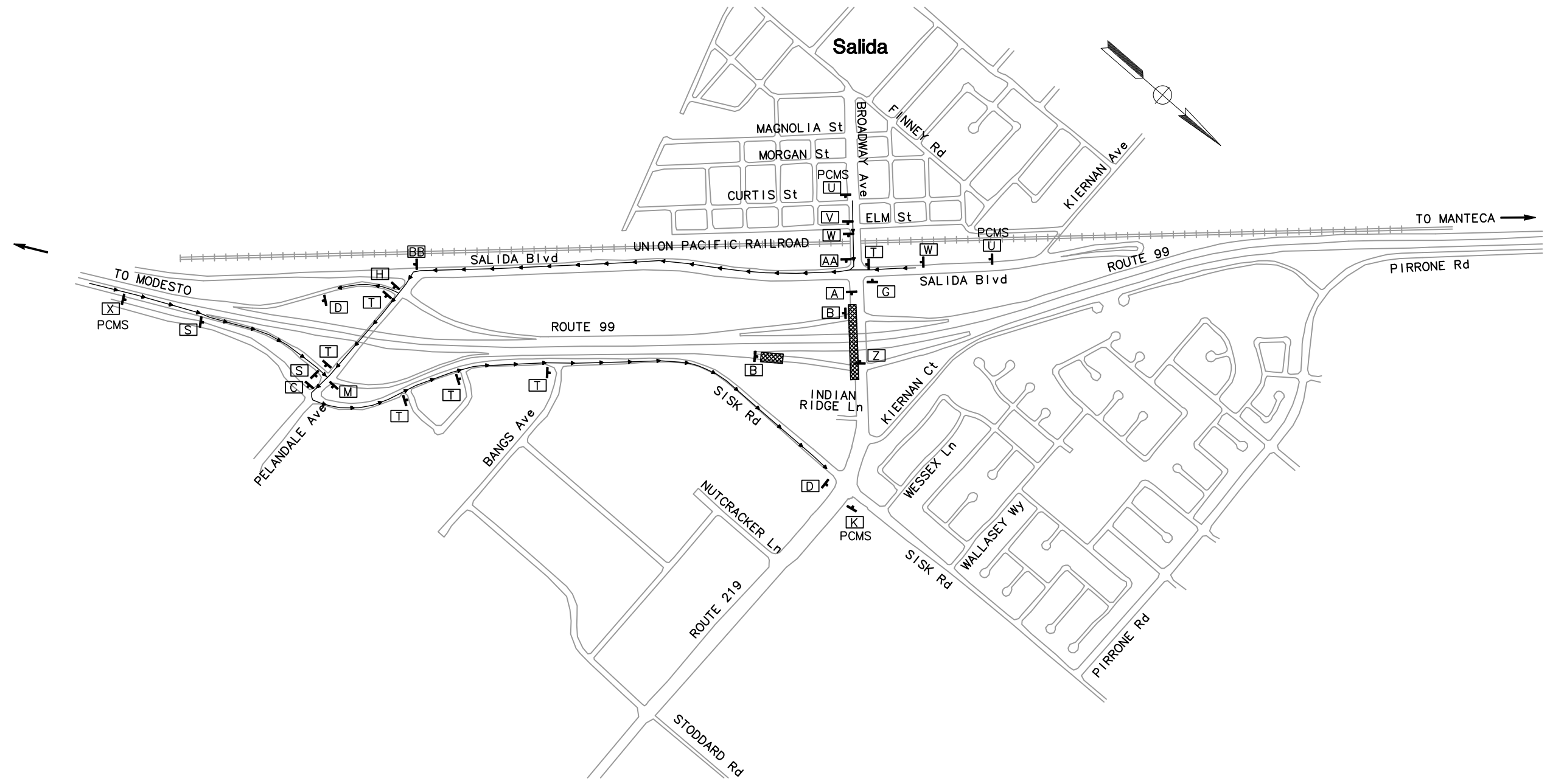
NO SCALE **DE-6**

APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED—DESIGNED BY: DAVID LIU
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: REVISOR: DATE
 REVISOR: DATE

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	184	423
			REGISTERED CIVIL ENGINEER	DATE	
			9/04/12		
			PLANS APPROVAL DATE		
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



DETOUR PLAN
(EB ROUTE 219 CLOSED
FROM SB ON-RAMP TO NB OFF-RAMP)

NO SCALE **DE-7**

APPROVED FOR DETOUR CONSTRUCTION WORK ONLY

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 DAVID LIU
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	185	423

REGISTERED CIVIL ENGINEER *Chuong Nguyen* DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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DETOUR SIGN QUANTITIES

SIGN NO.	SIGN CODE	PANEL SIZE	SIGN MESSAGE	No. OF POST AND SIZE	EACH	REMARKS
A	R11-2	48" x 30"	ROAD CLOSED	N/A	4	MOUNTED ON 3 TYPE III BARRICADE
B	SC6-4(CA)	48" x 60"	RAMP CLOSED (DATE, MONTH DATE) THRU (DATE, MONTH, DATE)	2-4" x 6"	8	SIGN SHALL BE POSTED AT LEAST 7 DAYS IN ADVANCE
C	G28-2(CA) M4-10(L)	36" x 36" 48" x 18"	ROUTE 219 DETOUR (LEFT ARROW)	1-4" x 6"	7	
D	M4-8A	24" x 18"	END DETOUR	1-4" x 6"	10	
E	PCMS	N/A	SB OFF RAMP CLOSED AT ROUTE 219	N/A	2	
F	PCMS	N/A	ROUTE 219 DETOUR USE NEXT EXIT RIGHT ARROW	N/A	7	
G	G28-2(CA) M3-3 M4-10(L)	36" x 36" 30" x 15" 48" x 18"	ROUTE 99 SOUTH DETOUR (LEFT ARROW)	1-6" x 6"	5	
H	G28-2(CA) M3-3 M4-10(R)	36" x 36" 30" x 15" 48" x 18"	ROUTE 99 SOUTH DETOUR (RIGHT ARROW)	1-6" x 6"	3	
I	G28-2(CA) M3-3 SC3(CA)	36" x 36" 30" x 15" 48" x 18"	ROUTE 99 SOUTH DETOUR (UP ARROW)	1-6" x 6"	1	
J	R3-1 SC3(CA)	30" x 30" 48" x 18"	NO RIGHT TURN DETOUR (UP ARROW)	1-4" x 6"	2	
K	PCMS	N/A	SB ON RAMP CLOSED AT ROUTE 219	N/A	3	
L	PCMS	N/A	NB OFF RAMP CLOSED AT ROUTE 219	N/A	1	
M	G28-2(CA) M3-1 M4-10(L)	36" x 36" 30" x 15" 48" x 18"	ROUTE 99 NORTH DETOUR (LEFT ARROW)	1-6" x 6"	6	
N	G28-2(CA) M3-1 M4-10(R)	36" x 36" 30" x 15" 48" x 18"	ROUTE 99 NORTH DETOUR (RIGHT ARROW)	1-6" x 6"	4	
O	G28-2(CA) M3-1 SC3(CA)	36" x 36" 30" x 15" 48" x 18"	ROUTE 99 NORTH DETOUR (UP ARROW)	1-6" x 6"	1	
P	PCMS	N/A	NB ON RAMP CLOSED AT ROUTE 219	N/A	3	
Q	PCMS	N/A	ROUTE 99 CLOSED USE NEXT EXIT	N/A	2	
R	SC3(CA)	48" x 18"	DETOUR (UP ARROW)	1-4" x 6"	2	
S	G28-2(CA) M4-10(R)	36" x 36" 48" x 18"	ROUTE 219 DETOUR (RIGHT ARROW)	1-4" x 6"	8	
T	G28-2(CA) SC3(CA)	48" x 18" 48" x 18"	ROUTE 219 DETOUR (UP ARROW)	1-4" x 6"	11	
U	PCMS	N/A	ROUTE 219 CLOSED AT ROUTE 99 USE ALT. ROUTE	N/A	5	
V	W20-3	36" x 36"	ROAD CLOSED AHEAD	1-4" x 6"	3	
W	W20-2	36" x 36"	DETOUR AHEAD	1-4" x 6"	8	
X	PCMS	N/A	NB OFF RAMP CLOSED AT ROUTE 219 USE PELANDALE AVE EXIT	N/A	1	
Z	R11-2	48" x 30"	ROAD CLOSED	N/A	4	MOUNTED ON 3 TYPE III BARRICADE TO CLOSE LEFT TURN LANE TO ON RAMPS.

NOTE: ALL CONSTRUCTION AREA SIGNS AND PCMS ARE NOT SEPARATE PAY ITEM, FOR INFORMATION ONLY.

DETOUR SIGN QUANTITIES

SIGN NO.	SIGN CODE	PANEL SIZE (in x in)	SIGN MESSAGE	No. OF POST AND SIZE	EACH	REMARKS
AA	G28-2(CA) G28-2(CA) M4-10(R)	36" x 36" 36" x 36" 48" x 18"	ROUTE 219 ROUTE 99 DETOUR (RIGHT ARROW)	1-4" x 6"	2	
BB	G28-2(CA) G28-2(CA) M4-10(L)	36" x 36" 36" x 36" 48" x 18"	ROUTE 219 ROUTE 99 DETOUR (LEFT ARROW)	1-4" x 6"	2	
CC	G28-2(CA) G28-2(CA) SC3(CA)	36" x 36" 36" x 36" 48" x 18"	ROUTE 219 ROUTE 99 DETOUR (UP ARROW)	1-4" x 6"	1	

DETOUR PLAN
 NO SCALE **DE-8**

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	186	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE
9/04/12 PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTES:

- REMOVE EXISTING DELINEATORS WHERE NEW DELINEATORS ARE SHOWN ON THE PLANS.
- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

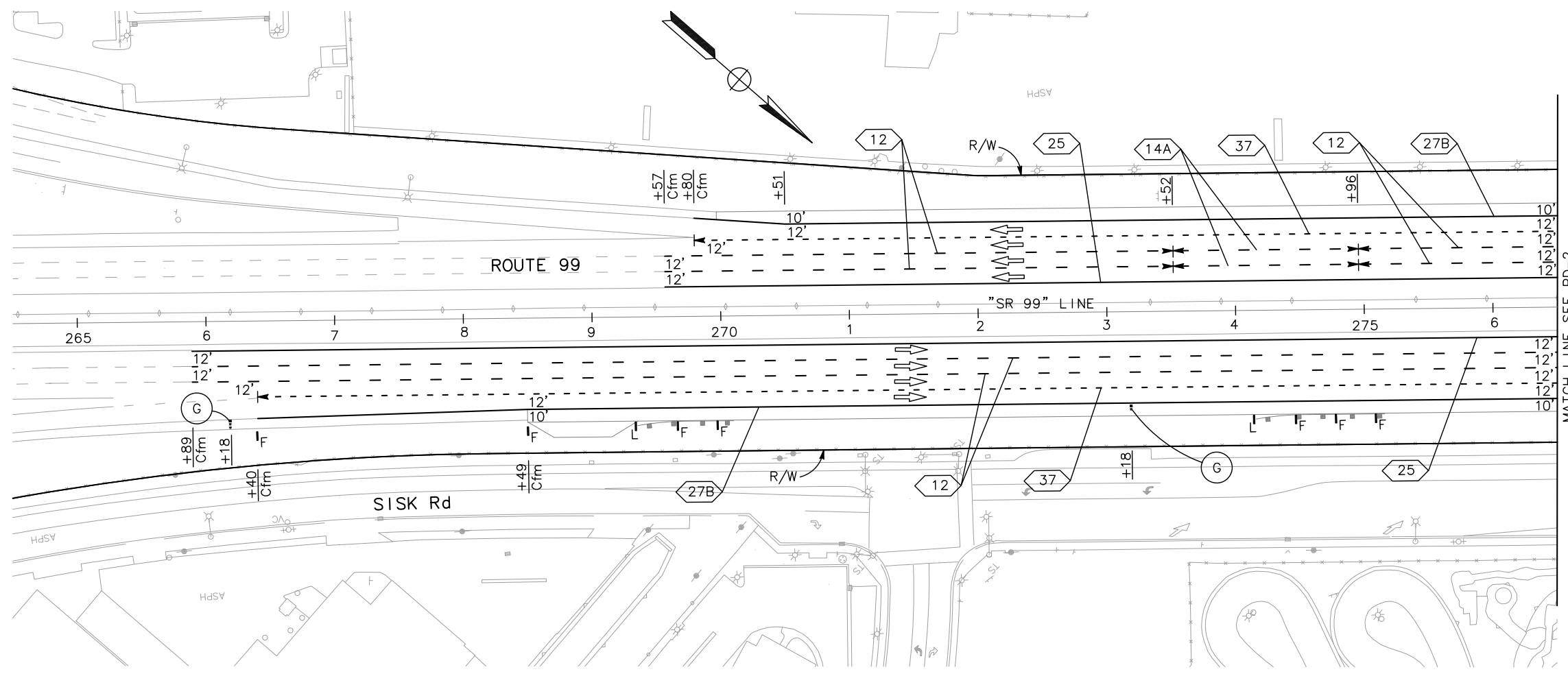
LEGEND

- PAVEMENT DELINEATION DETAIL
- LIMIT OF PAVEMENT DELINEATION DETAILS
- TYPE I 24'-0" ARROW
- TYPE II (R OR L) ARROW
- TYPE III (R OR L) ARROW
- TYPE VI (R OR L) ARROW
- HOV
- OBJECT MARKER (TYPE L)
- CLASS ONE DELINEATOR (TYPE F OR G)
- TRAFFIC DIRECTION
- TYPE G PAVEMENT MARKER

ABBREVIATIONS:

- Cfm CONFORM
- CW CROSSWALK
- LL LIMIT LINE

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	DESIGNED BY	REVISOR
Caltrans	KEITH G. MEYER	ANDY CHAN	CHUONG NGUYEN
	CHECKED BY	DATE REVISED	



PAVEMENT DELINEATION PLAN

SCALE 1" = 50' PD-1

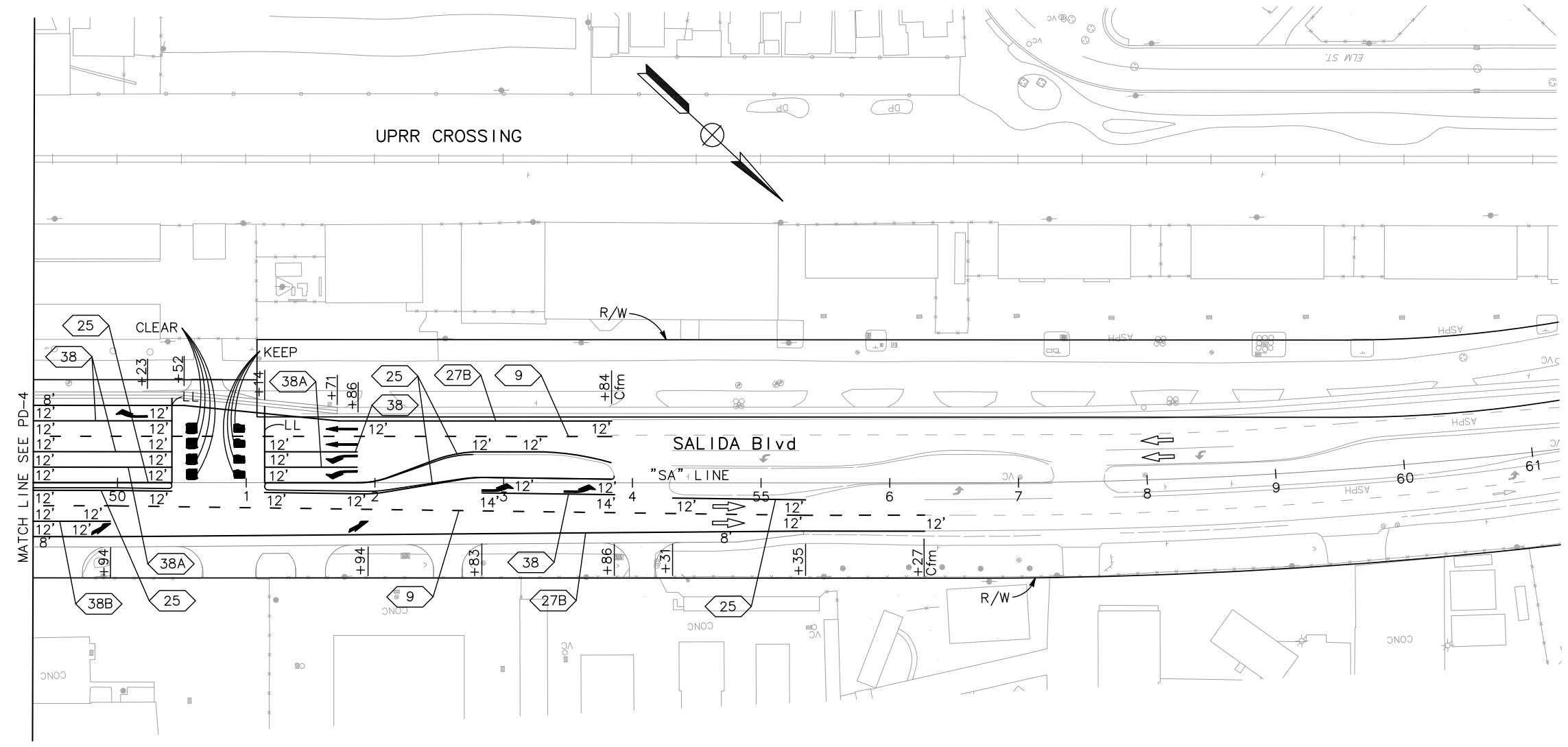
APPROVED FOR PAVEMENT DELINEATION WORK ONLY

LAST REVISION 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	188	423
			<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE	
			9/04/12 PLANS APPROVAL DATE		
			<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
FOR ACCURATE RIGHT OF WAY AND ACCESS DATA,
CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
	KEITH G. MEYER	CHECKED BY	ANDY CHAN
			CHUONG NGUYEN
			DATE REVISED




PAVEMENT DELINEATION PLAN
SCALE 1" = 50' **PD-3**

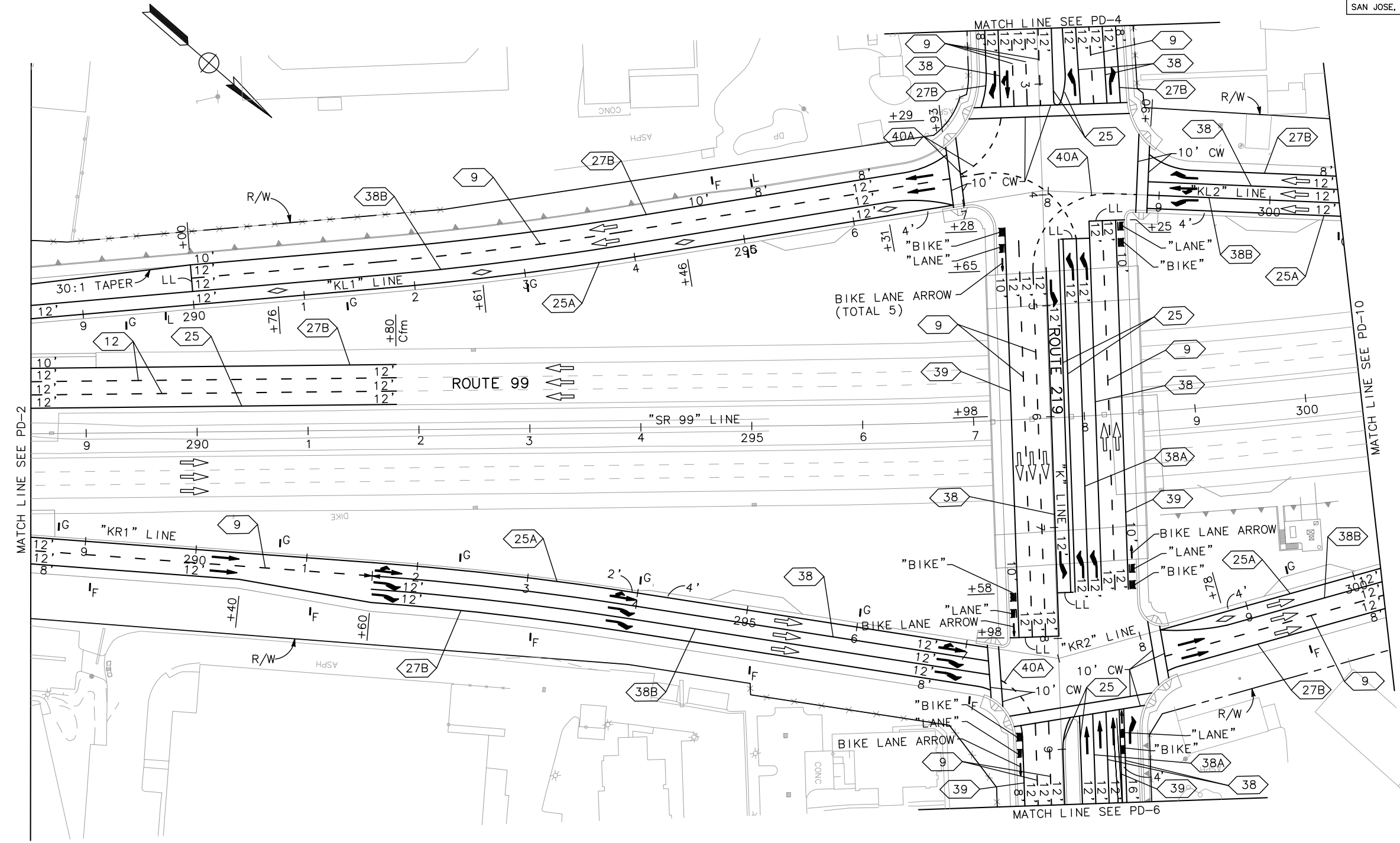
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LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	190	423


 REGISTERED CIVIL ENGINEER		05-06-12 DATE
9/04/12 PLANS APPROVAL DATE		
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NOTE:
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CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



PAVEMENT DELINEATION PLAN
SCALE 1" = 50' **PD-5**

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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
	KEITH G. MEYER	CHECKED BY	ANDY CHAN
			CHUONG NGUYEN
			DATE REVISED

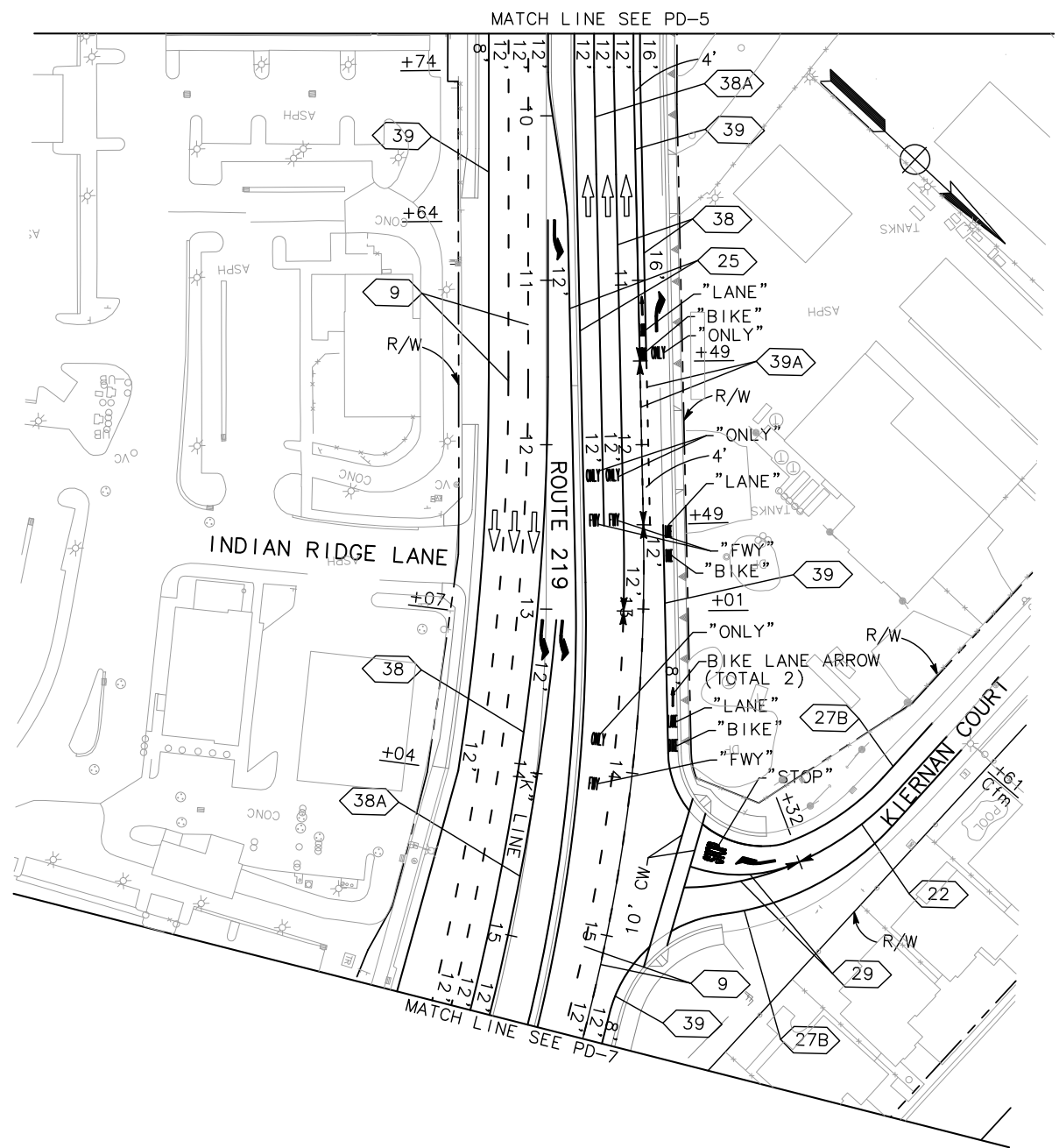
LAST REVISION DATE PLOTTED => \$DATE
05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	191	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
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NOTE:
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CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	DATE REVISED
			ANDY CHAN CHUONG NGUYEN



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PAVEMENT DELINEATION PLAN
SCALE 1" = 50' **PD-6**

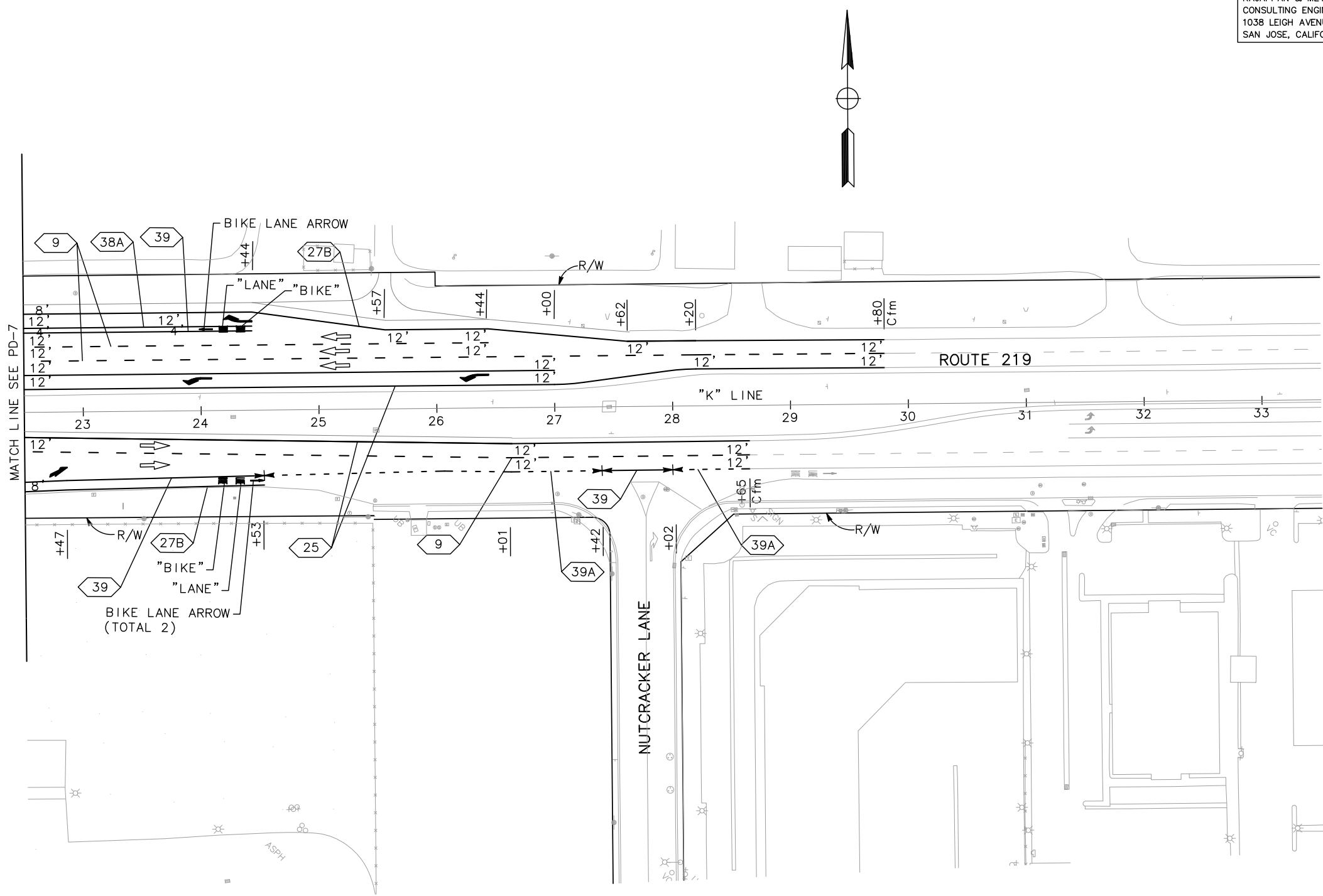
LAST REVISION: 05-01-12
DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	193	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
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NOTE:

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CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



PAVEMENT DELINEATION PLAN


SCALE 1" = 50'

PD-8

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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
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			CHUONG NGUYEN
			DATE REVISED

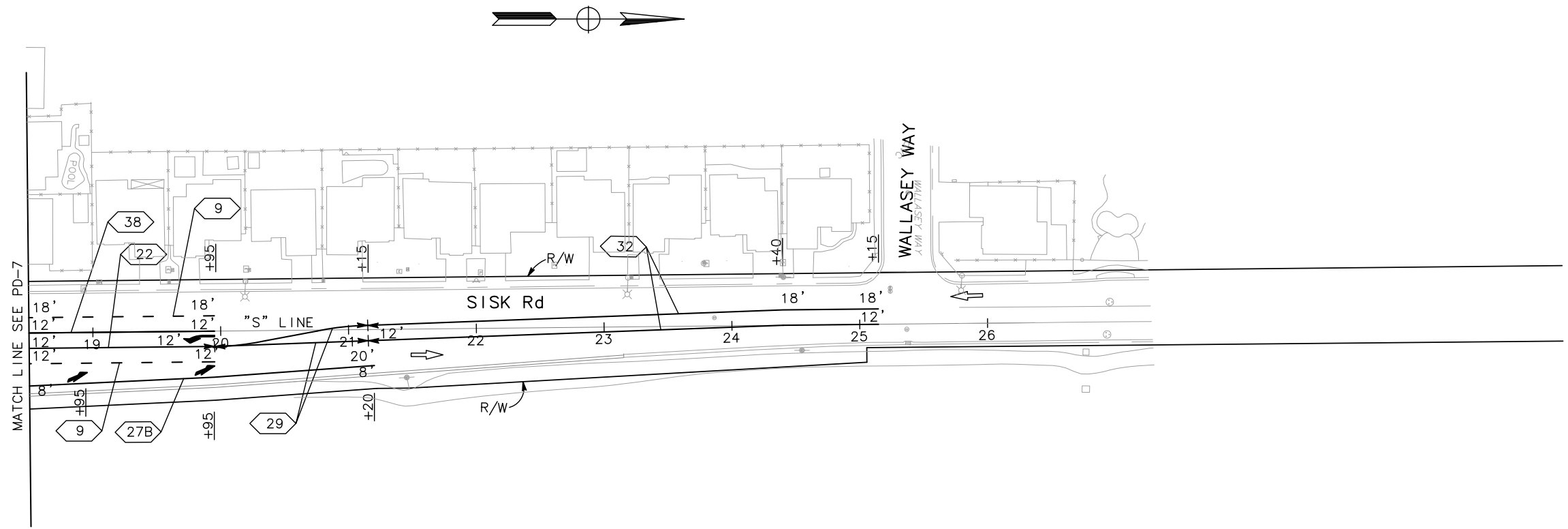
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	194	423

 REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:

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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
	KEITH G. MEYER	CHECKED BY	ANDY CHAN
			CHUONG NGUYEN
			DATE REVISED

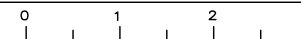


PAVEMENT DELINEATION PLAN

SCALE 1" = 50'

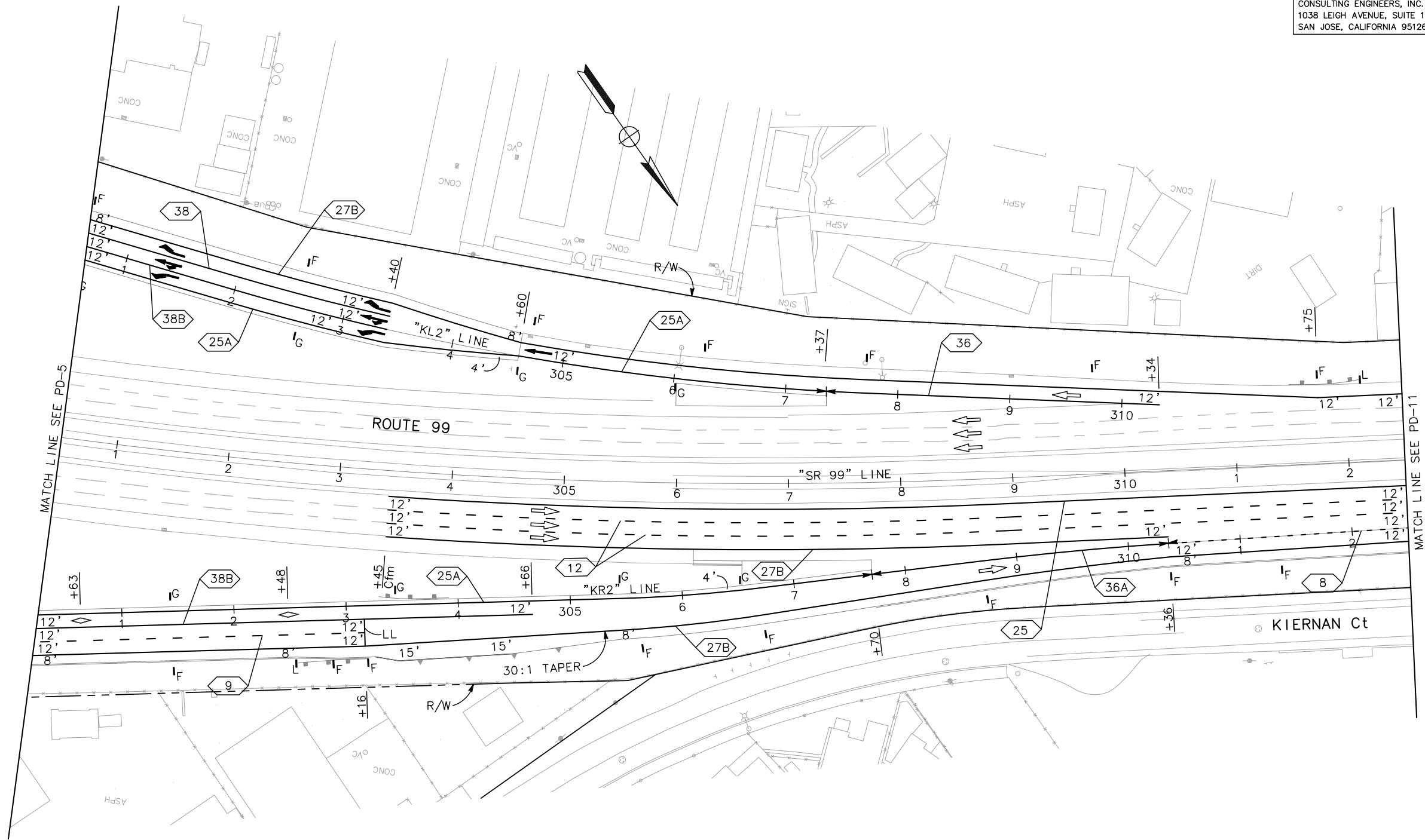
PD-9

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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	195	423
			<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER DATE 05-06-12		
PLANS APPROVAL DATE 9/04/12					
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NOTE:
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	DATE REVISED
		ANDY CHAN	CHUONG NGUYEN

PAVEMENT DELINEATION PLAN
 SCALE 1" = 50' PD-10

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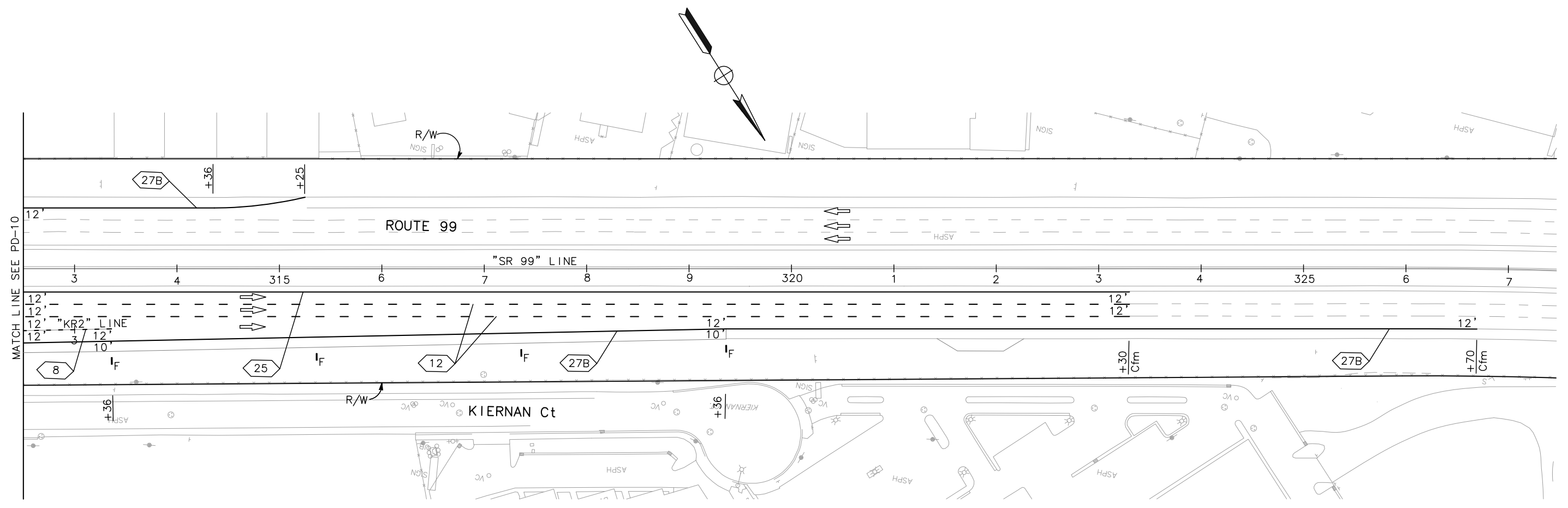
LAST REVISION 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	196	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISED BY
Caltrans	KEITH G. MEYER	CHECKED BY	ANDY CHAN
			CHUONG NGUYEN
			DATE REVISED



PAVEMENT DELINEATION PLAN
SCALE 1" = 50' **PD-11**

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 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
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 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	197	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

TRAFFIC STRIPE

SHEET	STATION	THERMOPLASTIC TRAFFIC STRIPE										THERMOPLASTIC TRAFFIC STRIPE (SRAYABLE)												
		4" WHITE					4" YELLOW		8" WHITE			4" WHITE		4" YELLOW			6" WHITE		8" WHITE					
		DETAIL 8	DETAIL 9	DETAIL 12	DETAIL 14A	DETAIL 27B	DETAIL 25	DETAIL 25A	DETAIL 36	DETAIL 36A	DETAIL 37	DETAIL 38	DETAIL 38B	DETAIL 9	DETAIL 27B	DETAIL 22	DETAIL 25	DETAIL 29	DETAIL 32	DETAIL 39	DETAIL 39A	DETAIL 38	DETAIL 38A	DETAIL 38B
PD-1	"SR 99" NB 265+89.00 TO 276+50.00			2,122		1,010	1,061																	
	"SR 99" SB 269+57.00 TO 276+50.00			1,100	288	671	693																	
PD-2	"SR 99" NB 276+50.00 TO 288+50.00			2,032	288	798	1,160																	
	"SR 99" SB 276+50.00 TO 288+50.00			2,400		900	1,200																	
	"KL1" SB ON-RAMP 279+80.00 TO 288+50.00					875		287		286	300													
	"KR1" NB OFF-RAMP 284+52.00 TO 288+50.00		403			496		131	541															
PD-3	"SA" 49+50.00 TO 56+27.00												1,142	1,143		864						391	180	60
PD-4	"K" 0+61.00 TO 2+50.00												716	276		364						490		161
	"B" 0+00.00 TO 7+47.09												212		82		334					41		
	"SA" 39+86.00 TO 49+50.00												925	836		772						884	130	129
PD-5	"SR 99" SB 288+50.00 TO 291+80.00			658		329	330																	
	"KR1" NB OFF-RAMP 288+50.00 TO 297+84.00		309			873		869				561	563											
	"KR2" NB ON-RAMP 297+24.00 TO 300+25.00		205			204		206				206												
	"KL1" SB ON-RAMP 288+54.00 TO 297+72.00		690			835		838				838												
	"KL2" SB OFF-RAMP 297+95.00 TO 300+64.00					172		175				171	173											
	"K" 2+50.00 TO 9+50.00													1,451	137		940				860		1,041	398
PD-6	"K" 9+50.00 TO 15+50.00													1,662	385	238	1,213	141			1,032	200	1,034	543
PD-7	"K" 15+50.00 TO 22+50.00													2,185	614		1,120				1,092		762	931
	"S" 6+56.00 TO 18+50.00													1,168	1,675	1,984							1,067	151
PD-8	"K" 22+50.00 TO 33+50.00													1,739	1,027		1,346				457	352	450	194
PD-9	"S" 18+50.00 TO 25+15.00													290	270	290		968	1600				145	
PD-10	"SR 99" NB 303+45.00 TO 312+50.00			1,816		698	908																	
	"SR 99" SB 310+34.00 TO 312+50.00					216																		
	"KR2" NB ON-RAMP 300+25.00 TO 312+50.00	214	295			1,232		745		266			443											
	"KL2" SB OFF-RAMP 300+63.00 TO 310+34					971		673	297			279	277											
PD-11	"KR2" NB ON-RAMP 312+50.00 TO 313+36.00	86																						
	"SR 99" NB 312+50.00 TO 326+70.00			2,160		1,419	1,080																	
	"SR 99" SB 312+50.00 TO 326+70.00					276																		
	SUBTOTAL	300	1,902	12,288	576	11,975	6,432	3,924	838	552	2,667	1,011	2,942	11,490	6,363	2,594	6,619	1,443	1,600	3,441	552	6,305	2,527	350
	TOTAL					37,397							8,010											43,284

PAVEMENT DELINEATION QUANTITIES

PDQ-1

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 05-01-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	198	423

REGISTERED CIVIL ENGINEER *Chuong Nguyen* DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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PAVEMENT MARKER

SHEET	STATION	PAVEMENT MARKER														NON-REFLECTIVE				
		RETROREFLECTIVE																		
		TYPE C		TYPE D			TYPE G						TYPE H		TYPE A					
14A	37	22	29	32	9	12	36	36A	37	38	38B	25	25A	40A						
		EA														EA				
PD-1	"SR 99" NB 265+89.00 TO 276+50.00		72									45			2			23		
	"SR 99" SB 269+57.00 TO 276+50.00	8	50									24			2			15		
	"SR 99" NB 276+50.00 TO 288+50.00	8	26									43					38	25		
PD-2	"SR 99" SB 276+50.00 TO 288+50.00		24									51						26		
	"KL1" SB ON-RAMP 279+80.00 TO 288+50.00		22										13					13		
	"KR1" NB OFF-RAMP 284+52.00 TO 288+50.00						9			24								6		
PD-3	"SA" 49+50.00 TO 56+27.00						25							17	8	19				
PD-4	"K" 0+61.00 TO 2+50.00						16							21	16	9				
	"B" 0+00.00 TO 7+47.09			10	30		5							3						
	"SA" 39+86.00 TO 49+50.00						20							38	12	17			93	
	"SR 99" SB 288+50.00 TO 291+80.00											15						8		
	"KR1" NB OFF-RAMP 288+50.00 TO 297+84.00						7							24	48			37		
PD-5	"KR2" NB ON-RAMP 297+24.00 TO 300+25.00						5								20			10		
	"KL1" SB ON-RAMP 288+54.00 TO 297+72.00						15								72			36		
	"KL2" SB OFF-RAMP 297+95.00 TO 300+64.00													8	16			8		
	"K" 2+50.00 TO 9+50.00						31							44		21			94	
PD-6	"K" 9+50.00 TO 15+50.00			22	14		36							44		26				
PD-7	"K" 15+50.00 TO 22+50.00						47							33		24				
	"S" 6+56.00 TO 18+50.00			166			25							45					141	
PD-8	"K" 22+50.00 TO 33+50.00						37							20		29				
PD-9	"S" 18+50.00 TO 25+15.00			26	82	173	7							7						
	"SR 99" NB 303+45.00 TO 312+50.00											39				20				
PD-10	"SR 99" SB 310+34.00 TO 312+50.00																			
	"KR2" NB ON-RAMP 300+25.00 TO 312+50.00										14			13	38			32		
	"KL2" SB OFF-RAMP 300+63.00 TO 310+34						7			12				26				29		
PD-11	"KR2" NB ON-RAMP 312+50.00 TO 313+36.00																			
	"SR 99" NB 312+50.00 TO 326+70.00						46									24				
	"SR 99" SB 312+50.00 TO 326+70.00																			
	SHEET TOTAL	16	194	224	126	173	292	263	38	25	4	317	294	286	171				328	
	SUBTOTAL																			2,751
	TYPE G MARKER (SHOULDER LOCATION)																			6
	FROM TRAFFIC HANDLING PLANS																			2,189
	TOTAL																			4,946

TYPE G MARKER

TYPE G MARKER (SHOULDER LOCATION)		
SHEET	STATION	QUANTITY
		EA
PD-1	"SR 99" NB 266+18	3
	"SR 99" NB 273+18	2
PD-2	"SR 99" NB 280+18	1
	TOTAL	6*

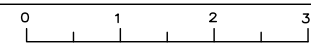
(*) QUANTITY INCLUDED IN PAVEMENT MARKER TABLE.

DELINEATOR & OBJECT MARKER

SHEET	STATION	OBJECT MARKER	DELINEATOR (CLASS 1)		GUARD RAILING DELINEATOR
		TYPE L	TYPE F	TYPE G	TYPE F
		EA	EA		
PD-1	"SR 99" NB	2	2		5
	"SR 99" NB	1			2
PD-2	"KL1" SB ON-RAMP		3	1	
	"KR1" NB OFF-RAMP		2		
	"KL1" SB ON-RAMP	2		4	1
PD-5	"KR1" NB OFF-RAMP		5	5	
	"KL2" SB OFF-RAMP			1	
	"KR2" NB ON-RAMP		1	1	
PD-10	"KL2" SB OFF-RAMP		6	3	1
	"KR2" NB ON-RAMP	1	6	4	2
PD-11	"SR 99" SB	1	1		
	"SR 99" NB		4		
	SUBTOTAL	7	30	19	11
	TOTAL	7	58		11

PAVEMENT DELINEATION QUANTITIES

PDQ-2



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: ANDY CHAN
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: ANDY CHAN
 DATE REVISED: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	199	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

PAVEMENT MARKING

SHEET	STATION	THERMOPLASTIC PAVEMENT MARKING															
		LIMIT LINE AND CROSSWALK	TYPE I ARROW	TYPE II ARROW	TYPE III ARROW	TYPE VI ARROW	DIAMOND SYMBOL	BIKE Ln ARROW	"BIKE" WORD	"LANE" WORD	"KEEP" WORD	"CLEAR" WORD	"FWY" WORD	"ONLY" WORD	"STOP" WORD	12" WHITE CHERVON	
																	SQFT
PD-3	"SA" 49+50 TO 61+00	120	62		210	84					96	108					
PD-4	"SA" 37+50.00 TO 49+50	462	186		378												
	"B" 5+49.00 TO 6+82.00	234	31	45	42	84											
	"K" 0+61.00 TO 2+50.00	305	186		126												
PD-5	"KL1" SB 288+50.00 TO 297+72.00	141	62				44										
	"KR1" NB 288+50.00 TO 297+84.00	102	62	135	252												
	"KL2" SB 297+95.00 TO 300+50.00	131		45	84												
	"KR2" NB 297+24.00 TO 300+50.00	105	62				11										
PD-6	"K" 2+50.00 TO 9+50.00	648	93	45	420			17.5	30	36							
PD-6	"K" 9+50.00 TO 15+50.00	189			210			7	15	18		48	88	22			
PD-7	"K" 15+50.00 TO 22+50.00	696			462	84		17.5	25	30						32	
	"S" 6+50.00 TO 18+50.00	395	155	90	462	210											
PD-8	"K" 22+50.00 TO 29+80.00				126	42		7	10	12							
PD-9	"S" 18+50.00 TO 25+15.00				42	84											
PD-10	"KL2" SB 300+50.00 TO 310+24.00		31	90	168												
	"KR2" NB 300+50.00 TO 312+50.00	24					22										
	SHEET TOTAL	3,552	930	450	2982	588	77	49	80	96	96	108	48	88	22	32	
	TOTAL																9,154

PAVEMENT DELINEATION QUANTITIES

PDQ-3

LAST REVISION: 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENTI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

LEGEND:

- ROADSIDE ONE POST SIGN
- ROADSIDE TWO POST SIGN
- POLE MOUNTED SIGN
- OVERHEAD SIGN (LETTER)
- EXISTING ONE POST SIGN
- EXISTING TWO POST SIGN
- ROADSIDE SIGN (NUMBER)
- EXISTING SIGN TO REMAIN
- REMOVE ROADSIDE SIGN
- TRAFFIC FLOW DIRECTION
- BRIDGE MOUNTED SIGN
- MAST ARM MOUNTED SIGN
- TYPE N-1 MARKER

NOTES:

1. EXACT LOCATION AND POSITION OF ROADSIDE SIGNS TO BE DETERMINED BY THE ENGINEER.
2. CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD BEFORE ORDERING AND FABRICATING MATERIAL.
3. POST SIZE AND LENGTH GIVEN ARE APPROXIMATE EXACT SIZE AND LENGTH TO BE DETERMINED BY THE ENGINEER.
4. ALL THE EXISTING FEATURES ARE SHOWN AS DASHED.
5. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

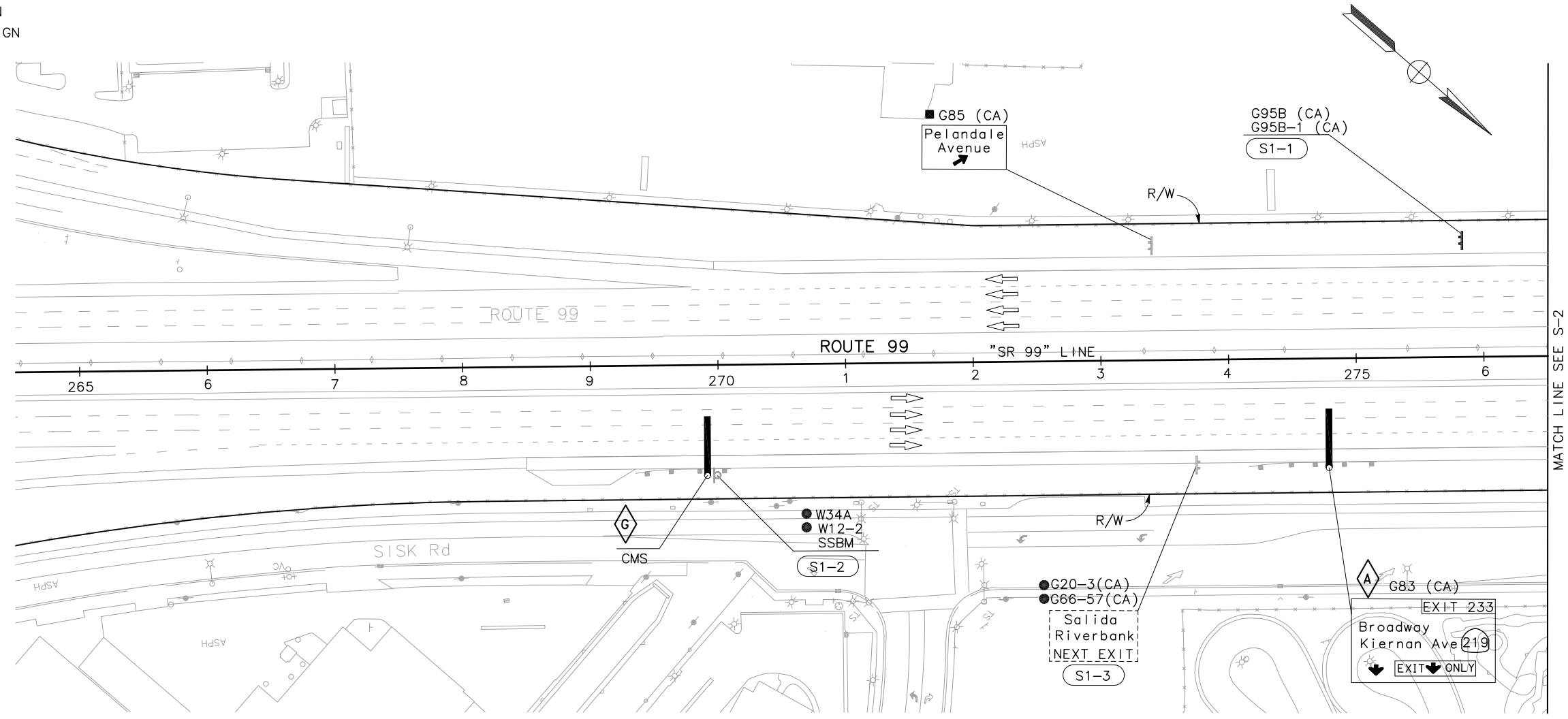
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	200	423

REGISTERED CIVIL ENGINEER
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 STATE OF CALIFORNIA
 REGISTERED PROFESSIONAL ENGINEER

05-06-12
 DATE
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--



SIGN PLAN
 SCALE: 1" = 50'
 S-1

APPROVED FOR SIGN WORK ONLY

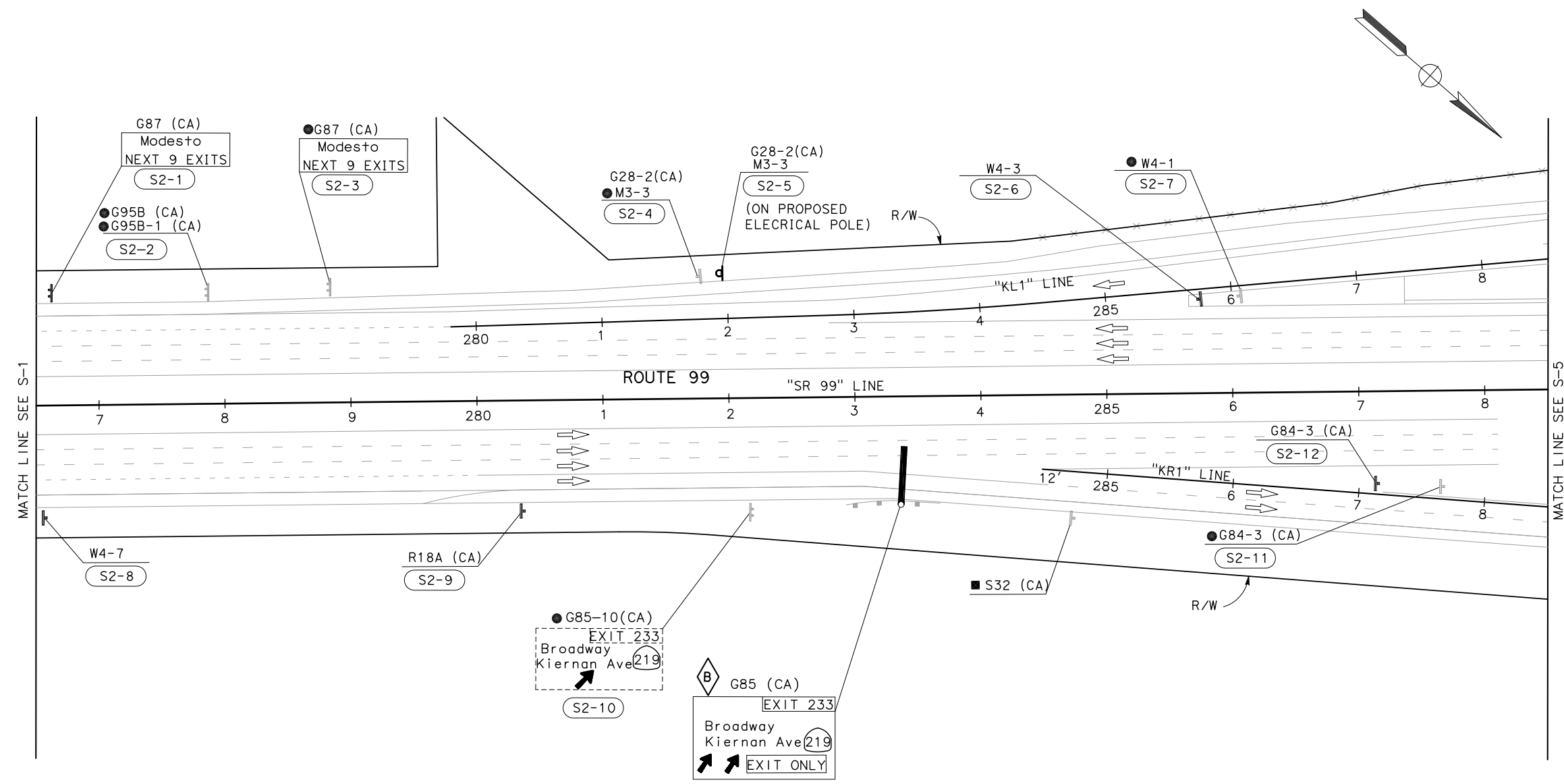
LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	201	423

<i>Chuong Nguyen</i>		05-06-12
REGISTERED CIVIL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANENT
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED:



SIGN PLAN
 SCALE: 1" = 50'
S-2

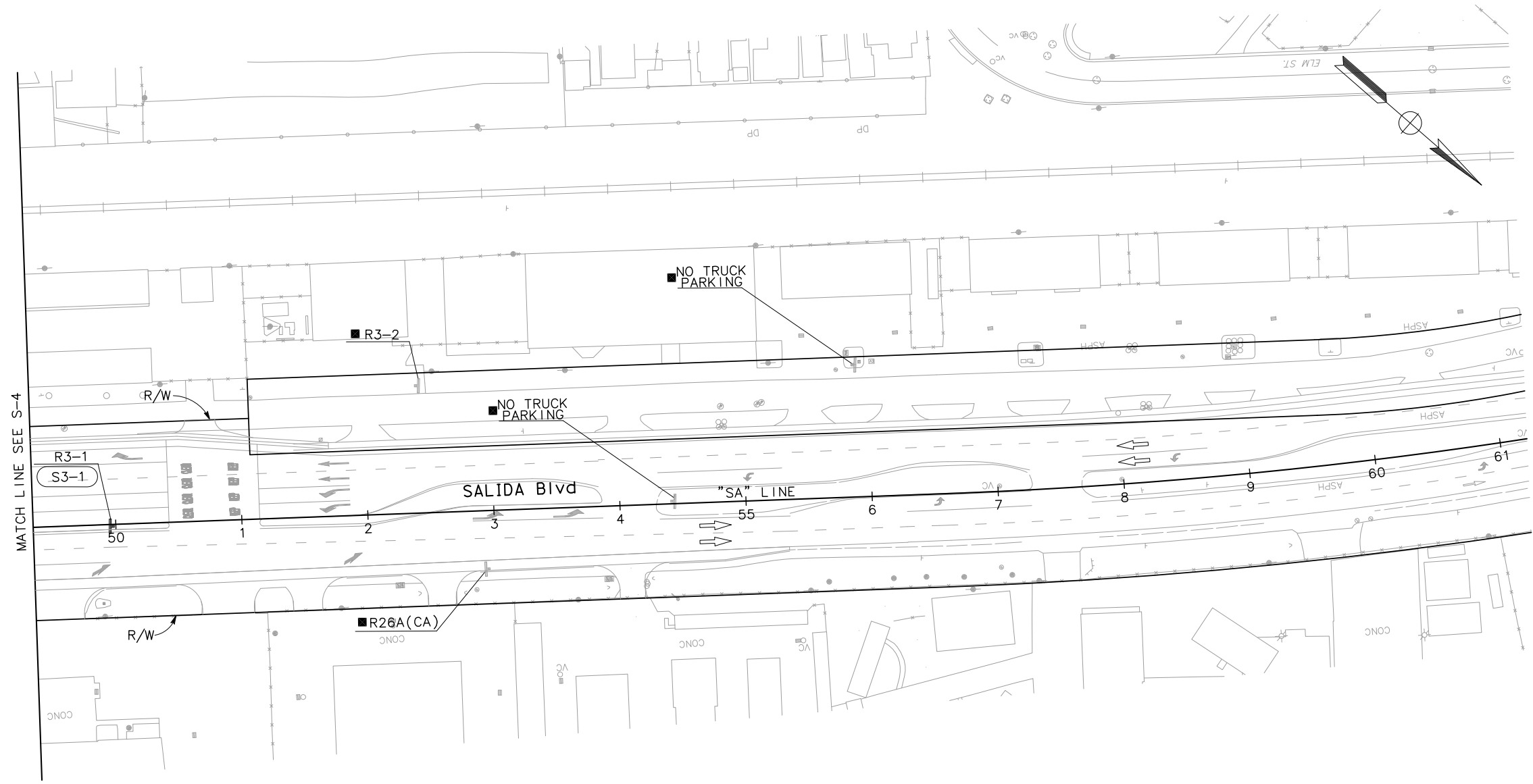
APPROVED FOR SIGN WORK ONLY

LAST REVISION 05-01-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	202	423
		<i>Chuong Nguyen</i>	05-06-12		
		REGISTERED CIVIL ENGINEER	DATE		
		9/04/12			
		PLANS APPROVAL DATE			
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENT
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED



SIGN PLAN
 SCALE: 1" = 50'
S-3

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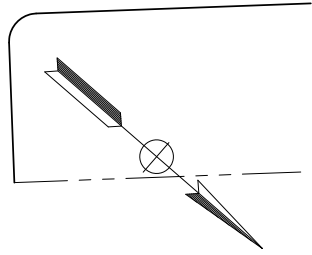
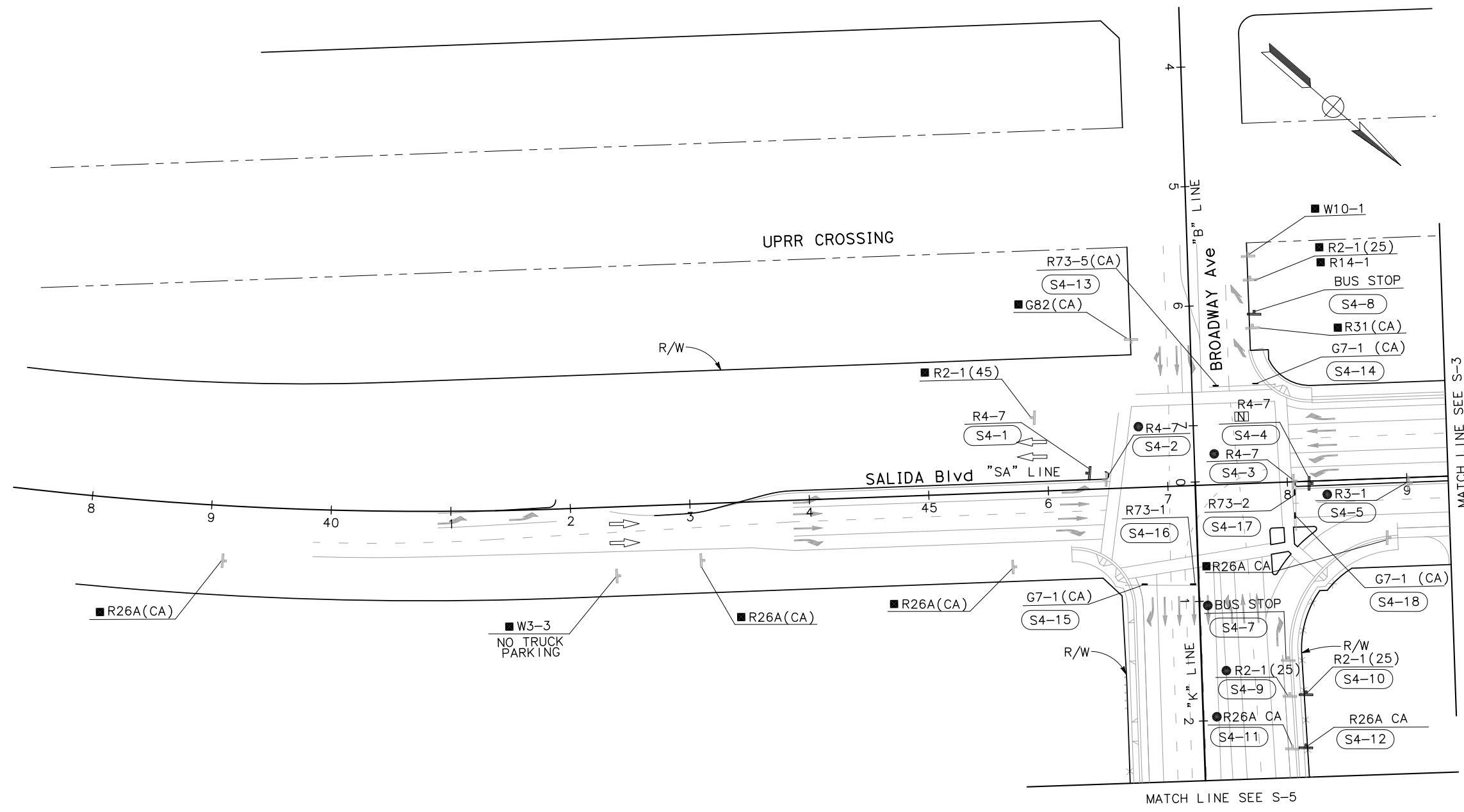
LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	203	423

<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER	05-06-12 DATE
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
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RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANENT
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED:



SIGN PLAN
SCALE: 1" = 50'
S-4

APPROVED FOR SIGN WORK ONLY

LAST REVISION 05-01-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

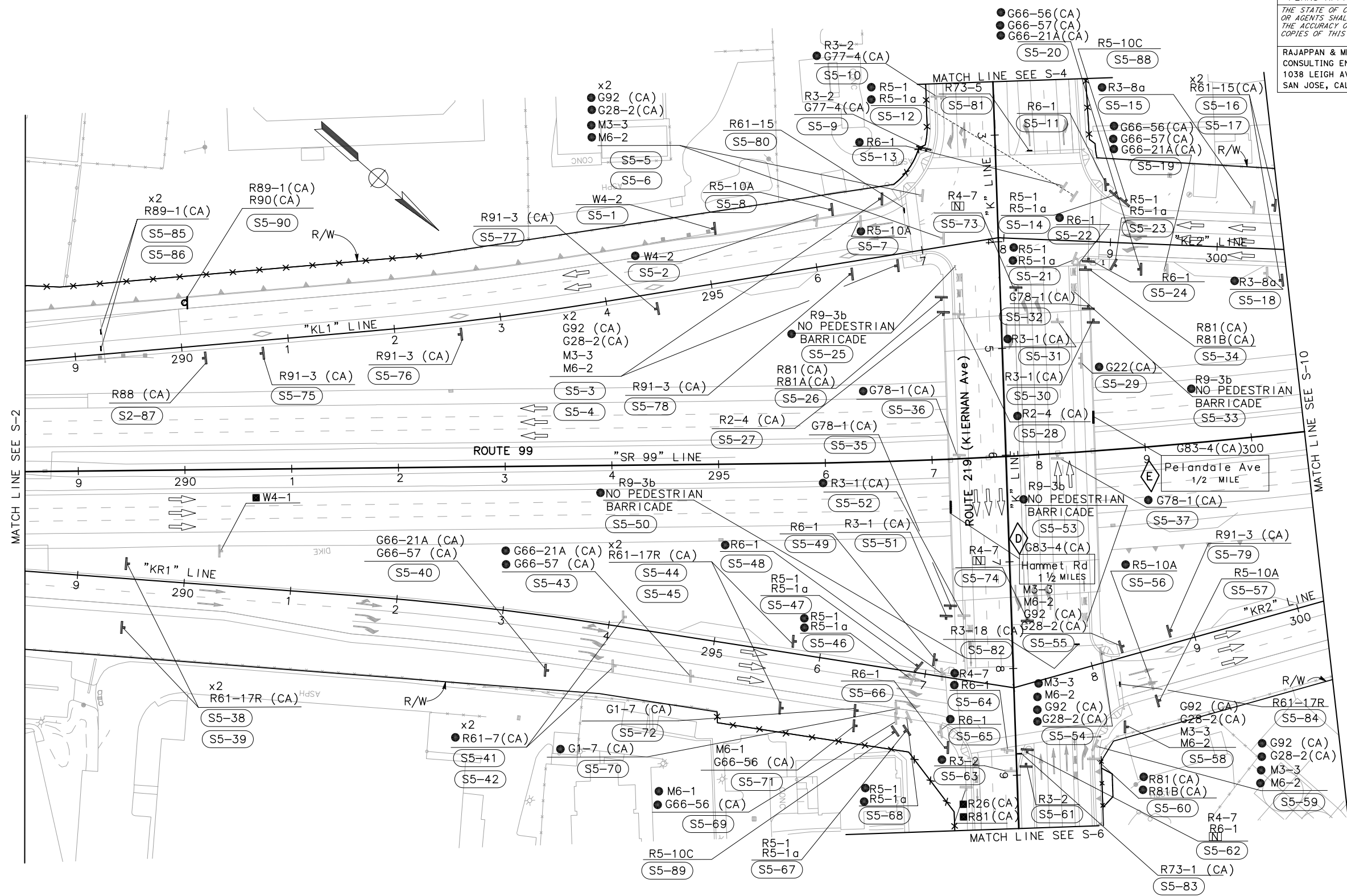
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	204	423

<i>Chuong Nguyen</i>	05-18-12
REGISTERED CIVIL ENGINEER	DATE

9/04/12
PLANS APPROVAL DATE

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
---	--

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: CHUONG NGUYEN
 REVISIONS: N MUMMANENTI, CHUONG NGUYEN, KEITH G. MEYER
 REVISED BY: DATE REVISIONS

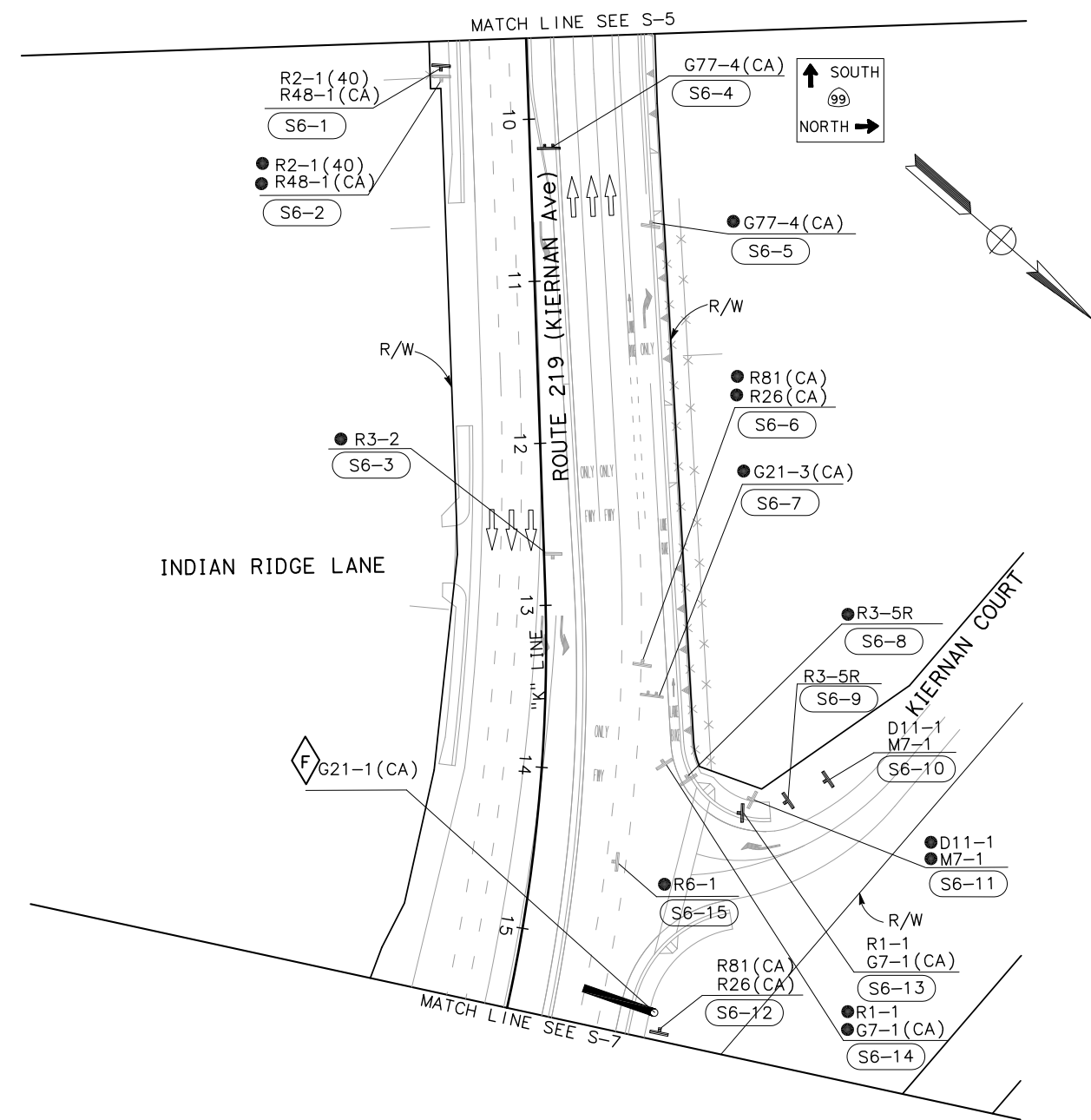
APPROVED FOR SIGN WORK ONLY

SIGN PLAN
SCALE: 1" = 50'
S-5

LAST REVISION DATE PLOTTED => \$DATE
06-08-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHECKED BY:
 N MUMMANENT: CHUONG NGUYEN
 REVISED BY: DATE REVISED:

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	205	423

05-18-12
 REGISTERED CIVIL ENGINEER DATE
 9/04/12
 PLANS APPROVAL DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

SIGN PLAN
 SCALE: 1" = 50'
S-6

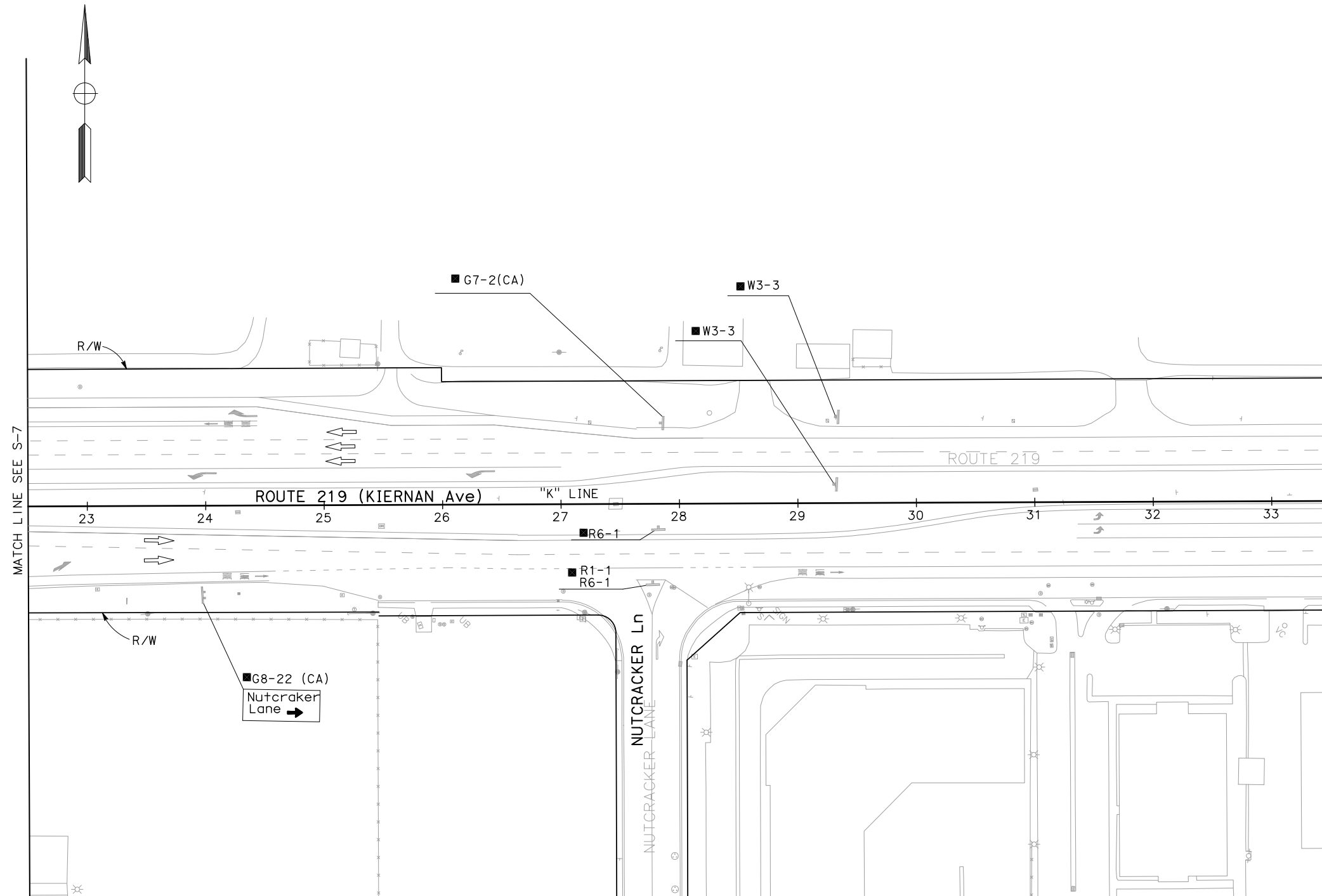
APPROVED FOR SIGN WORK ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 06-08-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	207	423
		<i>Chuong Nguyen</i> REGISTERED CIVIL ENGINEER		05-06-12 DATE	
		9/04/12 PLANS APPROVAL DATE			
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:

FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANENT
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: N MUMMANENT
 DATE REVISED: CHUONG NGUYEN

APPROVED FOR SIGN WORK ONLY

SIGN PLAN
 SCALE: 1" = 50'
S-8

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANENT
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED: []

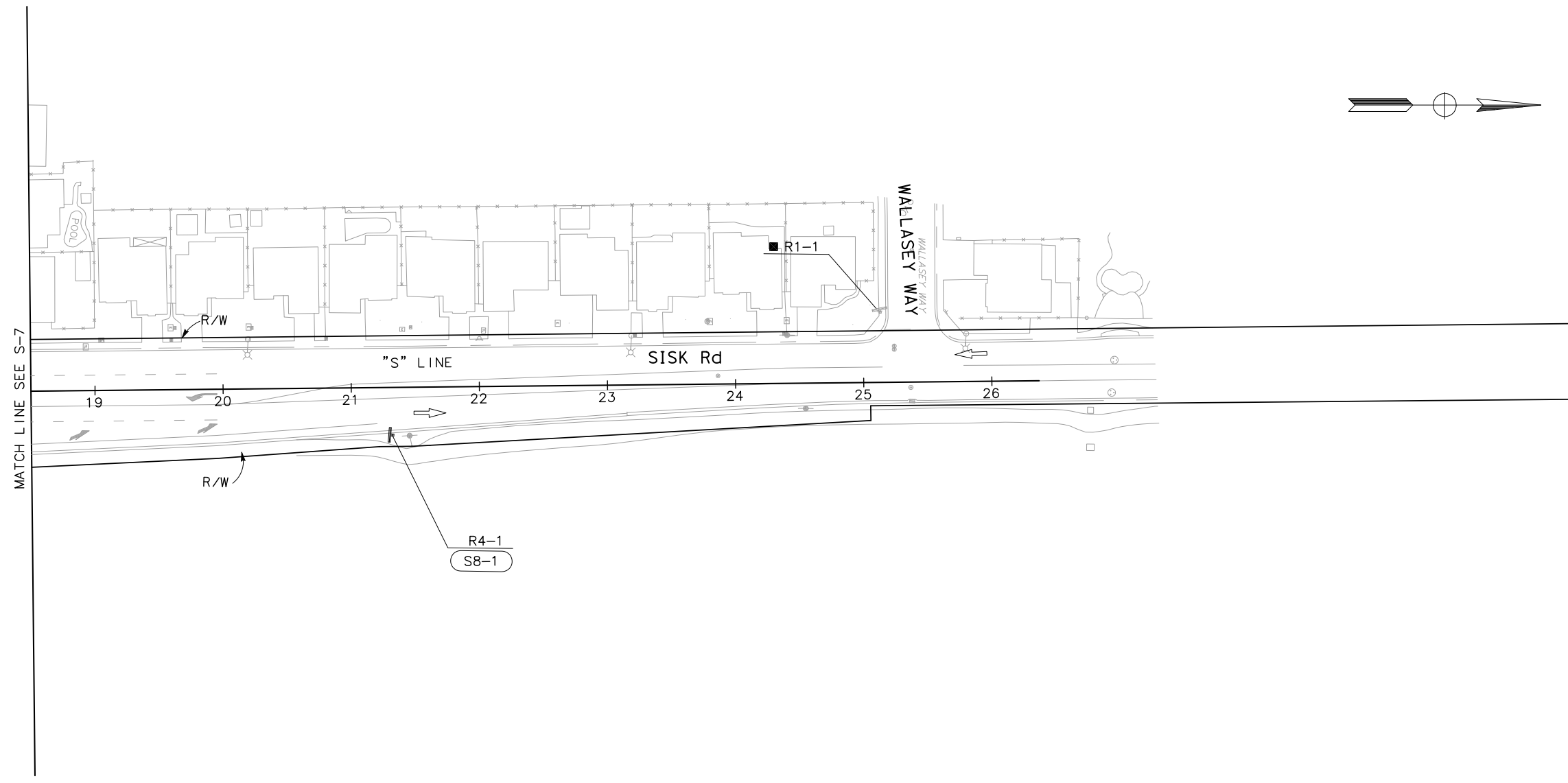
NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	208	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



SIGN PLAN
 SCALE: 1" = 50'
S-9

APPROVED FOR SIGN WORK ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION
 05-01-12

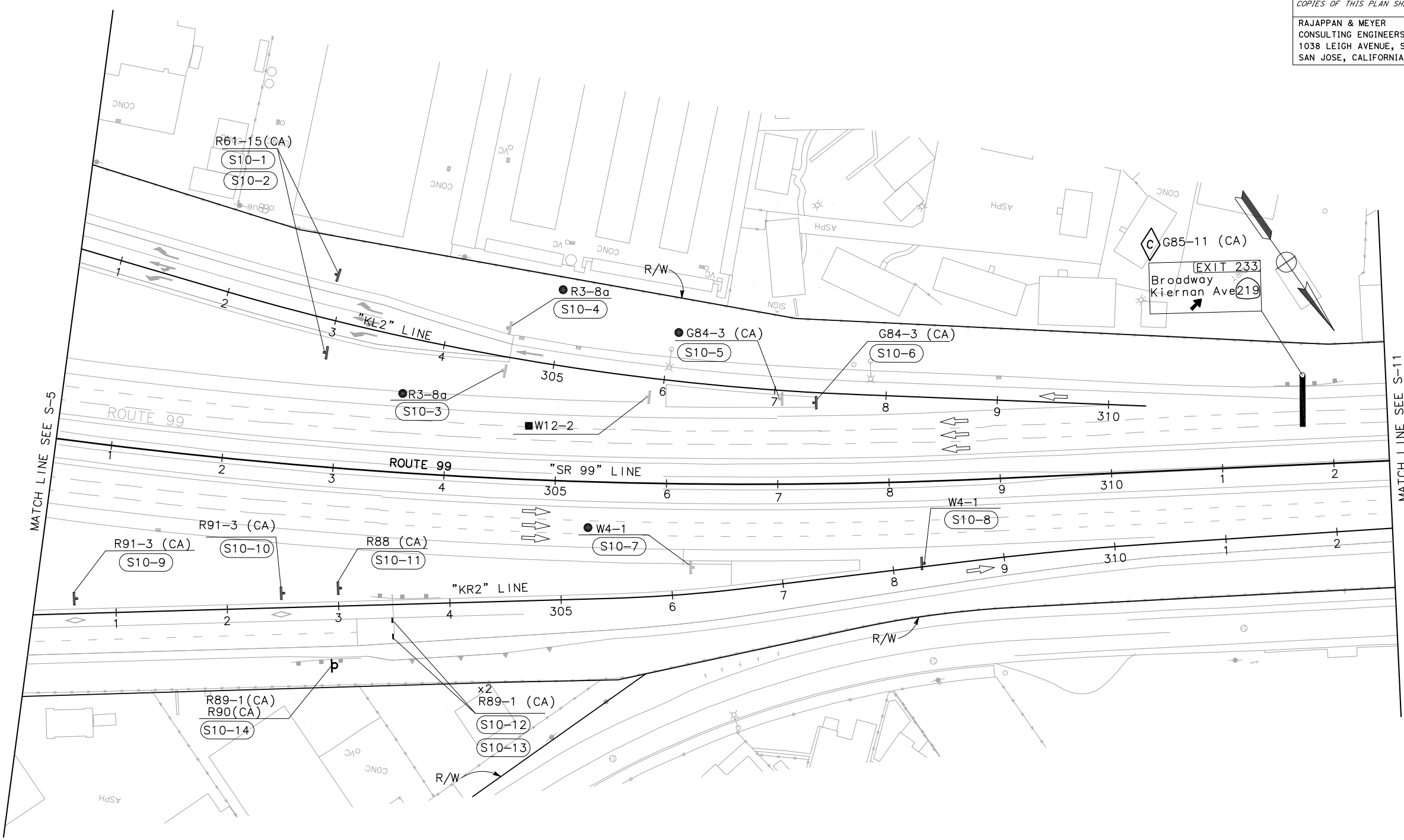
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	209	423

<i>Chuong Nguyen</i>	05-18-12
REGISTERED CIVIL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	

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---	---

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: CHUONG NGUYEN
 REVISOR: N. MUMMANENI
 REVISIONS: CHUONG NGUYEN
 DATE: 9/04/12

SIGN PLAN
 SCALE: 1" = 50'
S-10

APPROVED FOR SIGN WORK ONLY

LAST REVISION | DATE PLOTTED => \$DATE | 06-08-12 | TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: N MUMMANENT
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: CHUONG NGUYEN
 DATE REVISED: []

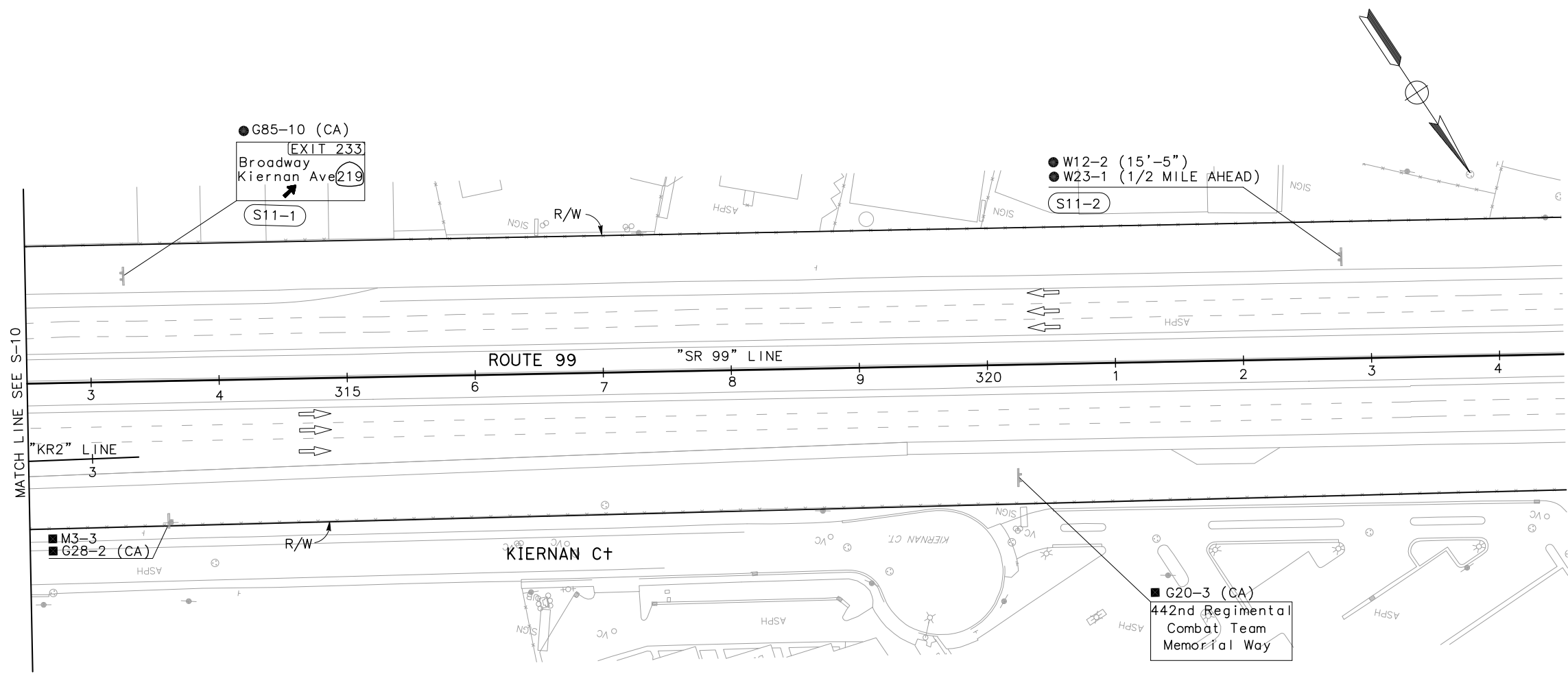
NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	210	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-06-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



SIGN PLAN
 SCALE: 1" = 50'

S-11

APPROVED FOR SIGN WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-01-12

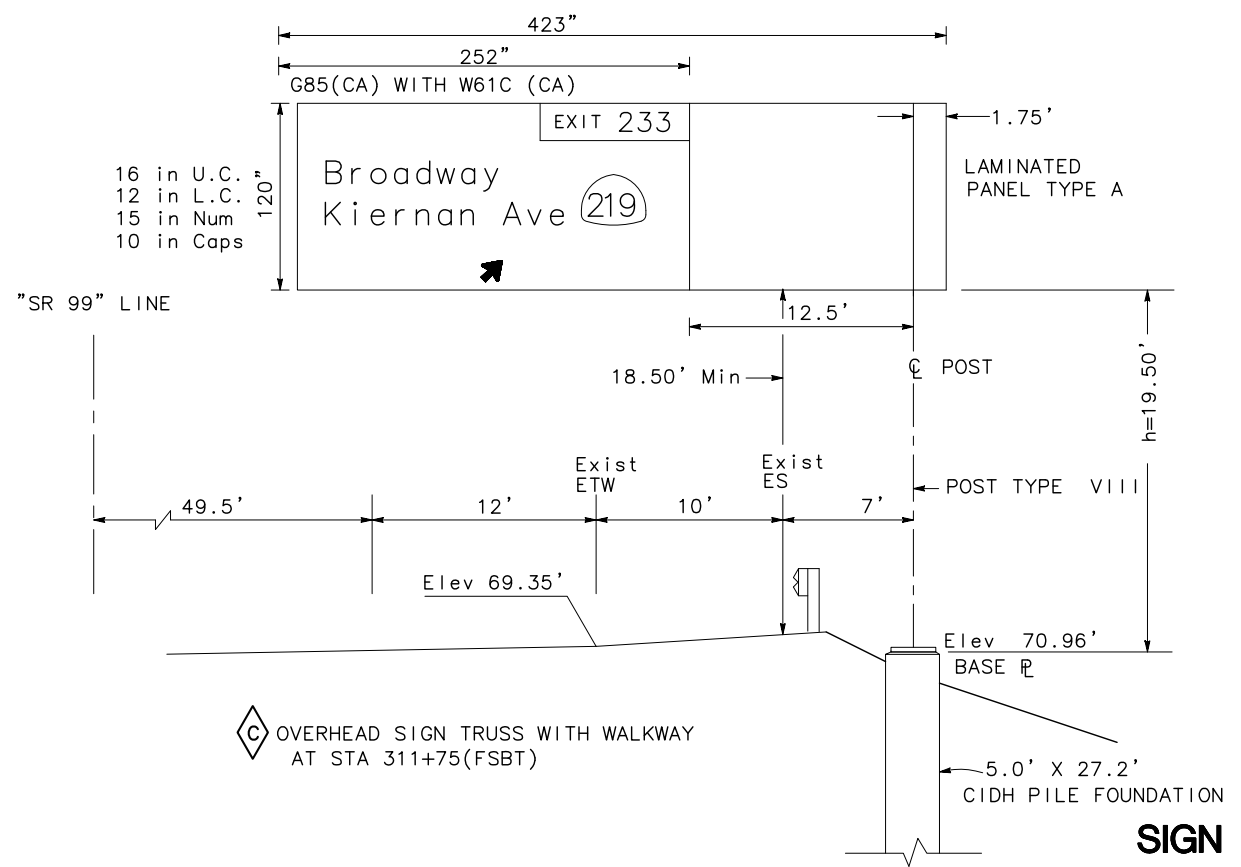
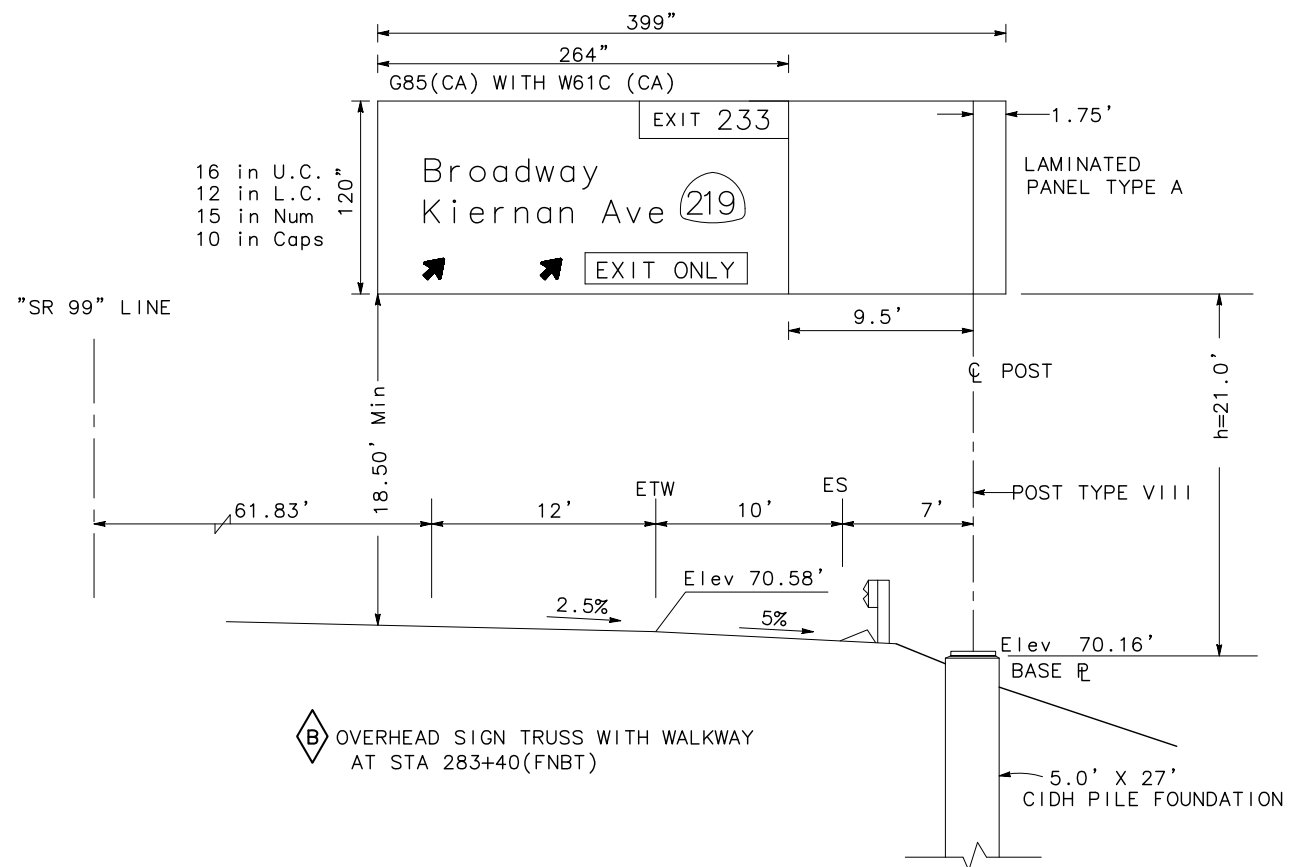
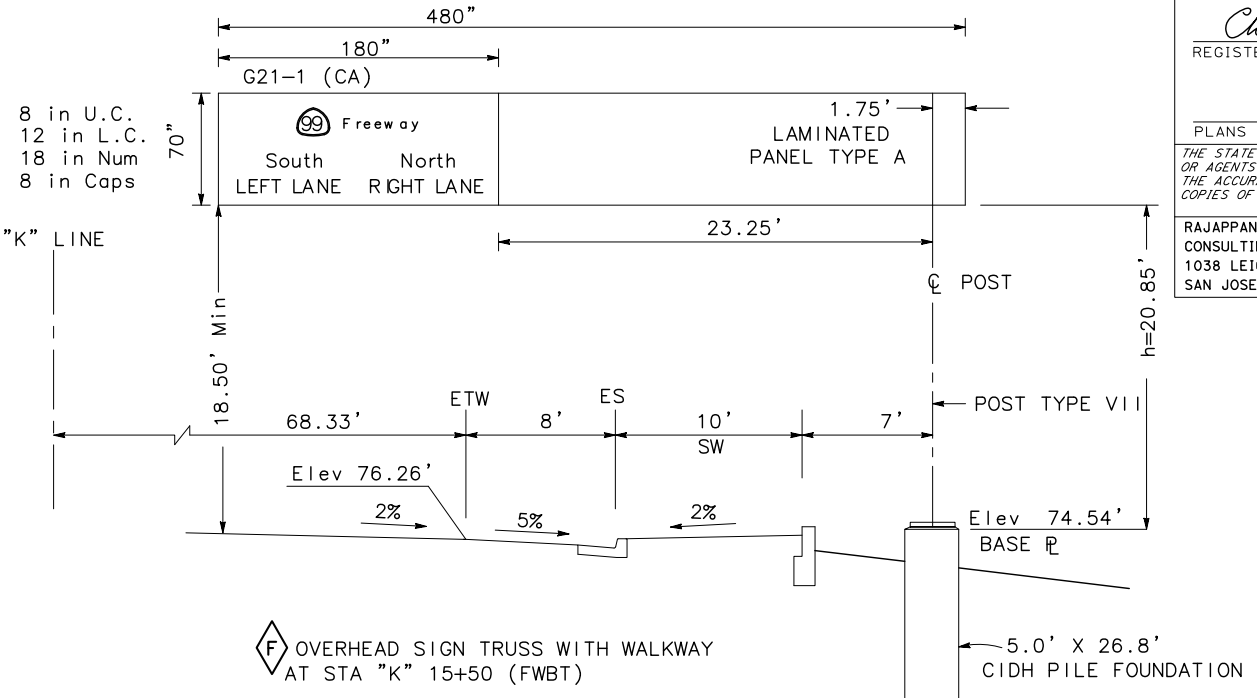
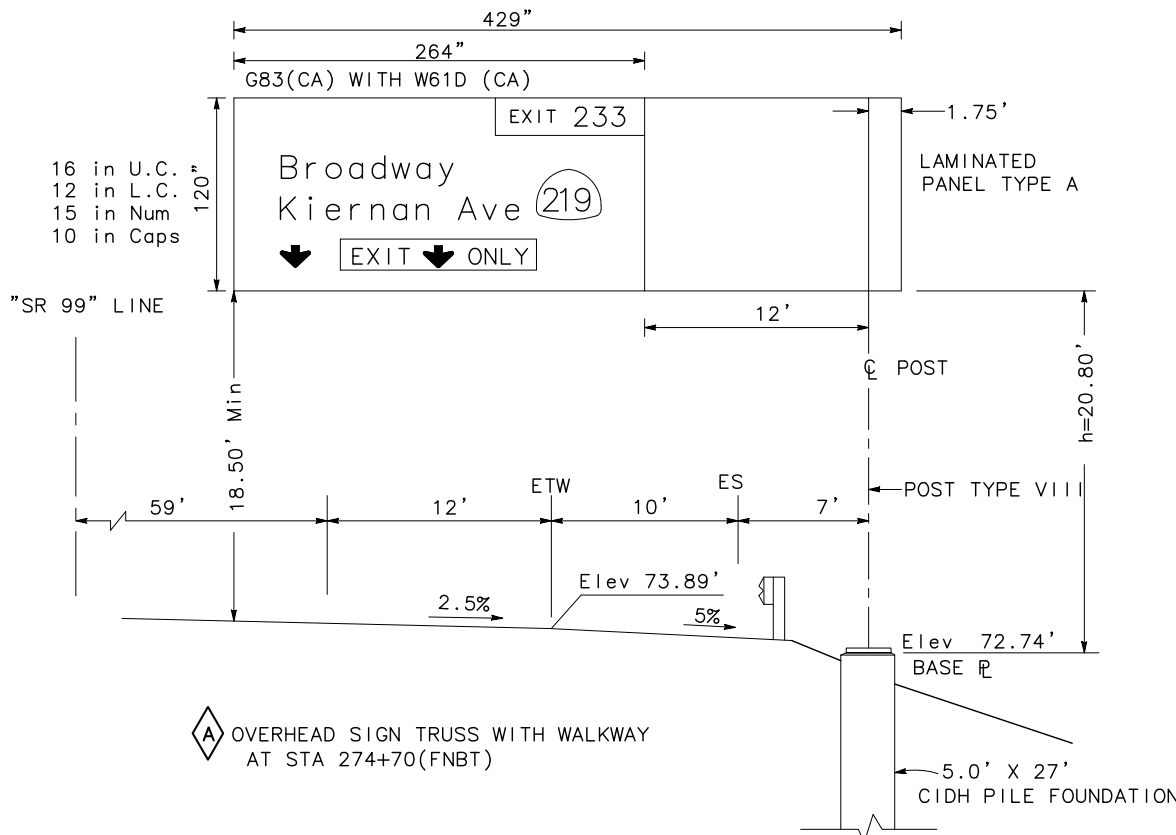
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS:
 1. N. MUMMANENI
 2. CHUONG NGUYEN
 3. KEITH G. MEYER

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	211	423

9/04/12
 REGISTERED CIVIL ENGINEER DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



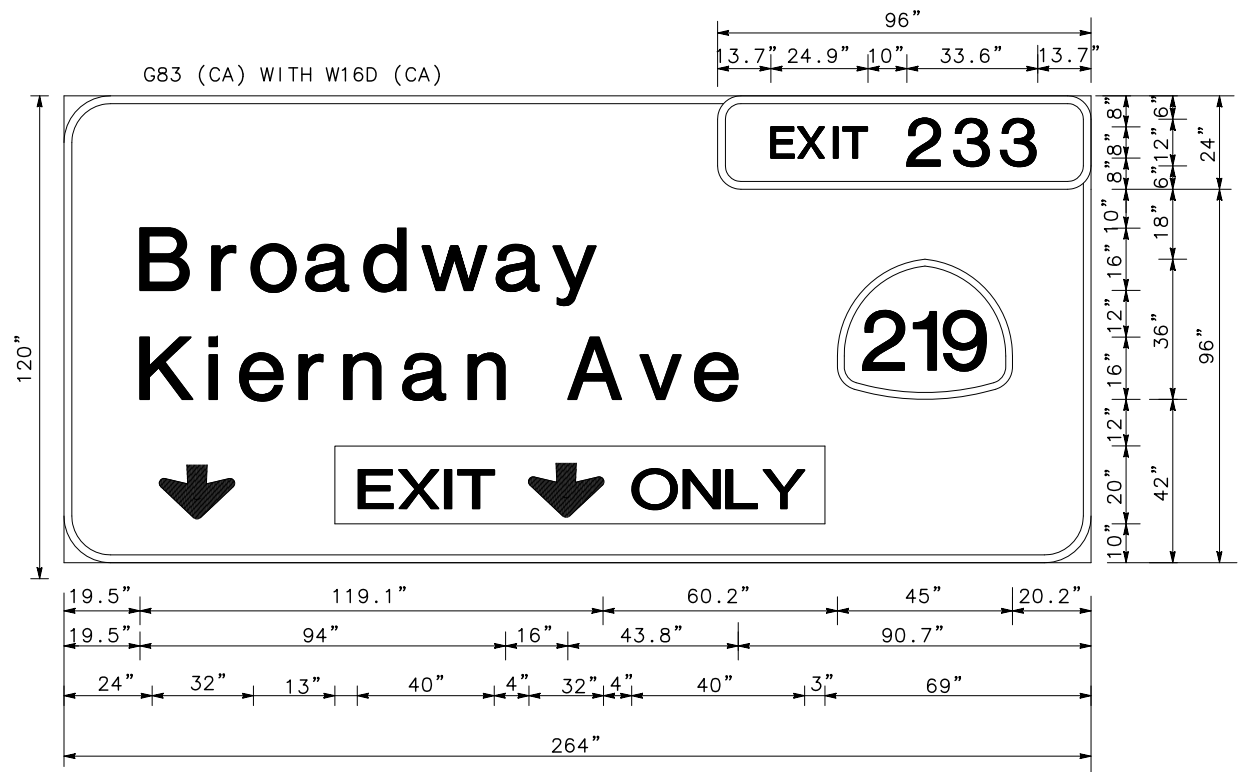
SIGN DETAILS
 NO SCALE
SD-1

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

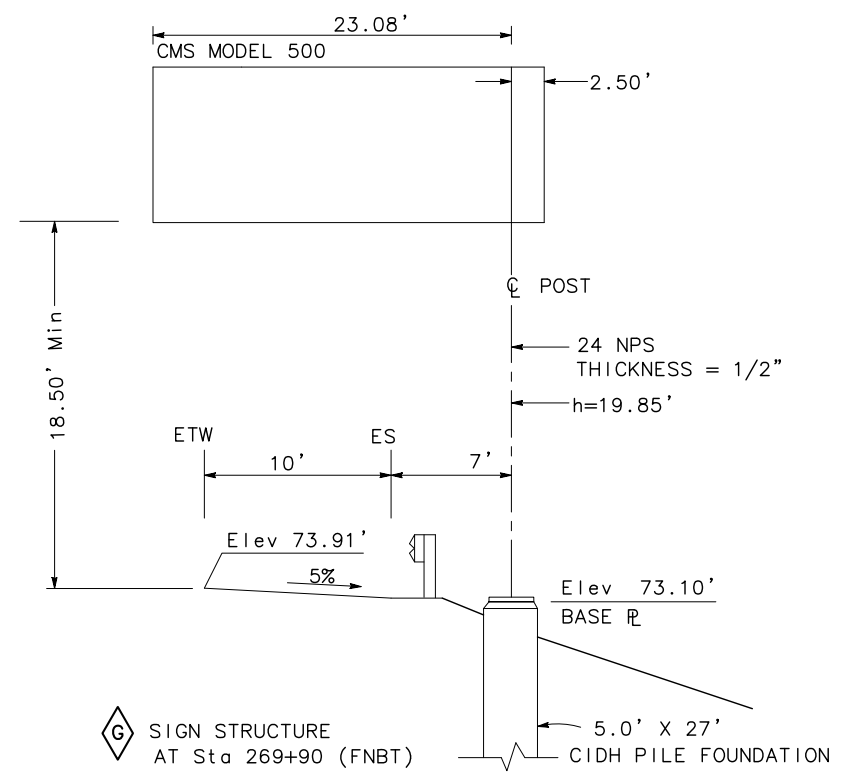
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	212	423

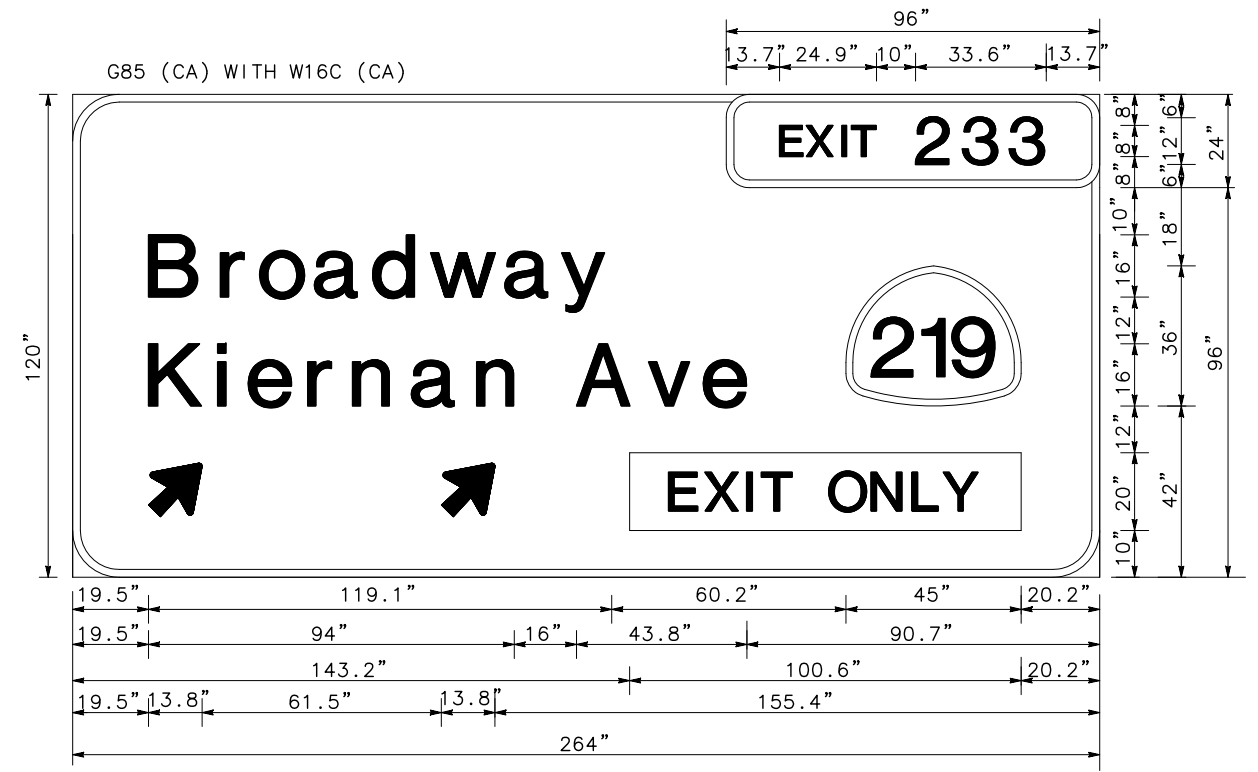
Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA
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 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



12.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [Broadway] E Mod; [Kiernan Ave] E Mod; RECTANGLE YELLOW;
 6.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [EXIT 233] E;



SIGN STRUCTURE
 AT Sta 269+90 (FNBT)

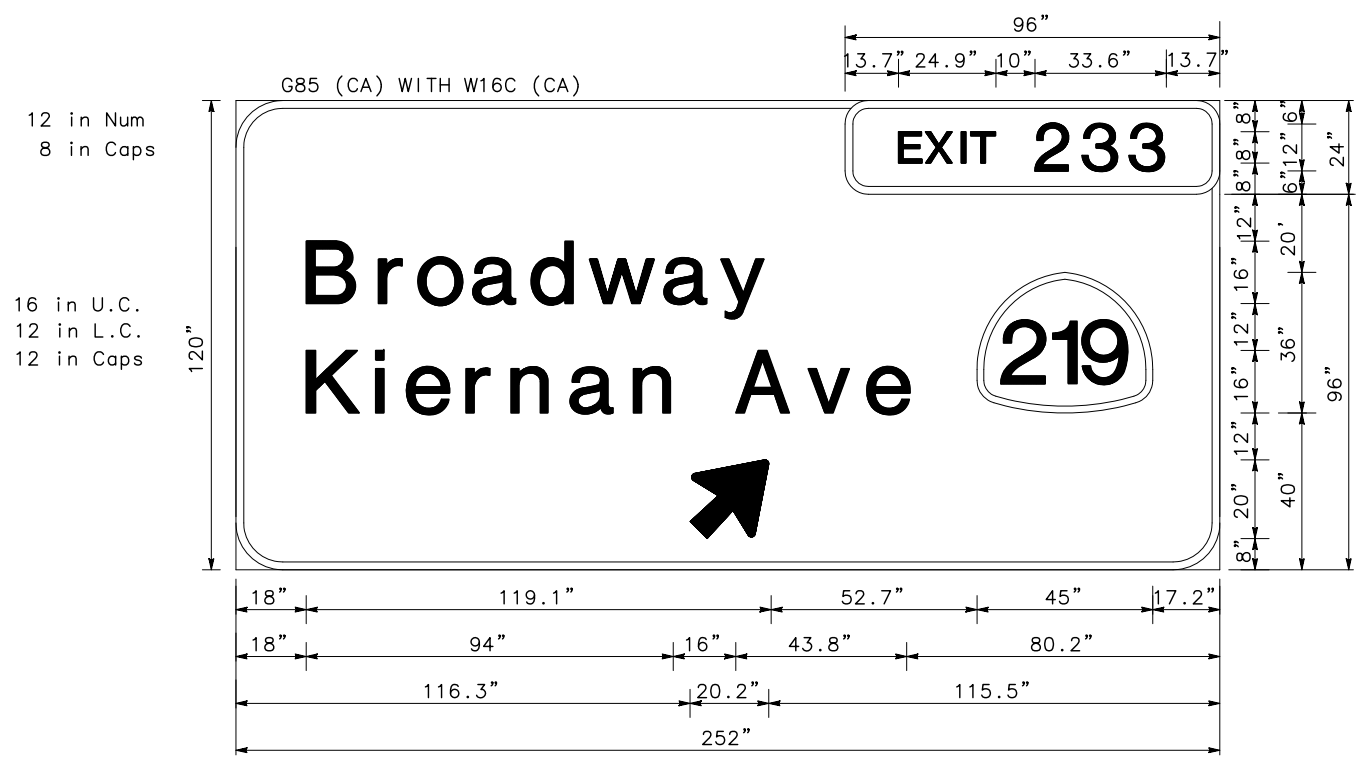
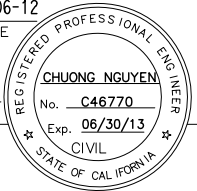


12.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [Broadway] E Mod; [Kiernan Ave] E Mod; RECTANGLE YELLOW;
 ARROW 10 CAP-1L - 17.0" 45 ; ARROW 10 CAP - 17.0" 45
 6.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [EXIT 233] E;

SIGN DETAILS
 NO SCALE
SD-2

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	213	423

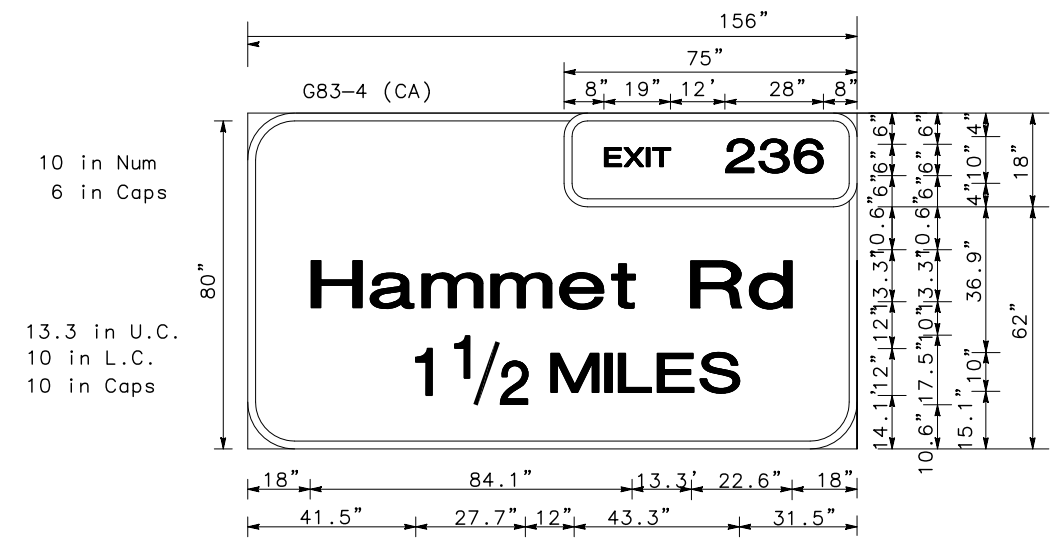
<i>Chuong Nguyen</i>	05-06-12
REGISTERED CIVIL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



12 in Num
 8 in Caps

16 in U.C.
 12 in L.C.
 12 in Caps

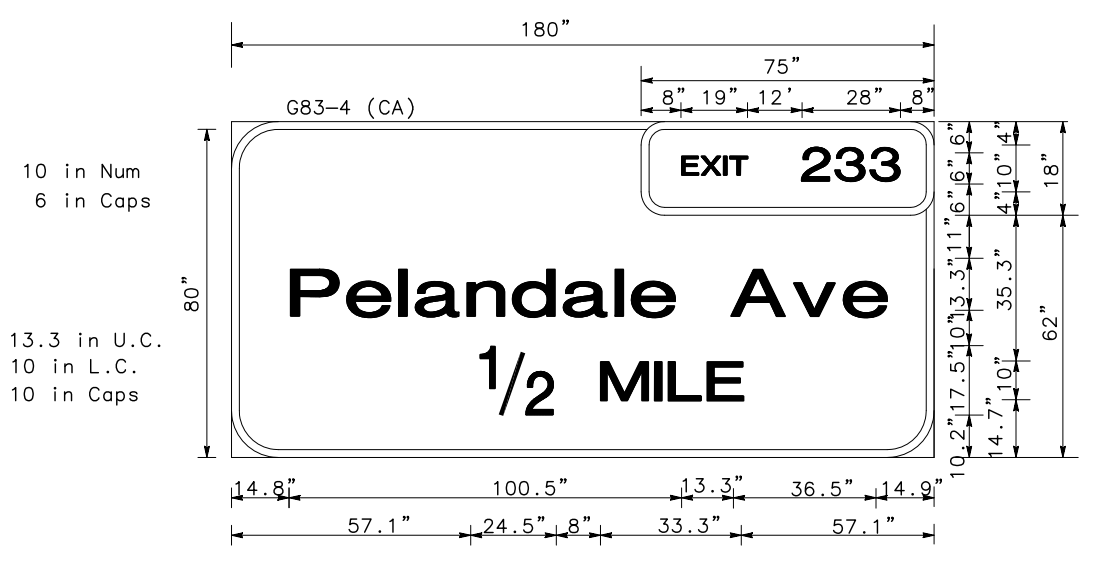
12.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [Broadway] E Mod; [Kiernan Ave] E Mod; ARROW 16UC-1L - 25.0" 45;
 6.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [EXIT 233] E;



10 in Num
 6 in Caps

13.3 in U.C.
 10 in L.C.
 10 in Caps

NO BORDER, WHITE ON GREEN;
 6.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [EXIT] E: [476] E;
 12.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [Hammet Rd] E Mod

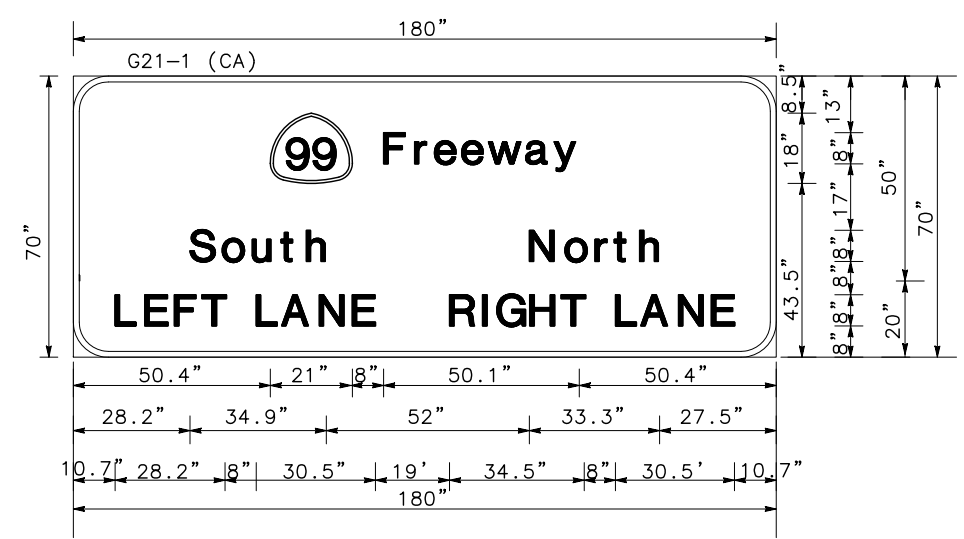


10 in Num
 6 in Caps

13.3 in U.C.
 10 in L.C.
 10 in Caps

NO BORDER, WHITE ON GREEN;
 6.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [EXIT] E: [476] E;
 12.0" RADIUS, 2.0" BORDER, WHITE ON GREEN;
 [Pelandale Ave] E Mod

8 in U.C.
 6 in L.C.
 8 in Caps



9.0" RADIUS, 1.5" BORDER, WHITE ON GREEN;
 [Freeway] E Mod; [South] E Mod; [North] E Mod;
 [LEFT LANE] E Mod; [RIGHT LANE] E Mod;

SIGN DETAILS
 NO SCALE
SD-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	REVISOR	DATE
	KEITH G. MEYER	N MUMMANANI	
		CHUONG NGUYEN	
		CHUONG NGUYEN	



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 CALCULATED-DESIGNED BY
 CHECKED BY
 SISSAY DESALEGN
 ALLEN WANG
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	214	423

Sissay Hailu
 REGISTERED CIVIL ENGINEER
 5/6/12
 DATE

9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

GENERAL NOTES

DESIGN: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS, FIFTH EDITION WITH INTERIM REVISION(S) (LTS-5).

WIND LOADING: NORMAL TO FACE OF SIGN: 40.3 psf ON 100% PANEL COVERAGE.

WALKWAY LOADING: DEAD LOAD + 500 LBS CONCENTRATED LIVE LOAD.

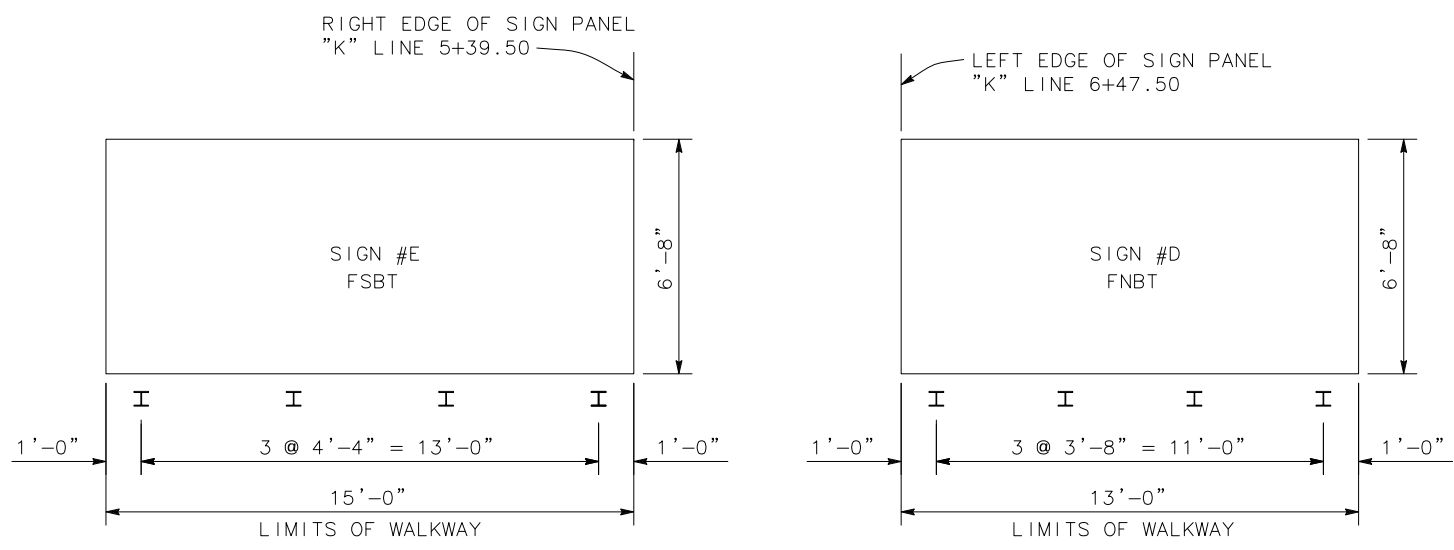
UNIT STRESSES: STRUCTURAL STEEL: $f_y = 36,000$ psi.

NOTES:

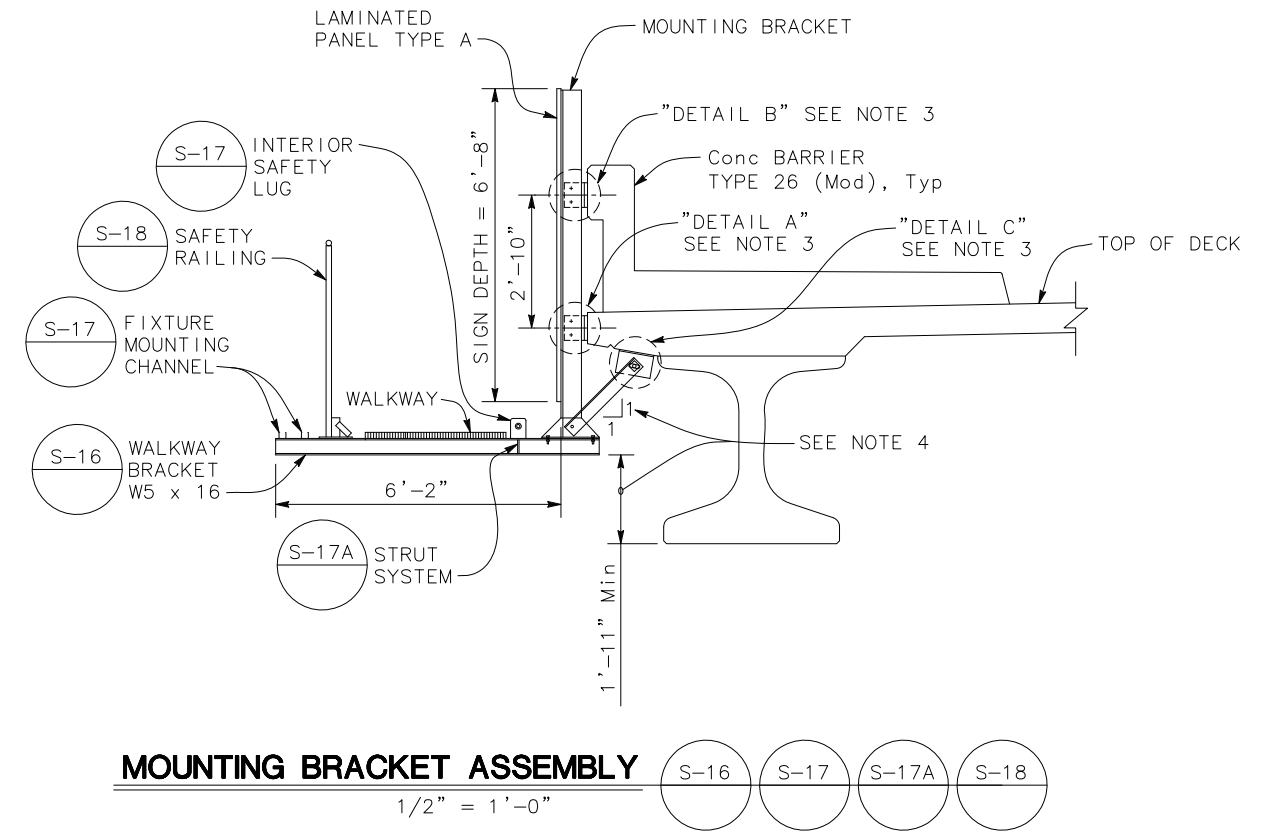
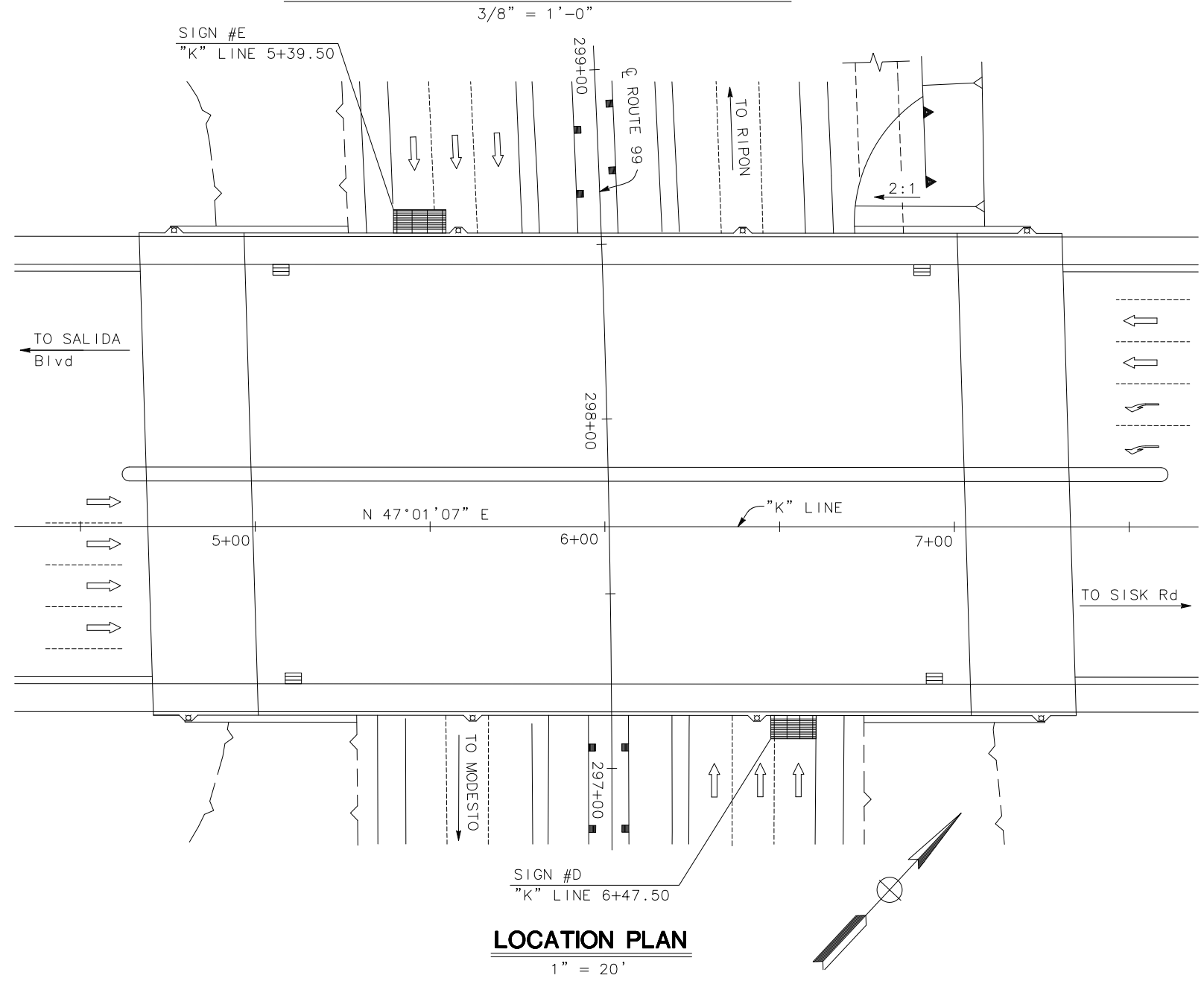
- For Details not shown see "STANDARD PLANS DATED 2010".
- The Contractor shall verify all controlling field dimensions before ordering or fabricating any materials.
- For "Detail A", "Detail B" and "Detail C", see "Mounting And Anchorage Details" sheet.
- Adjust the bottom of sign elevation to have the L 3"x3"x1/4" sloped 1 to 1 (horizontal to vertical).

STANDARD PLANS DATED 2010

- A10A ABBREVIATIONS (SHEETS 1 OF 2)
- A10B ABBREVIATIONS (SHEETS 2 OF 2)
- S16 OVERHEAD SIGNS - WALKWAY DETAILS No. 1
- S17 OVERHEAD SIGNS - WALKWAY DETAILS No. 2
- S17A OVERHEAD SIGNS - WALKWAY DETAILS No. 3
- S18 OVERHEAD SIGNS - WALKWAY SAFETY RAILING DETAILS



MOUNTING BRACKET ASSEMBLY SPACING



MOUNTING BRACKET ASSEMBLY S-16 S-17 S-17A S-18
 1/2" = 1'-0"

**ROUTE 219/99 SEPARATION (REPLACE)
 BRIDGE No. 38-0163
 BRIDGE SIGN LOCATION AND LAYOUT
 SD-4**

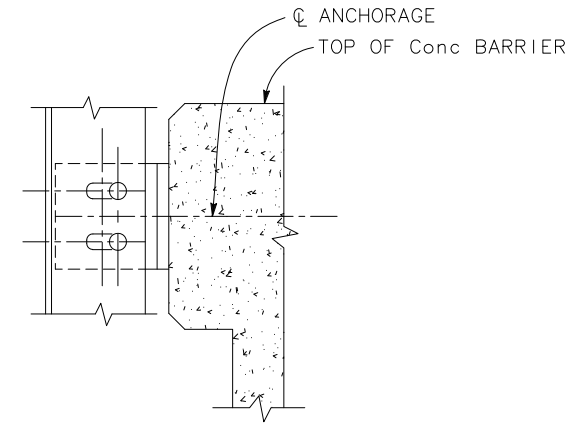
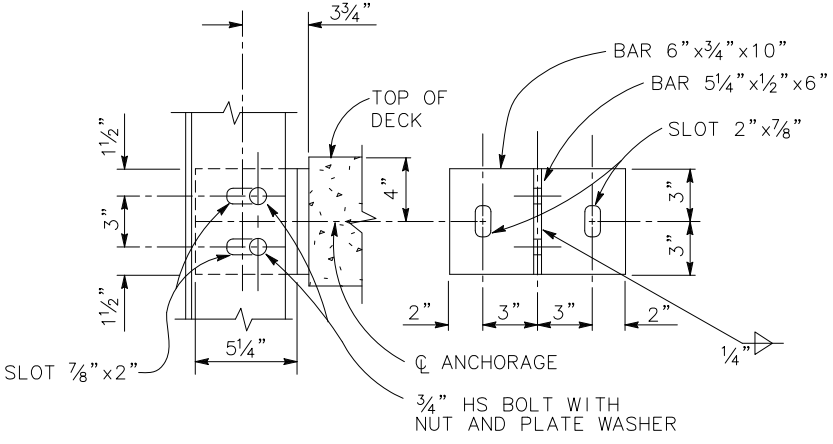
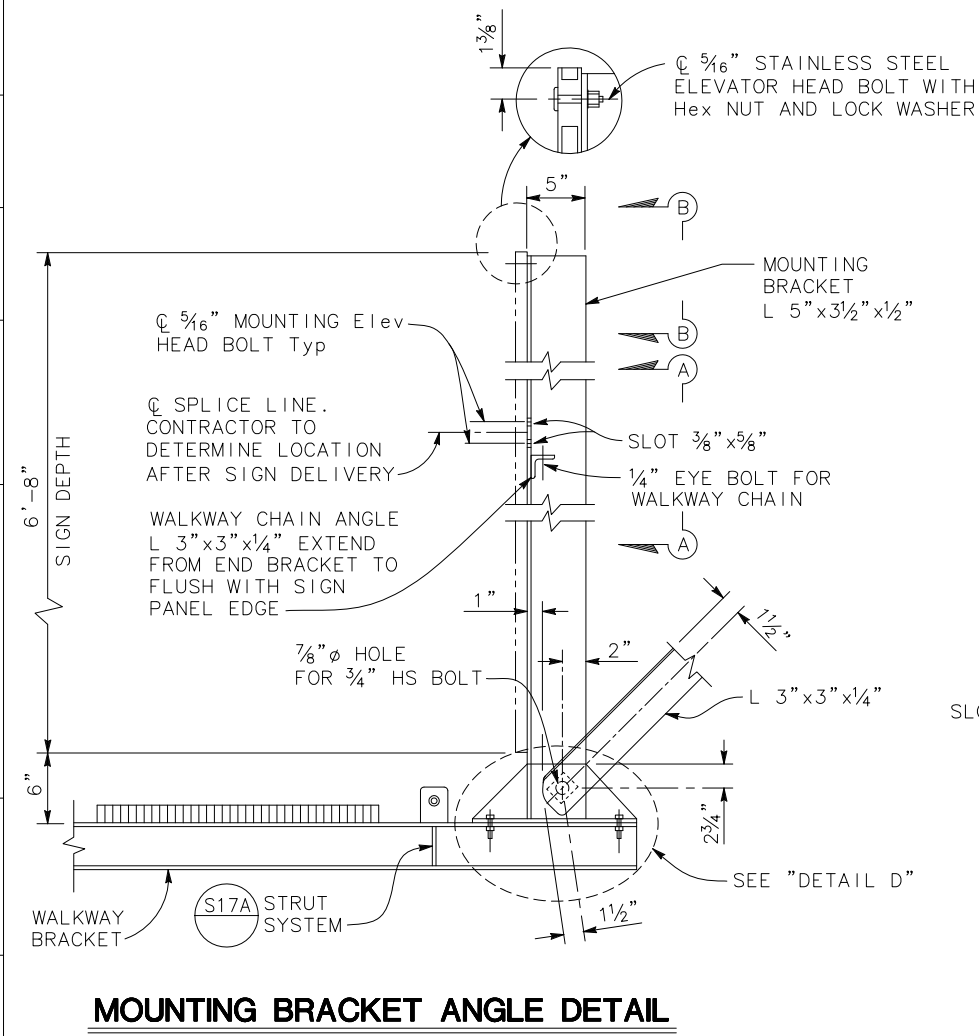
LAST REVISION 00-00-00 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 CALCULATED-DESIGNED BY
 CHECKED BY
 SISSAY DESALEGN
 ALLEN WANG
 REVISOR BY
 DATE REVISOR

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	215	423

Sissay Hailu
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

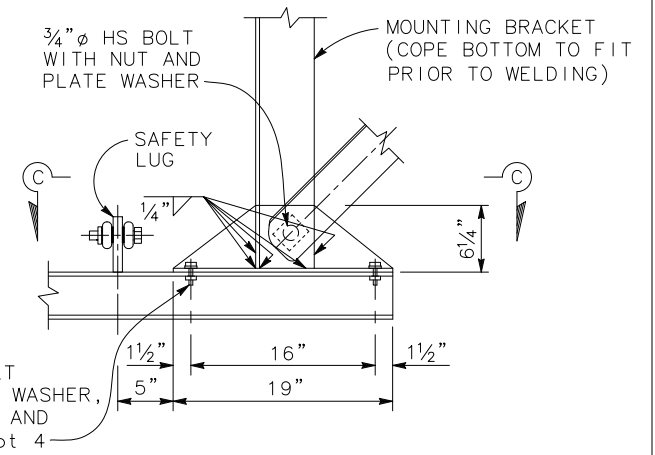
- NOTES:
1. For Details not shown see "STANDARD PLANS".
 2. The Contractor shall verify all controlling dimensions in the field before ordering or fabricating any material.
 3. The exterior mounting bracket to face outward to accommodate the walkway chain angle.
 4. Contractor to provide anchor bolt layout before anchor bolts are placed.
 5. All steel galvanized after fabrication.
 6. All high strength (hs) bolt shall be wrench tightened.



DETAIL A

DETAIL B

(For details not shown, see "DETAIL A")



DETAIL D

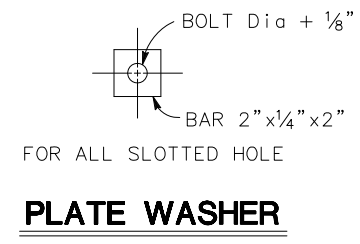
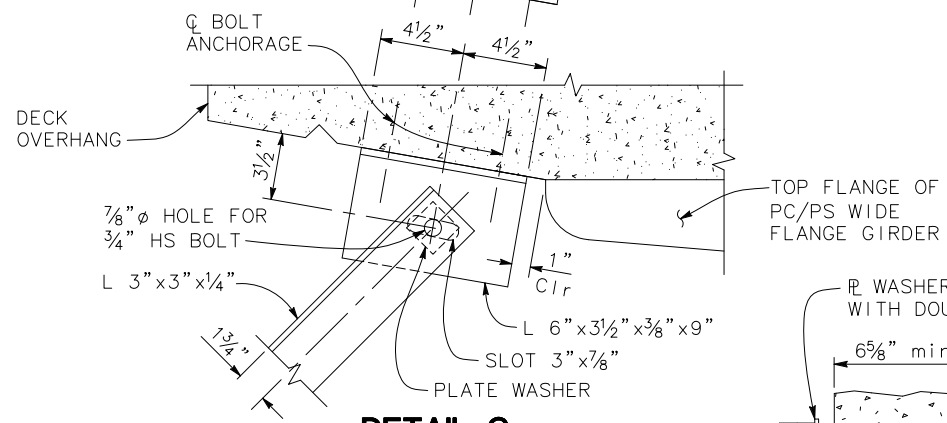
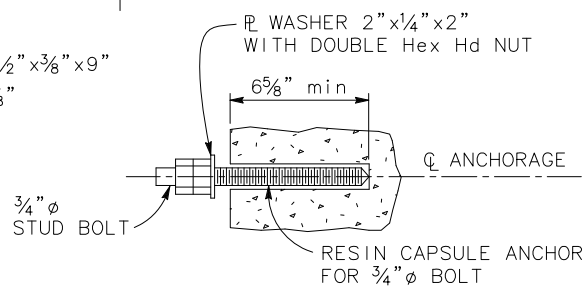


PLATE WASHER

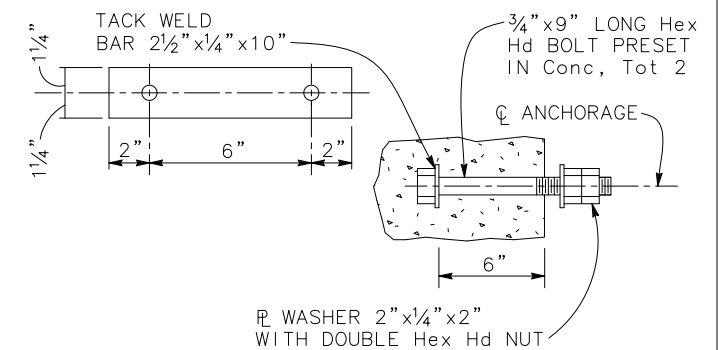


DETAIL C

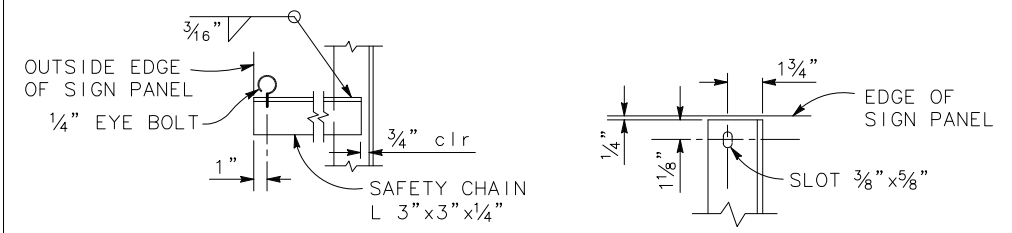


RESIN CAPSULE ANCHORAGE IS SUBJECT TO APPROVAL OF ENGINEER. INSTALLATION PROCEDURES SHALL COMPLY WITH MANUFACTURE'S INSTRUCTIONS.

RESIN CAPSULE BOLT ANCHORAGE

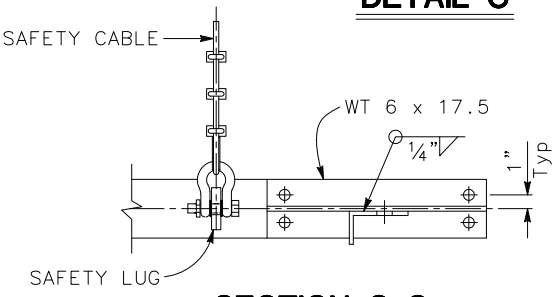


ALTERNATIVE CIP BOLT ANCHORAGE



SECTION A-A

SECTION B-B



SECTION C-C

**ROUTE 219/99 SEPARATION (REPLACE)
 BRIDGE No. 38-0163
 MOUNTING AND ANCHORAGE DETAILS
 NO SCALE SD-5**

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	216	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

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RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE PEDESTRIAN BARRICADE EA	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD) EA	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN EA	SIGN MESSAGE		
				4" x 4"	4" x 6"	6" x 6"	ONE POST EA	TWO POST EA					REMOVE ROADSIDE SIGN EA	REMOVE ROADSIDE SIGN (SSBM)	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063" UNFRAMED			0.080" FRAMED	0.063" FRAMED
S-1	S1-1	G95B (CA)	96 x 60		17'			1			X		GREEN	IV	WHITE	IV	X			40		PARK & RIDE NEXT EXIT			
		G95B-1 (CA)	96 x 18										GREEN	IV	WHITE	IV	X		12		COURTESY OF VINTAGE FAIR				
	S1-2	W34A																			1/2 MILE AHEAD				
		W12-2																			LOW CLEARANCE (15'-1")				
S-2	S1-3	G20-3 (CA)																			SALIDA RIVERBANK NEXT EXIT				
		G66-57 (CA)																			HIGHWAY PATROL				
	S2-1	G87 (CA)	132 x 54		15'			1			X		GREEN	IV	WHITE	IV	X		50		MODESTO NEXT 9 EXITS				
	S2-2	G95B (CA)																			PARK & RIDE NEXT EXIT				
		G95B-1 (CA)																			COURTESY OF VINTAGE FAIR				
	S2-3	G87 (CA)																			MODESTO NEXT 9 EXITS				
	S2-4	M3-3																			SOUTH				
		G28-2 (CA)																				ROUTE MARKER 99 CALIFORNIA			
	S2-5	M3-3	24 x 12							1	X		GREEN	IV	WHITE	IV	X	2				SOUTH			
		G28-2 (CA)	24 x 25							1	X		GREEN	IV	WHITE	IV	X	4.2				ROUTE MARKER 99 CALIFORNIA			
	S2-6	W4-3	48 x 48		15'				1		X		YELLOW	IV	BLACK		X		16			LANE MERGE			
	S2-7	W4-1																				LANE MERGE			
S2-8	W4-7	48 x 48		17'				1		X		YELLOW	IV	BLACK		X		16			THRU TRAFFIC MERGE LEFT				
S2-9	R18A (CA)	66 x 48		17'				1		X		WHITE	IV	BLACK		X		22			RIGHT LANE MUST EXIT				
S2-10	G85-10 (CA)																				BROADWAY KIERNAN Ave (219) ↗				
S2-11	G84-3 (CA)																				EXIT 233				
S2-12	G84-3 (CA)	48 x 60		14'				1		X		GREEN	IV	WHITE	IV	X		20			EXIT 233				
S-4	S4-1	R4-7	24 x 30	12'				1		X		WHITE	IV	BLACK		X		5			KEEP RIGHT				
	S4-2	R4-7																			KEEP RIGHT				
	S4-3	R4-7																			KEEP RIGHT				
	S4-4	R4-7	24 x 30	12'				1		X		WHITE	IV	BLACK		X		5			KEEP RIGHT				
		N	18 x 18																			OBJECT MARKER TYPE N			
	S4-5	R3-1																				TURN PROHIBITION			
	S4-6	R3-1	24 x 24	10'					1		X		WHITE	IV	BLACK		X		4			TURN PROHIBITION			
S4-7	BUS STOP																				BUS STOP				
SHEET TOTAL								7	2			11	2							20.2	86	90			

SIGN QUANTITIES
SQ-1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 N MUMMANENI
 CHUONG NGUYEN
 KEITH G. MEYER

LAST REVISION 05-01-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

ROADSIDE SIGNS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	217	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER 05-06-12
 DATE

9/04/12
 PLANS APPROVAL DATE

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST					SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063"	0.080"	0.063"		
				EA	EA	EA	EA	EA					EA	EA	EA	EA	EA	EA	EA	EA	EA		
	S4-8	BUS STOP			16'		1				X											BUS STOP	
	S4-9	R2-1						1														SPEED LIMIT 25	
	S4-10	R2-1	24 x 30		14'		1				X		WHITE	IV	BLACK		X		5			SPEED LIMIT 25	
	S4-11	R26 (CA)						1														NO PARKING ANY TIME	
	S4-12	R26 (CA)	12 x 18		14'		1				X		WHITE	II	RED	IV	X		1.5			NO PARKING ANY TIME	
	S4-13	R73-5(CA)	36 x 36		15'		1				X		WHITE	IV	BLACK		X		9			INTERSECTION LANE CONTROL	
	S4-14	G7-1(CA)	54 x 18								X		GREEN	IV	WHITE	IV		X	6.8			MAST ARM MOUNTED - SEE ELECTRICAL PLANS	
	S4-15	G7-1(CA)	54 x 18								X		GREEN	IV	WHITE	IV		X	6.8			MAST ARM MOUNTED - SEE ELECTRICAL PLANS	
	S4-16	R73-1(CA)	36 x 30		14'		1				X		WHITE	IV	BLACK		X		7.5			INTERSECTION LANE CONTROL	
	S4-17	R73-2(CA)	36 x 30		14'		1				X		WHITE	IV	BLACK		X		7.5			INTERSECTION LANE CONTROL	
	S4-18	G7-1(CA)	72 x 18								X		GREEN	IV	WHITE	IV		X	9			MAST ARM MOUNTED - SEE ELECTRICAL PLANS	
	S5-1	W4-2	30 x 30		14'		1				X		YELLOW	IV	BLACK			X	6.3			LANE ENDS	
	S5-2	W4-2						1														LANE ENDS	
	S5-3	G92 (CA)	48 x 30		15'		1						GREEN	IV	WHITE	IV		X	10			FREEWAY ENTRANCE	
		G28-2 (CA)	25 x 24							1		X		GREEN	IV	WHITE	IV		X	4.2			ROUTE MARKER 99 CALIFORNIA
		M3-3	24 x 12							1			X		GREEN	IV	WHITE	IV		X	2		SOUTH
		M6-2	21 x 15							1				GREEN	IV	WHITE	IV		X	2.2			DIRECTIONAL ARROW AUXILIARY
	S5-4	G92 (CA)	48 x 30		15'		1				X		GREEN	IV	WHITE	IV		X	10			FREEWAY ENTRANCE	
		G28-2 (CA)	24 x 24							1		X		GREEN	IV	WHITE	IV		X	4			ROUTE MARKER 99 CALIFORNIA
		M3-3	24 x 12							1			X		GREEN	IV	WHITE	IV		X	2		SOUTH
	S5-5	M6-2	21 x 15						1				GREEN	IV	WHITE	IV		X	2.2			DIRECTIONAL ARROW AUXILIARY	
		G92 (CA)								1												FREEWAY ENTRANCE	
		G28-2 (CA)								1												ROUTE MARKER 99 CALIFORNIA	
	S5-6	M3-3																				SOUTH	
		G92 (CA)								1												FREEWAY ENTRANCE	
		G28-2 (CA)								1												ROUTE MARKER 99 CALIFORNIA	
		M3-3																				SOUTH	
		M6-2																				DIRECTIONAL ARROW AUXILIARY	
SHEET TOTAL							9	5		6									69.7	26.3			

SIGN QUANTITIES

SQ-2

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

KEITH G. MEYER

CHUONG NGUYEN

REVISOR BY

DATE REVISED

DATE PLOTTED => \$DATE

TIME PLOTTED => \$TIME

05-01-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	218	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
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 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE ROADSIDE SIGN	REMOVE ROADSIDE SIGN (SSBM)	REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM	FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE		
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST							SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE		STANDARD	PREMIUM	0.063"			0.080"	0.063"
S5-7		R5-10A						EA	EA															PEDESTRIANS BICYCLES MOTOR-DRIVEN CYCLES PROHIBITED		
S5-8		R5-10A	30 x 36		14'		1					X		WHITE	IV	BLACK		X					20.25		PEDESTRIANS BICYCLES MOTOR-DRIVEN CYCLES PROHIBITED	
S5-9		G77-4(CA)	54 x 54		12'							X		GREEN	IV	WHITE	IV	X	X	3					↑NORTH Ⓣ SOUTH →	
		R3-2	24 x 24				1							WHITE	IV	BLACK		X			4				NO LEFT TURN	
S5-10		G77-4(CA)							1																↑NORTH Ⓣ SOUTH →	
S5-11		R6-1	36 x 12	8'			1					X		WHITE	IV	BLACK		X					3		ONE WAY	
S5-12		R5-1							1																DO NOT ENTER	
S5-13		R6-1							1																WRONG WAY	
S5-14		R5-1	36 x 36				1					X		WHITE	IV	RED	IV	X					9		DO NOT ENTER	
		R5-1a	36 x 24		12'		1							WHITE	IV	RED	IV	X					6		WRONG WAY	
S5-15		R3-8a							1																INTERSECTION LANE CONTROL	
S5-16		R61-15(CA)	48 x 30		14'		1					X		WHITE	IV	BLACK		X					10		INTERSECTION LANE CONTROL	
S5-17		R61-15(CA)	48 x 30		14'		1					X		WHITE	IV	BLACK		X					10		INTERSECTION LANE CONTROL	
S5-18		R3-8a							1																ADVANCE INTERSECTION LANE CONTROL	
S5-19		G66-56(CA)																								TRUCK TERMINAL ACCESS
		G66-57(CA)								1																HIGHWAY PATROL
		G66-21A(CA)																								1/2 MILE
S5-20		G66-56(CA)	24 x 24		15'		1					X		GREEN	IV	WHITE	IV	X					4			TRUCK TERMINAL ACCESS
		G66-57(CA)	24 x 24												GREEN	IV	WHITE	IV	X					4		HIGHWAY PATROL
		G66-21A(CA)	24 x 16												GREEN	IV	WHITE	IV	X					2.67		1/2 MILE
SHEET TOTAL							6	1	7												7	20	48.92			

SIGN QUANTITIES
SQ-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 N MUMMANENI
 CHUONG NGUYEN
 KEITH G. MEYER
 REVISOR BY
 DATE REVISED

LAST REVISION 05-01-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	219	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE
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REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
---	--

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE ROADSIDE SIGN (SSBM)	REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST						SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063" UNFRAMED	0.080"	0.063" FRAMED		
				EA	EA	EA	EA	EA																
S-5	S5-21	R5-1						1															DO NOT ENTER	
		R5-1a																					WRONG WAY	
	S5-22	R6-1						1															ONE WAY	
	S5-23	R5-1	36 x 36		12'		1					X		WHITE	IV	RED	IV	X		9				DO NOT ENTER
		R5-1a	36 x 24											WHITE	IV	RED	IV	X		6				WRONG WAY
	S5-24	R6-1	36 x 12	8'		1					X		WHITE	IV	BLACK		X		3				ONE WAY	
	S5-25	R9-3b							1														NO PEDESTRIAN BARRICADE	
	S5-26	R81(CA)	24 x 18		14'		1					X		WHITE	IV	BLACK		X		3				BIKE LANE
		R81A(CA)	12 x 5											WHITE	IV	BLACK		X		0.4				BEGIN
	S5-27	R2-4	24 x 30		15'		1					X		WHITE	IV	BLACK		X		5				40 ZONE AHEAD
	S5-28	R2-4						1															40 ZONE AHEAD	
	S5-29	G22(CA)						1																PELANDALE AVENUE EXIT 1/2 MILE
	S5-30	R3-1(CA)	24 x 24		14'		1					X		WHITE	IV	BLACK		X		4				NO RIGHT TURN
	S5-31	R3-1(CA)						1																NO RIGHT TURN
	S5-32	G78-1(CA)	48 x 42		17'		1					X		GREEN	IV	WHITE	IV		X			14		← SOUTH
	S5-33	R9-3b						1																NO PEDESTRIAN BARRICADE
	S5-34	R81(CA)	24 x 18		14'		1					X		WHITE	IV	BLACK		X		3				BIKE LANE
		R81B(CA)	8 x 5											WHITE	IV	BLACK		X		0.3				END
	S5-35	G78-1(CA)	48 x 42		17'		1					X		GREEN	IV	WHITE	IV		X			14		← NORTH
	S5-36	G78-1(CA)						1																← NORTH
	S5-37	G78-1(CA)						1																← SOUTH
S5-38	R61-17R(CA)	48 x 30		16'		1					X		WHITE	III	BLACK		X				10		INTERSECTION LANE CONTROL	
S5-39	R61-17R(CA)	48 x 30		16'		1					X		WHITE	III	BLACK		X				10		INTERSECTION LANE CONTROL	
S5-40	G66-21A(CA)	24 x 16		14'		1					X		GREEN	IV	WHITE	IV		X	2.6				DISTANCE WITH ARROW	
	G66-57(CA)	24 x 24											GREEN	IV	WHITE	IV		X	4				HIGHWAY PATROL	
S5-41	R61-17(CA)						1																INTERSECTION LANE CONTROL	
S5-42	R61-17(CA)						1																INTERSECTION LANE CONTROL	
S5-43	G66-21A(CA)						1																DISTANCE WITH ARROW	
	G66-57(CA)																						HIGHWAY PATROL	
SHEET TOTAL								11		11										40.3	20	38		

SIGN QUANTITIES

SQ-4

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 N MUMMANENI
 CHUONG NGUYEN
 REVISOR
 DATE REVISOR
 CALCULATED-DESIGNED BY
 CHECKED BY
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 DEPARTMENT OF TRANSPORTATION

LAST REVISION
 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: KEITH G. MEYER
 CHECKED BY: CHUONG NGUYEN
 DESIGNED BY: N. MUMMANENI
 REVISIONS: REVISOR: CHUONG NGUYEN, DATE: 05-06-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	220	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE: 05-06-12
 9/04/12
 PLANS APPROVAL DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE PEDESTRIAN BARRICADE (SSBM)	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	REMOVE PEDESTRIAN BARRICADE EA	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD) EA	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE	
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST							SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063" UNFRAMED	0.080"	0.063" FRAMED			
				EA	EA	EA	EA	EA							EA	EA	EA	EA	EA	EA	EA	EA	EA			EA
S5-44	R61-17R(CA)		48 x 30		15'		1					X		WHITE	IV	BLACK		X				10			INTERSECTION LANE CONTROL	
S5-45	R61-17R(CA)		48 x 30		15'		1					X		WHITE	IV	BLACK		X				10			INTERSECTION LANE CONTROL	
S5-46	R5-1							1																	DO NOT ENTER	
	R5-1a																								WRONG WAY	
S5-47	R5-1		36 x 36		12'		1					X		WHITE	IV	RED	IV	X				9			DO NOT ENTER	
	R5-1a		36 x 24											WHITE	IV	RED	IV	X				6			WRONG WAY	
S5-48	R6-1							1																	ONE WAY	
S5-49	R6-1		36 x 12	8'			1					X		WHITE	IV	BLACK		X				3			ONE WAY	
S5-50	R9-3b									1															NO PEDESTRIAN BARRICADE	
S5-51	R3-1		24 x 24		14'		1					X		WHITE	IV	BLACK		X				4			NO RIGHT TURN	
S5-52	R3-1							1																	NO RIGHT TURN	
S5-53	R9-3b									1															NO PEDESTRIAN BARRICADE	
S5-54	G92 (CA)																								FREEWAY ENTRANCE	
	G28-2 (CA)							1																		ROUTE MARKER 99 CALIFORNIA
	M3-3																									SOUTH
	M6-2																									DIRECTIONAL ARROW AUXILIARY
S5-55	G92 (CA)		48 x 30		14'		1							GREEN	IV	WHITE	IV		X				10			FREEWAY ENTRANCE
	G28-2 (CA)		24 x 24							1		X		GREEN	IV	WHITE	IV		X				4			ROUTE MARKER 99 CALIFORNIA
	M3-3		24 x 12							1				GREEN	IV	WHITE	IV		X				2			SOUTH
	M6-2		21 x 15							1				GREEN	IV	WHITE	IV		X				2.2			DIRECTIONAL ARROW AUXILIARY
S5-56	R5-10A								1																PEDESTRIANS BICYCLES MOTOR-DRIVEN CYCLES PROHIBITED	
S5-57	R5-10A		30 x 36		13'		1				X		WHITE	IV	BLACK		X				7.5				PEDESTRIANS BICYCLES MOTOR-DRIVEN CYCLES PROHIBITED	
S5-58	G92 (CA)		48 x 30		14'		1							GREEN	IV	WHITE	IV		X				10			FREEWAY ENTRANCE
	G28-2 (CA)		24 x 24							1		X		GREEN	IV	WHITE	IV		X				4			ROUTE MARKER 99 CALIFORNIA
	M3-3		24 x 12							1				GREEN	IV	WHITE	IV		X				2			SOUTH
	M6-2		21 x 15							1				GREEN	IV	WHITE	IV		X				2.2			DIRECTIONAL ARROW AUXILIARY
SHEET TOTAL								8		5		2		6								29.5	20	36.4		

SIGN QUANTITIES
SQ-5

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	221	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER
 DATE 05-06-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OR COMPLETENESS OF SCANNED
 COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE PEDESTRIAN BARRICADE (SSEM)	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE	
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST			SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063" UNFRAMED	0.080"	0.063" FRAMED			
				EA	EA	EA	EA	EA									SQFT					
S-5	S5-59	G92 (CA)																		FREEWAY ENTRANCE		
		G28-2 (CA)																		ROUTE MARKER 99 CALIFORNIA SOUTH		
		M3-3																			DIRECTIONAL ARROW AUXILIARY	
		M6-2																				
	S5-60	R81 (CA)																			BIKE LANE	
		R81B (CA)																			BEGIN	
	S5-61	R3-2	24 x 24		12'		1				X	WHITE	II	RED	IV	X		4			NO RIGHT TURN	
	S5-62	R4-7	24 x 30		12'		1				X	WHITE	IV	BLACK		X		5			KEEP RIGHT	
		R6-1	36 x 12									WHITE	IV	BLACK		X		3			ONE WAY	
	S5-63	R3-2						1													NO RIGHT TURN	
	S5-64	R4-7							1												KEEP RIGHT	
		R6-1							1												ONE WAY	
	S5-65	R6-1						1													ONE WAY	
	S5-66	R6-1	36 x 12	8'			1				X	WHITE	IV	BLACK		X		3			ONE WAY	
	S5-67	R5-1	36 x 36		12'		1				X	WHITE	IV	RED	IV	X		9			DO NOT ENTER	
		R5-1	36 x 24									WHITE	IV	RED	IV	X		6			WRONG WAY	
	S5-68	R5-1							1												DO NOT ENTER	
		R5-1							1												WRONG WAY	
	S5-69	M6-1							1													DIRECTIONAL ARROW AUXILIARY
		G66-56 (CA)							1													TRUCK TERMINAL ACCESS
S5-70	G1-7 (CA)	48 x 24						1													← SALIDA RIVERBANK →	
S5-71	M6-1	21 x 15		15'		1				X	GREEN	IV	WHITE	IV	X		2.25				DIRECTIONAL ARROW AUXILIARY	
	G66-56 (CA)	24 x 24									GREEN	IV	WHITE	IV	X		4				TRUCK TERMINAL ACCESS	
S5-72	G1-7 (CA)	48 x 24		14'		1				X	GREEN	IV	WHITE	IV	X		8				← SALIDA RIVERBANK →	
S5-73	R4-7	24 x 30		12'		1				X	WHITE	IV	BLACK		X		5				KEEP RIGHT	
	N	18 x 18																			OBJECT MARKER TYPE N	
S5-74	R4-7	24 x 30		12'		1				X	WHITE	IV	BLACK		X		5				KEEP RIGHT	
	N	18 x 18																			OBJECT MARKER TYPE N	
S5-75	R91-3 (CA)	36 x 54		15'		1				X	WHITE	IV	BLACK		X		13.5				CARPOOL 2 OR MORE ONLY	
S5-76	R91-3 (CA)	36 x 54		15'		1				X	WHITE	IV	BLACK		X		13.5				CARPOOL 2 OR MORE ONLY	
S5-77	R91-3 (CA)	36 x 54		15'		1				X	WHITE	IV	BLACK		X		13.5				CARPOOL 2 OR MORE ONLY	
SHEET TOTAL							11	8									40	31.5	14.25			

SIGN QUANTITIES

SQ-6

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Galtrans

REVISOR BY
 N MUMMANENI
 CHUONG NGUYEN

CALCULATED-DESIGNED BY
 KEITH G. MEYER

CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER

BORDER LAST REVISED 7/2/2010
 USERNAME => \$USER
 DGN FILE => \$REQUEST

RELATIVE BORDER SCALE IS IN INCHES
 0 1 2 3

UNIT 1457 PROJECT NUMBER & PHASE 100000100

LAST REVISION
 05-01-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: N MUMMANANI
 CHECKED BY: CHUONG NGUYEN
 REVISIONS: REVISOR: N MUMMANANI, DATE: 05-18-12, REVISION: 1, DESCRIPTION: REVISED BY DATE

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	222	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE ROADSIDE SIGN (SSBM)	REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE		
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST				SINGLE FACED	DOUBLE FACED	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063" UNFRAMED			0.080"	0.063" FRAMED
				EA	EA	EA	EA	EA				EA	EA	EA	EA	EA	EA	EA	EA	EA			EA	EA
S-5	S5-78	R91-3(CA)	36 x 54		15'		1					X	WHITE	IV	BLACK		X		13.5			CARPOOL 2 OR MORE ONLY		
	S5-79	R91-3(CA)	36 x 54		15'		1					X	WHITE	IV	BLACK		X		13.5			CARPOOL 2 OR MORE ONLY		
	S5-80	R61-15(CA)	48 x 30									X	WHITE	IV	BLACK		X		10			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-81	R73-5(CA)	36 x 36									X	WHITE	IV	BLACK		X		9			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-82	R3-18(CA)	36 x 36									X	WHITE	II	RED	II	X		9			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-83	R73-1(CA)	36 x 30									X	WHITE	IV	BLACK		X		7.5			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-84	R61-17R(CA)	48 x 30									X	WHITE	IV	BLACK		X		10			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-85	R89-1(CA)	45 x 20									X	WHITE	IV	BLACK		X		6.3			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-86	R89-1(CA)	45 x 20									X	WHITE	IV	BLACK		X		6.3			MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S5-87	R88(CA)	30 x 30		14'		1					X	WHITE	IV	BLACK		X		6.3			HOV LEFT LANE DO NOT STOP		
	S5-88	R5-10C	30 x 36		14'		1					X	WHITE	IV	BLACK		X		7.5			PEDESTRIANS PROHIBITED		
	S5-89	R5-10C	30 x 36		14'		1					X	WHITE	IV	BLACK		X		7.5			PEDESTRIANS PROHIBITED		
S5-90	R89-1(CA)	45 x 20								1		X	WHITE	IV	BLACK		X		6.3			1 CAR PER GREEN EACH LANE		
	R90(CA)	24 x 30								1		X	WHITE	IV	BLACK		X		5			STOP HERE ON RED		
S-6	S6-1	R2-1	24 x 30		15'		1					X	WHITE	IV	BLACK		X		5			SPEED LIMIT 40		
		R48-1(CA)	36 x 18										X	WHITE	IV	BLACK		X		4.5			RADAR ENFORCED	
	S6-2	R2-1							1														SPEED LIMIT 40	
		R48-1(CA)																					RADAR ENFORCED	
	S6-3	R3-2						1														NO LEFT TURN		
	S6-4	G77-4(CA)	36 x 36		14'		1					X	GREEN	IV	WHITE	IV	X		9			↑ SOUTH ↻ NORTH →		
	S6-5	G77-4(CA)						1														↑ SOUTH ↻ NORTH →		
	S6-6	R81(CA)							1														BIKE LANE	
		R26(CA)																					NO STOPPING ANY TIME	
	S6-7	G21-3(CA)						1														↻ FREEWAY SOUTH LEFT LANE NORTH RIGHT LANE		
	S6-8	R3-5R							1														MANDATORY MOVEMENT LANE CONTROL (RIGHT TURN ONLY)	
	S6-9	R3-5R	24 x 18		14'		1					X	WHITE	IV	BLACK		X		3				MANDATORY MOVEMENT LANE CONTROL (RIGHT TURN ONLY)	
S6-10	D11-1	36 x 24		14'		1					X	GREEN	IV	WHITE	IV	X		6				BIKE ROUTE		
	M7-1	12 x 9																0.75				BICYCLE DIRECTIONAL ARROW AUXILIARY		
S6-11	D11-1							1														BIKE ROUTE		
	M7-1																					BICYCLE DIRECTIONAL ARROW AUXILIARY		
S6-12	R81(CA)	24 x 18		14'		1					X	WHITE	IV	BLACK		X		3				BIKE LANE		
	R26(CA)	12 x 18																1.5				NO PARKING ANY TIME		
SHEET TOTAL							9	1	7		2							87.7	47	15.75				

SIGN QUANTITIES
SQ-7

LAST REVISION DATE PLOTTED => \$DATE 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	223	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE ROADSIDE SIGN (SSBM)	REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE
				4" x 4"	4" x 6"	6" x 6"	ONE POST EA	TWO POST EA						SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063" UNFRAMED	0.080" UNFRAMED	0.063" FRAMED		
S6-15	R1-1		24 x 24		15'		1				X		WHITE	IV	BLACK		X		4			STOP		
	G7-1(CA)		72 x 18										GREEN	IV	WHITE	IV	X			9		STREET NAME		
	S6-16	R1-1						1														STOP		
S6-17	G7-1(CA)																					STREET NAME		
S7-1	R6-1							1														ONE WAY		
S7-2	R28A(CA)		24 x 30		15'		1				X		WHITE	IV	BLACK		X		5			NO STOPPING ANY TIME (WITH ARROW)		
S7-3	R28A(CA)							1														NO STOPPING ANY TIME (WITH ARROW)		
S7-4	G8-7(CA)		48 x 12		15'		1				X		GREEN	IV	WHITE	IV	X			4		↑ @9 FREeway KIERNAN CT →		
S7-5	G8-7(CA)							1														↑ @9 FREeway KIERNAN CT →		
S7-6	R28A(CA)		24 x 30		15'		1				X		WHITE	IV	BLACK		X		5			NO STOPPING ANY TIME (WITH ARROW)		
S7-7	R81(CA)		24 x 18		14'		1				X		WHITE	IV	BLACK		X		3			BIKE LANE		
S7-8	R81(CA)							1														BIKE LANE		
S7-9	R6-1							1														ONE WAY		
S7-10	R4-7		36 x 12								X		WHITE	IV	BLACK		X		3			KEEP RIGHT		
S7-10	R4-7		24 x 30		12'		1						WHITE	IV	BLACK		X		5			KEEP RIGHT		
S7-10	N		18 x 18																			OBJECT MARKER TYPE N		
S7-11	D11-1							1														BIKE ROUTE		
S7-11	M7-1																					BICYCLE DIRECTIONAL ARROW AUXILIARY		
S7-12	D11-1		30 x 24		14'		1				X		GREEN	IV	WHITE	IV	X	5				BIKE ROUTE		
S7-12	M7-1		12 x 9										GREEN	IV	WHITE	IV	X	0.8				BICYCLE DIRECTIONAL ARROW AUXILIARY		
S7-13	G7-1(CA)		72 x 18								X		GREEN	IV	WHITE	IV	X	9				MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
S7-14	R73-2(CA)		24 x 24								X		WHITE	III	BLACK		X	4				MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
S7-15	R26(CA)							1														NO PARKING ANY TIME		
S7-16	R26A(CA)		24 x 30		14'		1				X		WHITE	II	RED	IV	X	5				NO PARKING ANY TIME		
S7-17	D11-1							1														BIKE ROUTE		
S7-17	M7-1																					BICYCLE DIRECTIONAL ARROW AUXILIARY		
S7-18	D11-1		30 x 24		15'		1				X		GREEN	IV	WHITE	IV	X	5				BIKE ROUTE		
S7-18	M7-1		12 x 9										GREEN	IV	WHITE	IV	X	0.8				BICYCLE DIRECTIONAL ARROW AUXILIARY		
S7-19	R28A(CA)							1														NO STOPPING ANY TIME (WITH ARROW)		
S7-20	R28A(CA)		24 x 30		15'		1				X		WHITE	IV	BLACK		X		5			NO STOPPING ANY TIME (WITH ARROW)		
SHEET TOTAL							9	2	11										59.6	13				

SIGN QUANTITIES

SQ-8



LAST REVISION | DATE PLOTTED => \$DATE | 06-08-12 | TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: N MUMMANENI
 CHECKED BY: CHUONG NGUYEN
 REVISOR BY: CHUONG NGUYEN
 DATE REVISED: 05-18-12

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	224	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER
CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE		
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST					REMOVE ROADSIDE SIGN	REMOVE ROADSIDE SIGN (SSBM)	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	0.063"			0.080"	0.063"
				EA	EA	EA	EA	EA					EA	EA	EA	EA	EA	EA	EA	EA	EA			EA	EA
S-7	S7-21	G7-1 (CA)	48 x 18								X		GREEN	IV	WHITE	IV		X			6		MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S7-22	R73 (3L)	48 x 30								X		WHITE	IV	BLACK		X		10				MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
	S7-23	R6-1R	36 x 12											WHITE	IV	BLACK		X		3			ONE WAY		
		R4-7	24 x 30		12'			1				X		WHITE	IV	BLACK		X		5			KEEP RIGHT		
		N	18 x 18																				OBJECT MARKER TYPE N		
	S7-24	D11-1	24 X 18		15'			1				X		GREEN	IV	WHITE	IV		X		3			BIKE ROUTE	
		M7-1	21 X 16											WHITE	IV	BLACK		X		2.3				DIRECTIONAL ARROW AUXILIARY	
	S7-25	G7-1 (CA)	72 x 18									X		GREEN	IV	WHITE	IV		X			9		MAST ARM MOUNTED - SEE ELECTRICAL PLANS	
	S7-26	R73-1 (CA)	36 X 30									X		WHITE	IV	BLACK		X		7.5				MAST ARM MOUNTED - SEE ELECTRICAL PLANS	
	S7-27	R2-1	24 x 30		15'			1				X		WHITE	IV	BLACK		X		5				SPEED LIMIT 35 MPH	
	S7-28	R2-1							1															SPEED LIMIT 35 MPH	
	S7-29	W9-1	30 x 30		15'			1				X		YELLOW	IV	BLACK			X		6.3				RIGHT LANE ENDS
	S7-30	W9-1							1															RIGHT LANE ENDS	
	S7-31	W4-2							1															LANE ENDS	
	S7-32	W4-2	30 x 30		15'			1				X		YELLOW	IV	BLACK		X		6.3				LANE ENDS	
	S7-33	R3-5R (CA)	24 x 30		12'			1				X		WHITE	IV	BLACK		X		5				RIGHT TURN ONLY	
	S7-34	R6-1R																						ONE WAY	
R4-7								1															KEEP RIGHT		
S7-35	G7-1 (CA)	48 x 18									X		GREEN	IV	WHITE	IV		X			6		MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
S7-36	R13A (CA)	24 X 24									X		WHITE	IV	BLACK		X		6				NO RIGHT TURN ON RED		
S7-37	R73-2 (CA)	24 X 36							1		X		WHITE	IV	BLACK		X				4		MAST ARM MOUNTED - SEE ELECTRICAL PLANS		
S-9	S9-1	R4-1	30 X 24		14'		1				X		WHITE	IV	BLACK		X		5				DO NOT PASS		
S-10	S10-1	R61-15 (CA)	48 X 30		15'		1				X		WHITE	III	BLACK		X			10			INTERSECTION LANE CONTROL		
	S10-2	R61-15 (CA)	48 X 30		15'		1				X		WHITE	III	BLACK		X			10			INTERSECTION LANE CONTROL		
	S10-3	R3-8A (CA)						1															INTERSECTION LANE CONTROL		
	S10-4	R3-8A (CA)						1															INTERSECTION LANE CONTROL		
	S10-5	G84-3 (CA)						1															EXIT 233		
	S10-6	G84-3 (CA)	48 x 60			16'	1				X		GREEN	IV	WHITE	IV		X			20			EXIT 233	
	S10-7	W4-1																					LANE MERGE		
	S10-8	W4-1	30 x 30		15'		1					X		YELLOW	IV	BLACK		X		6.3				LANE MERGE	
	S10-9	R93-1 (CA)	36 x 54		15'		1					X		WHITE	IV	BLACK		X			13.5			CARPPOOL 2 OR MORE ONLY	
	S10-10	R93-1 (CA)	36 x 54		15'		1					X		WHITE	IV	BLACK		X			13.5			CARPPOOL 2 OR MORE ONLY	
	S10-11	R88 (CA)	30 x 30		14'		1					X		WHITE	IV	BLACK		X		6.3				HOV LEFT LANE DO NOT STOP	
SHEET TOTAL							14				8									77	47	45			

SIGN QUANTITIES

SQ-9

LAST REVISION | DATE PLOTTED => \$DATE
06-08-12 | TIME PLOTTED => \$TIME

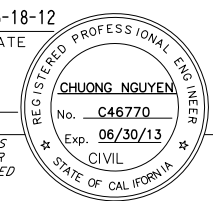
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	225	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



ROADSIDE SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE (in)	POST SIZE AND LENGTH			ROADSIDE SIGN		REMOVE PEDESTRIAN BARRICADE	INSTALL SIGN (STRAP AND SADDLE BRACKET METHOD)	SINGLE FACED	DOUBLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM		FURNISH SINGLE SHEET ALUMINIUM SIGN			RELOCATE ROADSIDE SIGN	SIGN MESSAGE			
				4" x 4"	4" x 6"	6" x 6"	ONE POST	TWO POST					REMOVE ROADSIDE SIGN	REMOVE ROADSIDE SIGN (SSBM)	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	STANDARD	PREMIUM	UNFRAMED			FRAMED		
				EA	EA	EA	EA	EA					EA	0.063"							0.080"				0.063"	SQFT
S-10	S10-12	R89-1(CA)	45 x 20								X		WHITE	IV	BLACK		X		6.3			MAST ARM MOUNTED - SEE ELECTRICAL PLANS				
	S10-13	R89-1(CA)	45 x 20								X		WHITE	IV	BLACK		X		6.3			MAST ARM MOUNTED - SEE ELECTRICAL PLANS				
	S10-14	R89-1(CA)	45 x 20							1		X		WHITE	IV	BLACK		X		6.3			1 CAR PER GREEN EACH LANE			
R90(CA)		24 x 30							1				WHITE	IV	BLACK		X		5			STOP HERE ON RED				
S-11	S11-1	G85-10																				Broadway Kiernan Ave (219) ↗				
	S11-2	W12-2																				LOW CLEARANCE (15'-5")				
		W23-1																					SLOW TRAFFIC AHEAD (1/2 MILE AHEAD)			
SHEET TOTAL																										
TOTAL FROM SHEET SQ-1							7	2	11	2											20.2	86	90			
TOTAL FROM SHEET SQ-2							9		5												69.7	26.3				
TOTAL FROM SHEET SQ-3							6	1	7												7	20	48.92			
TOTAL FROM SHEET SQ-4							11		11		1										40.3	20	38			
TOTAL FROM SHEET SQ-5							8		5		2										29.5	20	36.4			
TOTAL FROM SHEET SQ-6							11		6													40	31.5	14.25		
TOTAL FROM SHEET SQ-7							9	1	7													87.7	47	15.75		
TOTAL FROM SHEET SQ-8							9	2	11													59.6		13		
TOTAL FROM SHEET SQ-9							14		8													77	47	45		
TOTAL							84	6	73	2	3										454.9	297.8	301.32			

SIGN QUANTITIES
SQ-10

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Keith G. Meyer
 N. MUMMANENI
 CHUONG NGUYEN
 REVISOR BY
 DATE REVISOR

LAST REVISION DATE PLOTTED => \$DATE 06-08-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 N MUMMANENI
 CHECKED BY
 CHUONG NGUYEN
 REVISOR BY
 DATE REVISOR

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5		423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

OVERHEAD SIGNS

SHEET No.	SIGN No.	SIGN CODE	PANEL SIZE	FURNISH SIGN STRUCTURE (TRUSS)		INSTALL SIGN STRUCTURE (TRUSS)		CIDH CONCRETE PILING (SIGN FOUNDATION)	FURNISH LAMINATED PANEL SIGN (TYPE A) (OVERHEAD)	SINGLE FACED	BACKGROUND		LEGEND		GRAFFITI FILM	SIGN MESSAGE
				LB	LB	60" LF	SQFT				SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE	SHEETING COLOR	RETROREFLECTIVITY ASTM TYPE		
S-1	A	G83 (CA)	22' x 8.33'	16,142	16,142			27	183	X	GREEN	IV	WHITE	IV	X	Broadway Kiernan Ave ²¹⁹ EXIT ONLY
S-2	B	G85 (CA)	22' x 8.33'	17,158	17,158			27	183	X	GREEN	IV	WHITE	IV	X	Broadway Kiernan Ave ²¹⁹ EXIT ONLY
S-10	C	G85-11 (CA)	21' x 8.33'	21,447	21,447			27.2	175	X	GREEN	IV	WHITE	IV	X	Broadway Kiernan Ave
S-5	D	G83-4 (CA)	13' x 6.67'			1,798	1,798	25	85	X	GREEN	IV	WHITE	IV	X	Hammet Rd 1 1/2 MILES
S-5	E	G83-4 (CA)	15' x 6.67'			1,883	1,883	25	98	X	GREEN	IV	WHITE	IV	X	Pelandale Ave 1/2 MILE
S-6	F	G21-1 (CA)	15' x 5.83'	22,161	22,161			26.8	90	X	GREEN	IV	WHITE	IV	X	99 FREEWAY South LEFT LANE North RIGHT LANE
S-1	G	CMS		14,410	14,410			27		X						
TOTAL				91,318	91,318	3,681	3,681	185	814							

SIGN QUANTITIES
SQ-11

LAST REVISION 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 DAVID LIU
 REVISOR BY
 DATE REVISED

PLACE HMA DIKE

STATION	Loc	PLACE HMA DIKE				HMA-A	REMOVE HMA DIKE
		TYPE A	TYPE C	TYPE E	TYPE F		
		LF				TON	LF
"SA" 41+36.91 TO 46+48.02	L+	443				12.6	450
"SA" 45+82.66 TO 46+17.77	R+	36				1.1	51
"SA" 49+90.65 TO 55+33.09	L+ & R+	750				21.3	760
"SR99" 282+00.00 TO "KR1" 297+22.54	R+		63	1,630	25	45.3	
"SR99" 287+37.08 TO 289+09.55	L+		173			4.7	
"SR99" 292+62.00 TO 297+20.00	R+		458			12.5	
"SR99" 296+66.00 TO 298+72.00	L+		187			5.1	
"SR99" 298+66.04 TO 300+19.47	R+		65			1.8	
"SR99" 306+15.92 TO 306+58.68	R+		44			1.2	
"KL1" 282+50 TO 284+02.59	L+		152			4.1	
"KL1" 294+50 TO 296+85.23	L+		63	149	25	4.9	
"KR2" 298+23.10 TO 303+18.53	R+		63	408	25	12.0	
"KR2" 307+70.00 TO 313+36	R+			565		15.4	
FROM TRAFFIC HANDLING PLANS		434				12.4	
TOTAL		1,663	189	3,831	75	154.4*	1,261



(*) - QUANTITY INCLUDED IN ROADWAY QUANTITIES TABLE.

ROADWAY QUANTITIES

STATION	CLASS 2 AGGREGATE BASE	RUBBERIZED HOT MIXED ASPHALT (GAP GRADED)	HOT MIXED ASPHALT (TYPE A)
"S" 9+49.35 TO 13+05.12		65	217
"S" 15+10.98 TO 23+15.09		131	435
"SA" 42+61.25 TO 46+51.30	122		110
"SA" 47+97.93 TO 55+33.09	992		793
"K" 0+50.13 TO 4+69.26	1,150	529	1,833
"K" 7+33.26 TO 18+39.62	2,631	1,229	2,970
"K" 19+01.91 TO 27+62.01	1,397	471	1,430
"KR1" 284+48.20 TO 297+15.58	1,538		1,928
"KL1" 279+80 TO 296+86.06	2,279		3,605
"KR2" 298+27.41 TO 313+36.00	1,469		2,776
"KL2" 298+94.24 TO 304+60.00	590		1,012
"SR99" 269+97.39 TO 279+80.00	1,843	360	1,011
"SR99" 268+49.08 TO 284+48.20	2,454	468	1,253
"SR99" 313+36.00 TO 319+36.12	747	143	473
FROM HMA DIKE			155
FROM TRAFFIC HANDLING	1,175		3,782
AT SOUND WALL			2
AT MVPs	102		293
FOR UNDERDRAIN RECONSTRUCTION			73
TOTAL	18,489	3,396	24,151



MINOR CONCRETE (TEXTURED PAVING)

STATION	Loc	CONTRASTED GORE	ISLAND PAVING
		SQFT	
"KL1" 285+65.98 TO 287+37.88	R+	2,655	
"KR1" 287+18.92 TO 288+72.18	L+	2,670	
"KL2" 306+02.47 TO 307+36.50	R+	1,900	
"KR2" 306+53.71 TO 307+70.00	L+	1,767	
"SA" 48+06.36 TO 50+42.26	R+		955
"SA" 47+83.95 TO 48+22.94	L+		522
"K" 4+40.40 TO 7+58.90	L+		1,271
"K" 8+73.29 TO 11+89.91	L+		2,270
"K" 17+00.00 TO 18+07.38	L+		545
"K" 19+19.50 TO 19+50.44	L+		223
TOTAL		8,992	5,786

REMOVE SOUND WALL

LOCATION	Loc	LENGTH
		LF
"KK" 16+01.71 TO 17+84.87	Lt	252
TOTAL		252

SHOULDER BACKING

LOCATION	Loc	SHOULDER BACKING
		TON
"K" 19+46.34 TO 26+63.36	Med	85
TOTAL		85

UTILITIES

LOCATION	16" WATER PIPE	EXPANSION JOINT (N)	WATER VALVE (N)	SAND BACKFILL
				LF
WATER LINE "K" 3+06 TO 8+95	589	2	3	34.7
MID FACILITIES "K" 8+96 TO 13+00				36.9
FROM DRAINAGE				50.1
TOTAL	589	2	3	121.7



(N)- NOT A SEPARATE PAY ITEM, FOR INFORMATION ONLY.



REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012



REVISED PER ADDENDUM No. 3 DATED OCTOBER 10, 2012

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	227	423

05-18-12
 REGISTERED CIVIL ENGINEER DATE
 CHUONG NGUYEN
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

DESTROY WELL

STATION	Loc	DESTROY WELL
		EA
"KL2" 299+86	L+	1
TOTAL		1

BOLLARD

STATION	Loc	6" BOLLARD
		EA
"K" 1+74	R+	6
"K" 9+12	L+	7
TOTAL		13

8" PERFORATED PLASTIC PIPE UNDERDRAIN

STATION	Loc	8" PPP UD
		LF
"SR 99" 296+75 TO 299+17	L+	403
"SR 99" 292+63 TO 298+83	R+	579
TOTAL		982

TEMPORARY CULVERT

LOCATION	Loc	LENGTH
		LF
"SR99" 296+90 TO 299+17	Med	234
TOTAL		234

SUMMARY OF QUANTITIES

Q-1

LAST REVISION DATE PLOTTED => \$DATE
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 N MUMMANENI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

EARTHWORK QUANTITIES

STATION	ROADWAY EXCAVATION	ROADWAY EXCAVATION (TYPE Z-2) (ADL)	EMBANKMENT (N)	PULVERIZE PAVEMENT
	CY			SY
"S" 9+49.35 TO 13+05.41	345			
"S" 15+10.98 TO 23+15.09	870			
"SA" 41+37.19 TO 55+33.09	1,663			
"K" 0+39.89 TO 4+69.26	1,223		759	2,427
"K" 7+33.26 TO 27+63.08	4,372		2,786	2,897
"KR1" 284+48.20 TO 297+15.58	4,809		1,016	455
"KL1" 278+70.00 TO 296+85.44	9,590		729	1,226
"KR2" 298+25.16 TO 313+61.00	2,874		3,151	2,734
"KL2" 298+91.66 TO 304+20.00	3,093		694	1,470
"SR99" 267+58.21 TO 284+47.18	5,234	1,001	764	
"SR99" 298+61.63 TO 300+21.63	530			
"SR99" 313+61.00 TO 319+61.00	872		33	
BASIN EXCAVATION	2,488			
REMOVE TEMPORARY PAVEMENT	2,742			
TOTAL	40,705	1,001	9,932	11,209

(N)- NOT A SEPARATE PAY ITEM, FOR INFORMATION ONLY.

REMOVE CONCRETE

STATION	Loc	CURB & GUTTER	SIDEWALK	MEDIAN/ ISLAND	ROADWAY
		CY			
"S" 9+49.35 TO 13+00.42	R+	22.6	50.8		
"S" 9+49.35 TO 14+63.92	R+	51.9	5.4		
"SA" 48+23.20 TO 51+43.04	L+	37.7			42.9
"B" 6+31.86 TO 6+93.33	L+	5.5	9.9		
"K" 0+47.96 TO 3+65.82	R+	40.5	51.2		
"K" 4+09.34 TO 4+99.86	R+	16.5	13.8		
"K" 7+02.70 TO 8+08.31	R+	12.2	15.1		
"K" 8+46.34 TO 10+50.00	R+	12.4	21.5		
"K" 8+38.62 TO 14+18.79	L+	39.2	85.1		
"K" 11+87.65 TO 14+02.92	R+	10.0	28.4		
"K" 17+25.40 TO "S" 14+81.67	L+	9.3	10.9		
"K" 8+20.17 TO 13+66.69	R+	78.0		83.9	47.7
"K" 19+02.32 TO 19+33.59	R+	2.8		4.7	
SUBTOTAL		338.6	292.1	88.6	90.6
TOTAL			809.9		

COLD PLANE AC PAVEMENT (0.15' Max)

STATION	Loc	AREA
		SQYD
"K" 1+50.00 TO 1+80.00	L+ & R+	225
"K" 14+20.45 TO 14+72.75	L+	310
"K" 17+73.76 TO 18+02.93	L+	63
"K" 19+21.00 TO 23+17.90	L+	431
"KR1" 287+50.00 TO 287+90.00	L+ & R+	63
"KL2" 303+40.00 TO 304+60.00	L+	160
FROM TRAFFIC HANDLING		31
TOTAL		1,283

MINOR CONCRETE (MISCELLANEOUS CONSTRUCTION)

STATION	Loc	CONCRETE CURB TYPE			SIDEWALK/ PATHWAY	CURB RAMP	DRIVEWAY	MEDIAN	RAISED ISLAND	GUTTER
		(TYPE A1-6)	(TYPE A2-6)	RETAINING						
CY										
"B" 6+38.61 TO 6+81.74	L+		4.4		7.6	0.8				
"K" 0+53.13 TO 3+56.33	R+		20.7		21.0	3.1	5.0			
"K" 0+52.49 TO 3+61.56	L+		22.0		28.6	2.4				
"K" 4+08.82 TO 4+70.62	R+	1.5	6.0		9.9	0.8				
"K" 4+18.35 TO 4+67.00	L+		4.6		6.8	0.8		1.5		
"K" 7+34.62 TO 8+04.25	R+		5.9		9.0	0.8				
"K" 7+30.00 TO 7+89.73	L+	1.4	4.7		6.8	0.8		1.3		
"K" 8+50.21 TO 10+50.00	R+		12.6		14.7	1.6				
"K" 8+37.61 TO 14+18.71	L+		38.2		38.0	2.4	8.8			
"K" 11+87.65 TO 14+02.82	R+		12.4		16.9					
"K" 14+72.75 TO 19+48.68	L+	2.1	26.0	0.4	54.3	2.4		4.4		
"K" 8+76.26 TO 18+07.96	Med		24.7	55.1						36
"SA" 48+06.36 TO 50+42.26	R+		16.6	0.9	3.4			12.4	10.2	39.6
"S" 9+49.35 TO 13+02.00	L+		22.4		27.5	1.6				
"S" 14+76.27 TO 23+15.09	R+		51.3		7.5	1.6				
"KL1" 284+56.01 TO 286+27.88	R+							49.2		
"KR1" 287+18.92 TO 288+48.69	L+							49.3		
"KL2" 306+02.47 TO 307+36.50	R+							35.2		
"KR2" 306+53.71 TO 307+70.08	L+							32.7		
SUBTOTAL		46.3	286.3	1.3	252	19.1	13.8	181.6	14.6	75.6
FROM DRAINAGE						109.6				
FROM RETAINING WALL						44				
TOTAL						848				

6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

4 REVISED PER ADDENDUM No. 4 DATED OCTOBER 16, 2012

SUMMARY OF QUANTITIES

Q-2

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	228	423

REGISTERED CIVIL ENGINEER *Chuong Nguyen* DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

6

6

4

4

LAST REVISION DATE PLOTTED => \$DATE 06-08-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	229	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

FENCE

STATION	Loc	REMOVE FENCE	RECONSTRUCT CHAIN LINK FENCE (TYPE CL-6)	CHAIN LINK FENCE (TYPE CL-6 SLATTED)	6' CL GATE (TYPE CL-6)	FENCE (TYPE BW, 5 STRAND METAL POST)	TEMPORARY FENCE (TYPE CL-6)	TEMPORARY FENCE (TYPE ESA)
		LF	LF	EA	EA	LF		
"K" 2+14.90 TO 3+42.80	L+		160					
"K" 0+77.07 TO 3+24.87	L+	85	271					
"K" 0+77.74 TO 3+42.80	R+						294	
"K" 0+76+41 TO 2+60.86	L+						498	
"K" 8+66.19 TO 13+93.42	L+	184	343					
"K" 8+69.30 TO 10+52.15	L+						193	
"S" 10+17.18 TO 12+73.08	R+	295						
"K" 10+52.15 TO 13+97.38	L+					354		
"K" 14+83.10 TO 18+26.61	L+	455		292				
"K" 15+85.74	L+	73			2			
"K" 15+83.12	L+						99	
"KL1" 282+90.03 TO 293+93.40	L+						1,097	
"KL1" 284+29.47 TO 292+33.16	L+		811					
"KR1" 295+12.72 TO "K" 9+79.04	R+		315					
"KL2" 298+84.11 TO 301+86.72	L+	81						
"S" 9+68.29 TO 12+54.68	L+						304	
"SR 99" 314+38	R+				1			
FROM PLANT REMOVAL PLANS								1,160
TOTAL		1,173	1,900	292	3	354	2,485	1,160



METAL BEAM GUARD RAIL & TRANSITION RAILING

STATION	Loc	METAL BEAM GUARD RAILING	TRANSITION RAILING (TYPE WB)	ALTERNATIVE FLARED TERMINAL SYSTEM	END ANCHOR ASSEMBLY (TYPE CA)	END ANCHOR ASSEMBLY (TYPE SFT)
		LF	EA			
"SR 99" 269+34.59 TO 270+10	R+	37.5		1		1
"SR 99" 274+15.16 TO 275+15.00	R+	163		1		1
"SR 99" 282+92.09 TO 283+66.00	R+	37.5		1		1
"SR 99" 311+49.45 TO 312+11.79	L+	37.5		1		1
"KL1" 289+10.61 TO 289+72.95	R+	37.5		1		1
"KL1" 294+50.00 TO 295+12.45	L+	37.5	1		1	
"KR2" 302+56.15 TO 303+18.53	R+	37.5	1		1	
"KR2" 303+29.37 TO 303+91.72	L+	37.5		1		1
FROM TRAFFIC HANDLING PLANS					8	
TOTAL		425.5	2	6	10	6

RETAINING WALL AND SOUND WALL

LOCATION	STRUCTURE EXCAVATION (F)	STRUCTURE BACKFILL (F)	STRUCTURE CONCRETE (F)	PERVIOUS BACKFILL MATERIAL (N)	BAR REINFORCING STEEL (F)	ARCHITECTURAL TREATMENT (F)	CL FENCE (TYPE CL-4) BLACK VINYL CLAD	HAND RAILING	6" PLASTIC PIPE	Conc BARRIER (TYPE 60D)	Conc BARRIER (TYPE 736S)	Conc BARRIER (TYPE 736SV)	CIDH PILES	SOUND WALL (BARRIER) (MASONRY BLOCK)	MINOR CONCRETE (GUTTER)
	CY				LB	SQFT	LF						SQFT	CY	
RW No. 1	4,288	3,053	910	122	93,300	2,457	1,049		14	1,044					31
RW No. 2	27	27		11			191		6		191		280		7
RW No. 3	1,384	935	327	49	32,060	2,163	161		54						6
CURB WALL	96	154	123		5,835			50							
SW1											277		576	2,030	
TOTAL	5,795	4,169	1,360	182	131,195	4,620	1,401	50	74	1,044	191	277	856	2,030	44*

(*) - QUANTITY INCLUDED IN MINOR CONCRETE (MISCELLANEOUS CONSTRUCTION)
 (N)- NOT A SEPARATE PAY ITEM, FOR INFORMATION ONLY.

6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

SUMMARY OF QUANTITIES Q-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 CHUONG NGUYEN
 DAVID LIU
 REVISOR BY
 DATE REVISED

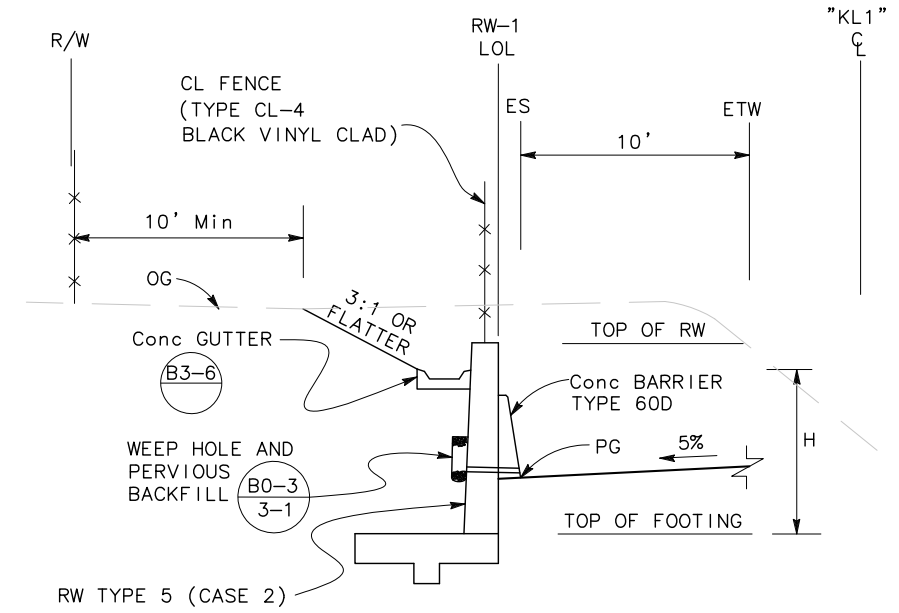
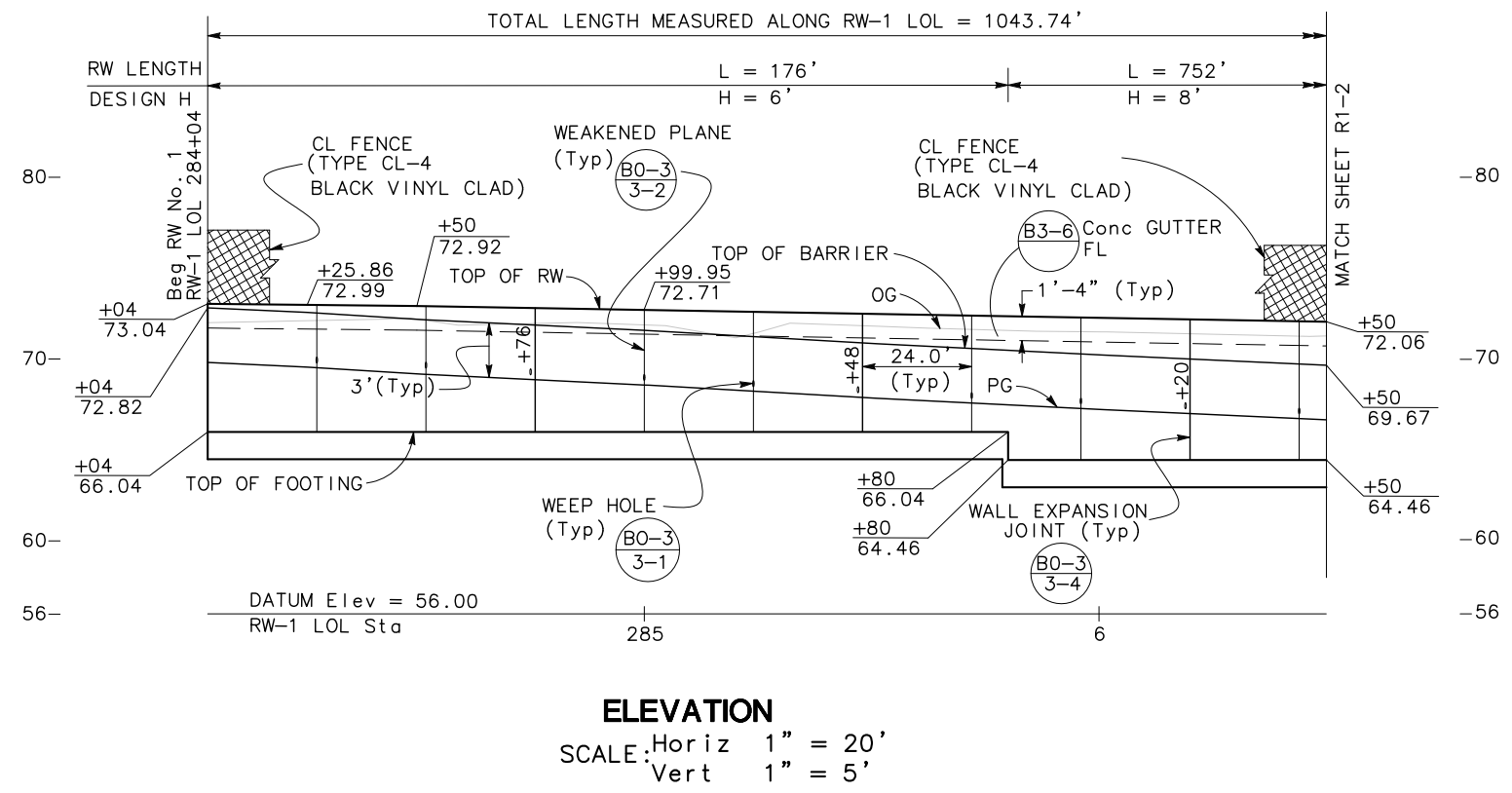
LAST REVISION | DATE PLOTTED => \$DATE
 06-08-12 | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	230	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen*
 DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

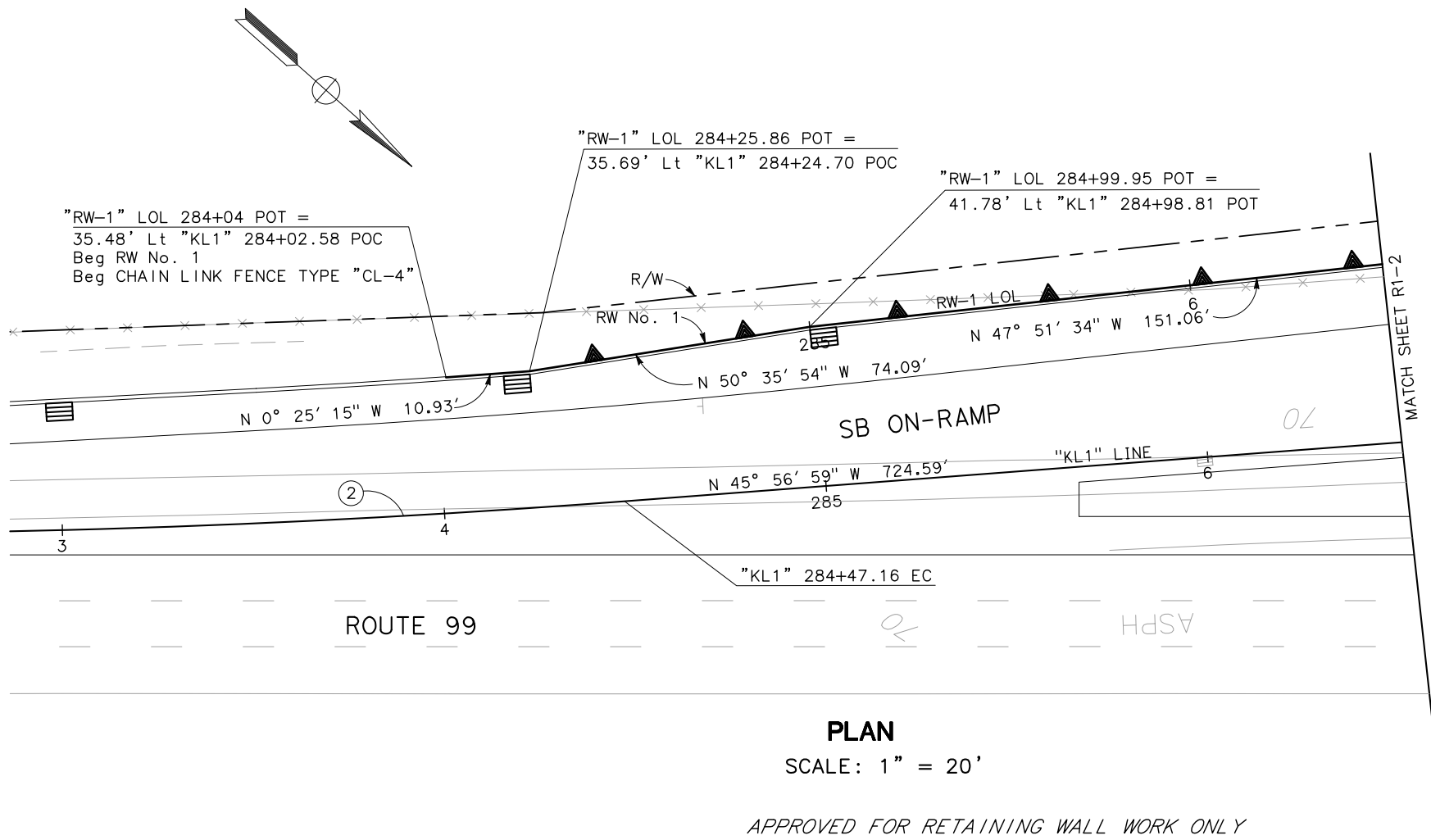


TYPICAL SECTION
 RETAINING WALL TYPE 5 (CASE 2)
 RW-1 Sta 284+04 TO Sta 284+63
 RW-1 Sta 286+90 TO Sta 294+47.74
 NO SCALE

NOTE:
 FOR REINFORCEMENT AND OTHER DETAILS NOT SHOWN, SEE REVISED STD PLAN RSP B3-4B

CURVE DATA

No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
②	3000.00'	3°11'29"	83.57'	167.10'	2077949.98	6392342.66



REVISIONS:
 REVISED BY: ANDY CHAN
 DATE REVISED:
 CHECKED BY: CHUONG NGUYEN
 CALCULATED/DESIGNED BY:
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 DEPARTMENT OF TRANSPORTATION
 STATE OF CALIFORNIA - Caltrans

RETAINING WALL PLAN
RETAINING WALL No. 1
 SCALE AS SHOWN
R1-1

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	231	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12

9/04/12
 PLANS APPROVAL DATE

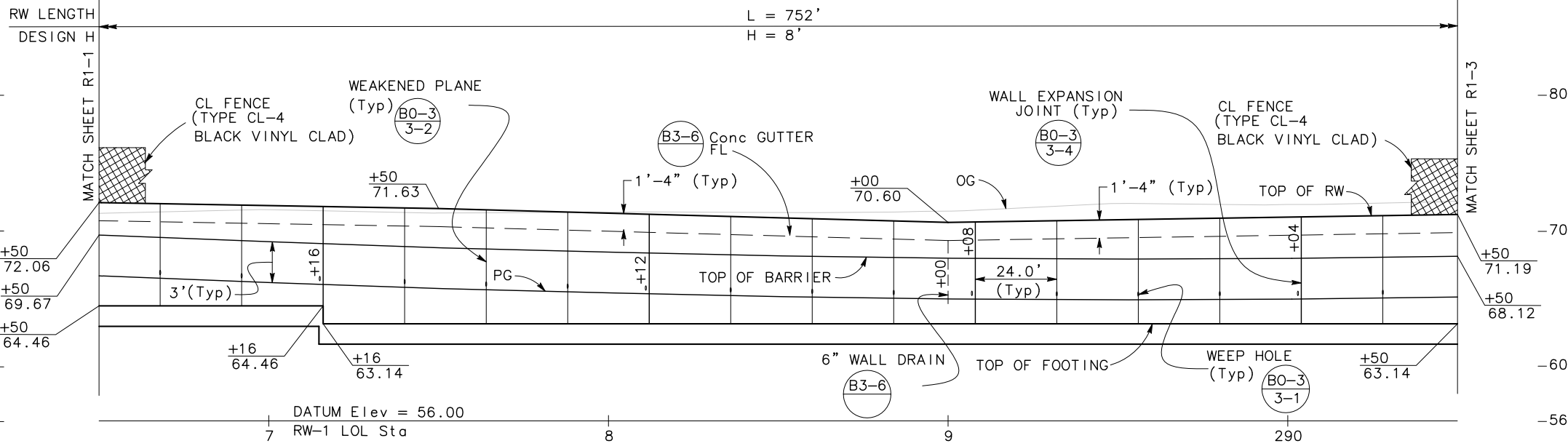
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126

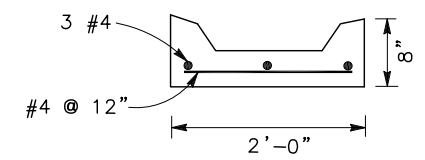
CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA

STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

TOTAL LENGTH MEASURED ALONG RW-1 LOL = 1043.74'

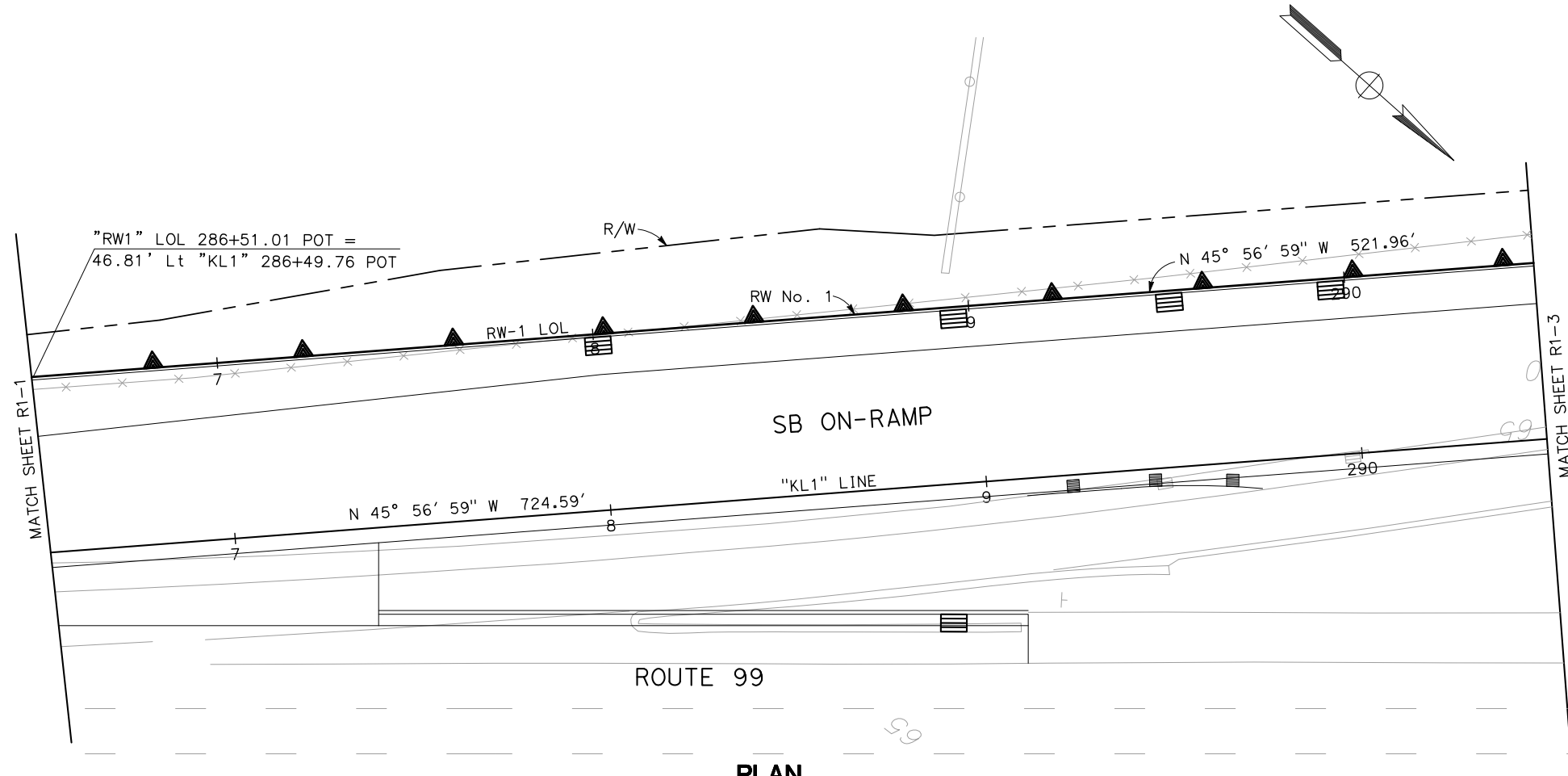


ELEVATION
 SCALE: Horiz 1" = 20'
 Vert 1" = 5'



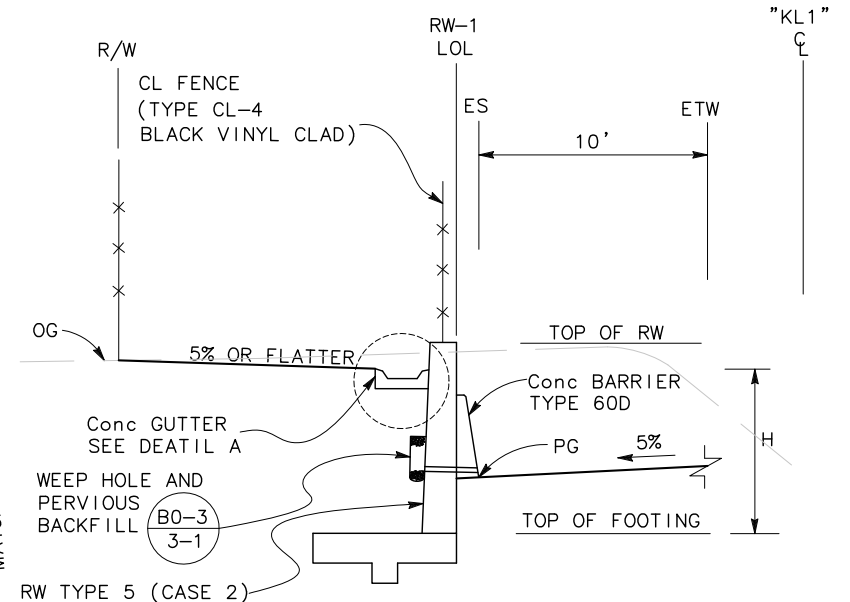
DETAIL A
 NO SCALE

FOR DIMENSION AND OTHER DETAILS NOT SHOWN, SEE Std PLAN B3-6



PLAN
 SCALE: 1" = 20'

APPROVED FOR RETAINING WALL WORK ONLY



TYPICAL SECTION

RETAINING WALL TYPE 5 (CASE 2)
 RW-1 Sta 284+63 TO Sta 286+90
 NO SCALE

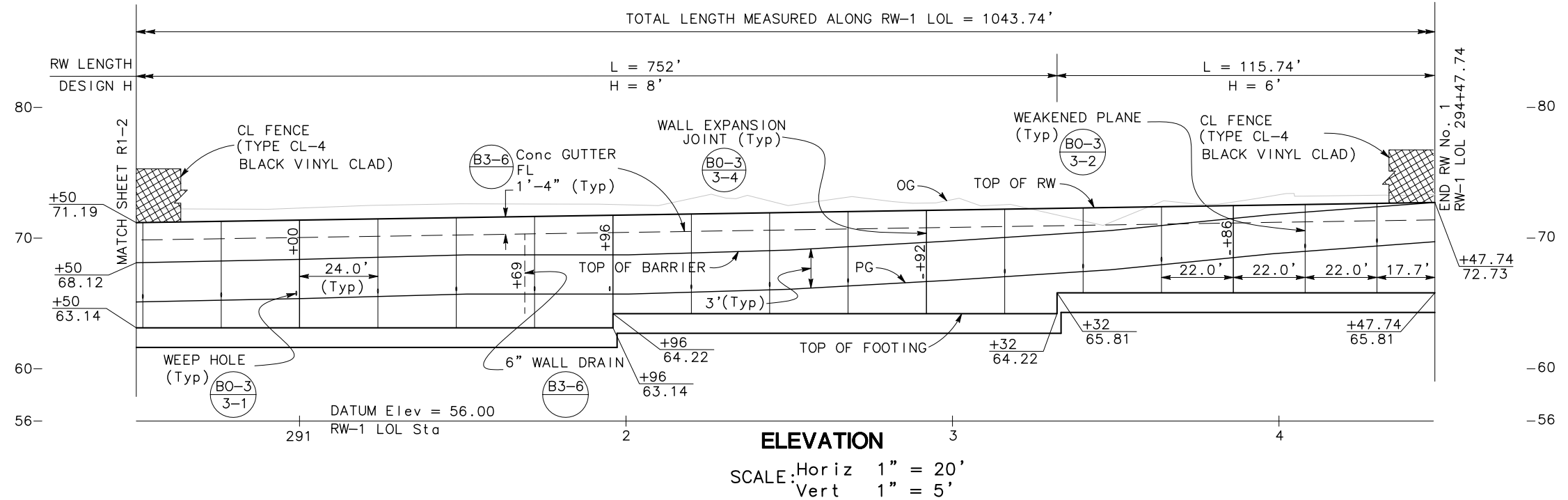
NOTE:
 FOR REINFORCEMENT AND OTHER DETAILS NOT SHOWN, SEE REVISED Std PLAN

RETAINING WALL PLAN
RETAINING WALL No. 1
 SCALE AS SHOWN

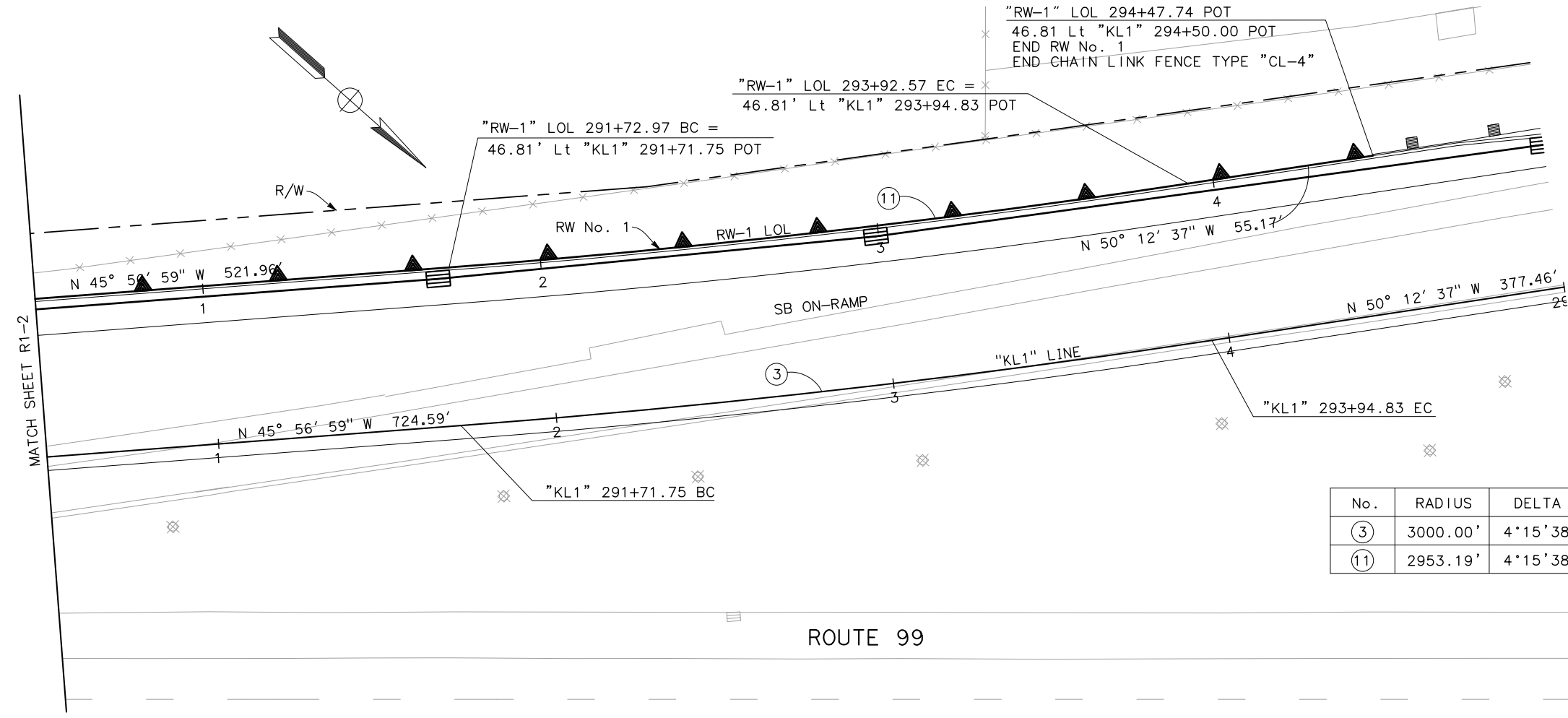
R1-2

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS:
 REVISED BY: ANDY CHAN
 DATE REVISED: CHUONG NGUYEN
 CALCULATED-DESIGNED BY: ANDY CHAN
 CHECKED BY: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	232	423
REGISTERED CIVIL ENGINEER		DATE		9/04/12	
PLANS APPROVAL DATE		REGISTERED PROFESSIONAL ENGINEER		CHUONG NGUYEN	
No. C46770		Exp. 06/30/13		CIVIL	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



ELEVATION
 SCALE: Horiz 1" = 20'
 Vert 1" = 5'



CURVE DATA

No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
③	3000.00'	4°15'38"	111.59'	223.08'	2078453.78	6391821.88
⑪	2953.19'	4°15'38"	109.85'	219.60'	2078453.78	6391821.88

PLAN
 SCALE: 1" = 20'

APPROVED FOR RETAINING WALL WORK ONLY

RETAINING WALL PLAN
RETAINING WALL No. 1
 SCALE AS SHOWN
R1-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED/DESIGNED BY
 CHECKED BY
 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

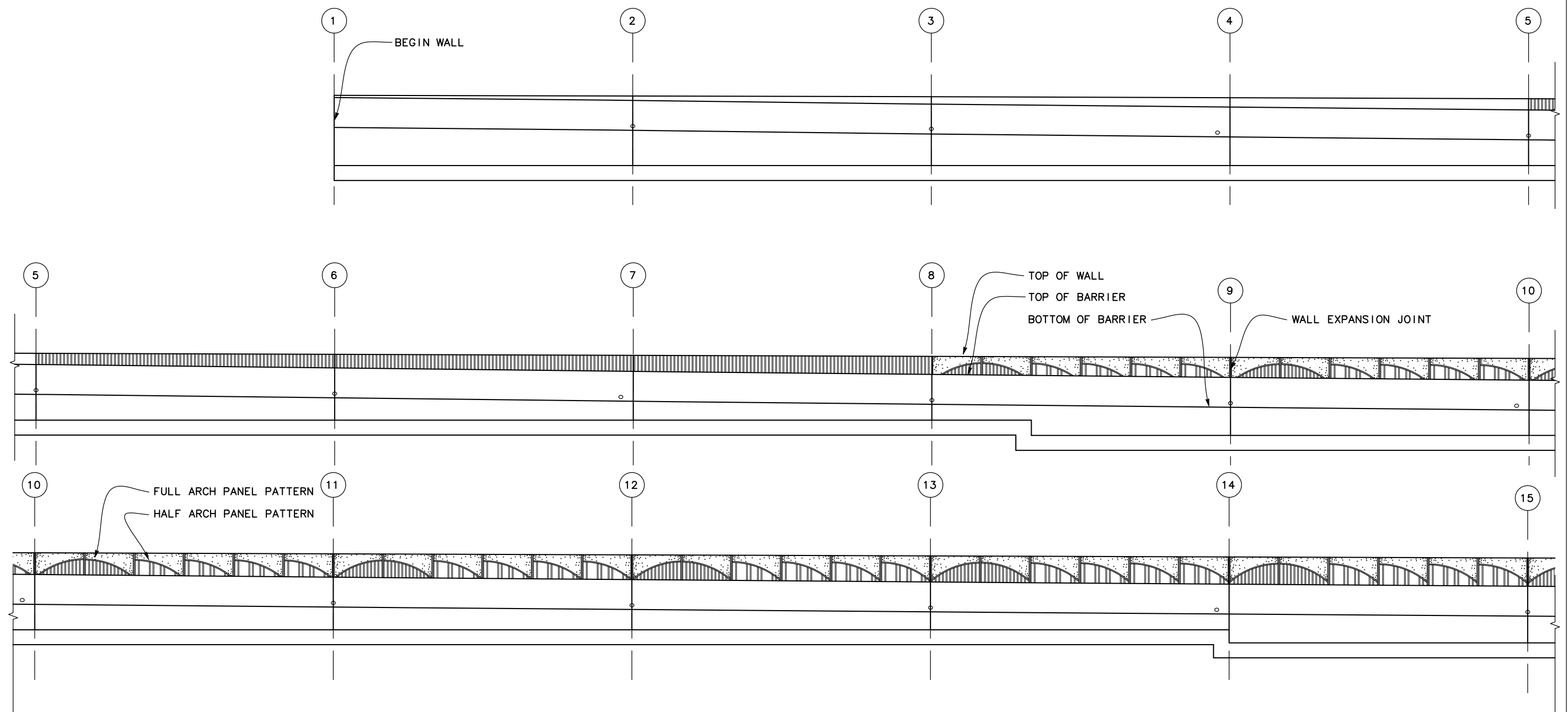
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: L. M. DERIVI
 CALCULATED-DESIGNED BY: L. M. DERIVI
 CHECKED BY: L. M. DERIVI
 REVISIONS:
 REVISION NO. | DATE | BY | DESCRIPTION

GENERAL NOTES

- GRIDS ALIGN WITH CONSTRUCTION CONTROL JOINTS / WEAKENED PLANES - SEE RW PLANS.
- BEGINNING POINT OF PATTERN IS WHERE TOP LEFT OF PATTERN AND TOP OF WALL ALIGN. RIGHT OF BEGINNING POINT, TOP OF WALL MAY MOVE UP OR DOWN.
- EACH RETAINING WALL ELEVATION IS ELEVATED INDEPENDENTLY. SEE RETAINING WALL GENERAL PLANS FOR MORE INFORMATION.
- WHERE NO PATTERN SHOWN ASSUME NO TREATMENT.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	233	423

MUSTA MFRANAL
 LICENSED ARCHITECT
 DATE: 08.30.2012
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CA 95203
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



RETAINING WALL AESTHETICS
RETAINING WALL No. 1
 SCALE: 1" = 4'
R1-4

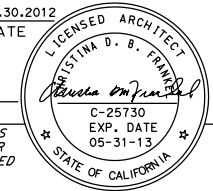
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 L. M. DERIVI
 CALCULATED-DESIGNED BY
 CHECKED BY
 L. M. DERIVI
 C. D. B. FRANKEL
 L. M. DERIVI
 REVISED BY
 DATE
 REVISED
 DATE

GENERAL NOTES

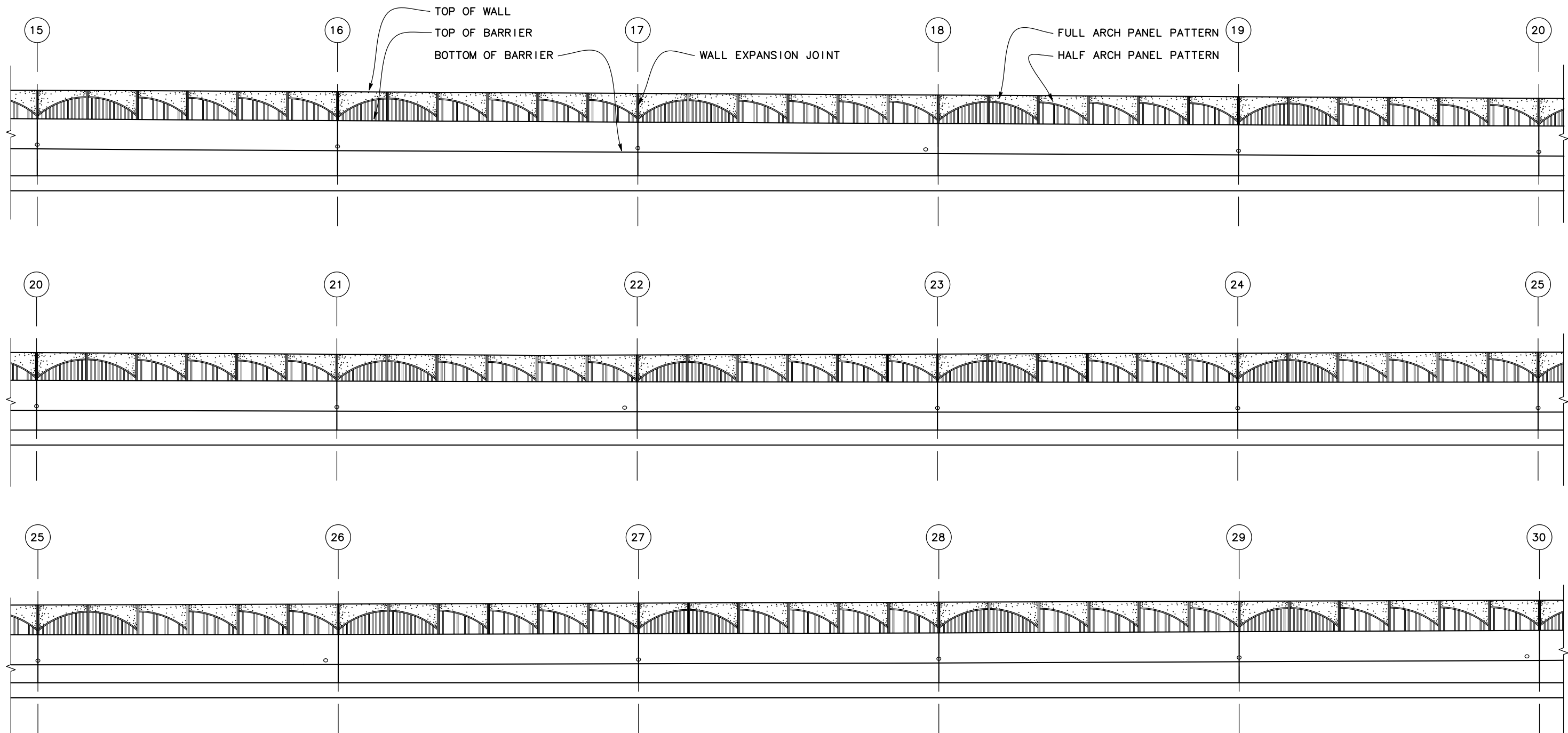
1. GRIDS ALIGN WITH CONSTRUCTION CONTROL JOINTS / WEAKENED PLANES - SEE RW PLANS.
2. BEGINNING POINT OF PATTERN IS WHERE TOP LEFT OF PATTERN AND TOP OF WALL ALIGN. RIGHT OF BEGINNING POINT, TOP OF WALL MAY MOVE UP OR DOWN.
3. EACH RETAINING WALL ELEVATION IS ELEVATED INDEPENDENTLY. SEE RETAINING WALL GENERAL PLANS FOR MORE INFORMATION.
4. WHERE NO PATTERN SHOWN ASSUME NO TREATMENT.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	234	423

Christina D. B. Frankel
 LICENSED ARCHITECT
 DATE 08.30.2012
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CA 95203
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



RETAINING WALL AESTHETICS
RETAINING WALL No. 1
 SCALE: 1" = 4'
R1-5

LAST REVISION | DATE PLOTTED => \$DATE
 00-00-00 | TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 L. M. DERIVI
 CALCULATED-DESIGNED BY
 CHECKED BY
 C. D. B. FRANKEL
 L. M. DERIVI
 REVISED BY
 DATE REVISION

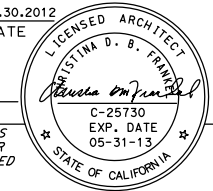
GENERAL NOTES

1. GRIDS ALIGN WITH CONSTRUCTION CONTROL JOINTS / WEAKENED PLANES - SEE RW PLANS.
2. BEGINNING POINT OF PATTERN IS WHERE TOP LEFT OF PATTERN AND TOP OF WALL ALIGN. RIGHT OF BEGINNING POINT, TOP OF WALL MAY MOVE UP OR DOWN.

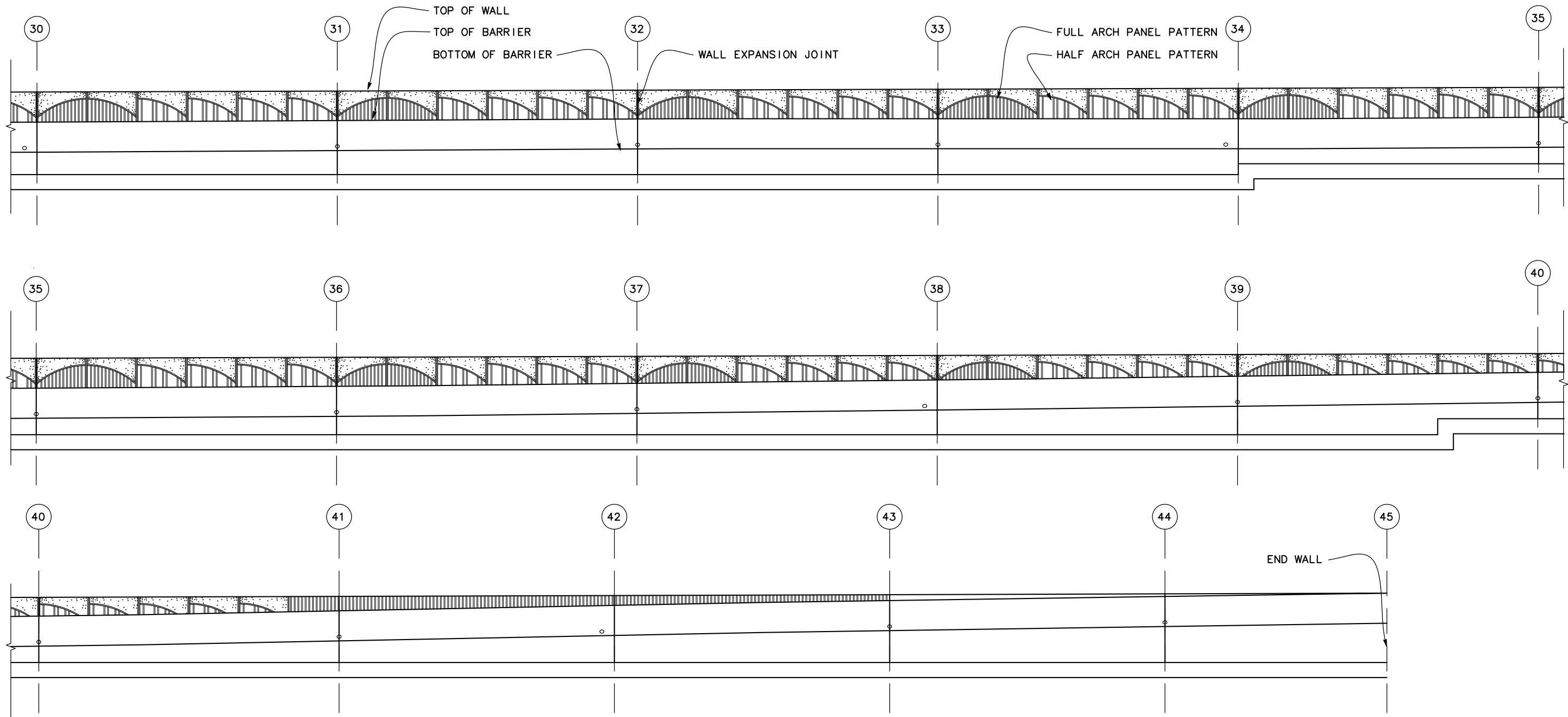
3. EACH RETAINING WALL ELEVATION IS ELEVATED INDEPENDENTLY. SEE RETAINING WALL GENERAL PLANS FOR MORE INFORMATION.
4. WHERE NO PATTERN SHOWN ASSUME NO TREATMENT.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	235	423

Christina D. B. Frankel
 LICENSED ARCHITECT
 DATE 08.30.2012
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CA 95203
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

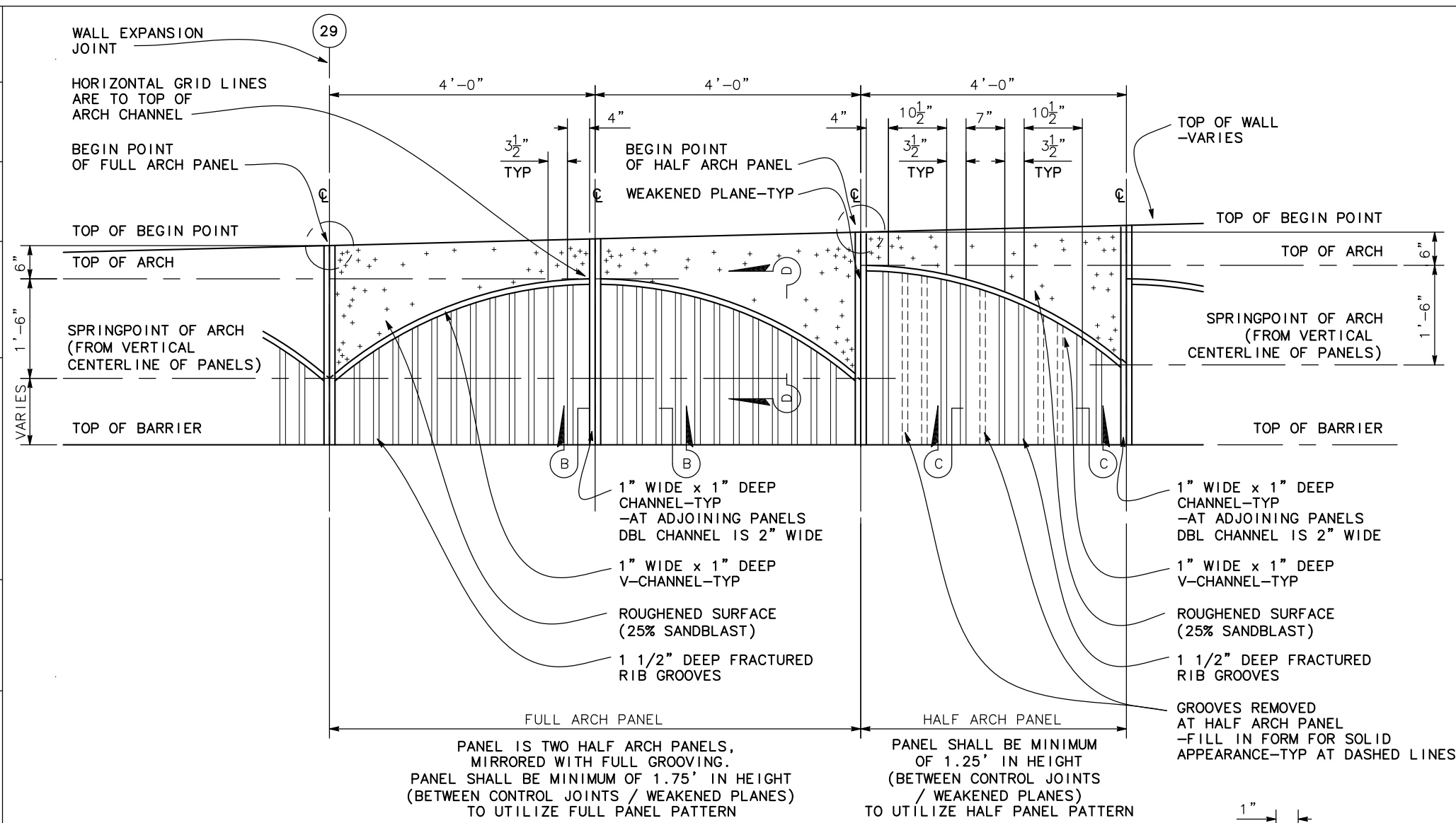
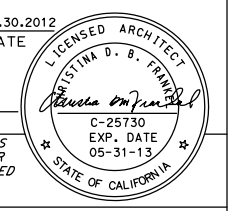


RETAINING WALL AESTHETICS
RETAINING WALL No. 1
 SCALE: 1" = 4'
R1-6

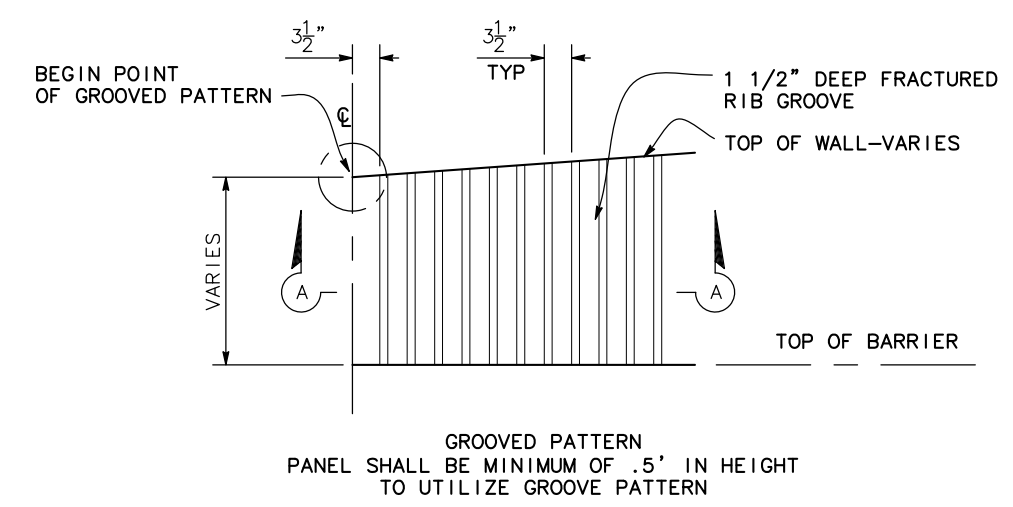
LAST REVISION | DATE PLOTTED => \$DATE
 00-00-00 | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES	SHEET	TOTAL
10	Sta	99, 219	22.0/23.0, 0.0/0.5	236	423

<i>Christina D. B. Frankel</i>		08.30.2012
LICENSED ARCHITECT		DATE
9/04/12		
PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.		
DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CA 95203		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

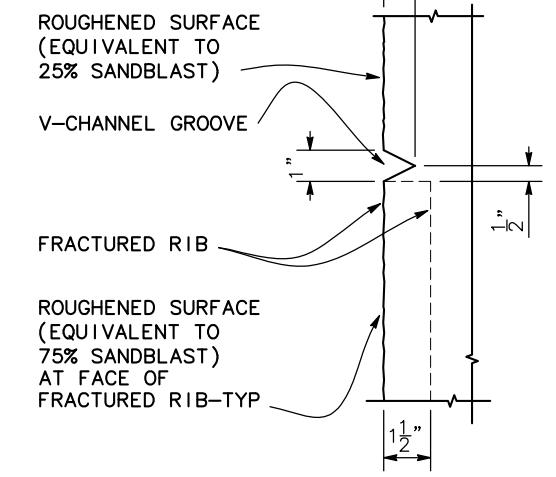


DETAIL A-FULL ARCH AND HALF ARCH PANEL



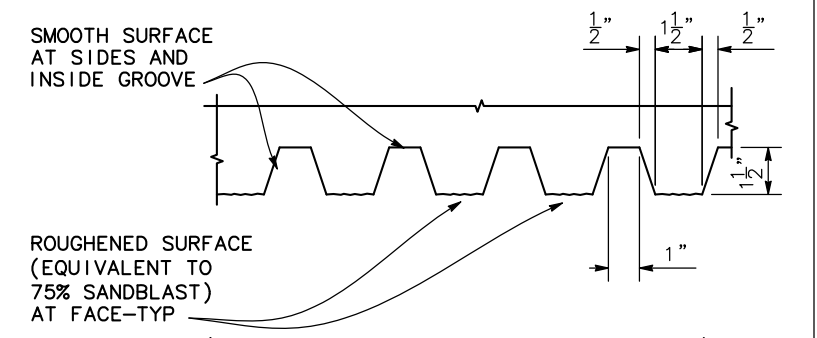
DETAIL B-NO ARCH

PANEL PATTERN DETAIL



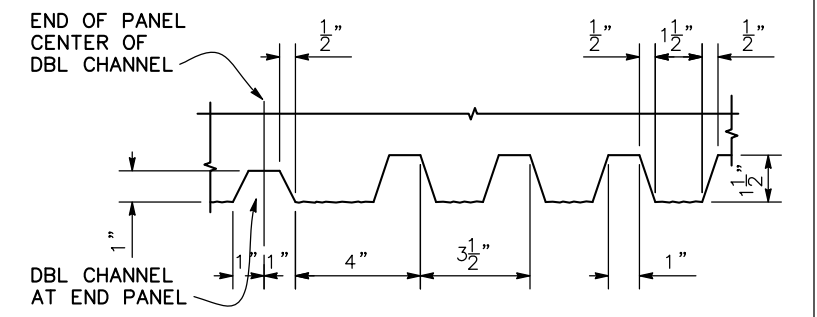
VIEW D-D (SECTION)

SCALE: 4" = 1'



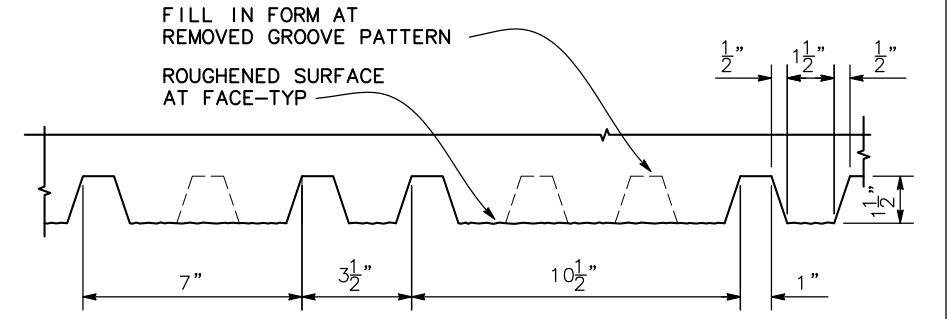
VIEW A-A (FRACTURED RIB PATTERN-TYP)

SCALE: 4" = 1'



VIEW B-B (PATTERN AT END PANEL)

SCALE: 4" = 1'



VIEW C-C (PATTERN AT REMOVED GROOVES)

SCALE: 4" = 1'

RETAINING WALL AESTHETICS
RETAINING WALL No. 1
SCALE: 1" = 1'
R1-7

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR: L. M. DERIVI
 CALCULATED-DESIGNED BY: L. M. DERIVI
 CHECKED BY: L. M. DERIVI
 REVISIONS: REVISOR: C. D. B. FRANKEL, DATE: 08/30/12, BY: L. M. DERIVI
 BORDER LAST REVISED 7/2/2010

USERNAME => \$USER
 DGN FILE => \$REQUEST

RELATIVE BORDER SCALE IS IN INCHES
 0 1 2 3

UNIT PROJECT NUMBER & PHASE 10 0000 0100

LAST REVISION: DATE PLOTTED => \$DATE
 00-00-00 TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	237	423

REGISTERED GEOTECHNICAL ENG. DATE 04/30/12

PLANS APPROVAL DATE 9/04/12

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

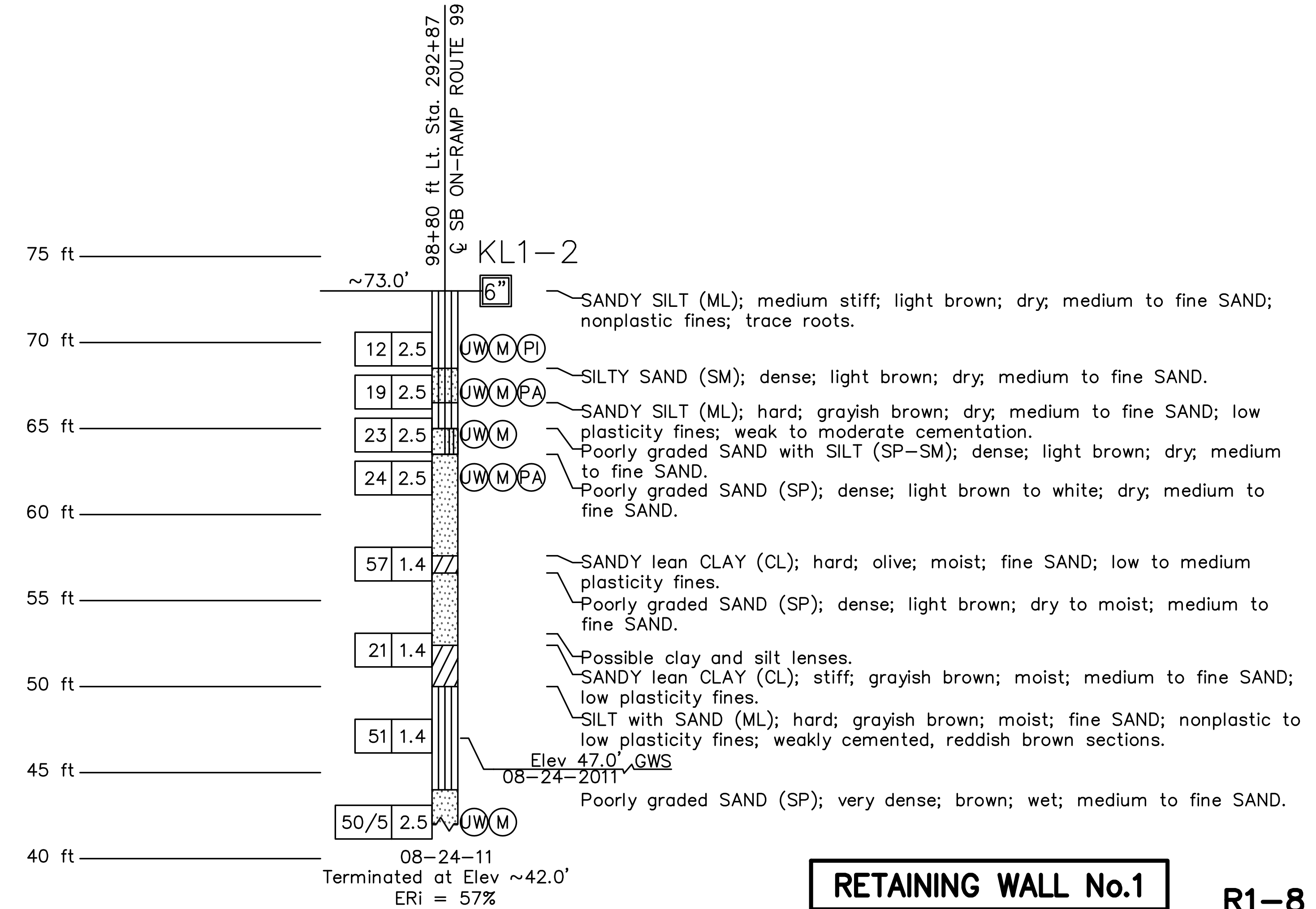
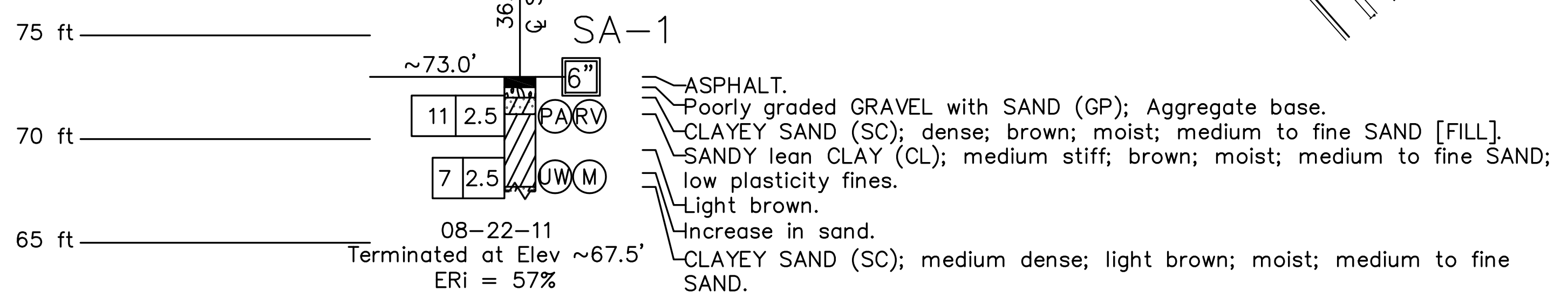
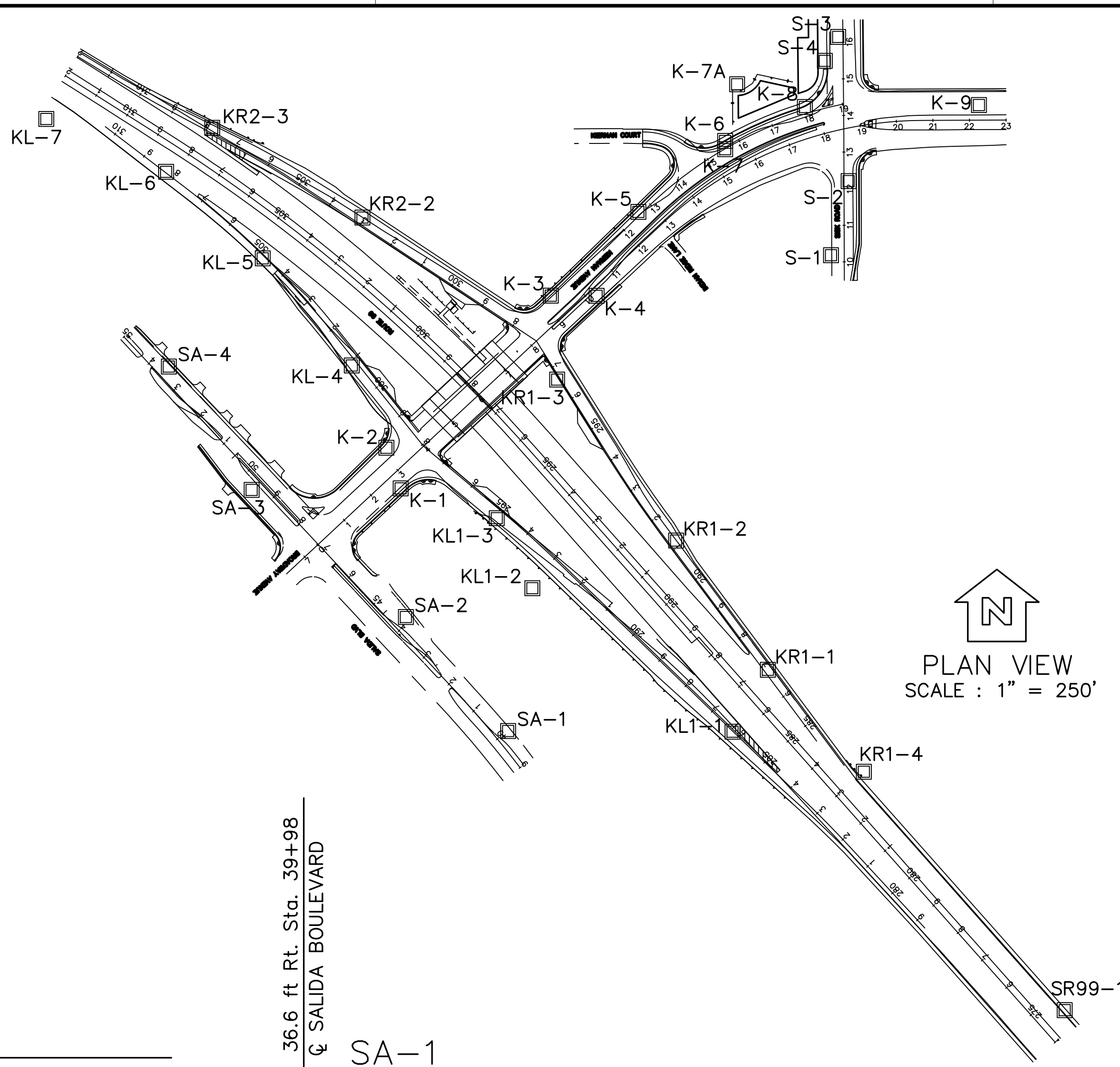
RAJAPPAN & MEYER
CONSULTING ENGINEERS, INC.
1038 LEIGH AVE, SUITE 100
SAN JOSE, CA 95126

GEOCON
CONSULTANTS, INC.
6671 BRISA STREET, LIVERMORE, CALIFORNIA 94550
TEL 925 371-5900 - FAX 925 371-5915

REGISTERED PROFESSIONAL ENGINEER
JAMES A. RODACKER
No. 2915
Exp. 06/30/12
GEOTECHNICAL
STATE OF CALIFORNIA

NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
- 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
- 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
- 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
- 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
- 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



RETAINING WALL No.1 R1-8

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY:	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER	DATE: 08/22/2011 THROUGH 03/02/2012			POST MILES R22.56	LOG OF TEST BORINGS	

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	238	423

REGISTERED GEOTECHNICAL ENG. DATE 04/30/12

9/04/12
PLANS APPROVAL DATE

The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.

DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

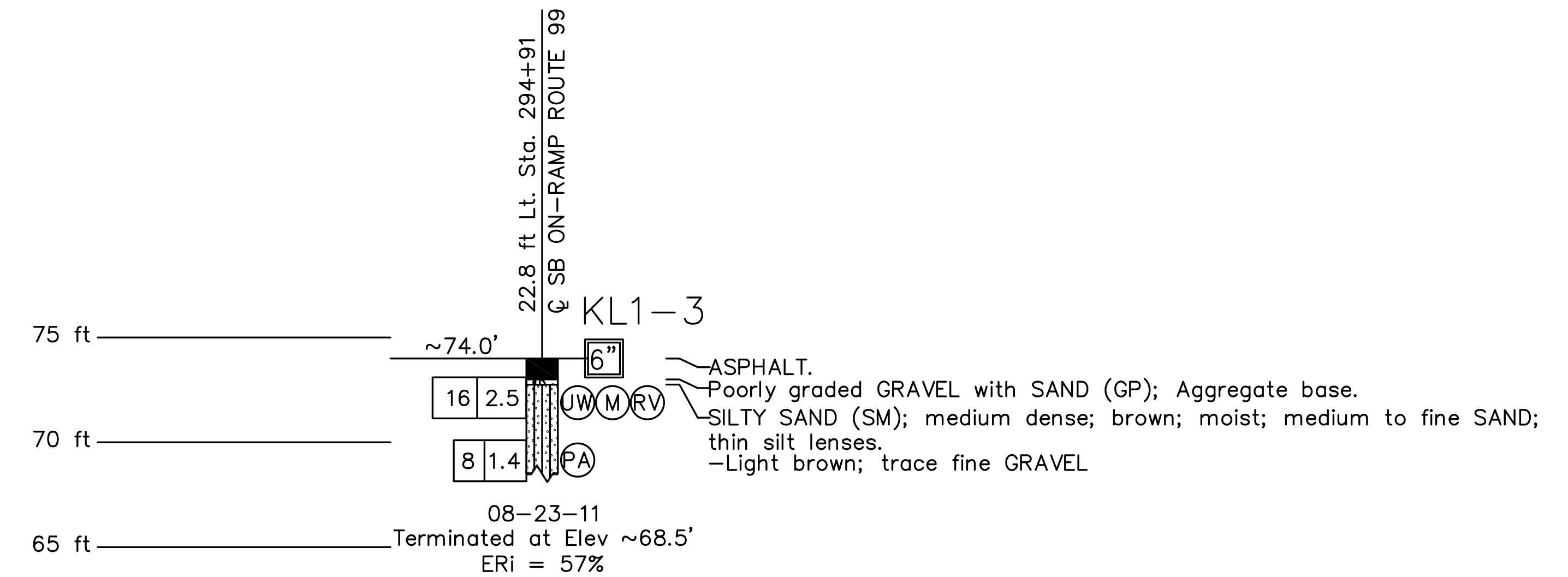
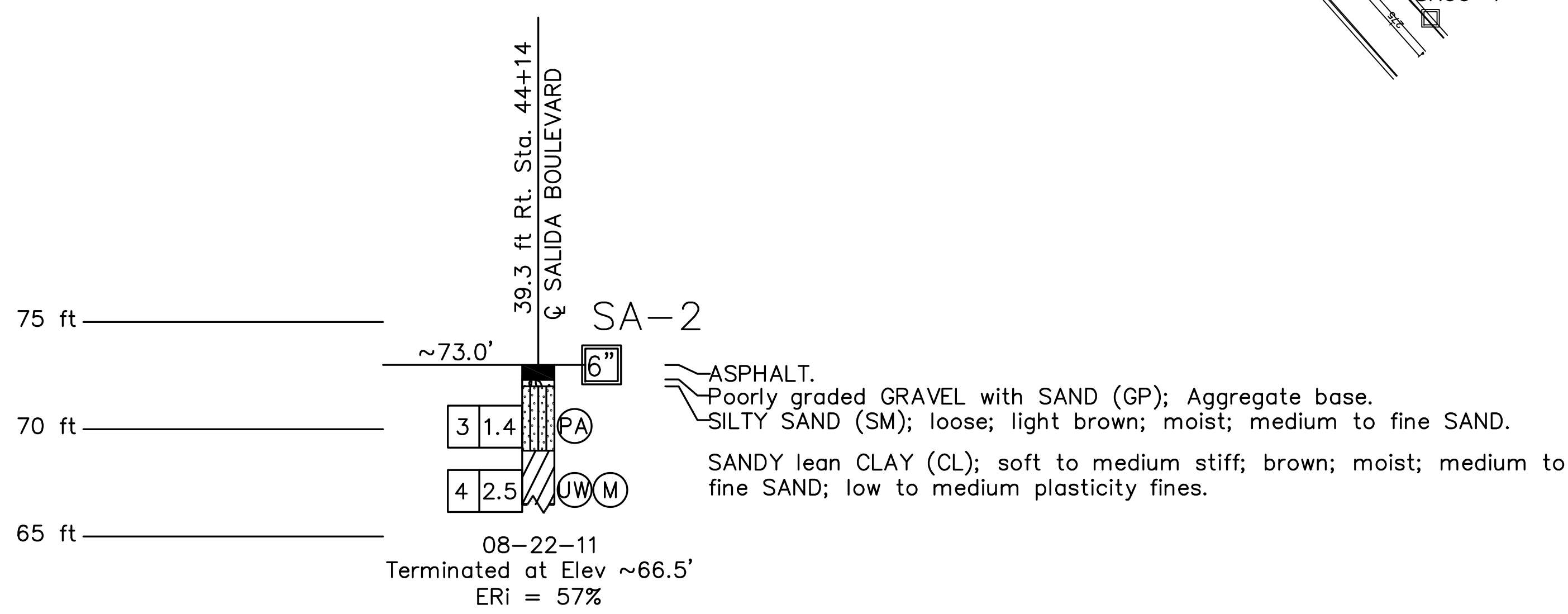
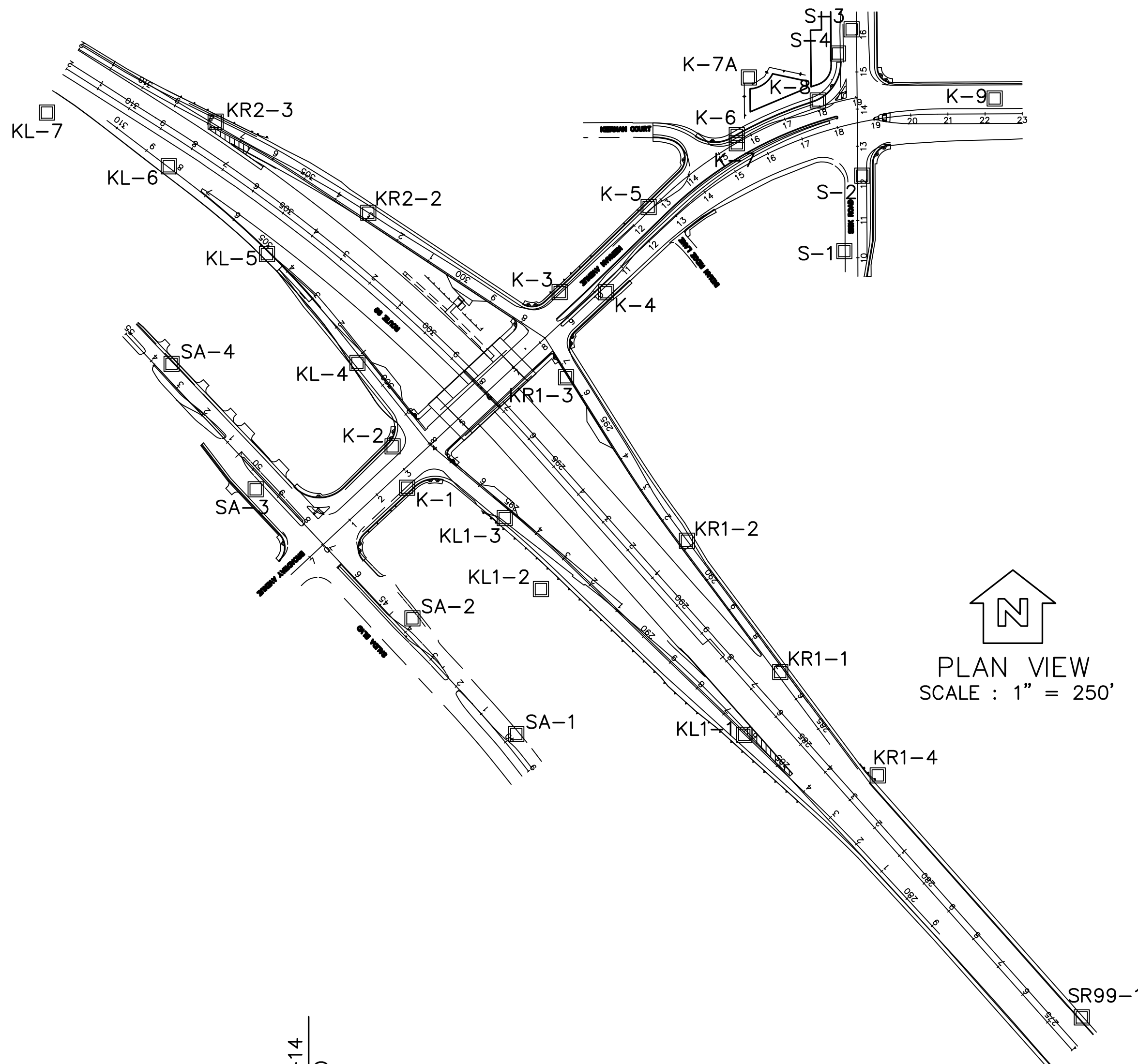
RAJAPPAN & MEYER
CONSULTING ENGINEERS, INC.
1038 LEIGH AVE, SUITE 100
SAN JOSE, CA 95126

GEOCON
CONSULTANTS, INC.
6671 BRISA STREET, LIVERMORE, CALIFORNIA 94550
TEL 925 371-5900 - FAX 925 371-5915

REGISTERED PROFESSIONAL ENGINEER
JAMES A. RODACKER
No. 2915
Exp. 06/30/12
GEOTECHNICAL
STATE OF CALIFORNIA

NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
- 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
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- 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
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- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



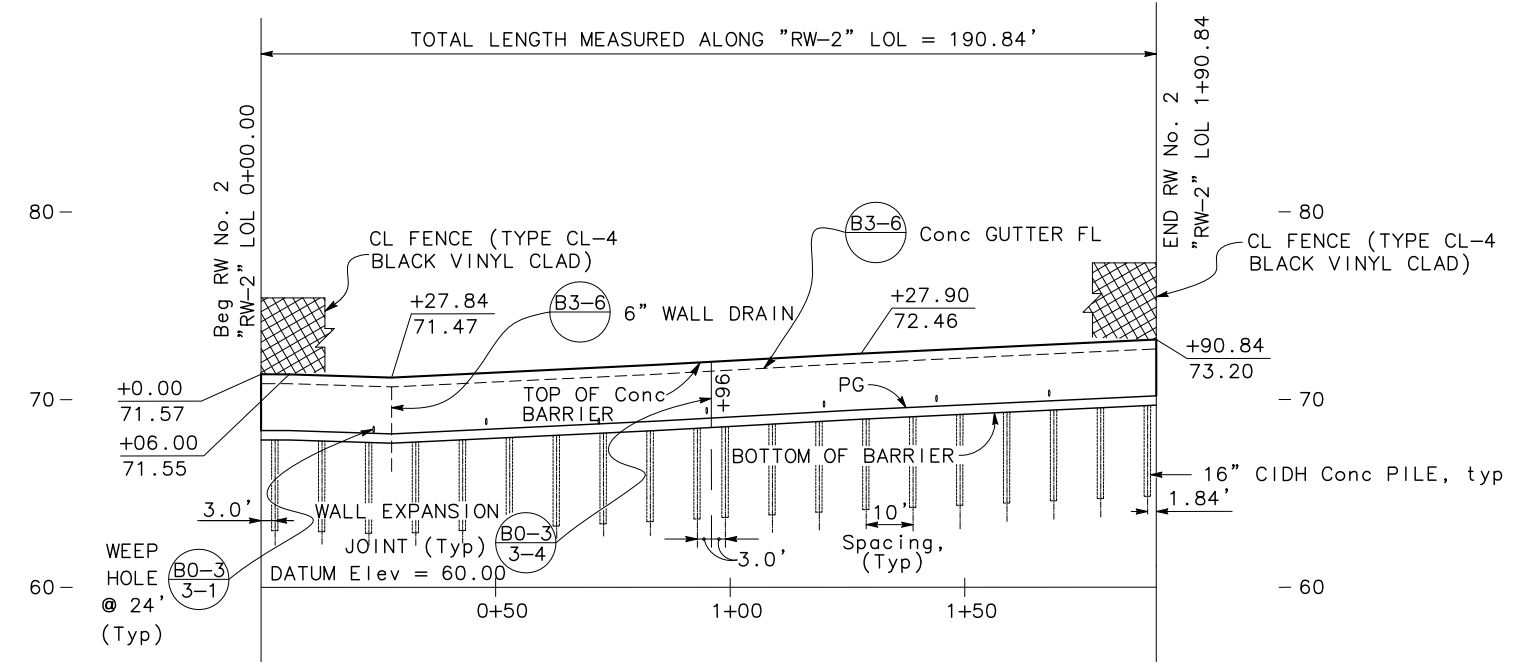
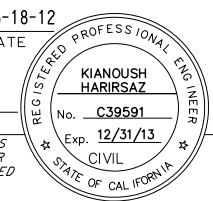
PROFILE VIEW
VERT. SCALE : 1" = 5'

RETAINING WALL No.1 R1-9

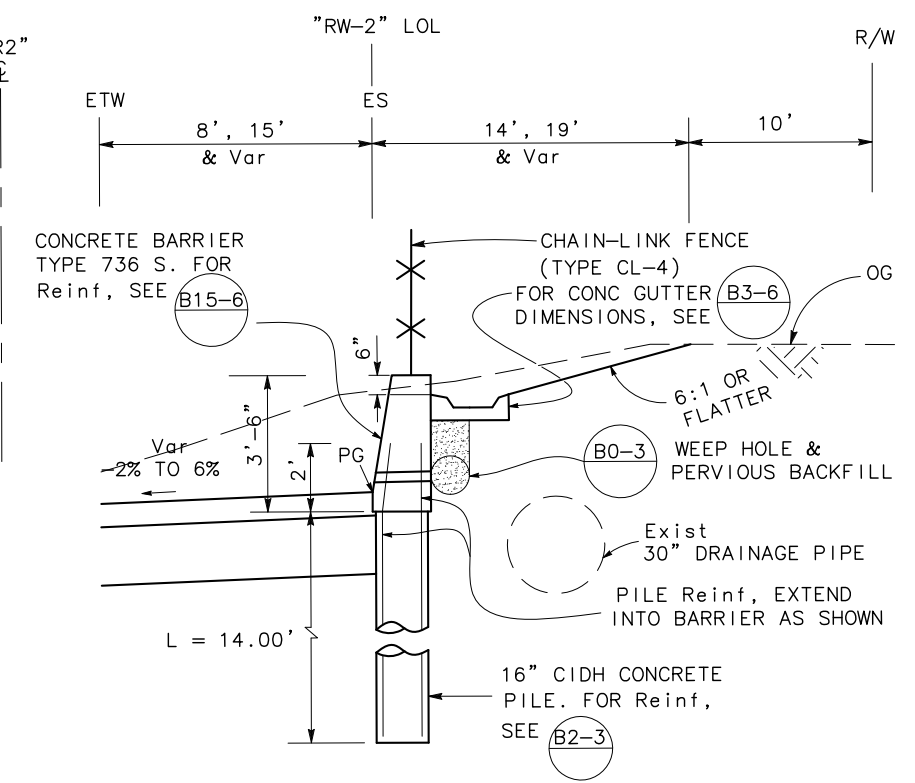
DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY:	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER	DATE: 08/22/2011 THROUGH 03/02/2012			R22.56	LOG OF TEST BORINGS	
GS GEOTECHNICAL LOG OF TEST BORINGS SHEET (ENGLISH) (REV. 7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: PROJECT NUMBER & PHASE: 1000000100	1457 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES X-XX-XX
				FILE => \$REQUEST		SHEET XX	OF XX

DATE PLOTTED => \$TIME
DATE PLOTTED => \$DATE
USERNAME => \$USER

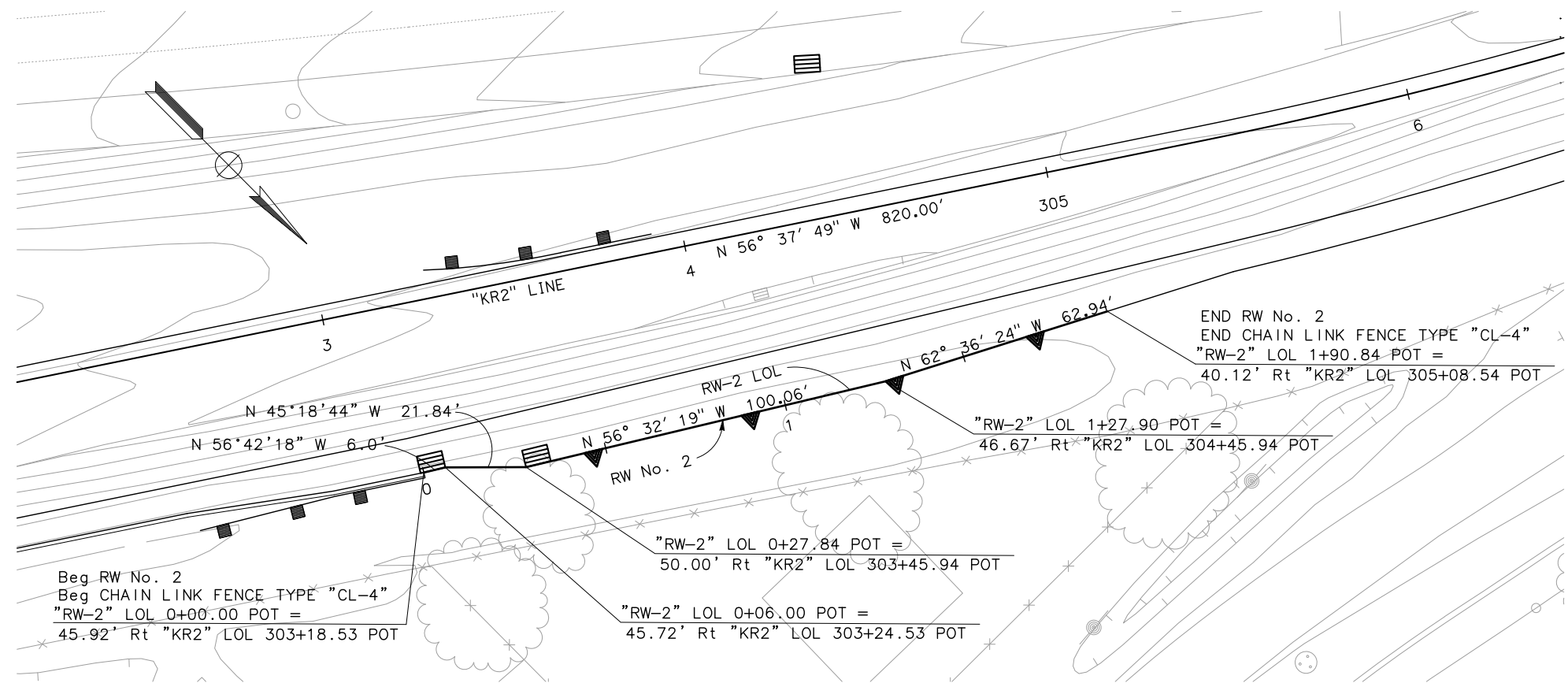
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	239	423
REGISTERED CIVIL ENGINEER			DATE	05-18-12	
9/04/12			PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



ELEVATION
 SCALE: Horiz 1"=20'
 Vert 1"=5'



TYPICAL SECTION
 RW-2 Sta 0+00.00 TO Sta 1+90.84
 NO SCALE





PLAN
 SCALE: 1"=20'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: BALA SWARUP
 CHECKED BY: KIANOUSH HARIRSAZ
 REVISIONS: REVISOR, DATE, REVISIONS

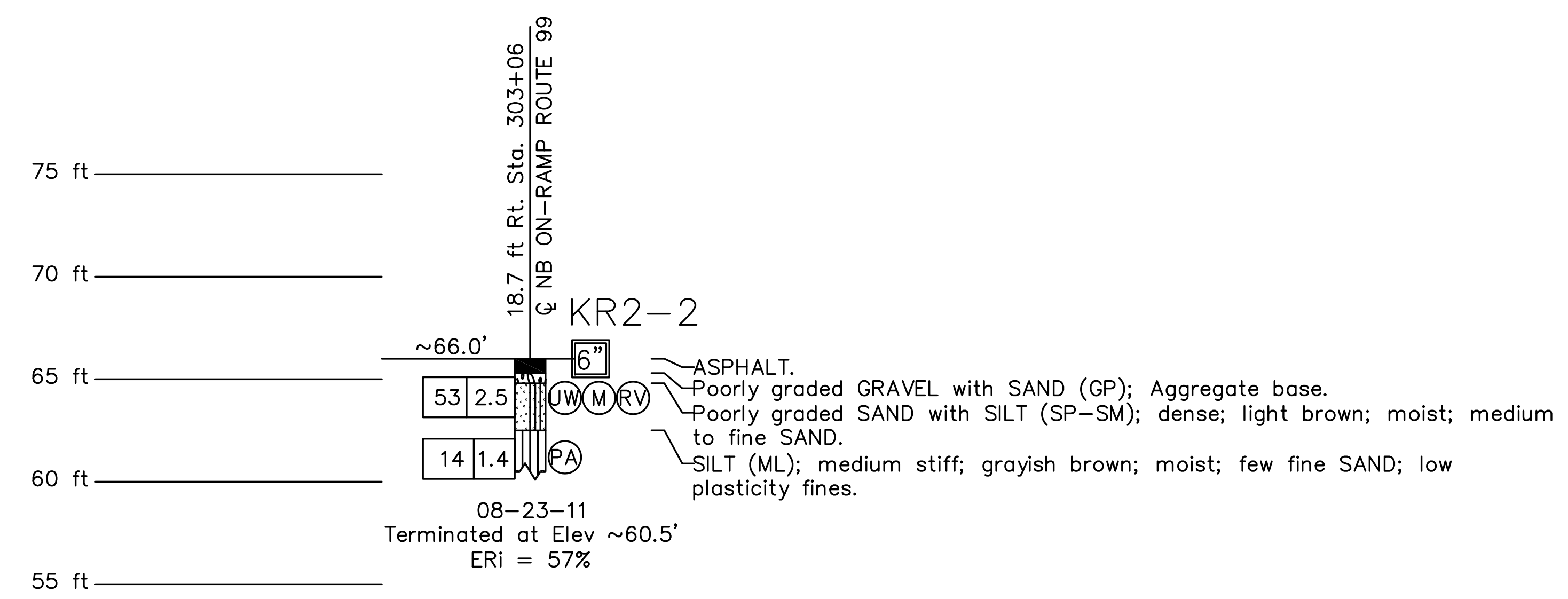
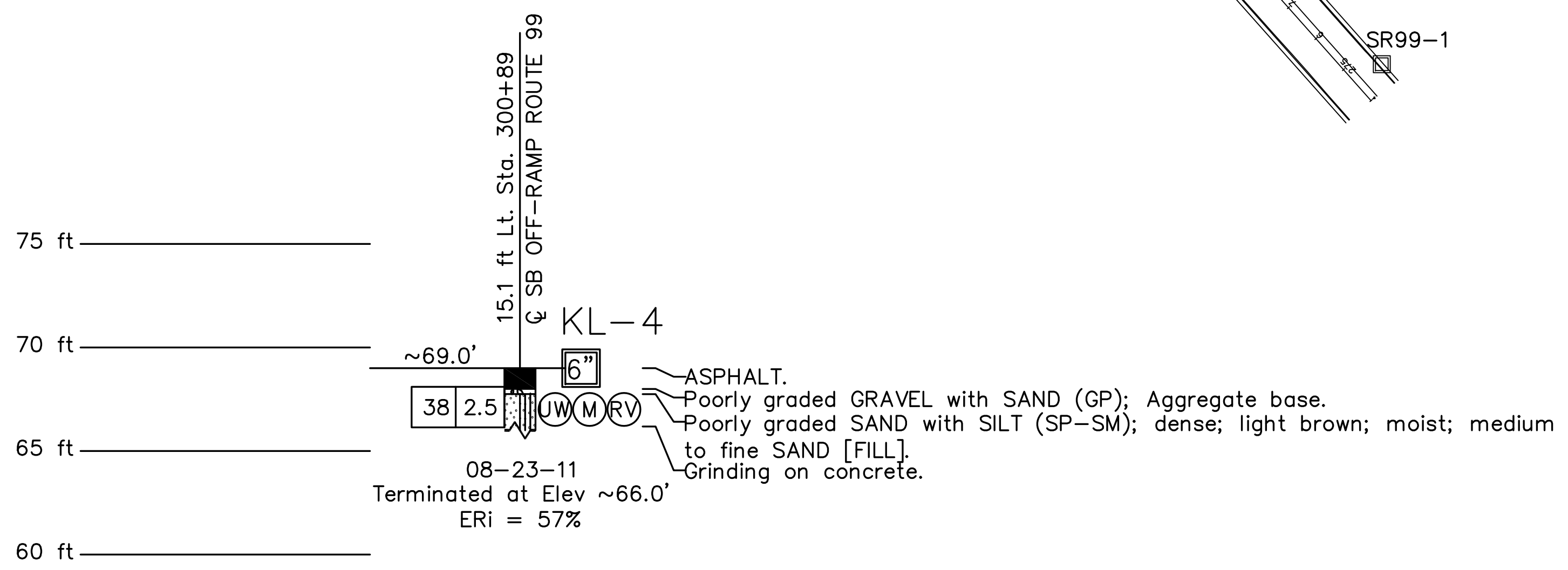
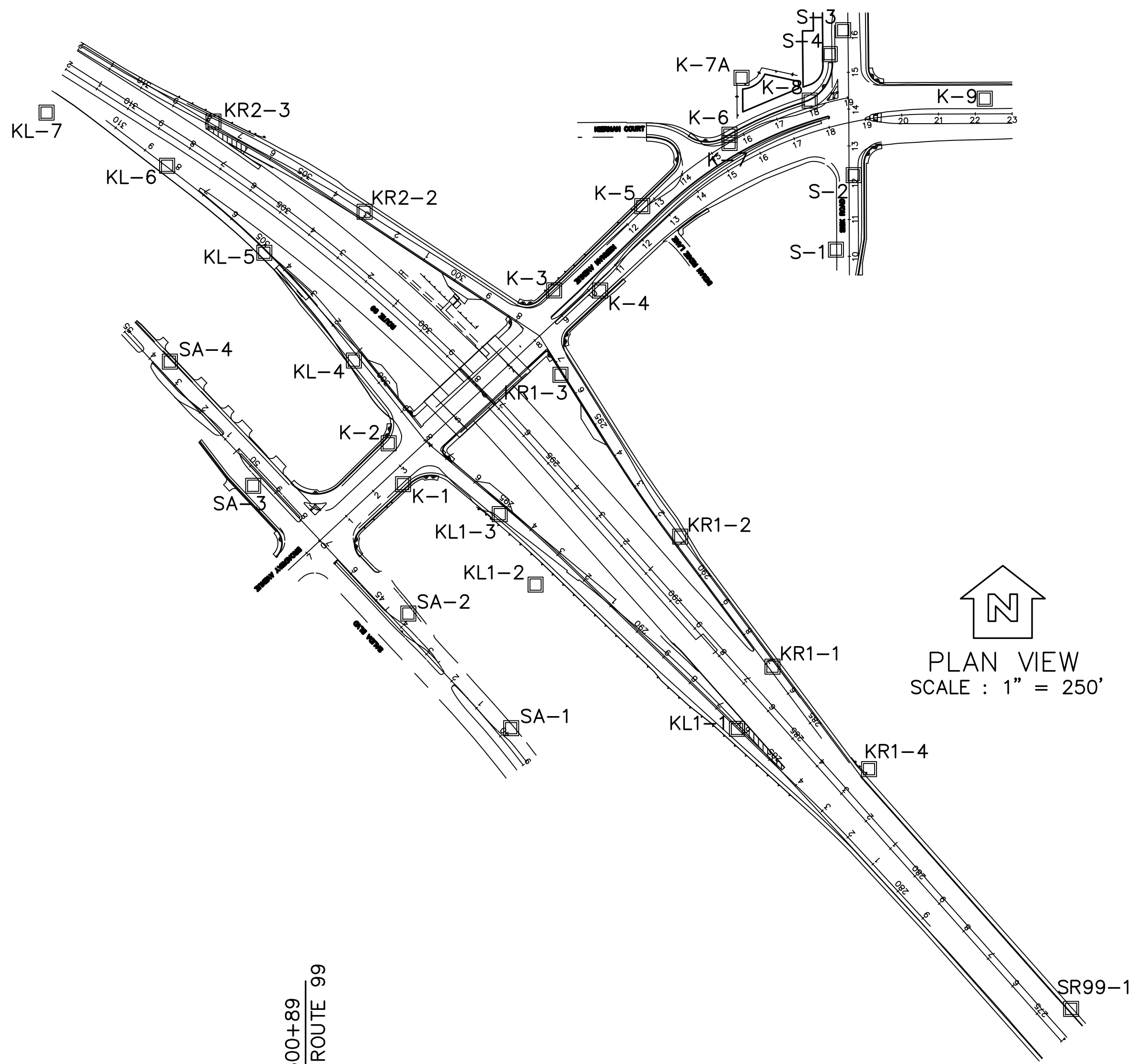
APPROVED FOR RETAINING WALL WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE
 06-08-12 TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	240	423
 REGISTERED GEOTECHNICAL ENG. DATE 04/30/12			9/04/12 PLANS APPROVAL DATE		
The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVE, SUITE 100 SAN JOSE, CA 95126					
					

NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
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PROFILE VIEW
VERT. SCALE : 1" = 5'

RETAINING WALL No.2 R2-2

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY: DATE: 08/22/2011 THROUGH 03/02/2012	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163 POST MILES R22.56	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER					LOG OF TEST BORINGS	
GS GEOTECHNICAL LOG OF TEST BORINGS SHEET (ENGLISH) (REV. 7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: PROJECT NUMBER & PHASE: 1000000100	1457 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES X-XX-XX
			0 1 2 3	FILE => \$REQUEST		SHEET XX	OF XX

DATE PLOTTED => \$TIME
DATE PLOTTED => \$DATE
DATE PLOTTED => \$USER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	241	423

REGISTERED GEOTECHNICAL ENG. DATE 04/30/12

9/04/12
PLANS APPROVAL DATE

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

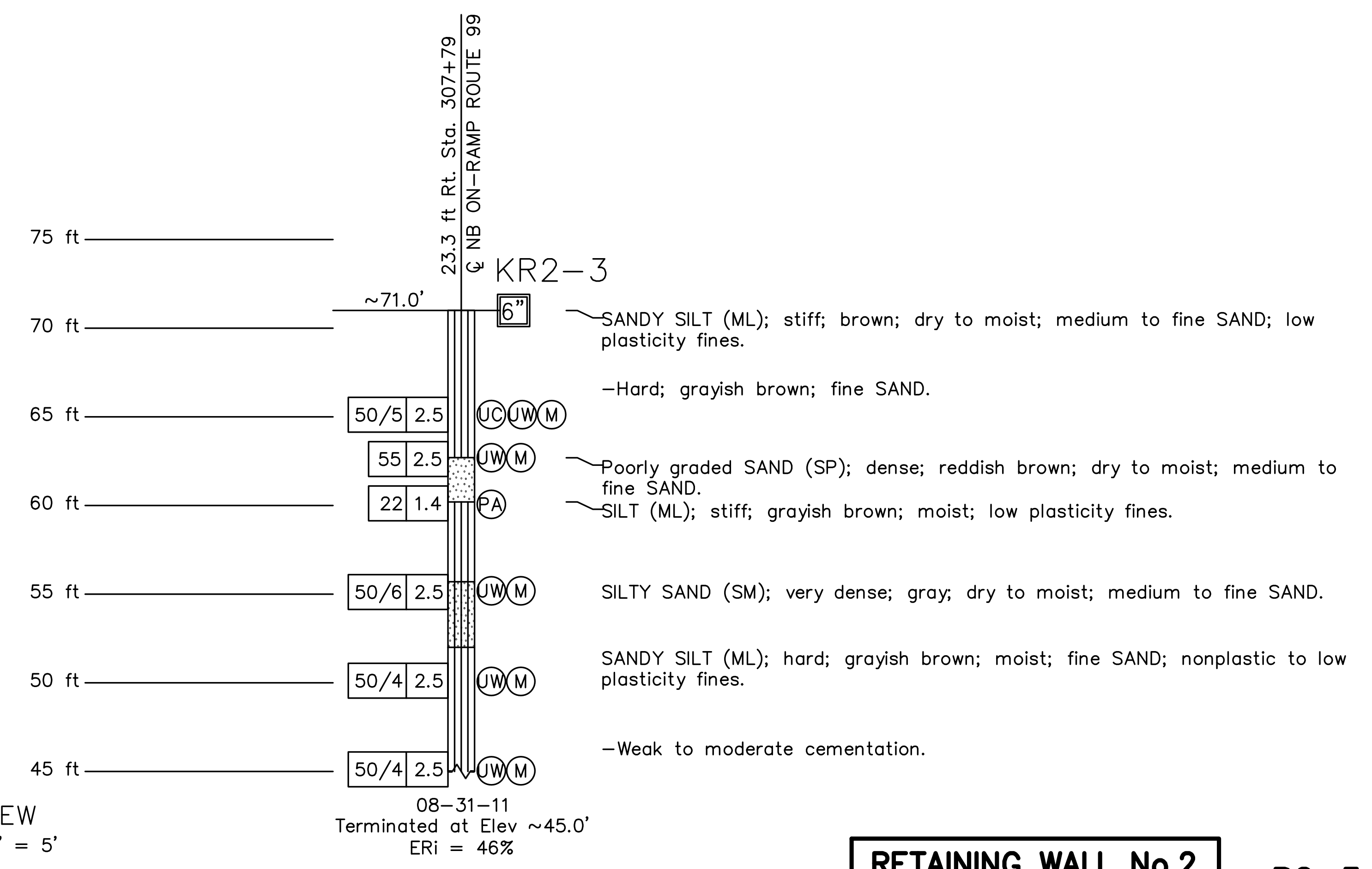
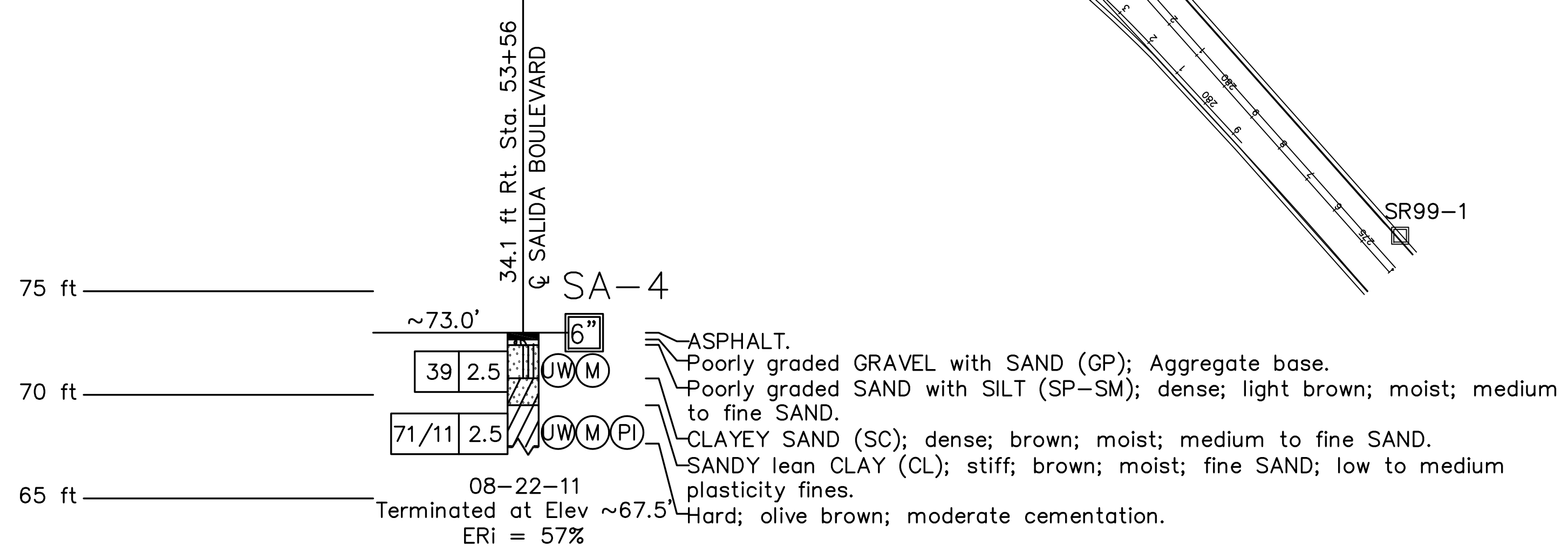
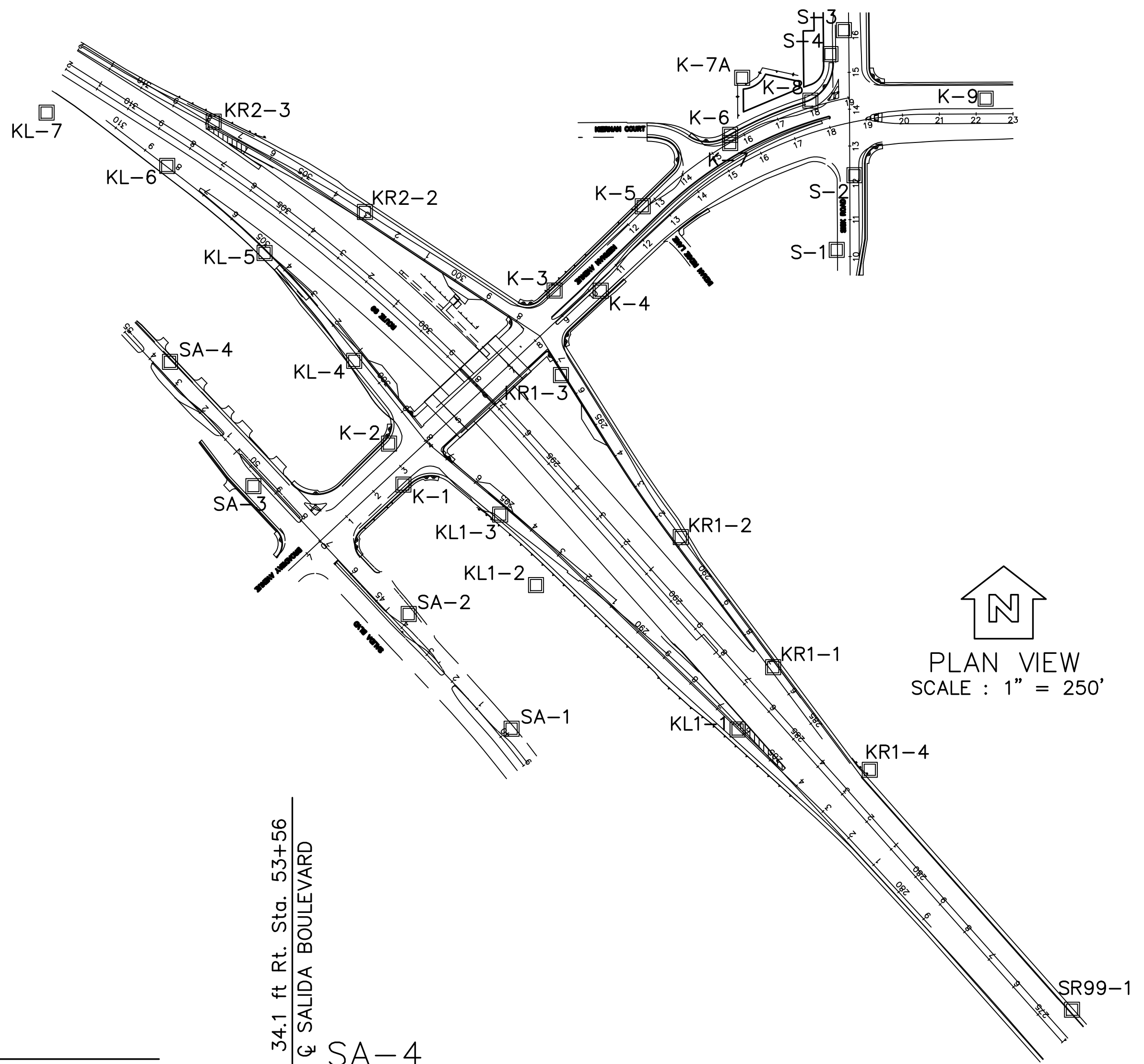
RAJAPPAN & MEYER
CONSULTING ENGINEERS, INC.
1038 LEIGH AVE, SUITE 100
SAN JOSE, CA 95126

GEOCON
CONSULTANTS, INC.
6671 BRISA STREET, LIVERMORE, CALIFORNIA 94550
TEL 925 371-5900 - FAX 925 371-5915

REGISTERED PROFESSIONAL ENGINEER
JAMES A. RODACKER
No. 2915
Exp. 06/30/12
GEOTECHNICAL
STATE OF CALIFORNIA

NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
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RETAINING WALL No.2 R2-3

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY:	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER	DATE: 08/22/2011 THROUGH 03/02/2012			POST MILES R22.56	LOG OF TEST BORINGS	

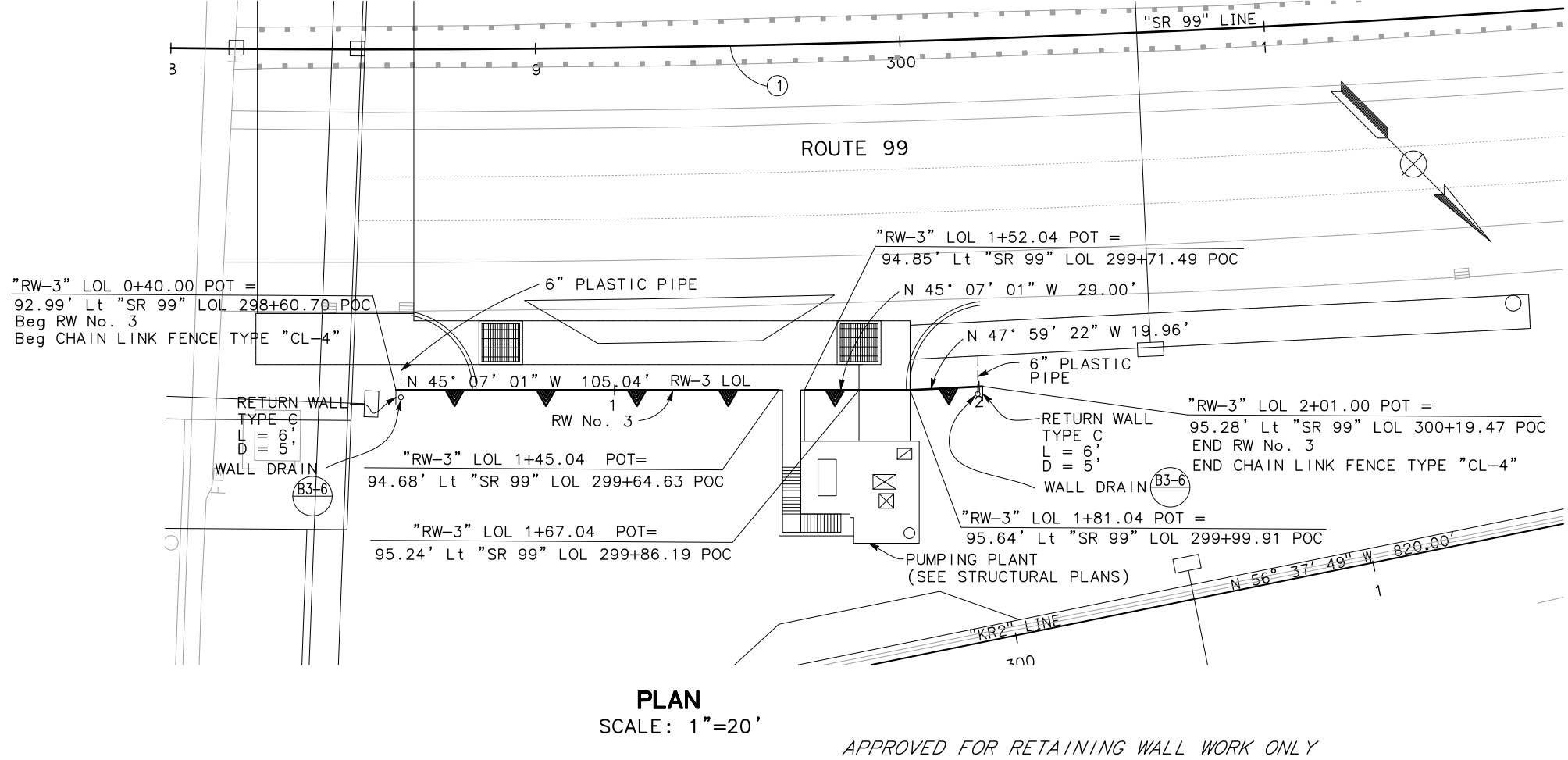
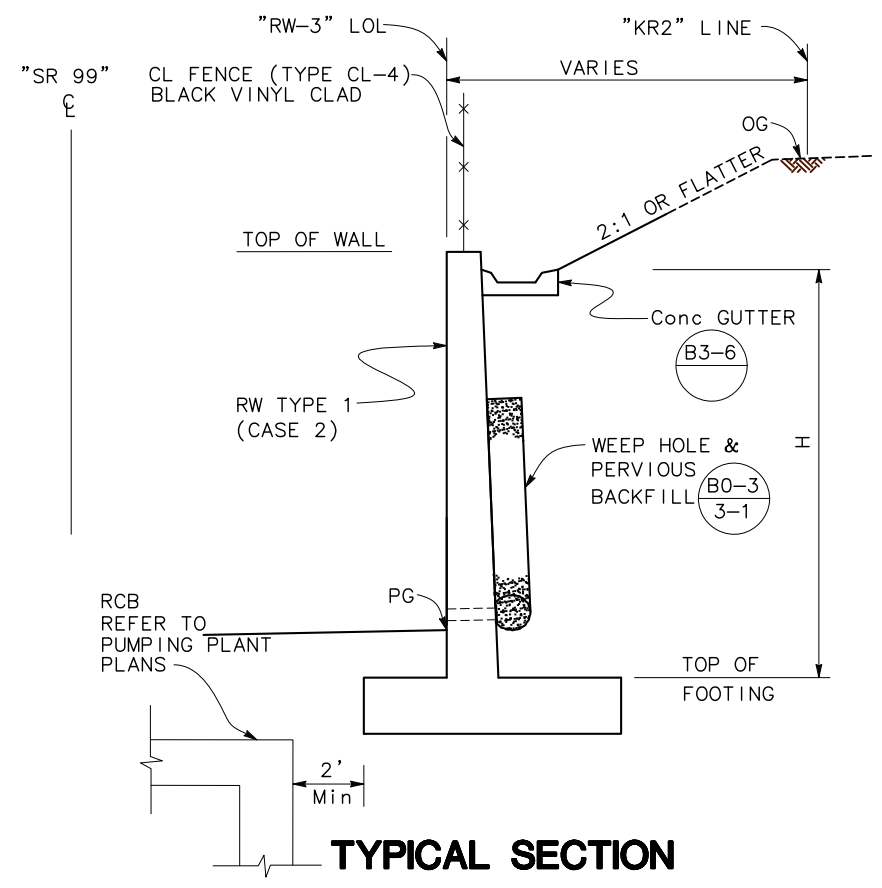
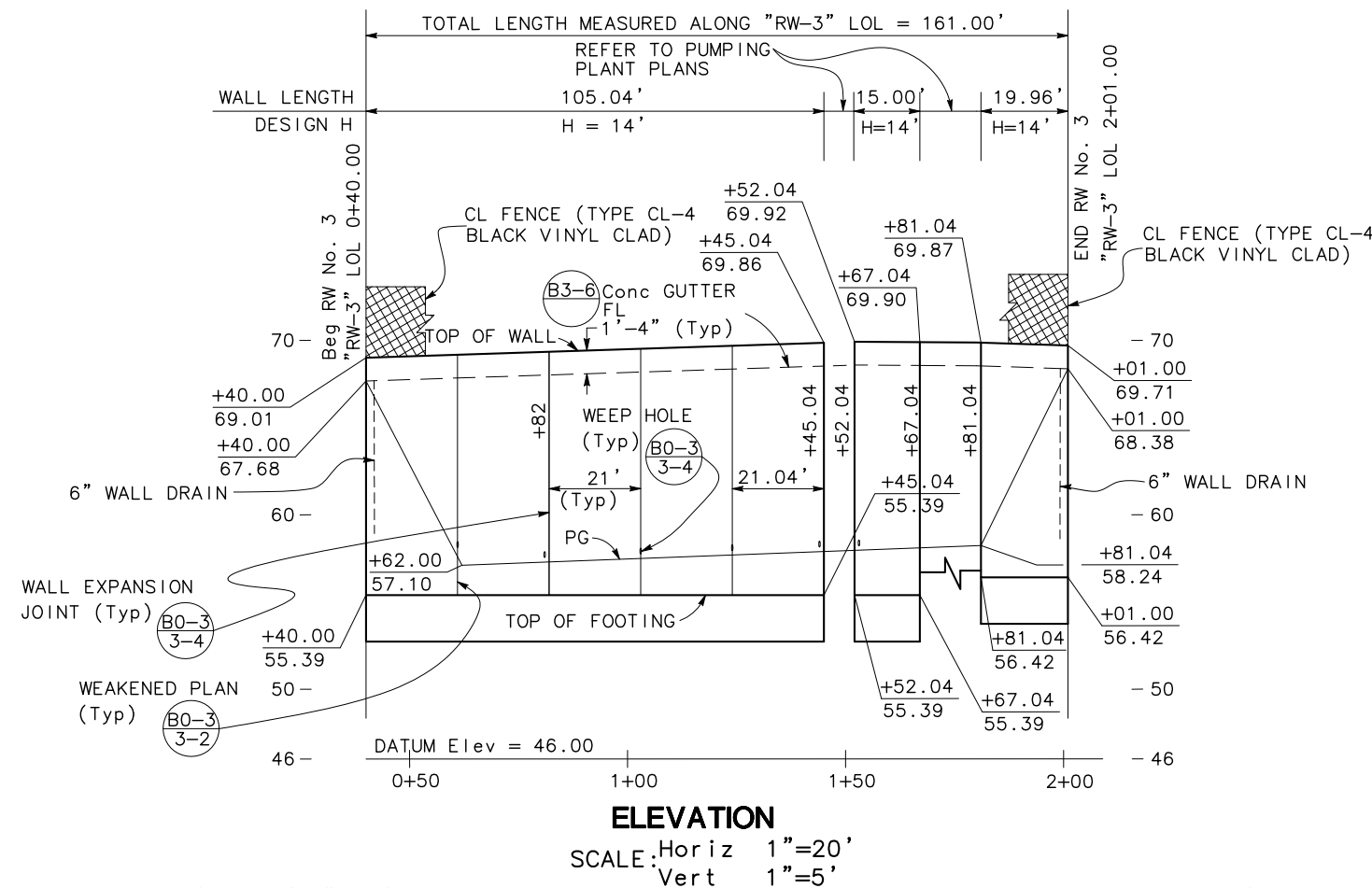
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	242	423

Chuong Nguyen
 REGISTERED CIVIL ENGINEER DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE
 No. C46770
 Exp. 06/30/13
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



CURVE DATA						
No.	RADIUS	DELTA	TANGENT	LENGTH	N	E
①	4,750.00'	15°59'12"	667.00'	1325.34'	2077836.67	6390188.78

RETAINING WALL PLAN
RETAINING WALL No. 3
 SCALE AS SHOWN
R3-1

LAST REVISION 06-08-12 DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 L. M. DERIVI
 CALCULATED-DESIGNED BY
 CHECKED BY
 C. D. B. FRANKEL
 L. M. DERIVI
 REVISED BY
 DATE REVISED

GENERAL NOTES

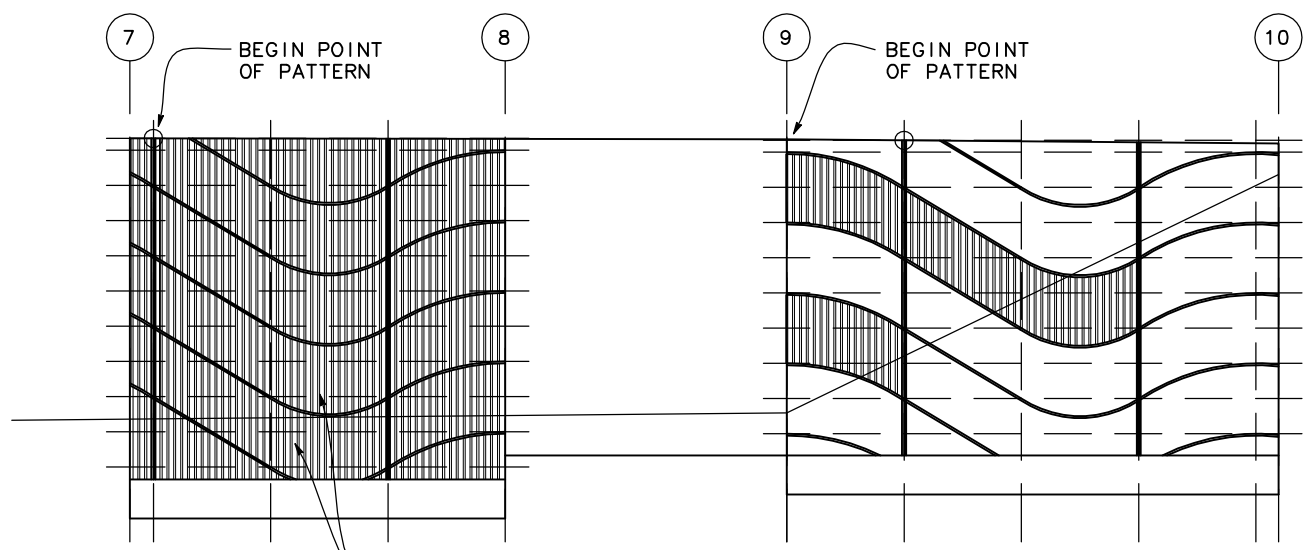
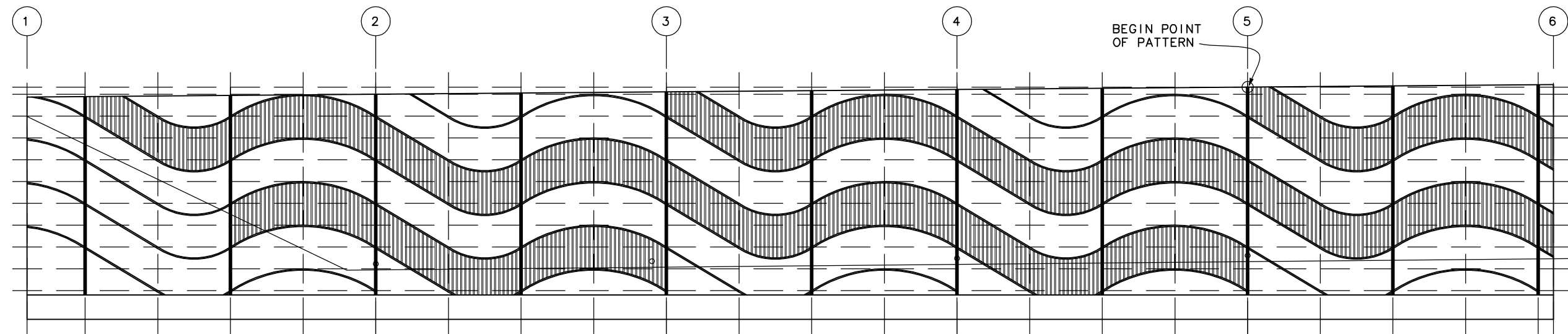
- GRIDS ALIGN WITH CONSTRUCTION CONTROL JOINTS / WEAKENED PLANES - SEE RW PLANS.
- BEGINNING POINT OF PATTERN IS WHERE TOP OF PATTERN AND TOP OF WALL ALIGN. RIGHT OR LEFT OF BEGIN POINT, TOP OF WALL MAY MOVE UP OR DOWN.
- EACH RETAINING WALL ELEVATION IS ELEVATED INDEPENDENTLY. SEE RETAINING WALL GENERAL PLANS FOR MORE INFORMATION.
- WHERE NO PATTERN SHOWN ASSUME NO TREATMENT.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	243	423

Musta M. Frankel
 LICENSED ARCHITECT
 DATE 04.30.2012
 PLANS APPROVAL DATE
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



1
 FRACTURED RIB PATTERN (GROOVED PATTERN) OCCURS AT EACH SECTION THIS WALL SECTION ONLY

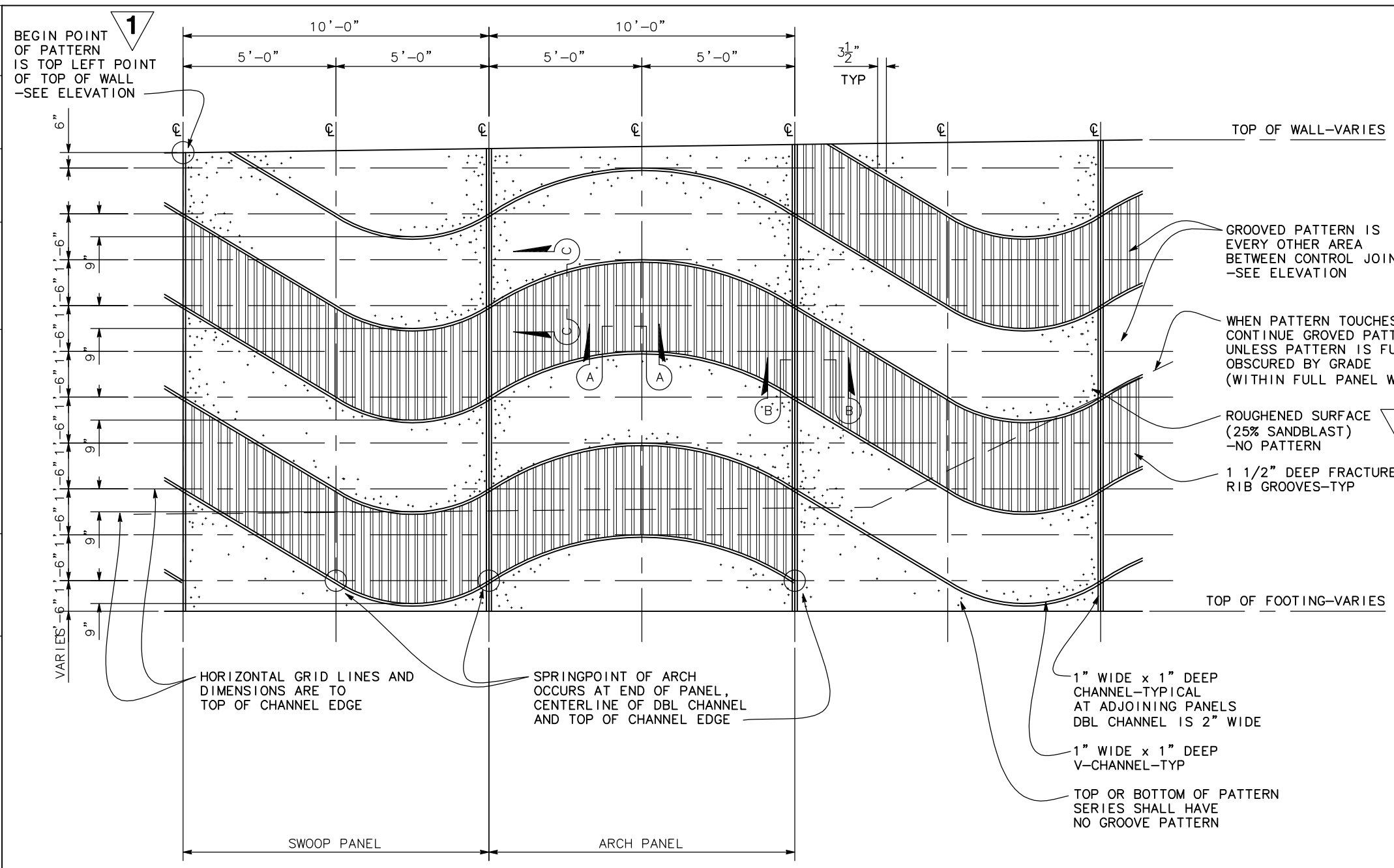
1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

RETAINING WALL AESTHETICS
RETAINING WALL No. 3
 SCALE: 1" = 4'
R3-2

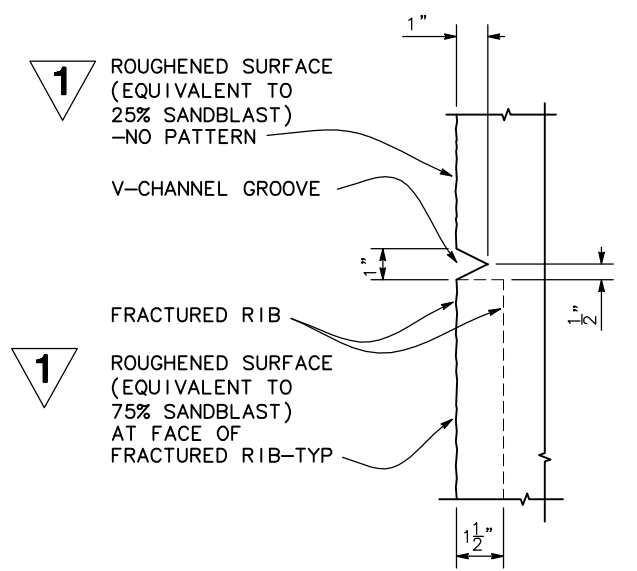
APPROVED FOR RETAINING WALL AESTHETIC TREATMENT WORK ONLY

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	244	423

<i>Christa Frankel</i> LICENSED ARCHITECT	04.30.2012 DATE
PLANS APPROVAL DATE	
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
DERIV CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

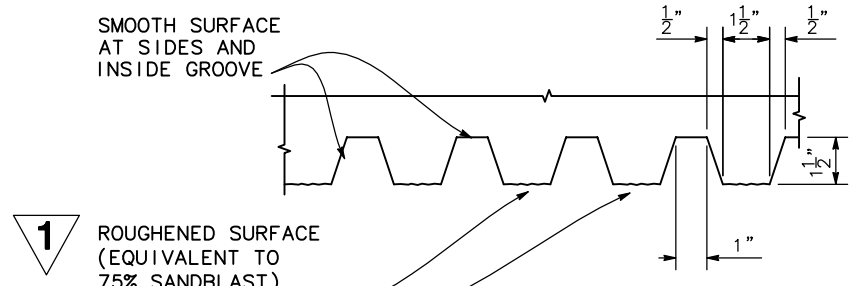


- GROOVED PATTERN IS EVERY OTHER AREA BETWEEN CONTROL JOINTS UON -SEE ELEVATION
- WHEN PATTERN TOUCHES GRADE CONTINUE GROOVED PATTERN UNLESS PATTERN IS FULLY OBSCURED BY GRADE (WITHIN FULL PANEL WIDTH)
- ROUGHENED SURFACE (25% SANDBLAST) -NO PATTERN
- 1 1/2" DEEP FRACTURED RIB GROOVES-TYP
- 1" WIDE x 1" DEEP CHANNEL-TYPICAL AT ADJOINING PANELS DBL CHANNEL IS 2" WIDE
- 1" WIDE x 1" DEEP V-CHANNEL-TYP
- TOP OR BOTTOM OF PATTERN SERIES SHALL HAVE NO GROOVE PATTERN

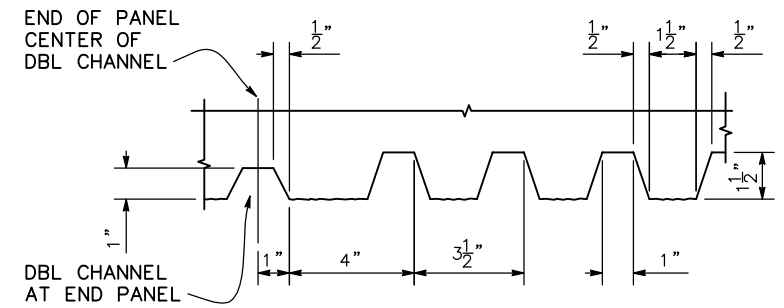


VIEW C-C (SECTION)
SCALE: 4" = 1'

DETAIL A-PANEL PATTERN



VIEW A-A (FRACTURED RIB PATTERN-TYP)
SCALE 4" = 1'



VIEW B-B (PATTERN AT END PANEL)
SCALE: 4" = 1'

PANEL PATTERN DETAIL

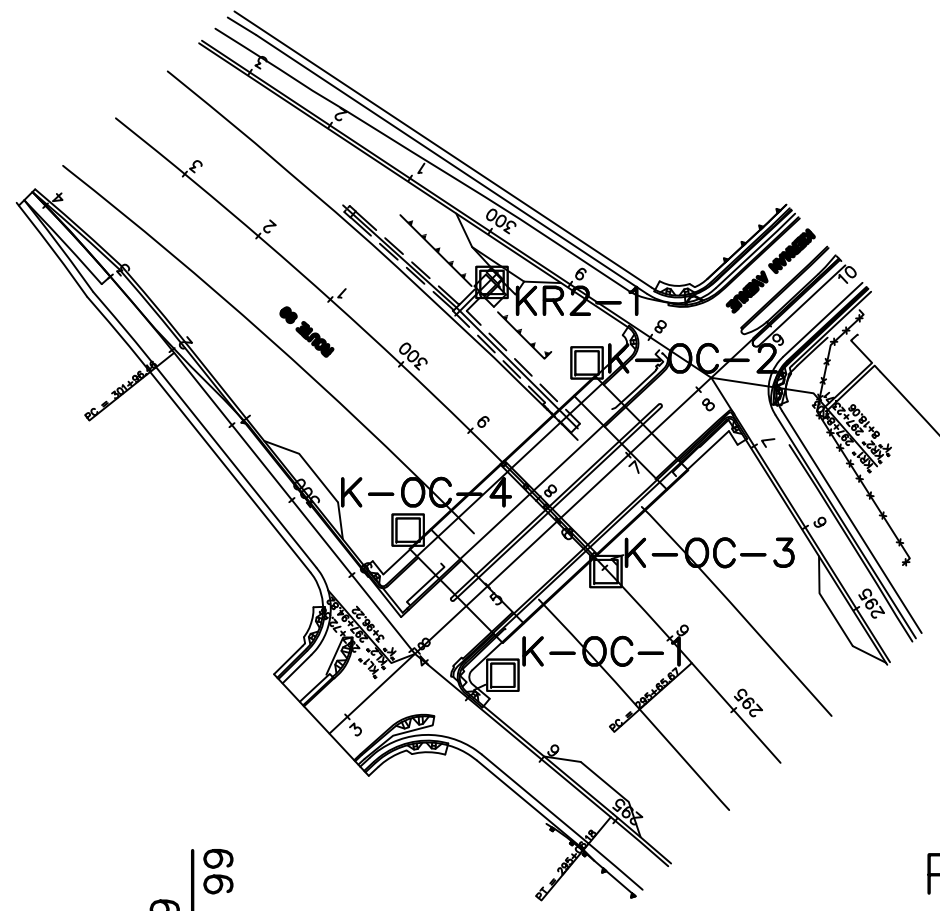
RETAINING WALL AESTHETICS
RETAINING WALL No. 3
SCALE: 1" = 2'
R3-3

1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

APPROVED FOR RETAINING WALL AESTHETIC TREATMENT WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans

CONSULTANT SUPERVISOR: L. M. DERIVI
DESIGNED BY: L. M. DERIVI
CHECKED BY: L. M. DERIVI
REVISOR: C. D. B. FRANKEL
DATE: L. M. DERIVI



PLAN VIEW
SCALE : 1" = 200'

NOTES:

- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
- 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
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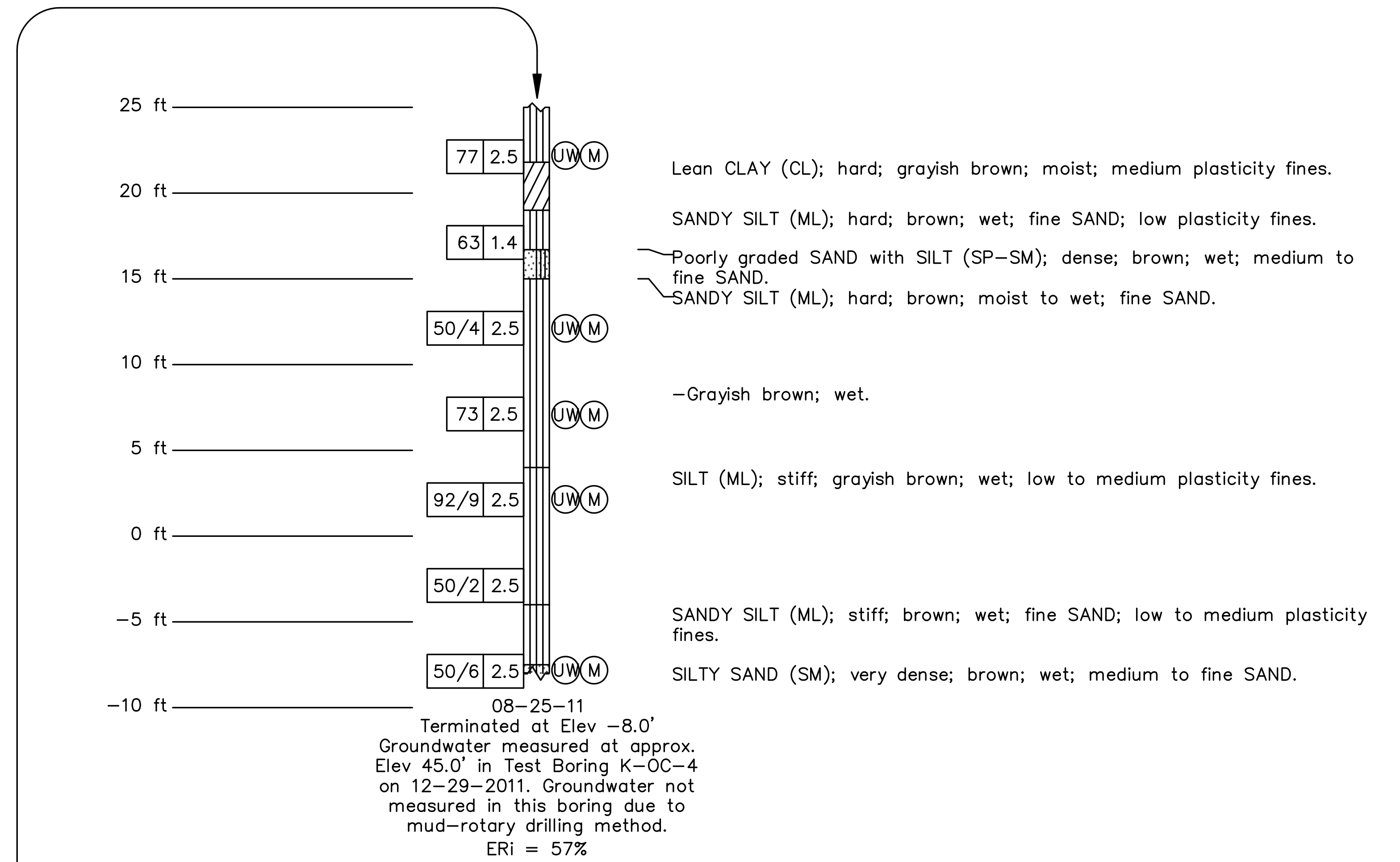
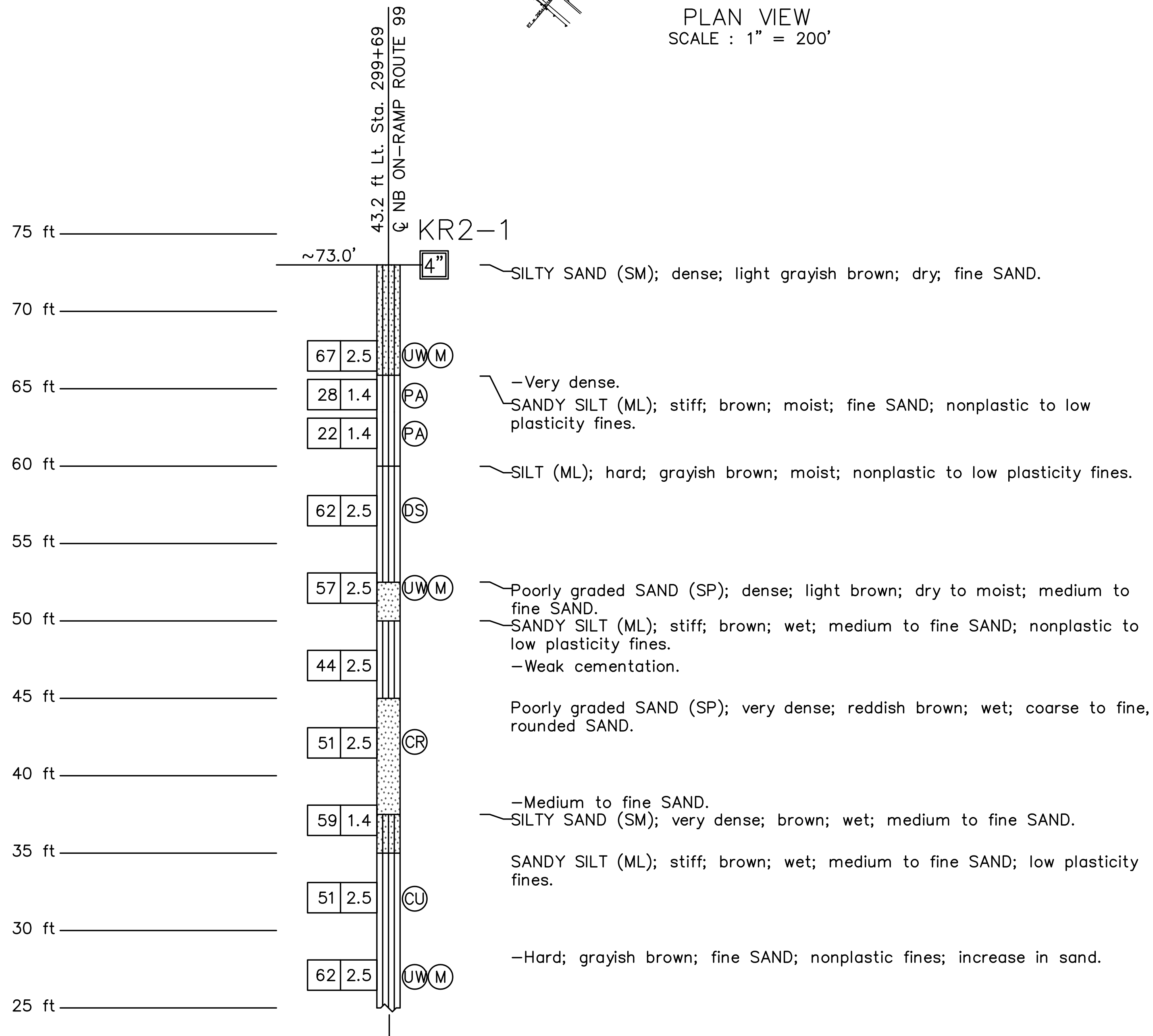
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	245	423

REGISTERED GEOTECHNICAL ENG. DATE 04/30/12
 PLANS APPROVAL DATE 9/04/12
 The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE, SUITE 100
 SAN JOSE, CA 95126

GEOCON CONSULTANTS, INC.
 6671 BRISA STREET, LIVERMORE, CALIFORNIA 94550
 TEL 925 371-5900 - FAX 925 371-5915



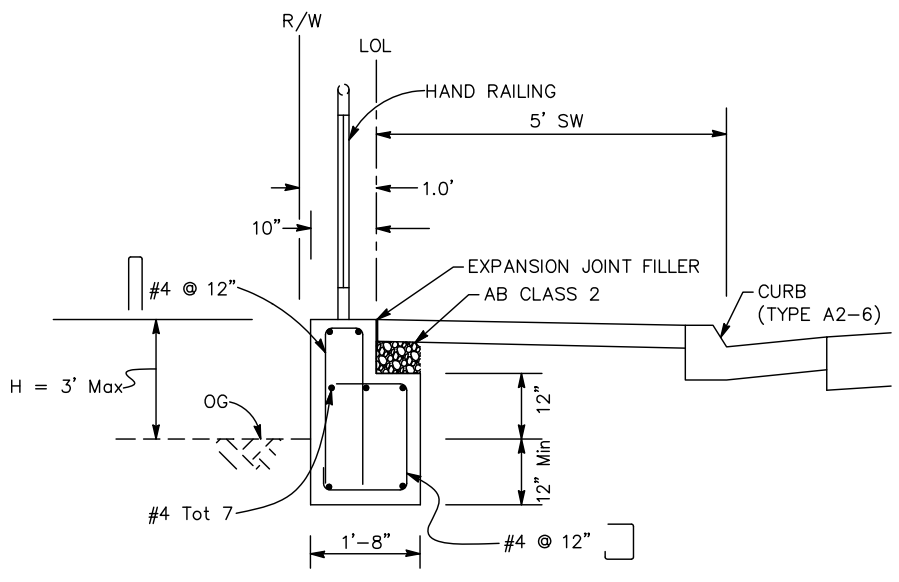
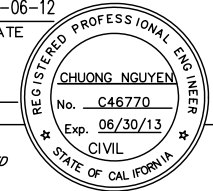
PROFILE VIEW
VERT. SCALE : 1" = 5'

RETAINING WALL No.3 R3-4

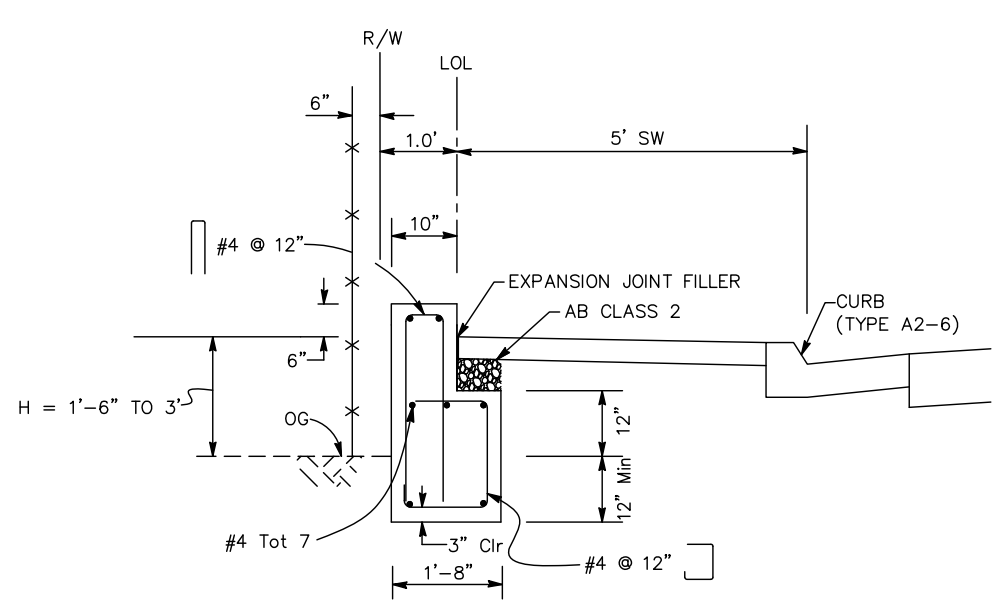
DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY:	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER	DATE: 08/22/2011-08/31/2011, 12/29/2011			R22.56	LOG OF TEST BORINGS	
GS GEOTECHNICAL LOG OF TEST BORINGS SHEET (ENGLISH) (REV. 7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: PROJECT NUMBER & PHASE: 1457 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES X-XX-XX
				FILE => \$REQUEST		SHEET XX	OF XX

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	246	423

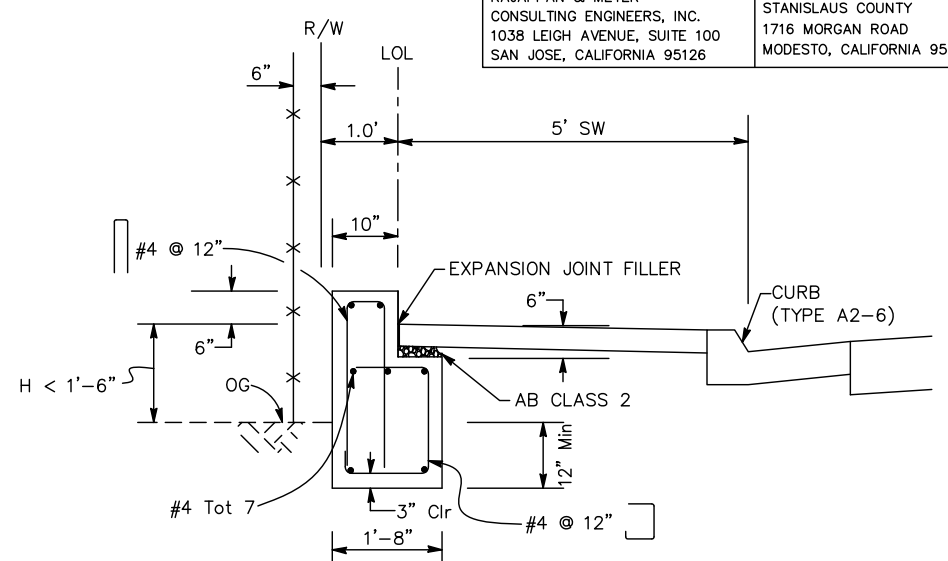
REGISTERED CIVIL ENGINEER	DATE
9/04/12	05-06-12
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



CURB WALL WITH HAND RAILING
 Sta 8+85.18 TO Sta 10+16.00
 NO SCALE

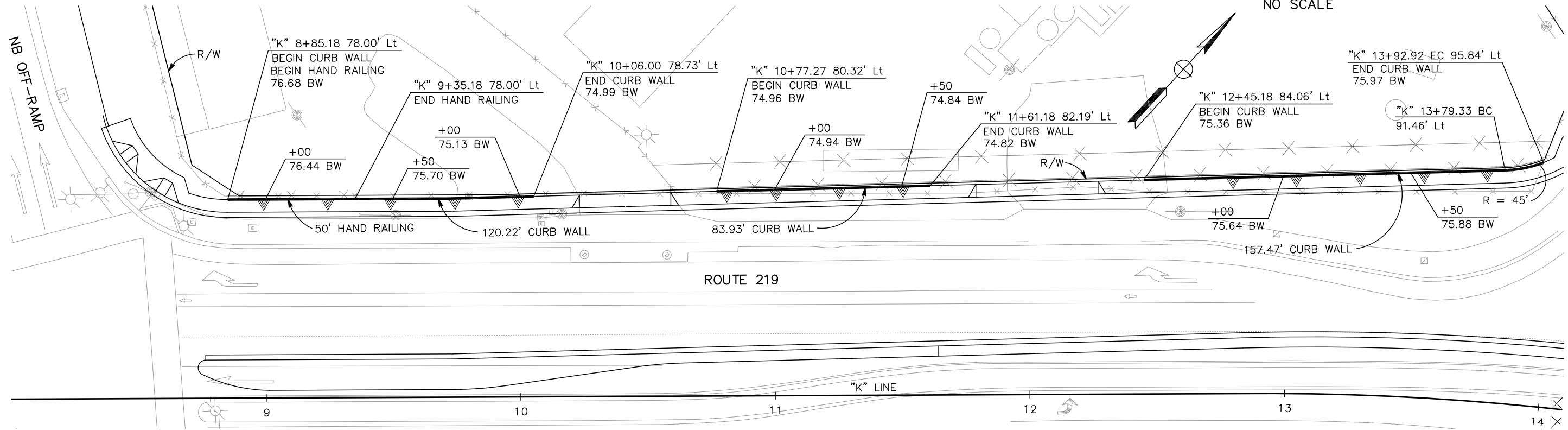


CURB WALL
 (H = 1'-6" - 3.0')
 NO SCALE



CURB WALL
 (H < 1'-6")
 NO SCALE

ABBREVIATION:
 BW BACK OF SIDEWALK ELEVATION



PLAN
 SCALE: 1" = 20'

RETAINING WALL PLAN (CURB WALL)
 SCALE AS SHOWN

RW4-1

APPROVED FOR RETAINING WALL WORK ONLY

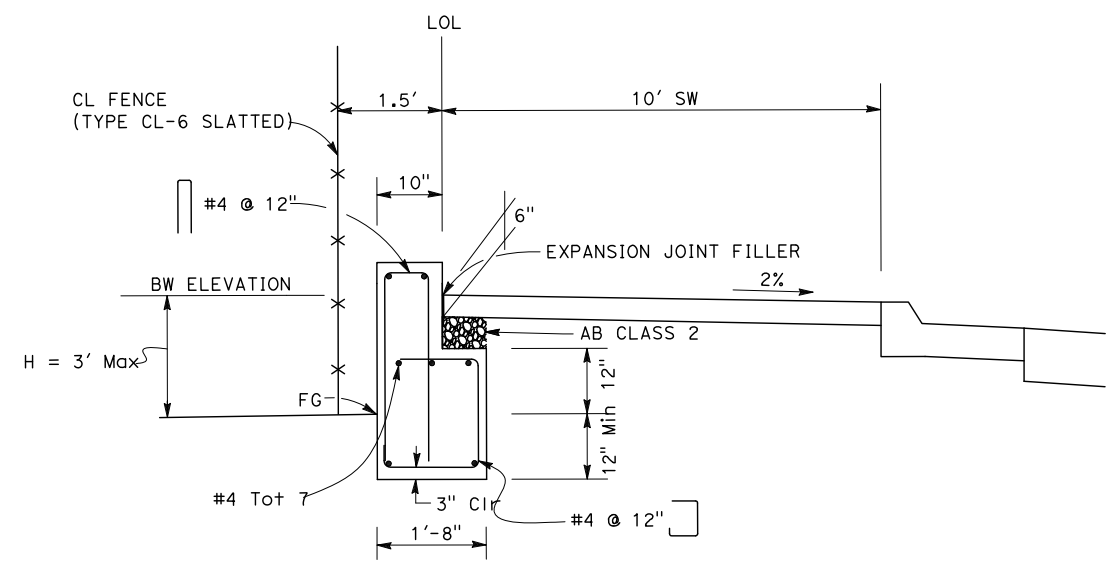
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 REVISIONS: ANDY CHAN, CHUONG NGUYEN, REVISED BY, DATE REVIS
 CALCULATED-DESIGNED BY, CHECKED BY
 Galtrans

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	247	423

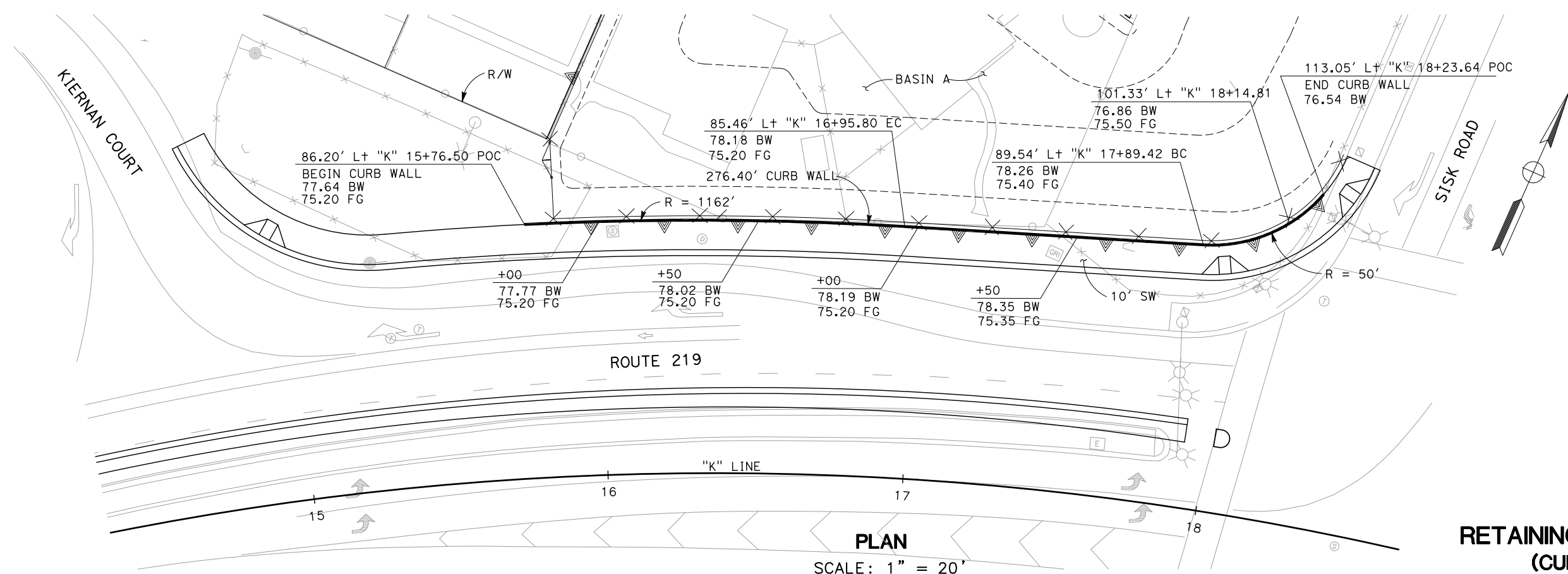
<i>Chuong Nguyen</i>		05-18-12
REGISTERED CIVIL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358	

ABBREVIATION:

BW BACK OF SIDEWALK
 FG FINISH GRADE AT BACK OF WALK



TYPICAL SECTION
NO SCALE



PLAN

SCALE: 1" = 20'

RETAINING WALL PLAN
(CURB WALL)

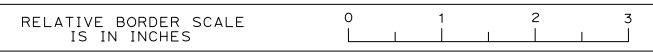
SCALE AS SHOWN

RW4-2

APPROVED FOR RETAINING WALL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 ANDY CHAN
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED
 \$t Caltrans

USERNAME => \$USER
 DGN FILE => \$REQUEST




UNIT 1457

PROJECT NUMBER & PHASE 1000000100

LAST REVISION
 06-08-12
 DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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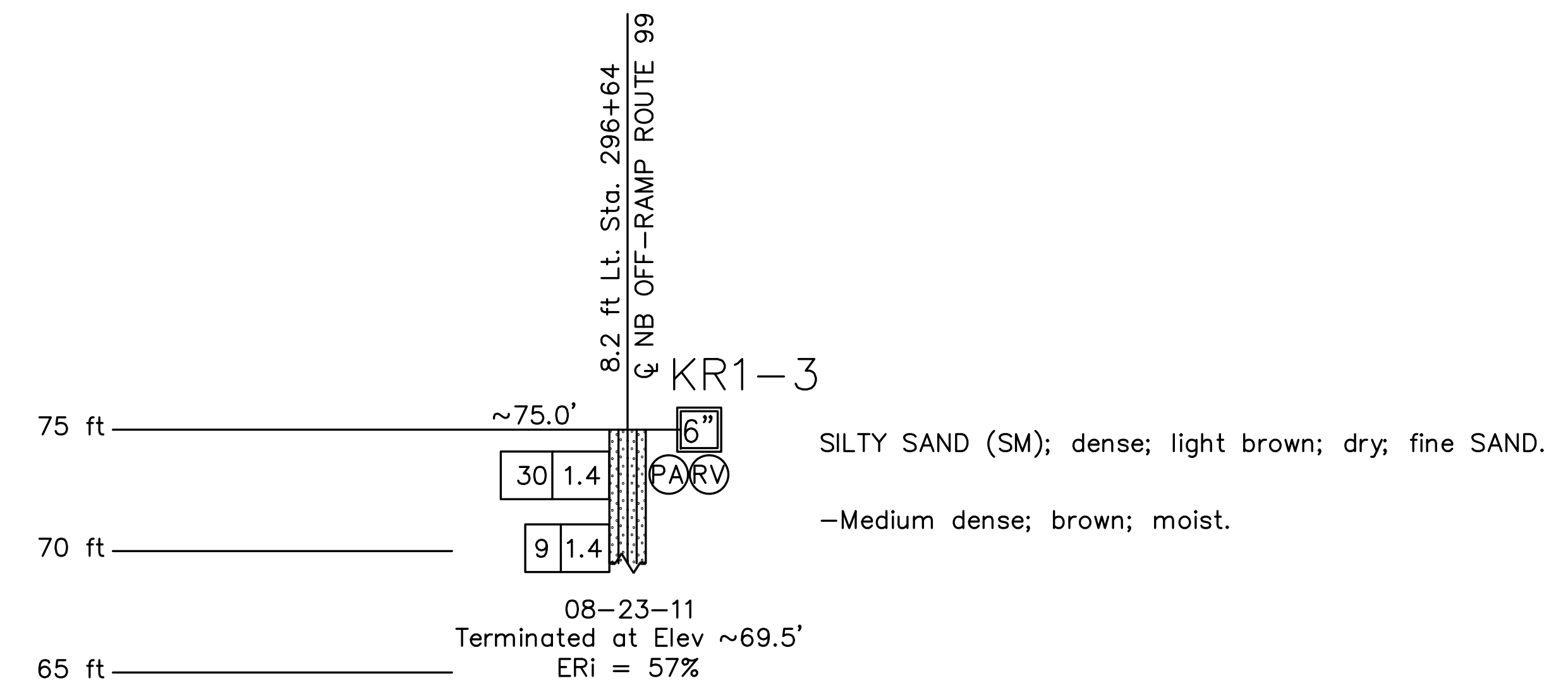
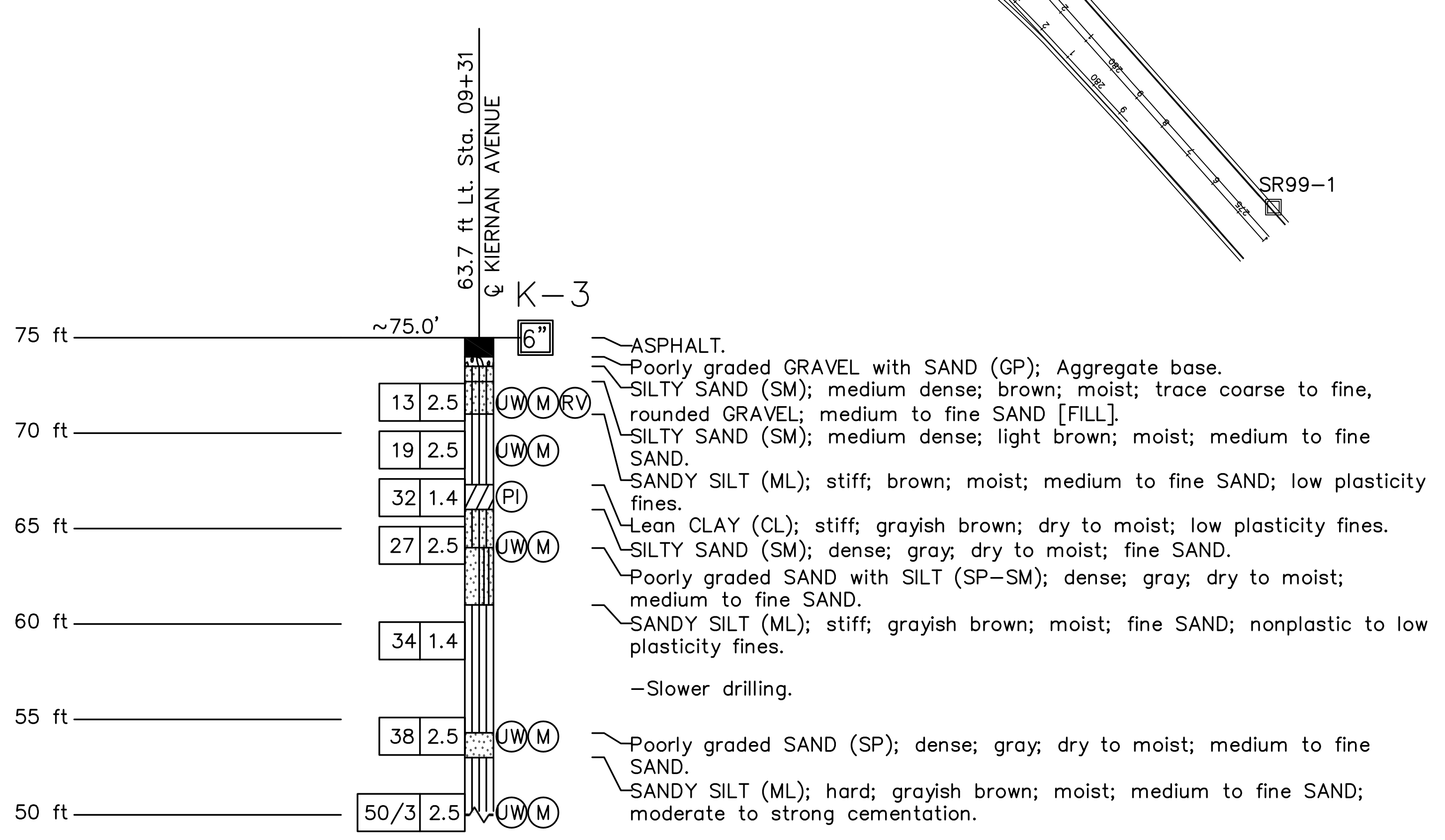
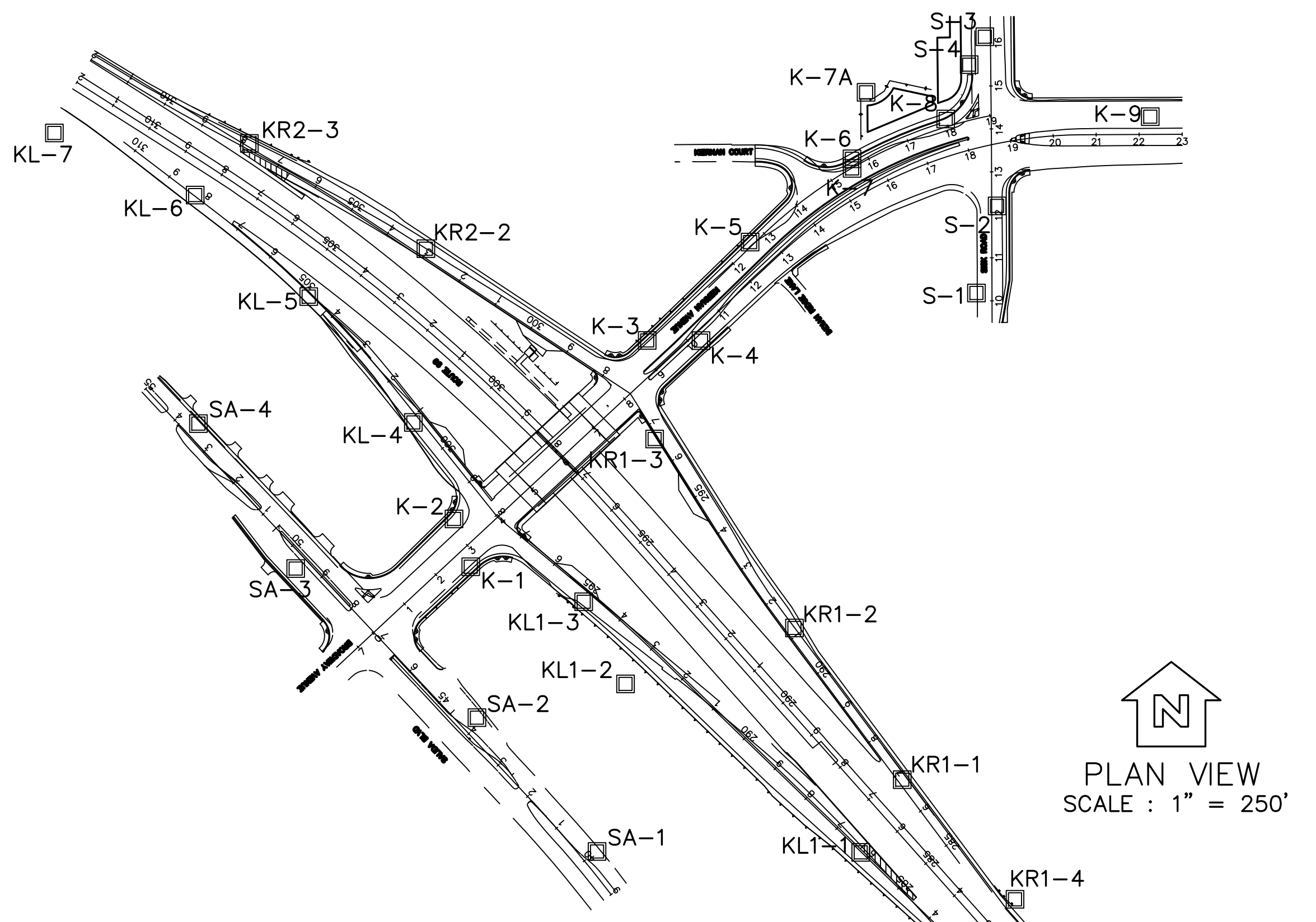

 REGISTERED GEOTECHNICAL ENG. DATE 04/30/12
 PLANS APPROVAL DATE 9/04/12
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE, SUITE 100
 SAN JOSE, CA 95126
GEOCON
 CONSULTANTS, INC.
 6671 BRISA STREET, LIVERMORE, CALIFORNIA 94550
 TEL 925 371-5900 - FAX 925 371-5915




NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
- 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
- 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
- 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
- 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
- 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



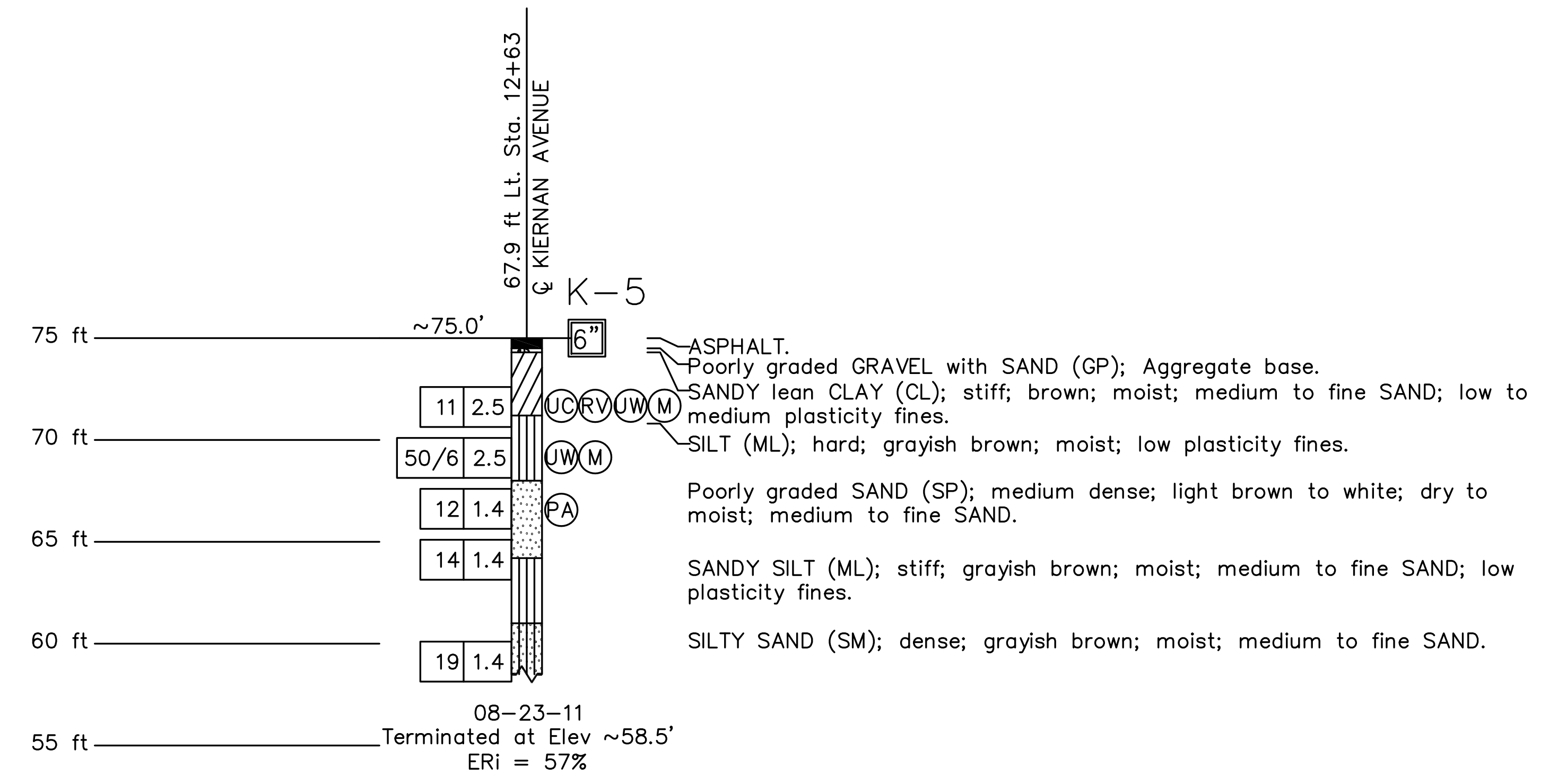
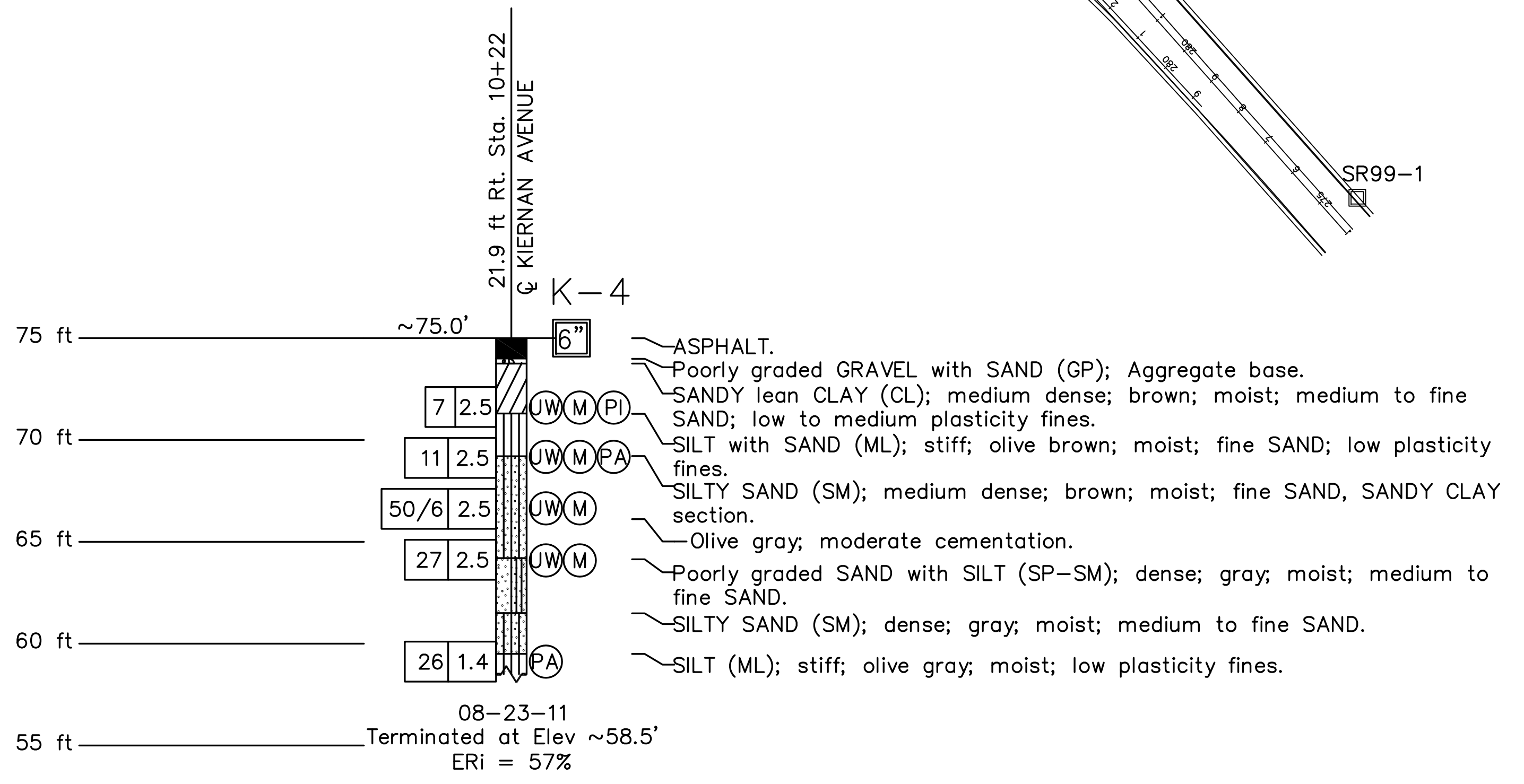
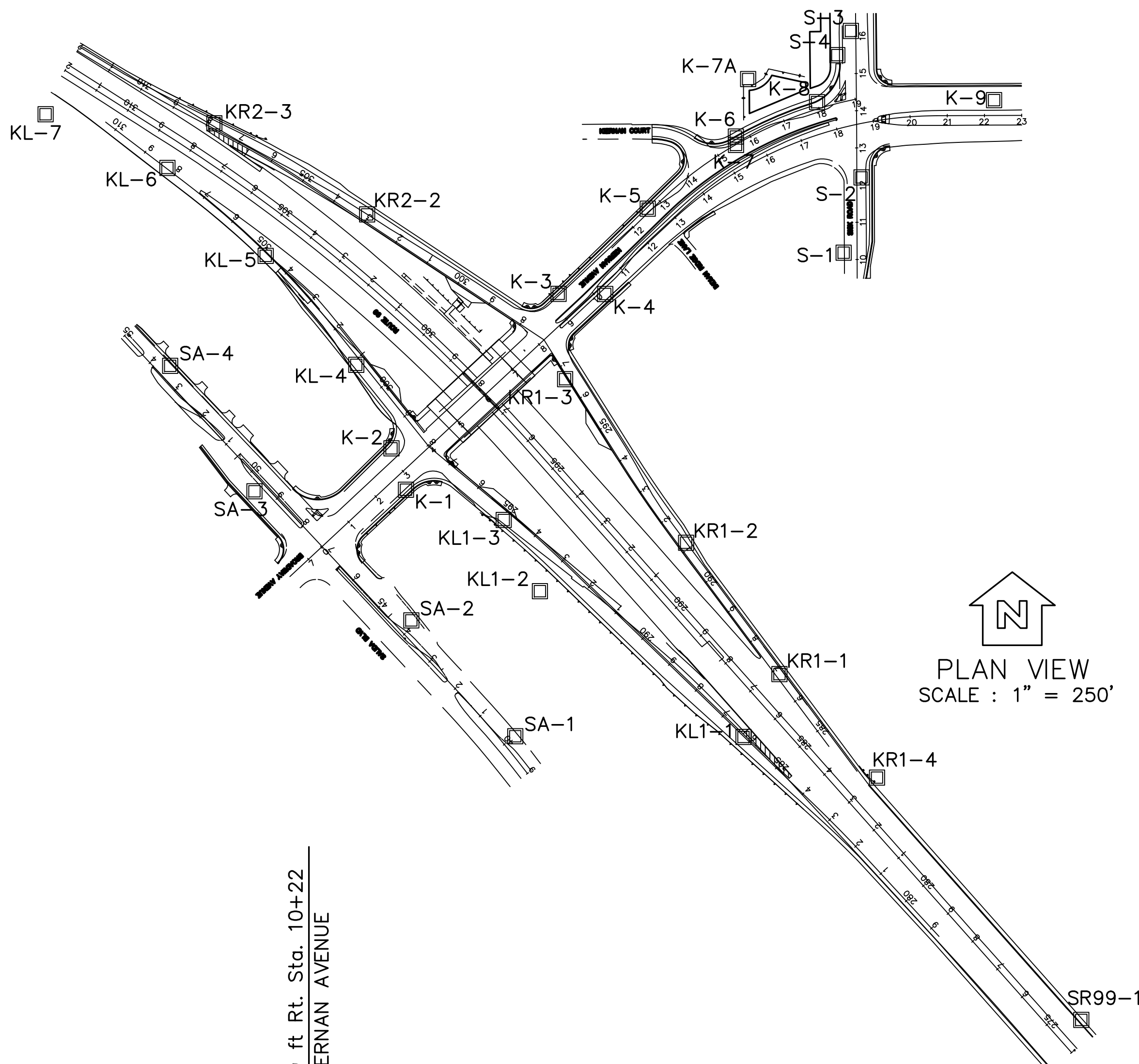
CURB WALL R4-3

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER	FIELD INVESTIGATION BY: DATE: 08/22/2011 THROUGH 03/02/2012			POST MILES R22.56	LOG OF TEST BORINGS	

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	249	423
 REGISTERED GEOTECHNICAL ENG. DATE 04/30/12					
PLANS APPROVAL DATE 9/04/12					
<small>The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.</small>					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVE, SUITE 100 SAN JOSE, CA 95126					
					

NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
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- 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
- 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
- 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).




PROFILE VIEW
 VERT. SCALE : 1" = 5'

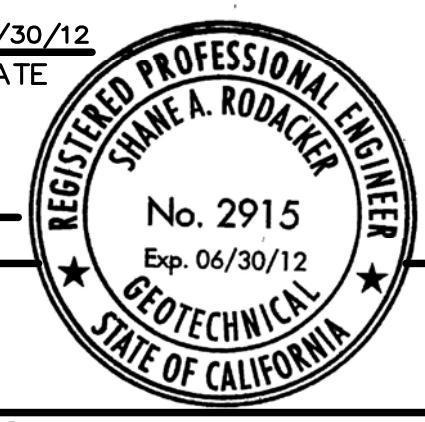
CURB WALL R4-4

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY:	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163 POST MILES R22.56	ROUTE 99/KIERNAN INTERCHANGE	
SIGN OFF DATE	CHECKED BY S. RODACKER	DATE: 08/22/2011 THROUGH 03/02/2012			LOG OF TEST BORINGS		
GS GEOTECHNICAL LOG OF TEST BORINGS SHEET (ENGLISH) (REV. 7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: PROJECT NUMBER & PHASE: 1000000100	1457 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES X-XX-XX
				FILE => \$REQUEST		SHEET XX	OF XX

DATE PLOTTED => \$TIME
 USERNAME => \$USER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	250	423

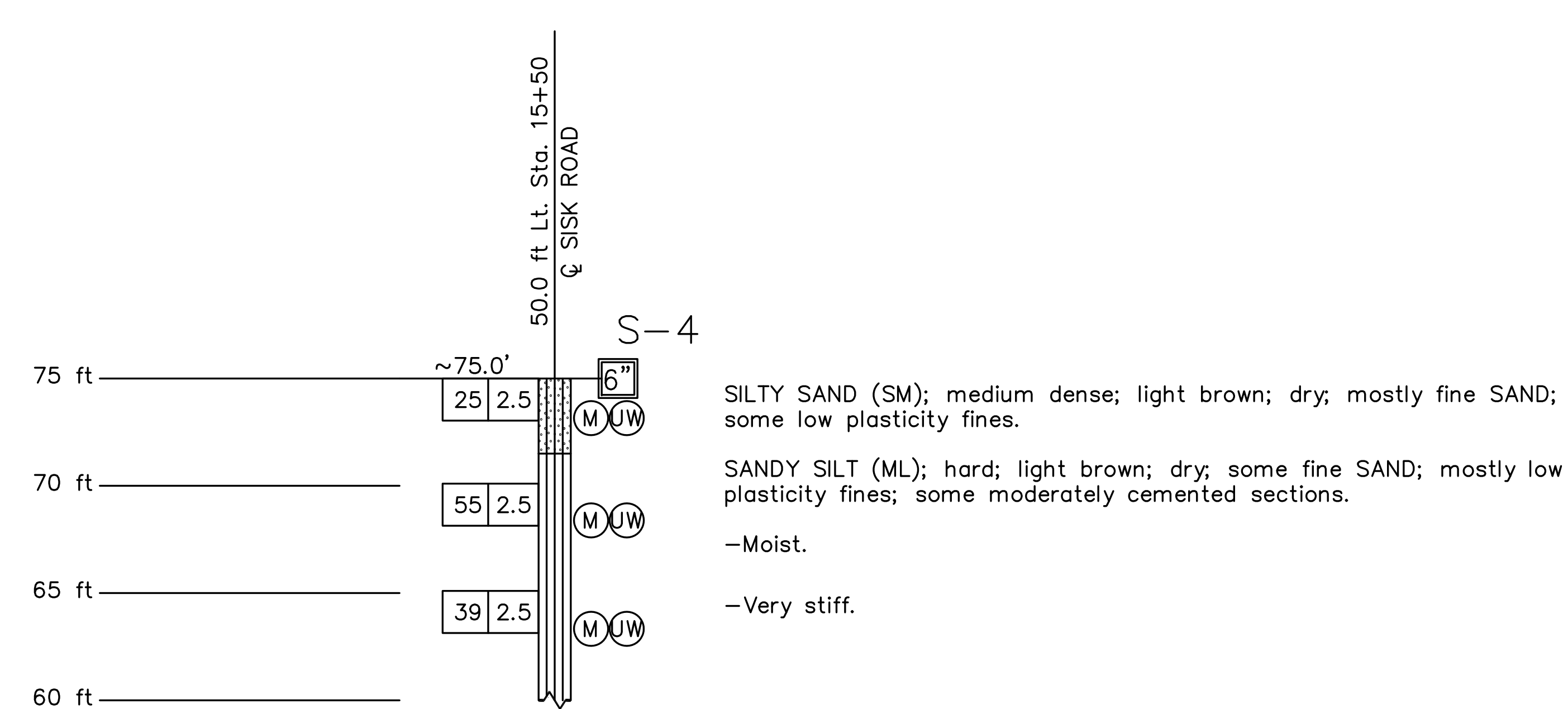
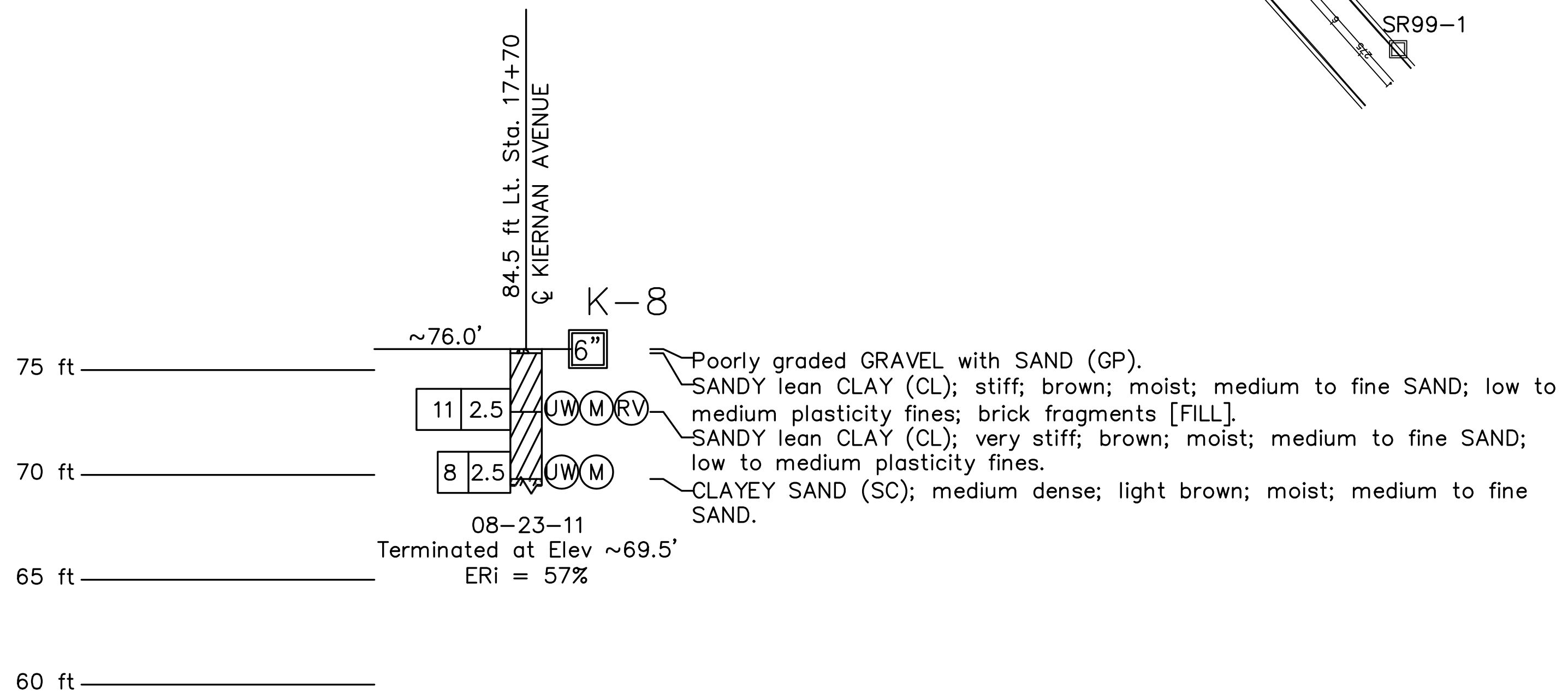
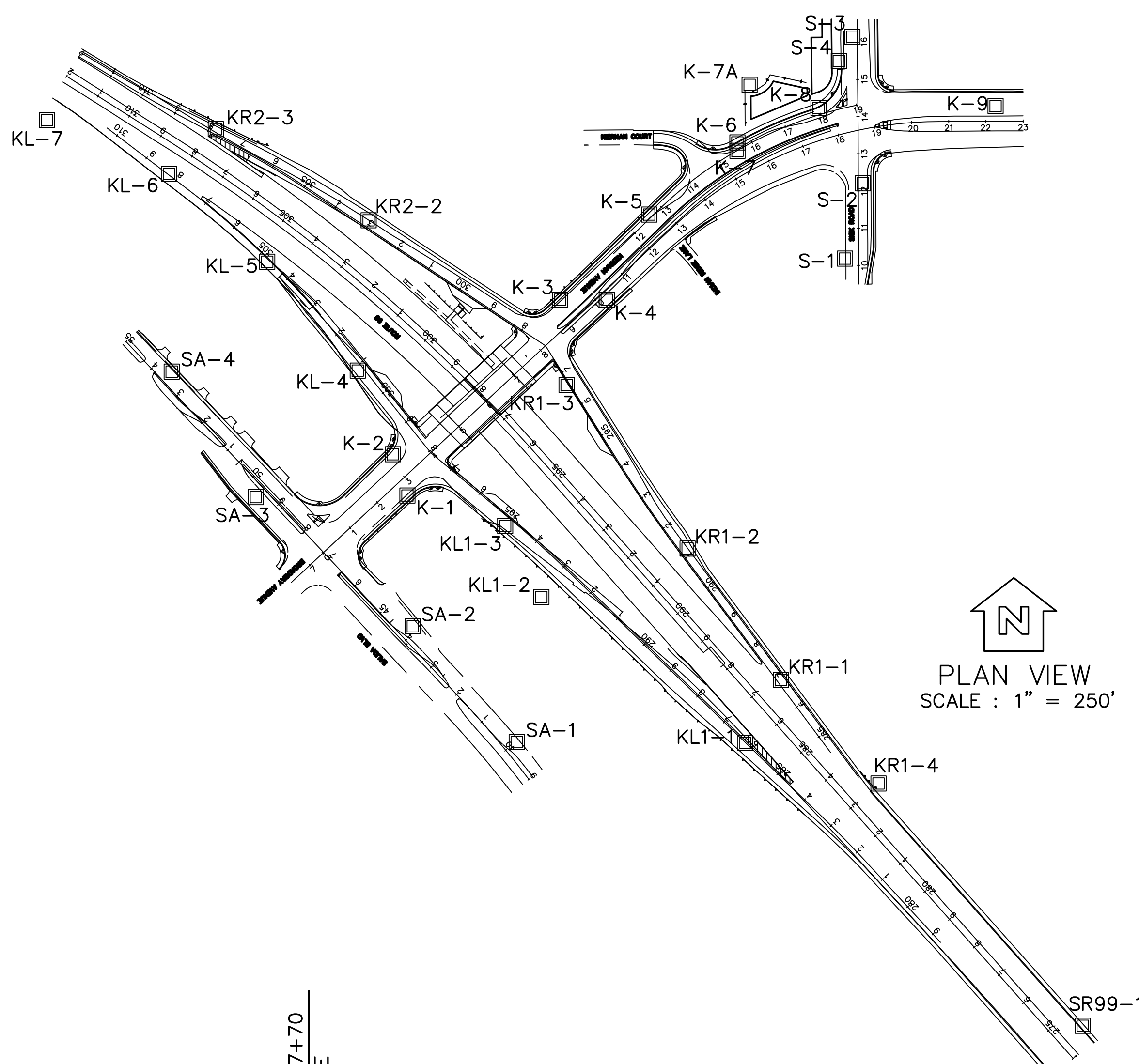

 REGISTERED GEOTECHNICAL ENG. DATE 04/30/12
 9/04/12
 PLANS APPROVAL DATE
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE, SUITE 100
 SAN JOSE, CA 95126
GEOCON
 CONSULTANTS, INC.
 6671 BRISA STREET, LIVERMORE, CALIFORNIA 94550
 TEL 925 371-5900 - FAX 925 371-5915

NOTES:

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- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



PROFILE VIEW
VERT. SCALE : 1" = 5'

CURB WALL R4-5

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY: DATE: 08/22/2011 THROUGH 03/02/2012	BRIDGE NO. 38-0163	ROUTE 99/KIERNAN INTERCHANGE
SIGN OFF DATE	CHECKED BY S. RODACKER	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	POST MILES R22.56	LOG OF TEST BORINGS
PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304			SHEET OF XX XX	

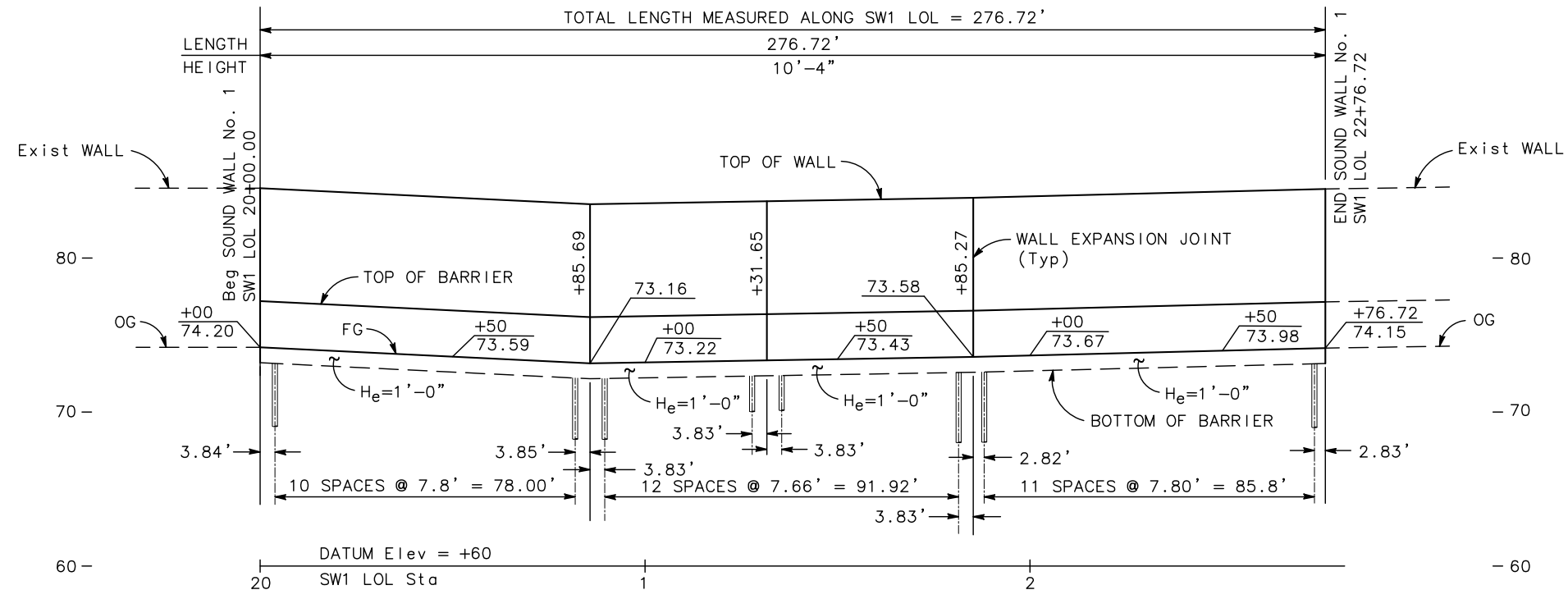
TIME PLOTTED => \$DATE
 USERNAME => \$USER

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 REVISIONS: REVISED BY ANDY DO, DATE CHUONG NGUYEN, CHECKED BY KEITH G. MEYER, CALCULATED/DESIGNED BY, CHECKED BY

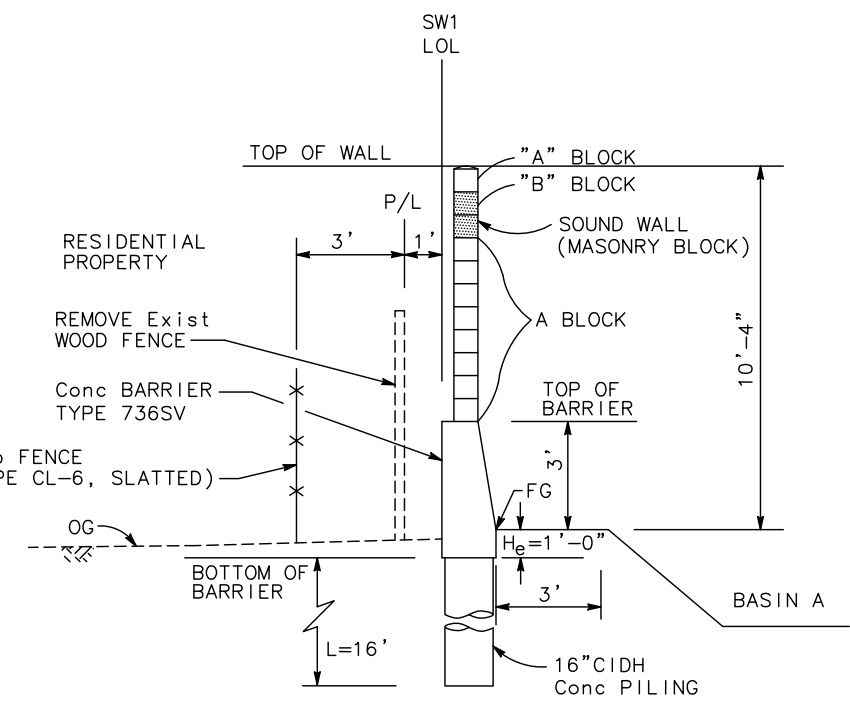
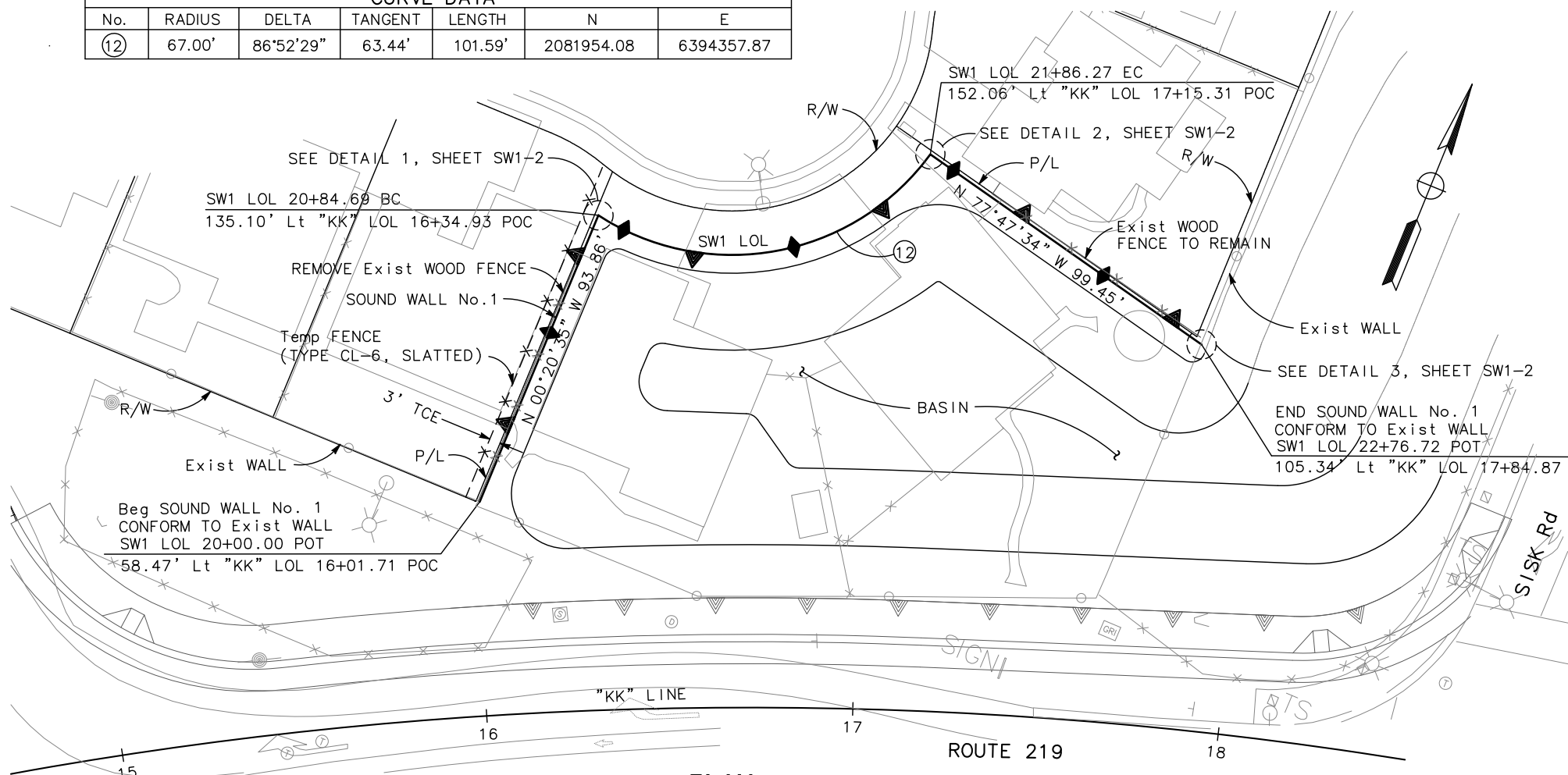
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	251	423

REGISTERED CIVIL ENGINEER: *Chuong Nguyen* DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 REGISTERED PROFESSIONAL ENGINEER: CHUONG NGUYEN No. C46770 Exp. 06/30/13 CIVIL STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358


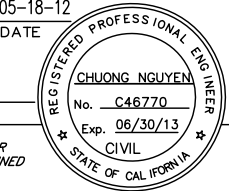


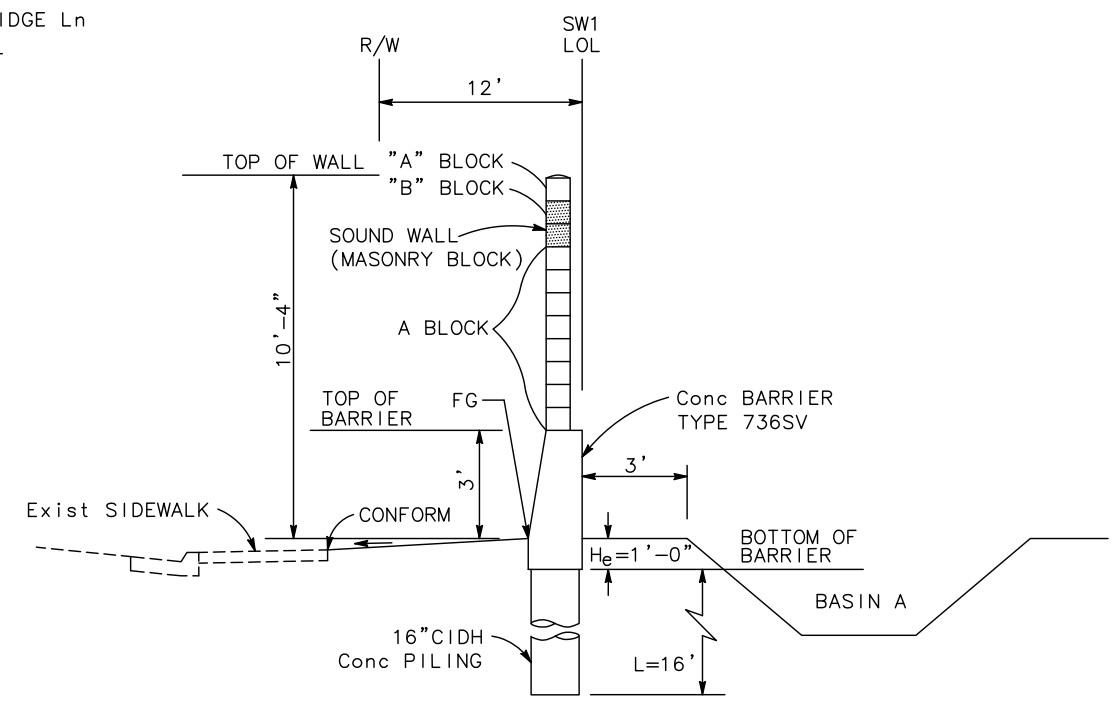
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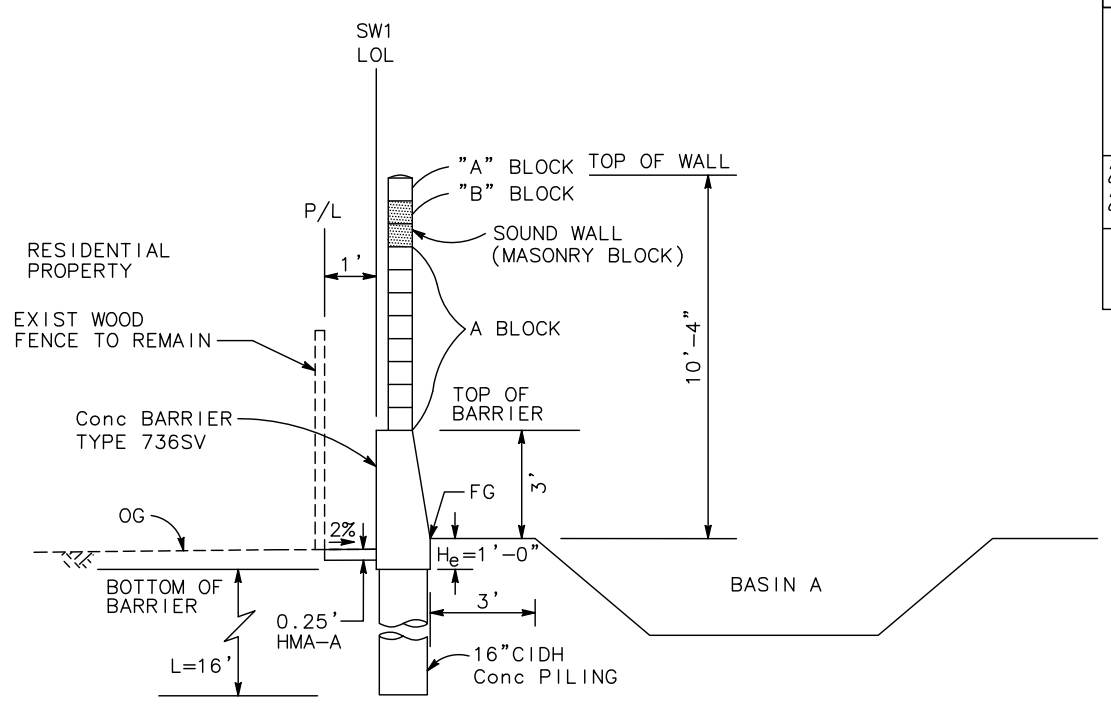
- NOTES:**
- FOR DETAILS OF SOUND WALL ON BARRIER, SEE Std PLAN (B15-6) (B15-7) (B15-8)
 - "A" BLOCKS SHALL BE 8" X 8" X 16", BROWN COLOR (MATCH Exist BLOCK), NOMINAL SIZE AND PLAIN FACE BOTH SIDES.
 - "B" BLOCKS SHALL BE 8" X 8" X 16", GRAY COLOR (MATCH Exist BLOCK), SPLIT-FACED (BOTH SIDES).
 - TEMPORARY FENCE (TYPE CL-6, SLATTED) SHALL BE REMOVED AFTER THE CONSTRUCTION.

SOUND WALL PLAN
 SCALE AS SHOWN
SW1-1

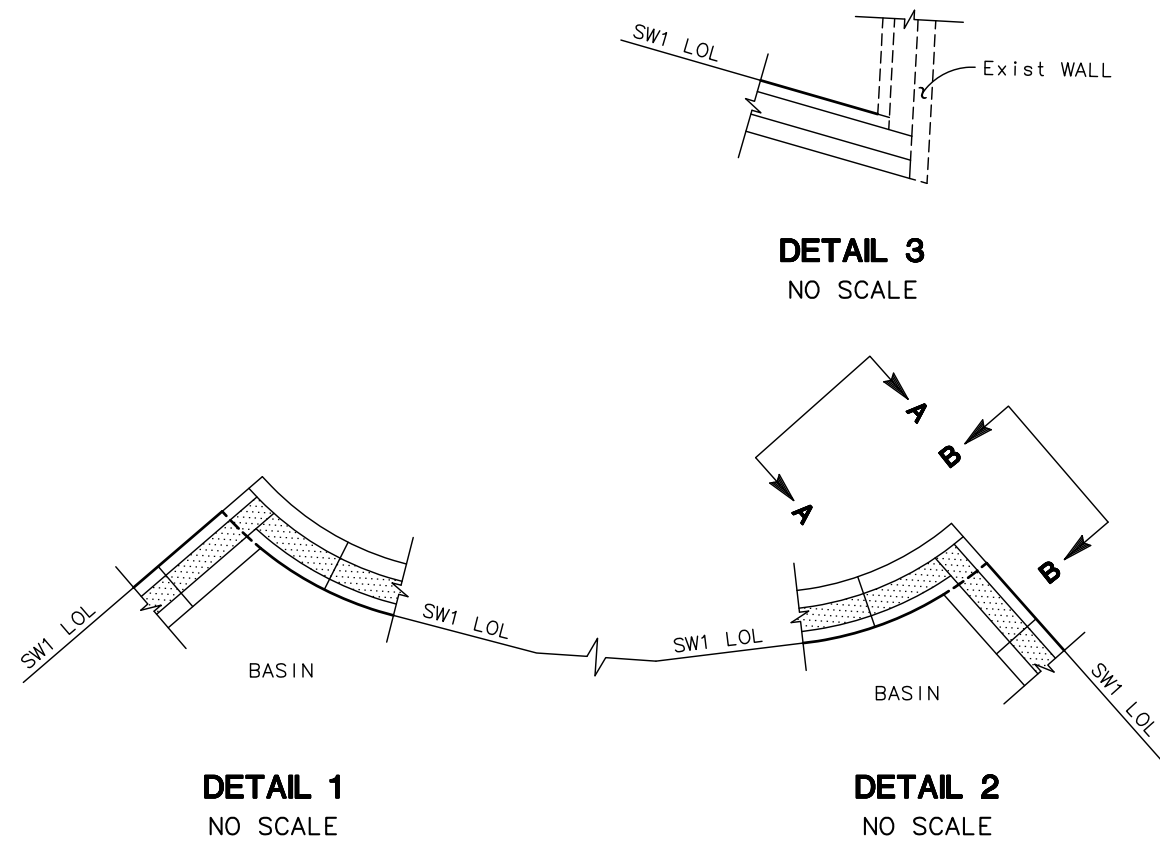
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	252	423
 REGISTERED CIVIL ENGINEER		05-18-12 DATE			
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



TYPICAL SECTION
 SW1 Sta 20+84.69 TO Sta 21+86.24
 NO SCALE



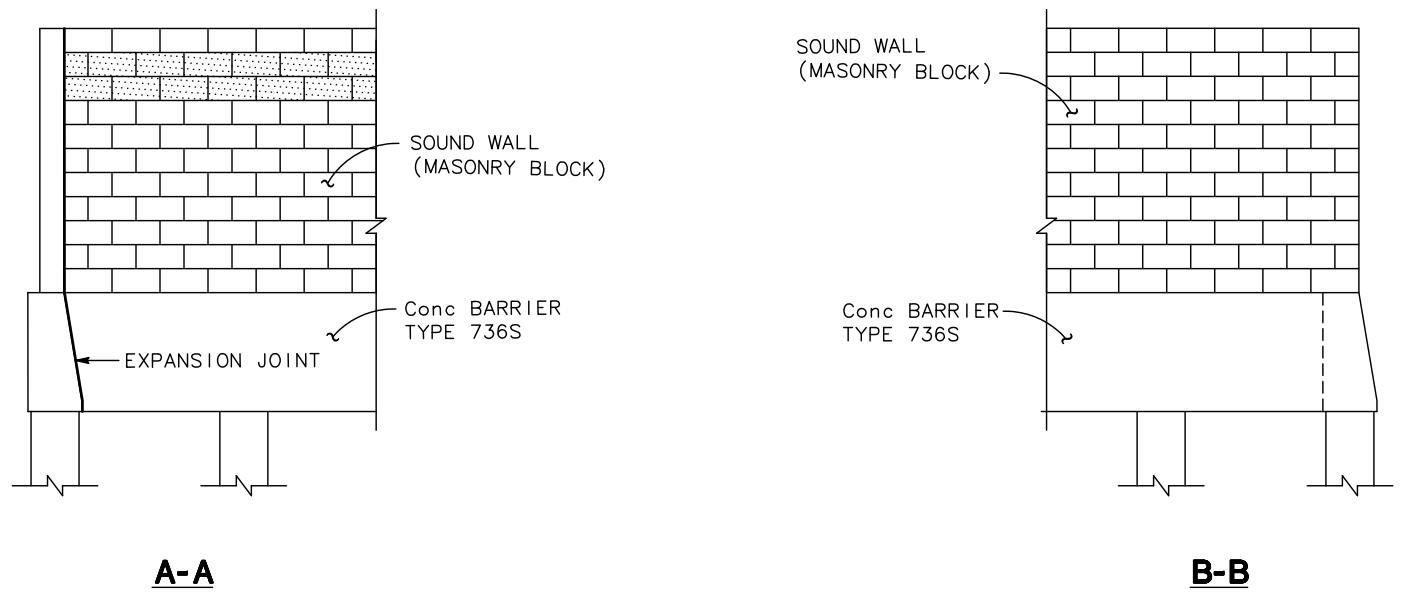
TYPICAL SECTION
 SW1 Sta 21+86.24 TO Sta 22+76.72
 NO SCALE



DETAIL 1
 NO SCALE

DETAIL 2
 NO SCALE

DETAIL 3
 NO SCALE



A-A

B-B




SOUND WALL DETAILS
 SCALE AS SHOWN

SW1-2

APPROVED FOR SOUND WALL WORK ONLY

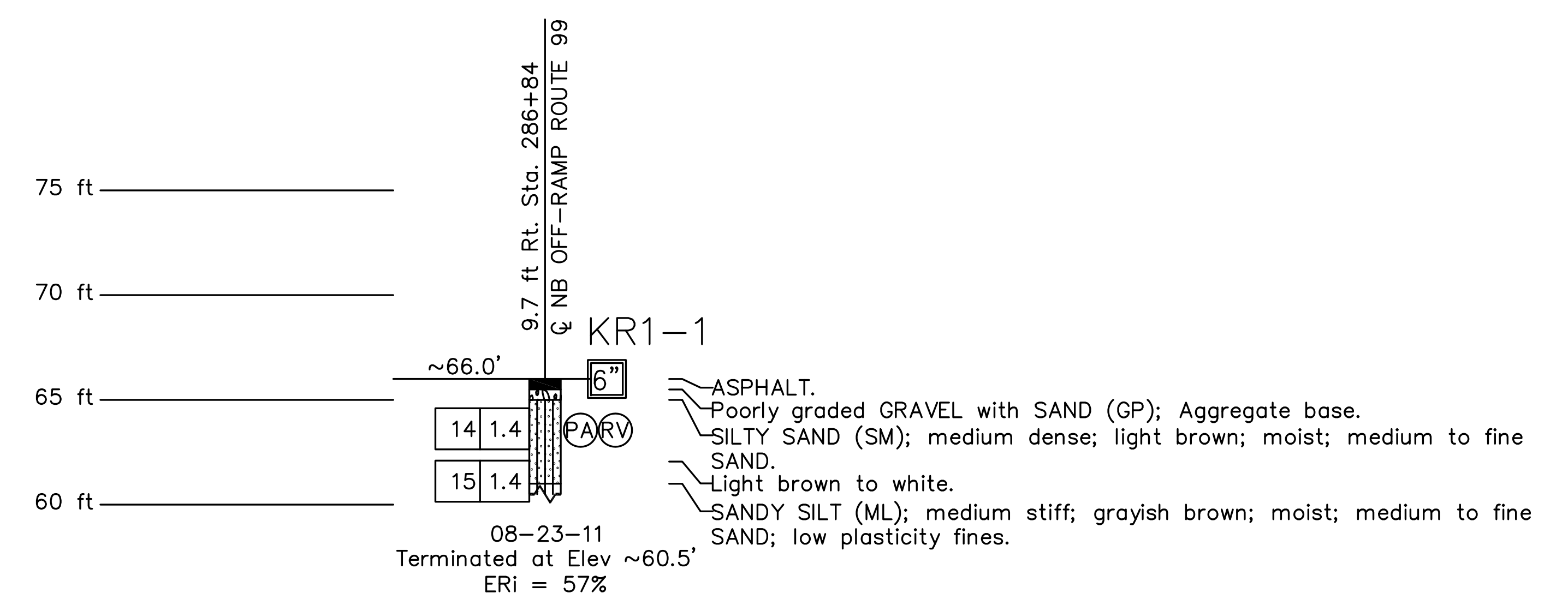
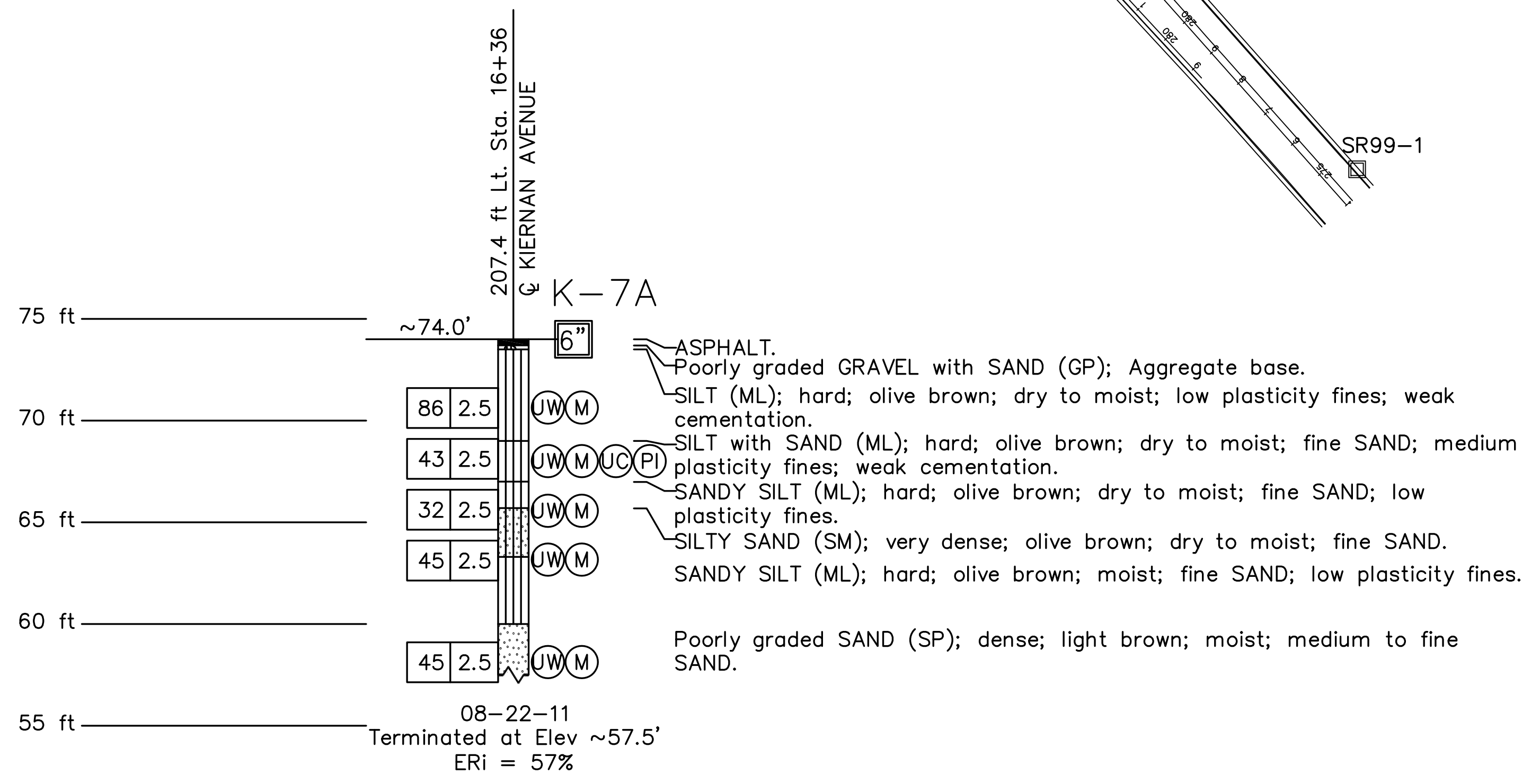
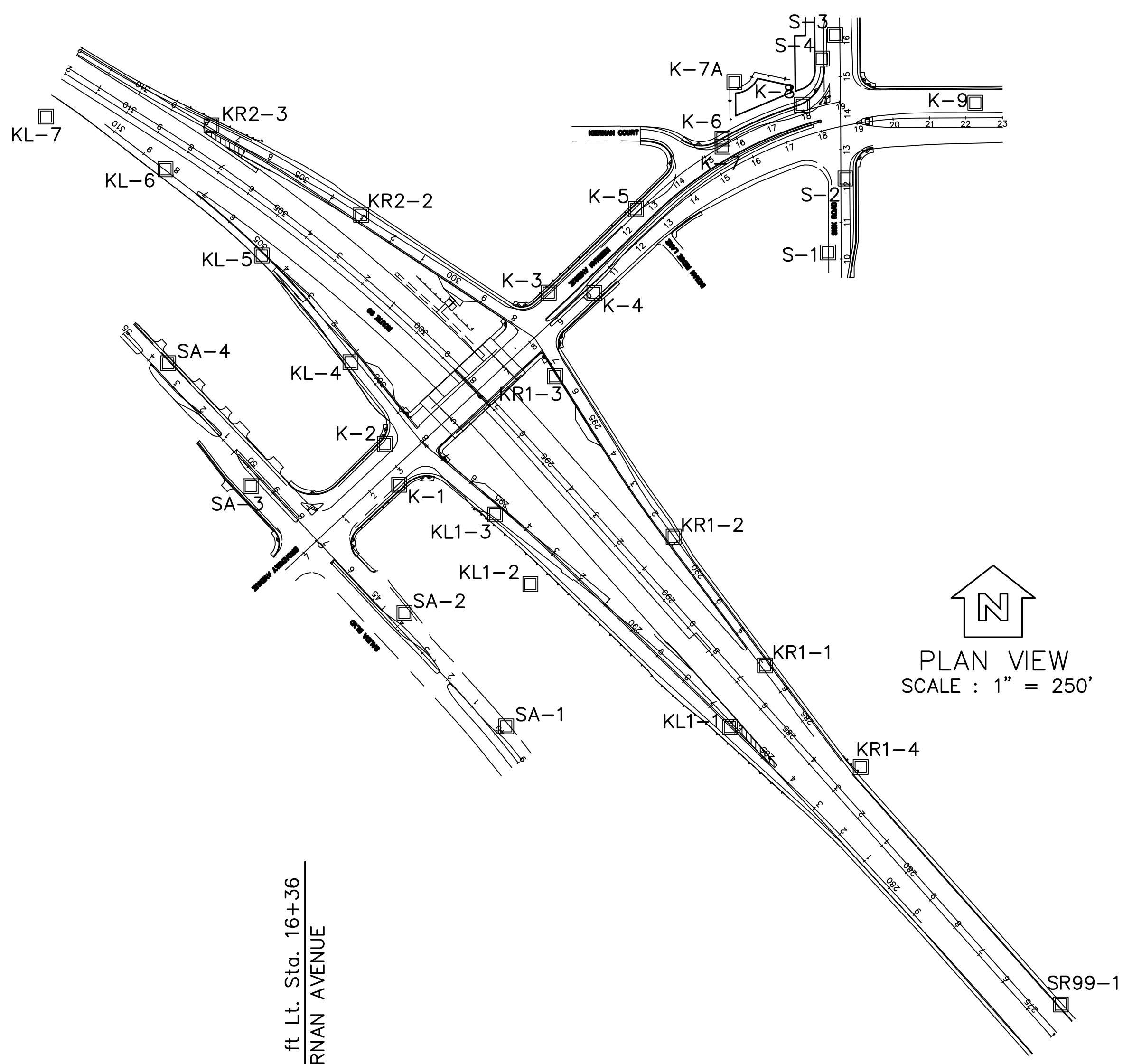
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 REVISIONS:
 1. REVISED BY: ANDY DO
 2. DATE REVIS: CHUONG NGUYEN
 3. CALCULATED-DESIGNED BY: ANDY DO
 4. CHECKED BY: CHUONG NGUYEN

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION 06-08-12

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99.219	22.0/23.0,0.0/0.5	253	423
 REGISTERED GEOTECHNICAL ENG. DATE 04/30/12					
9/04/12 PLANS APPROVAL DATE					
<i>The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.</i>					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVE, SUITE 100 SAN JOSE, CA 95126					
					

NOTES:

- 1) TEST BORINGS WHERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS. TEST BORINGS KL-7, S-4 AND KR1-4 WERE DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH A 6-INCH HOLLOW STEM AUGERS.
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PROFILE VIEW
VERT. SCALE : 1" = 5'

SOUND WALL No.1 SW1-3

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. VON DESSONNECK FIELD INVESTIGATION BY: DATE: 08/22/2011 THROUGH 03/02/2012	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163 POST MILES R22.56	ROUTE 99/KIERNAN INTERCHANGE LOG OF TEST BORINGS
SIGN OFF DATE	CHECKED BY S. RODACKER					

DATE PLOTTED => \$TIME USERNAME => \$USER

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
St. Gibbons
 CONSULTANT FUNCTIONAL SUPERVISOR
 BRAD COLE
 CALCULATED-DESIGNED BY
 CHECKED BY
 LINDA DERIVI
 DAVID HARRINGTON
 REVISOR BY
 DATE REVISED

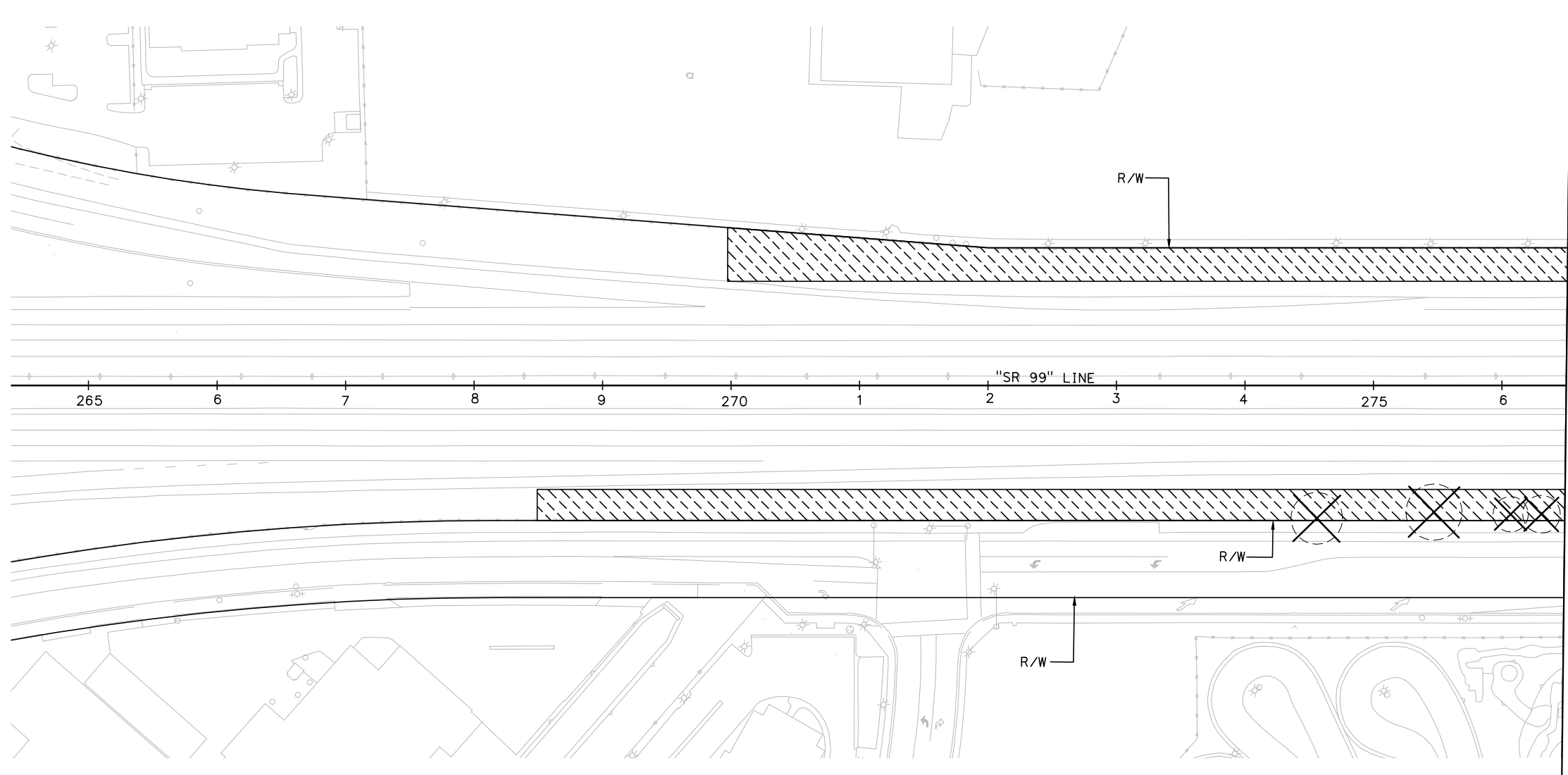
NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	254	423

David Harrington 05-03-12
 LICENSED LANDSCAPE ARCHITECT DATE
 PLANS APPROVAL DATE
 9/04/12
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



MATCH LINE SEE PR-2

APPROVED FOR PLANT REMOVAL WORK ONLY

PLANT REMOVAL PLAN

SCALE: 1" = 50'

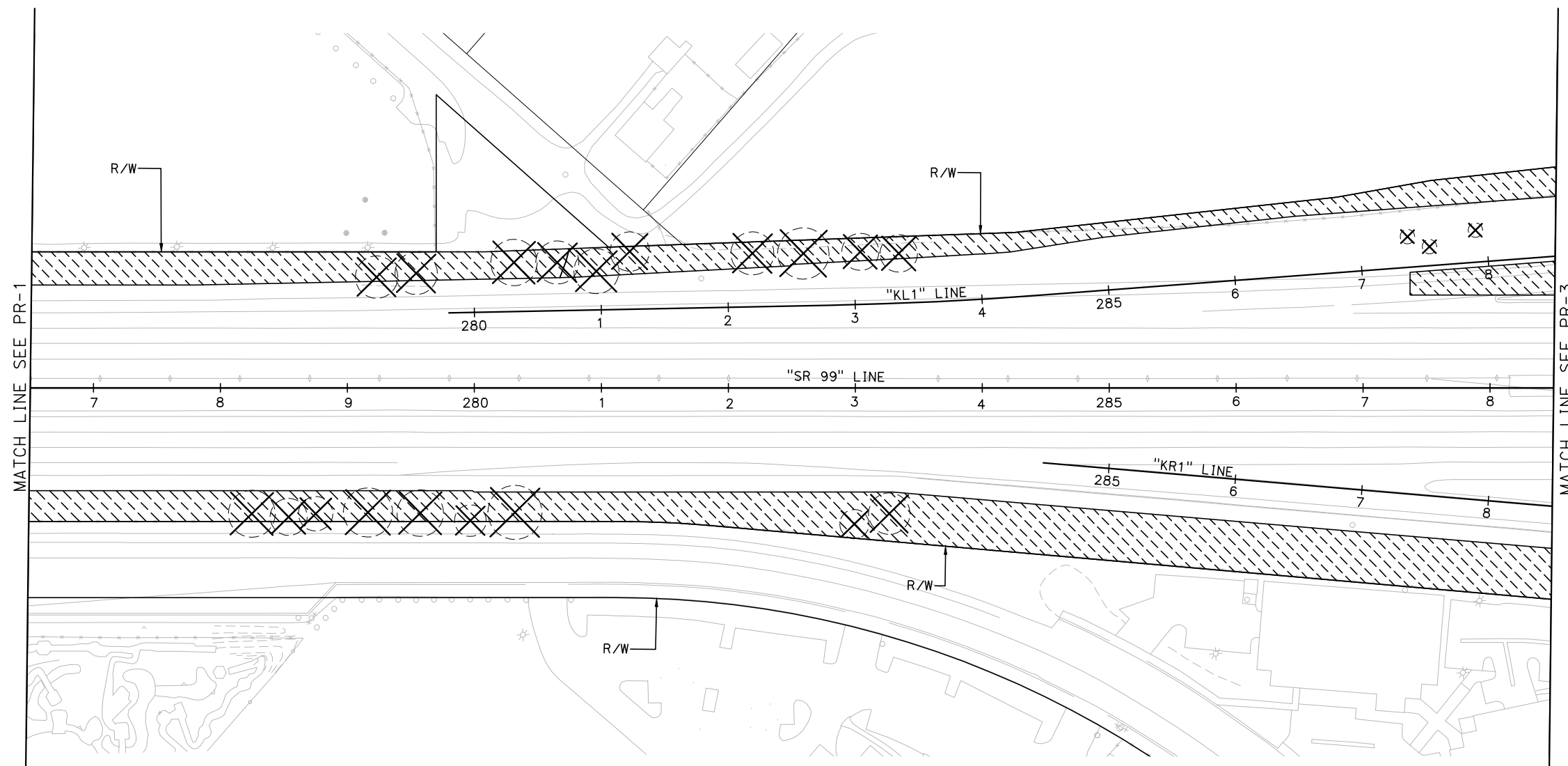
PR-1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
St. Gibbons
 CONSULTANT FUNCTIONAL SUPERVISOR
 BRAD COLE
 CALCULATED-DESIGNED BY
 CHECKED BY
 LINDA DERIVI
 DAVID HARRINGTON
 REVISOR BY
 DATE REVISED

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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	255	423

05-03-12
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 9/04/12
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 STOCKTON, CALIFORNIA 95203
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 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358




APPROVED FOR PLANT REMOVAL WORK ONLY

PLANT REMOVAL PLAN
 SCALE: 1" = 50'
PR-2

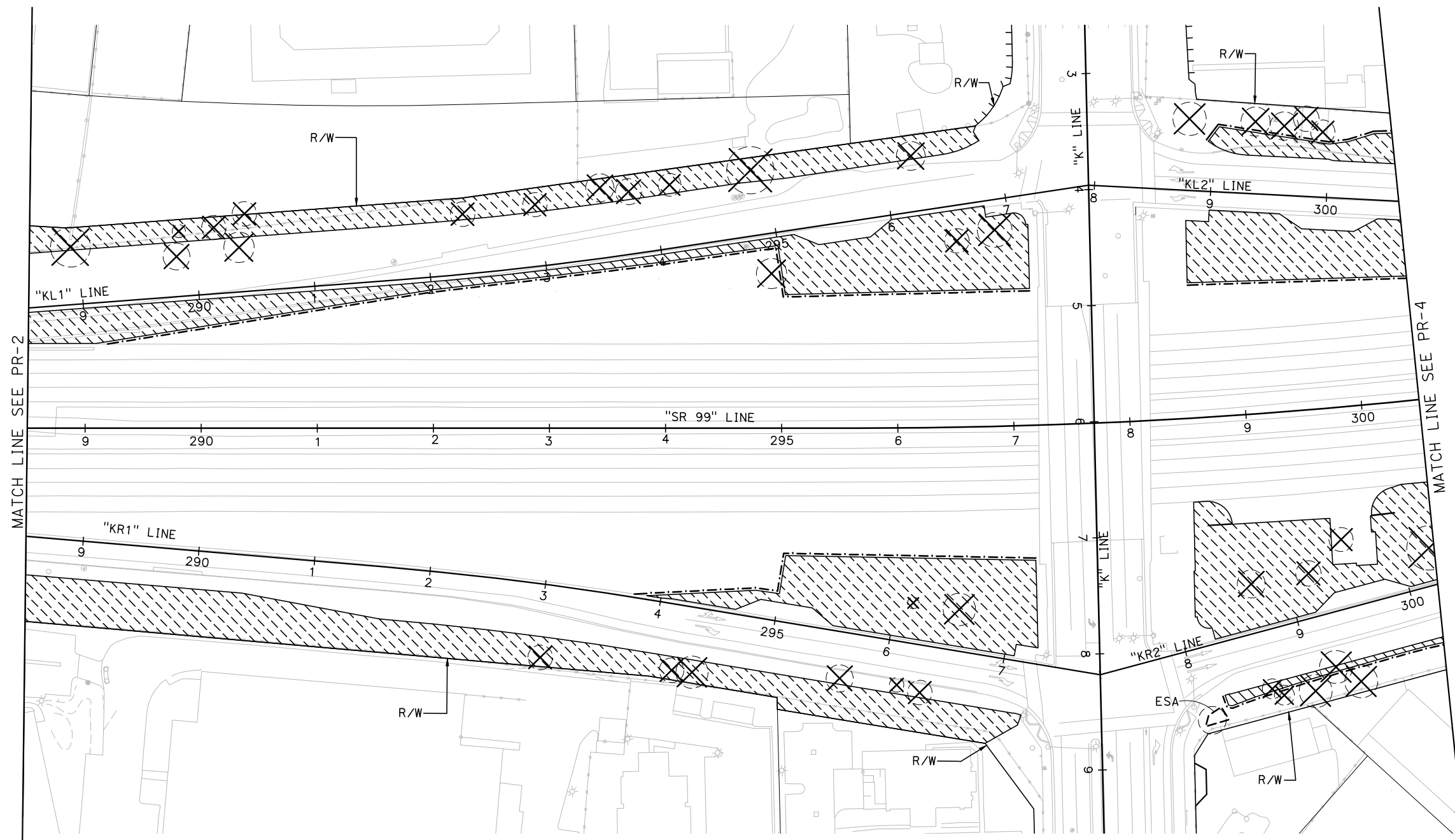
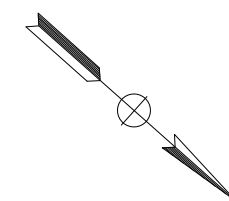
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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	256	423


 05-03-12
 LICENSED LANDSCAPE ARCHITECT ARCHITECT DATE
 9/04/12
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DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
---	--



APPROVED FOR PLANT REMOVAL WORK ONLY

PLANT REMOVAL PLAN
SCALE: 1" = 50'
PR-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

 CONSULTANT FUNCTIONAL SUPERVISOR
 BRAD COLE
 CHECKED BY
 LINDA DERIVI
 REVISOR
 DATE REVISOR

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME
LAST REVISION 05-14-12

NOTE:
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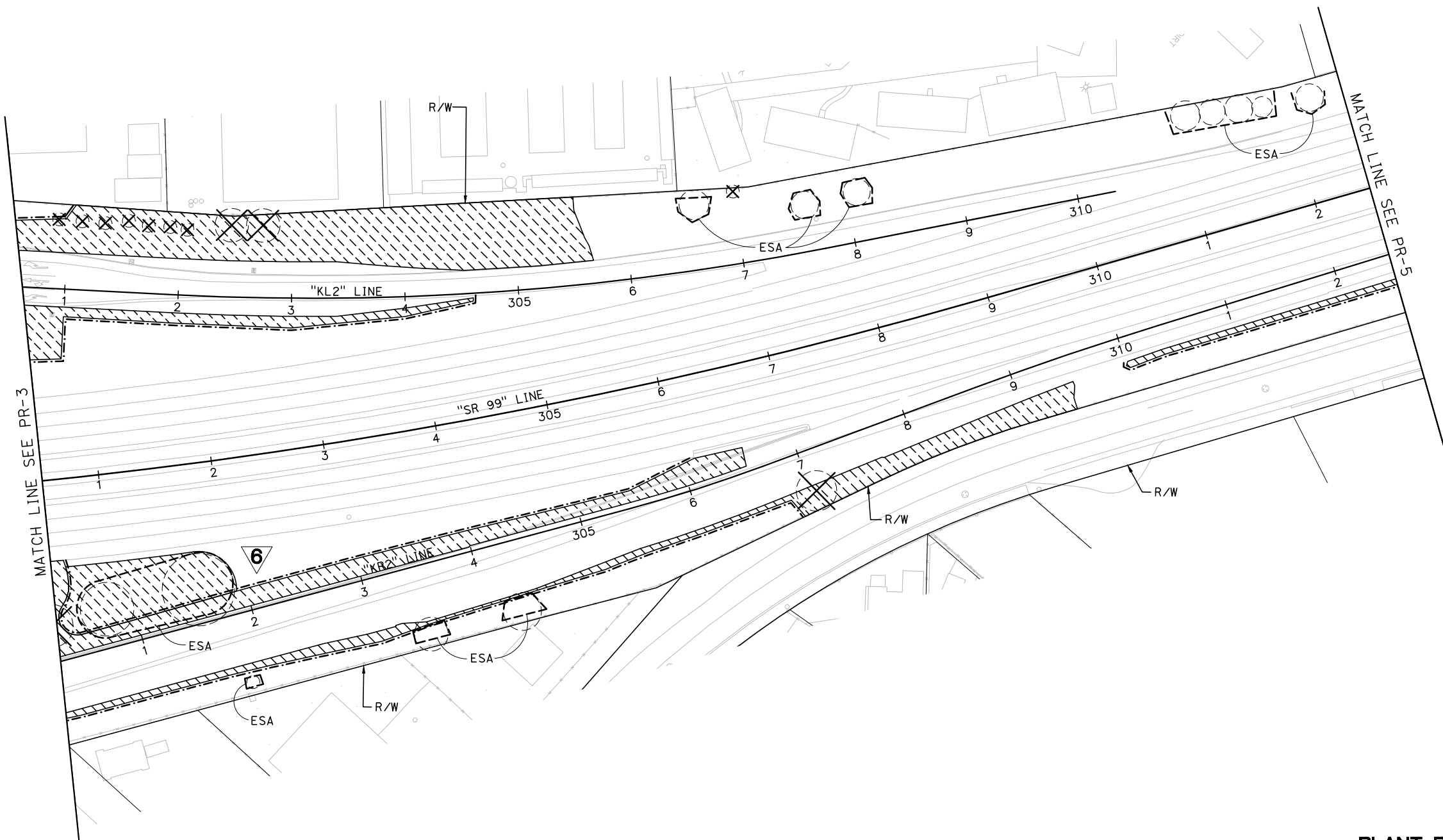
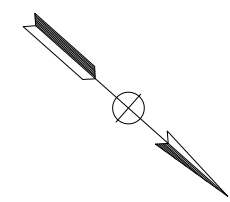
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5		

Signature: *David Harrington* 05-03-12
 LICENSED LANDSCAPE ARCHITECT DATE

PLANS APPROVAL DATE
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DERIVI CONSTRUCTION &
 ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

APPROVED FOR PLANT REMOVAL WORK ONLY

PLANT REMOVAL PLAN

SCALE: 1" = 50'

PR-4

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Giltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: BRAD COLE
 CHECKED BY: LINDA DERIVI
 DESIGNED BY: DAVID HARRINGTON
 REVISIONS: SB 10/24/12

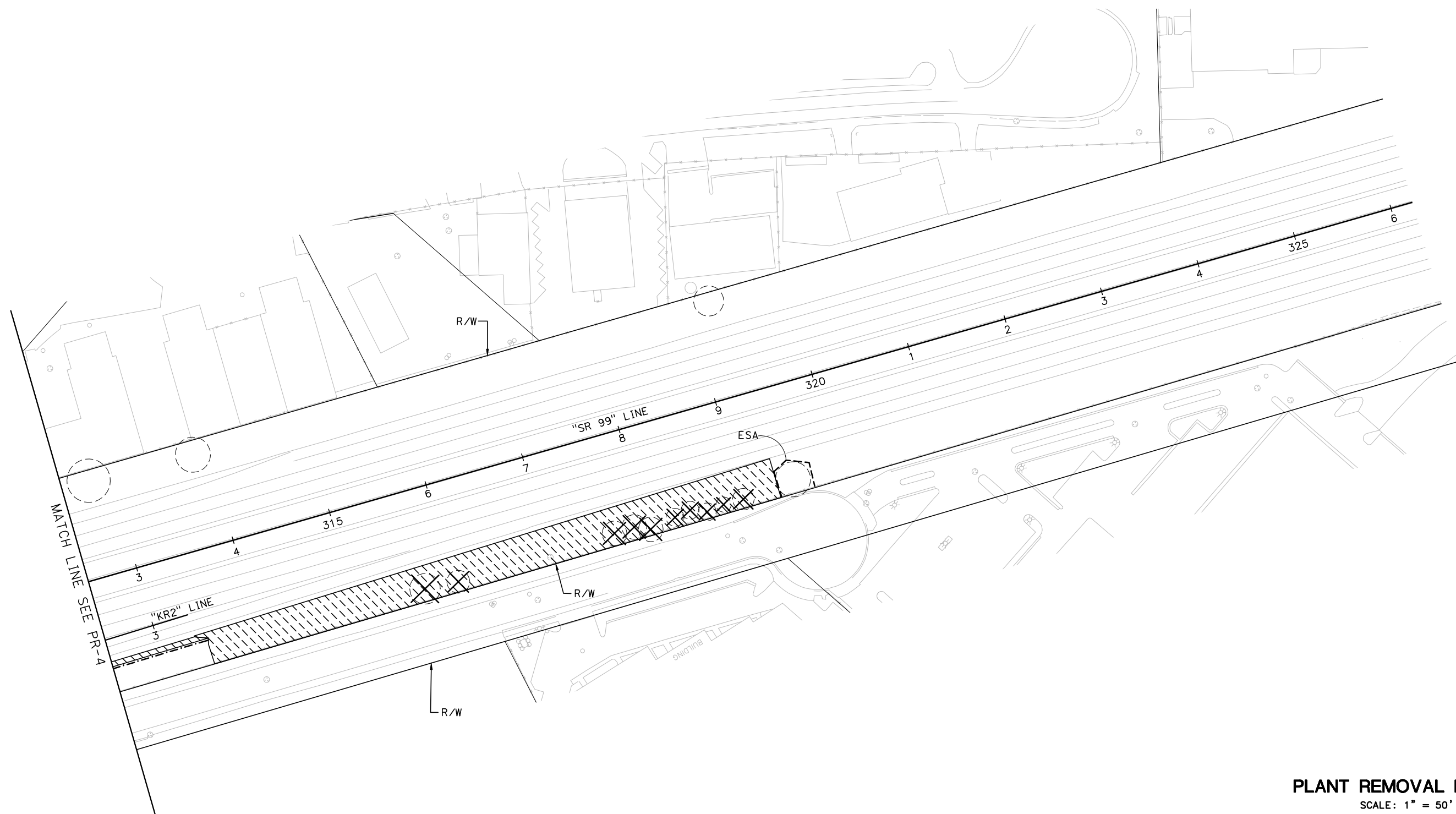
LAST REVISION | DATE PLOTTED => \$DATE
 05-14-12 | TIME PLOTTED => \$TIME

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	258	423

David Harrington 05-03-12
 LICENSED LANDSCAPE ARCHITECT ARCHITECT DATE
 PLANS APPROVAL DATE
 9/04/12
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DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
---	--




APPROVED FOR PLANT REMOVAL WORK ONLY

PLANT REMOVAL PLAN
SCALE: 1" = 50'
PR-5

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED- DESIGNED BY	REVISOR
St. Gibbons	BRAD COLE	CHECKED BY	DATE REVISED
		DAVID HARRINGTON	
		LINDA DERIVI	

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	259	423

 05-03-12
 LICENSED LANDSCAPE ARCHITECT DATE

PLANS APPROVAL DATE _____
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DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--





ABBREVIATIONS:

AMEND.....AMENDMENT	NTS.....NOT TO SCALE
B&B.....BALLED AND BURLAPPED	OZ.....OUNCE
CF.....CUBIC FOOT/FEET	PKT.....PACKET
CY.....CUBIC YARD	PLT ESTB..PLANT ESTABLISHMENT
DIA.....DIAMETER	PSFM.....POLYMER STABILIZED FIBER MATRIX
EA.....EACH	PVMT.....PAVEMENT
FT.....FOOT/FEET	R/W.....RIGHT OF WAY
FT ²SQUARE FOOT	SF.....STATE FURNISHED
FT ³CUBIC FOOT	SQFT.....SQUARE FOOT/FEET
IN.....INCH	SQYD.....SQUARE YARD
LB.....POUND	TAB.....TABLET(S)
MAX.....MAXIMUM	TRVD.....TRAVELED
MIN.....MINIMUM	YD.....YARD
N.C.N.NO COMMON NAME	YD ²SQUARE YARD
NO.....NUMBER	YD ³CUBIC YARD

NOTE:

1. Underlined portions of botanical name indicate abbreviations used on Planting Plans.



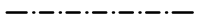





PLANT LIST AND PLANTING SPECIFICATIONS

PLANT GROUP	PLANT No.	SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITY EACH	HOLE SIZE		BASIN TYPE	IRON SULFATE	SOIL AMEND (CF)	COMMERCIAL FERTILIZER ①		BASIN MULCH (CF)	STAKING	PLANTING LIMITS					REMARKS		
							DIA	DEPTH				PLANTING	PLT ESTB			MINIMUM DISTANCE (ft) FROM							
																TRVD WAY	PVMT	FENCE	WALL	PAVED DITCH		EARTH DITCH	ON CENTER (ft)
H	1		CARPOBROTUS EDULUS	ICE PLANT	<u>6</u>	CUTTING	157,726	⑧	-	-	⑦	⑦⑨	⑦⑨	-	-	-	6	6	6	6	8	1.5	GROUND COVER
U	2		GEIJERA PARVIFLORA	AUSTRALIAN WILLOW	No. 15	23	②	11	-	1 CF	9 TABS	.5 LB	.5 CF	⑥	30	-	10	10	10	12	-	TREE	
	3		PISTACIA CHINENSIS "KEITH DAVIES"	CHINESE PISTACHE	No. 15	10	②	11	-	1 CF	9 TABS	.5 LB	.5 CF	⑥	30	-	20	20	20	22	-	TREE	
	4		QUERCUS LOBATA	VALLEY OAK	No. 15	7	②	11	-	1 CF	9 TABS	.5 LB	.5 CF	⑥	40	-	20	20	20	22	-	TREE	

APPLICABLE WHEN CIRCLED:

- ① Quantities shown are "per plant" unless shown as CF application rates.
- ② Sufficient to receive root ball. Equal amounts of soil amendment and import top soil.
3. Does not apply to mulch areas.
- ④ As shown on plans.
5. Unless otherwise shown on plans.
- ⑥ See standard details.
- ⑦ See Special Provisions.
- ⑧ See Standard Specifications.
- ⑨ Per manufacturer's recommended rate.

LEGEND:

-  EXISTING TREE TO REMAIN
-  (E) TREE (TOTAL OF 85) TO BE REMOVED AS PART OF CLEARING AND GRUBBING WORK.
-  ROADSIDE CLEARING (3,943 LF)
-  (E) GROUND COVER (ICE PLANT) TO BE REMOVED AS PART OF CLEARING AND GRUBBING WORK. 6
-  MAINTAIN (E) PLANTED AREAS (123,326.3 SQFT)
-  3' TRENCH (E) GROUND COVER TO BE REMOVED DURING CLEARING & GRUBBING (17,630.0 SQFT)
-  EDGE OF TRAVELED WAY
-  TEMPORARY FENCE (TYPE ESA) TREE PROTECTION (1160 LF) PROTECT WITHIN ENCLOSURE TREE DRIPLINE SEE STD. DETAIL T65 SEE STD. SPEC. 80-1.07, 80-1.10

6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

PLANTING LIST
SCALE: NONE

PL-1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 CHECKED BY
 DESIGNED BY
 CALCULATED-
 CHECKED BY
 REVISIONS:

BRAD COLE

DAVID HARRINGTON
LINDA DERIVI

REVISOR BY
DATE REVISED

SB
10/24/12

DATE

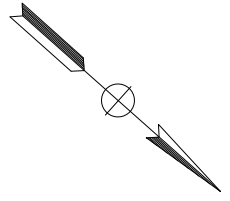
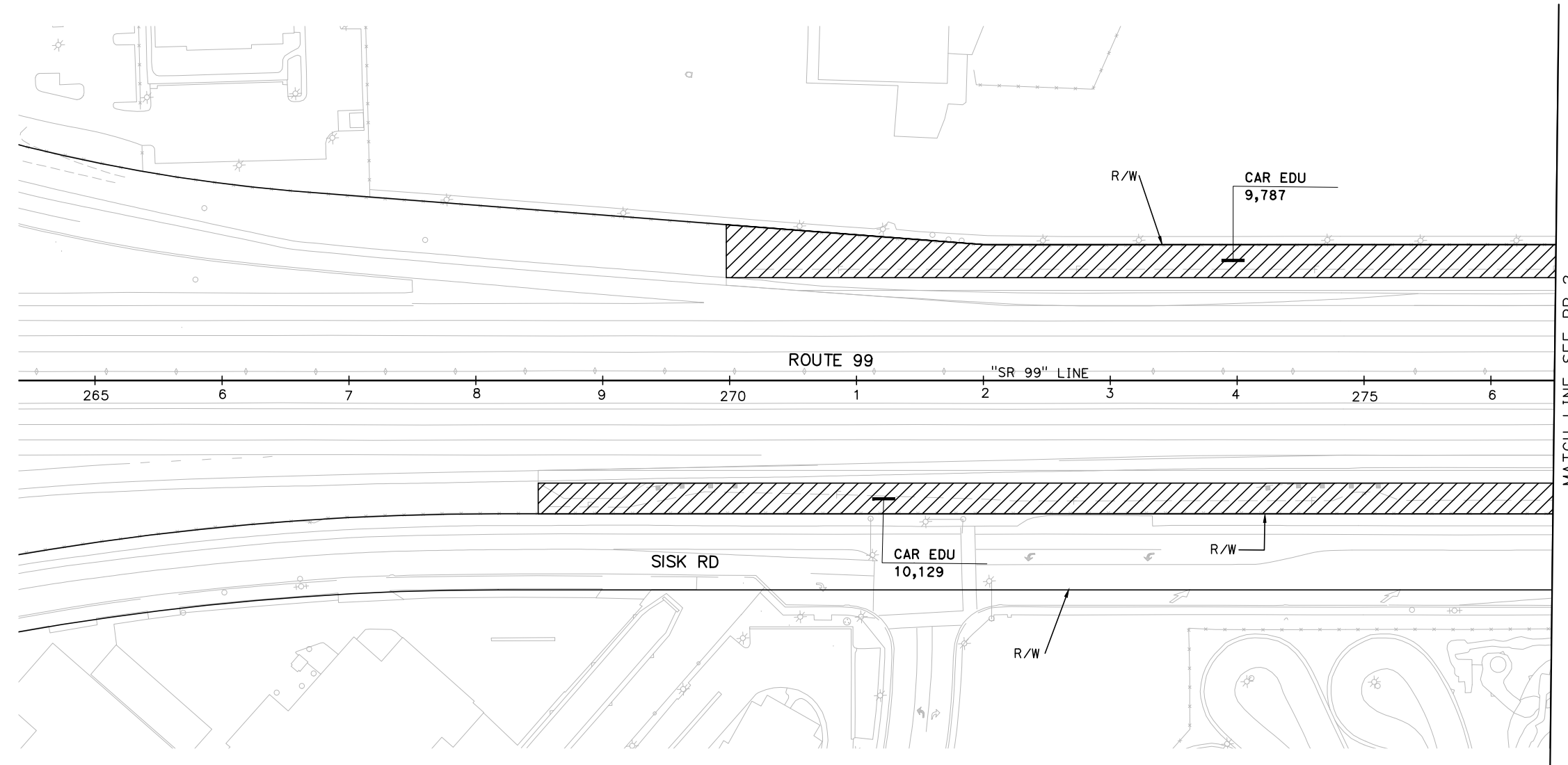
Caltrans

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Gilbert
 CONSULTANT FUNCTIONAL SUPERVISOR
 BRAD COLE
 CALCULATED-DESIGNED BY
 CHECKED BY
 DAVID HARRINGTON
 LINDA DERIVI
 REVISED BY
 DATE REVISED

NOTE:
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	260	423

05-03-12
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 9/04/12
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 DERIVI CONSTRUCTION & ARCHITECTURE, INC.
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 STOCKTON, CALIFORNIA 95203
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



APPROVED FOR PLANTING WORK ONLY

PLANTING PLAN
 SCALE: 1" = 50'
PP-1

NOTE:
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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
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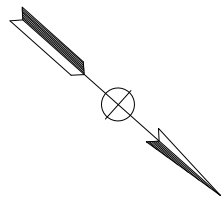
David Harrington 05-03-12
LICENSED LANDSCAPE ARCHITECT ARCHITECT DATE

9/04/12
PLANS APPROVAL DATE

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STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CALIFORNIA 95358



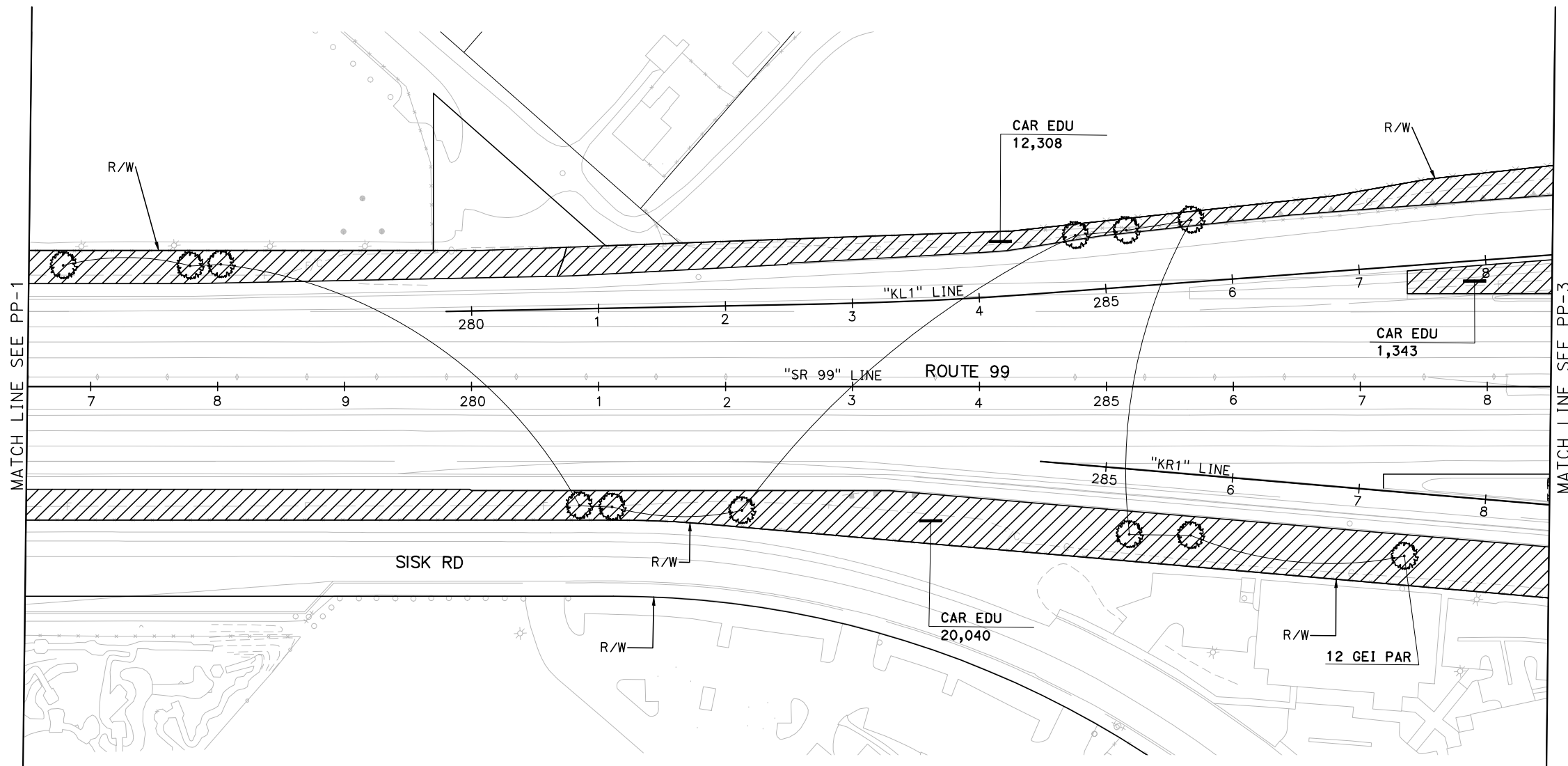
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
St. Gibbons

CONSULTANT FUNCTIONAL SUPERVISOR
BRAD COLE

CALCULATED-
DESIGNED BY
CHECKED BY

DAVID HARRINGTON
LINDA DERIVI

REVISED BY
DATE REVISED



APPROVED FOR PLANTING WORK ONLY

PLANTING PLAN

SCALE: 1" = 50'

PP-2

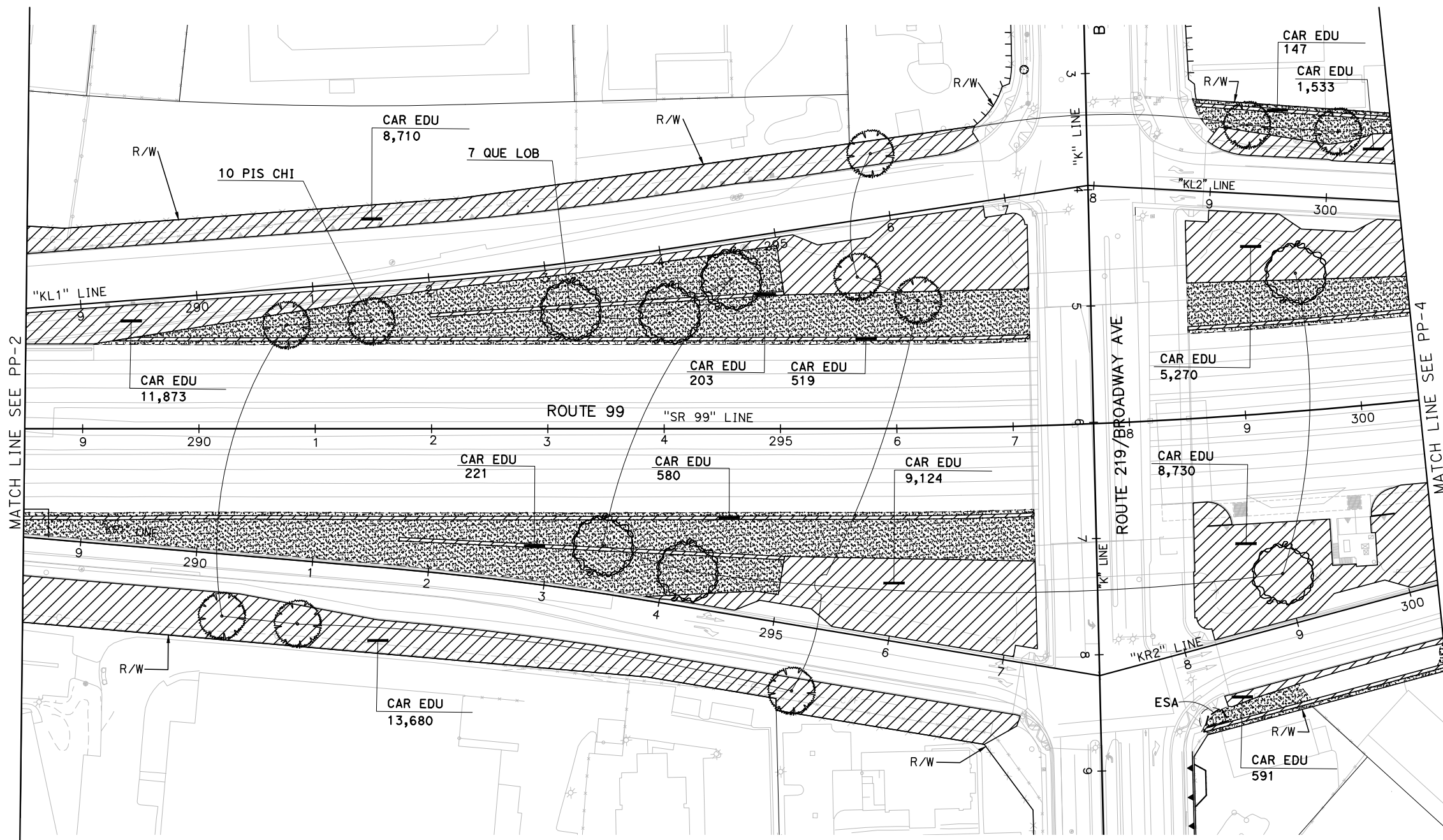
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 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



APPROVED FOR PLANTING WORK ONLY

PLANTING PLAN

SCALE: 1" = 50'

PP-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
St. Gibbons
 CONSULTANT FUNCTIONAL SUPERVISOR
 BRAD COLE
 CALCULATED BY
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 DAVID HARRINGTON
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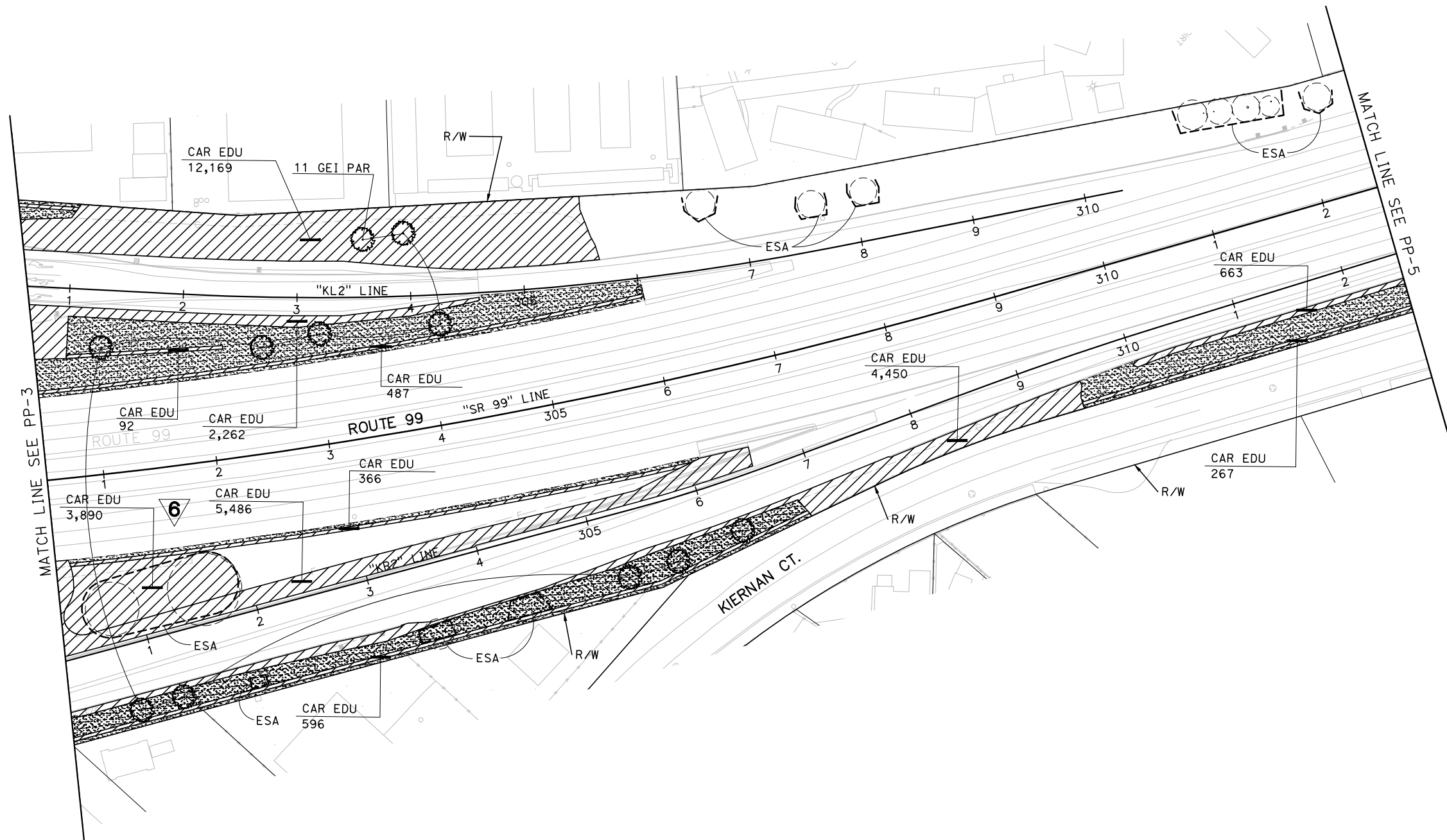
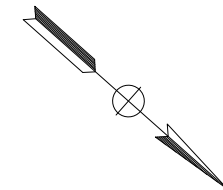
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Signature: *David Harrington* 05-03-12
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6 REVISED PER ADDENDUM No. 6 DATED OCTOBER 24, 2012

APPROVED FOR PLANTING WORK ONLY

PLANTING PLAN
SCALE: 1" = 50'

PP-4

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

Caltrans

CONSULTANT FUNCTIONAL SUPERVISOR
BRAD COLE

CALCULATED/DESIGNED BY
CHECKED BY

DAVID HARRINGTON
LINDA DERIVI

REVISOR
DATE REVISED

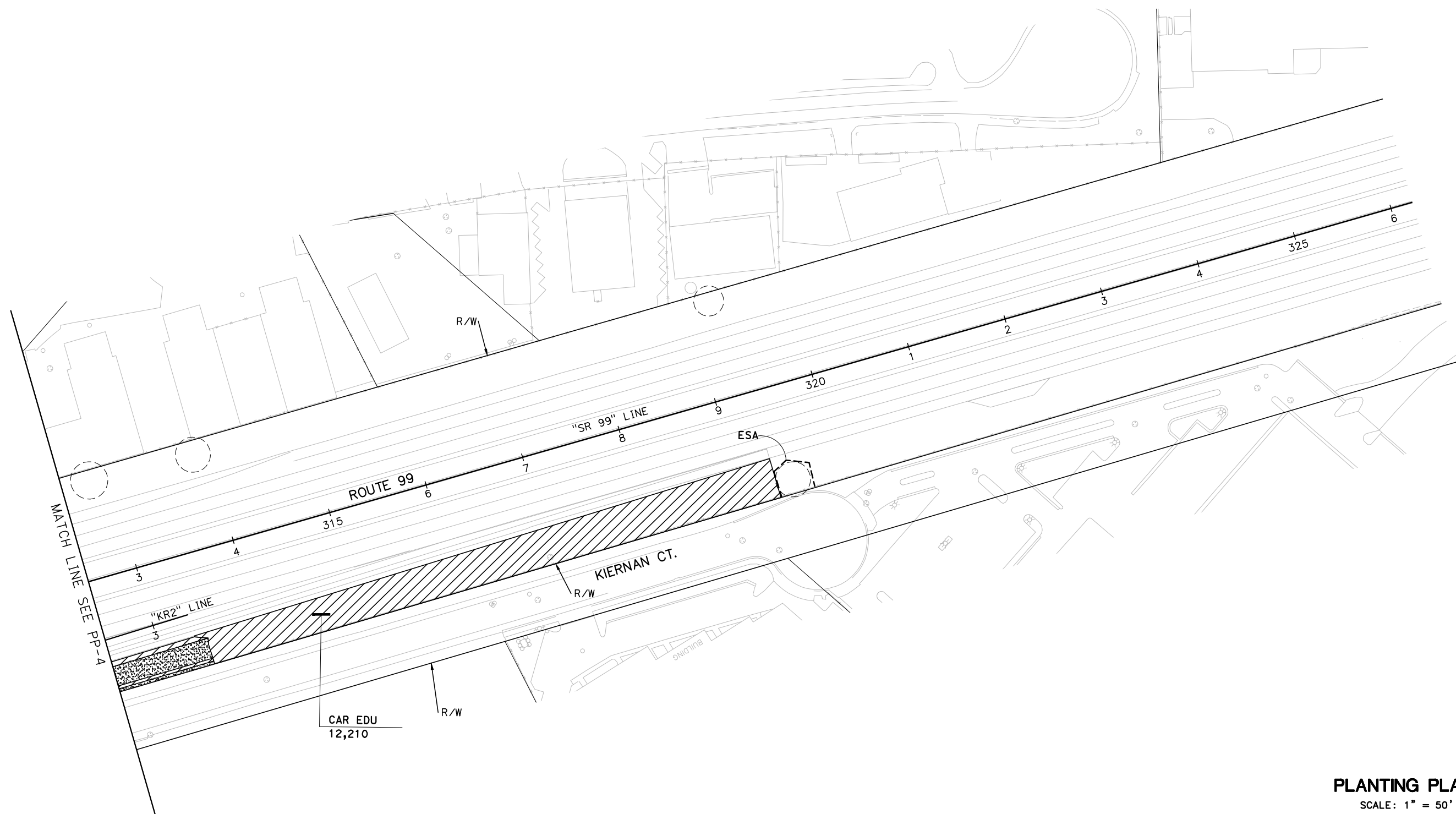
SB
10/24/12

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David Harrington 05-03-12
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DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--



APPROVED FOR PLANTING WORK ONLY

PLANTING PLAN

SCALE: 1" = 50'

PP-5



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
St. Gibbons	BRAD COLE	CHECKED BY	DATE REVISED
		DAVID HARRINGTON	
		LINDA DERIVI	

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 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

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GENERAL NOTES

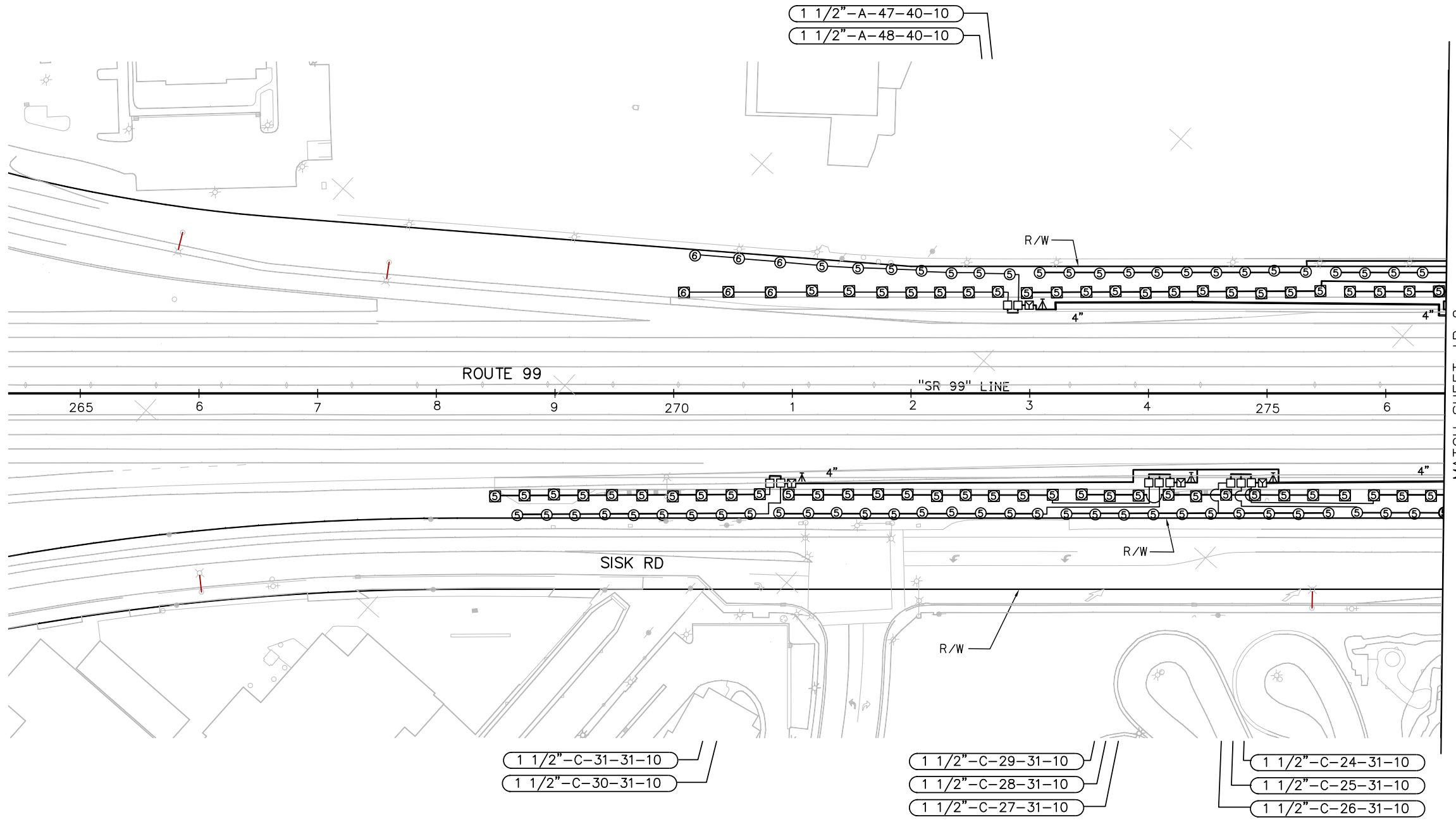
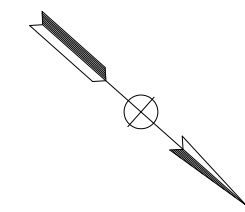
- FOR CLARITY SOME IRRIGATION FACILITIES ARE SHOWN IN THE ROADWAY. LOCATE FACILITIES IN LANDSCAPED AREAS.
- WHERE THERE ARE MVP'S IN THE QUADRANTS, REMOTE CONTROL VALVES ARE INTENDED TO BE GROUPED AND LOCATED AT THE MVPS.
- WHERE VALVES ARE SHOWN ALONG THE ROADWAY AND RAMP WHERE THERE ARE NO MVP'S, GROUP AND LOCATE THE VALVES BEHIND THE WALLS OR AGAINST THE RIGHT OF WAY FENCE.
- EXISTING IRRIGATION SYSTEM SHALL BE ABANDONED. DESTROY WATER WELL AND REMOVE EXISTING STAND PIPE.
- ALL GATE VALVES ARE LINE SIZE
- ALL WYE STRAINERS SHALL BE 2"
- ALL BALL VALVES SHALL BE 2"
- SYSTEM IS DESIGNED TO OPERATE TO OPERATE CONTROLLER "A" WITH A MAXIMUM OF 150 GPM. WHEN CONTROLLER "A" HAS COMPLETED ITS OPERATION, CONTROLLER "B" AND "C" SHALL OPERATE SIMULTANEOUSLY WITH MAXIMUM OF 150 GPM.
- A PHONE LINE SHALL BE RUN FROM THE RELOCATED UTILITY BOX TO THE LR-HUB.

LATERAL PIPE SIZING CHART FOR ROTOR HEADS

GALLONS/MINUTE	PIPE SIZE(IN)
1 - 10	1 INCH
11 - 23	1 1/4 INCH
24 - 31	1 1/2 INCH
32 - 55	2 INCH
55-UP	2 1/2 INCH

LATERAL PIPE SIZING CHART FOR BUBBLERS

GALLONS/MINUTE	PIPE SIZE(IN)
1 - 3	3/4 INCH
4 - 5	1 INCH
6 - 26	1 1/4 INCH
27 - 34	1 1/2 INCH
35 - up	2 INCH



REVISIONS:
 REVISOR: DAVID HARRINGTON
 DATE: 05-03-12
 CHECKED BY: LINDA DERIVI
 CALCULATED/DESIGNED BY: BRAD COLE
 CONSULTANT SUPERVISOR: BRAD COLE
 DEPARTMENT OF TRANSPORTATION: CALTRANS

APPROVED FOR IRRIGATION WORK ONLY

IRRIGATION PLAN
 SCALE 1"=50'

IP-1

LAST REVISION | DATE PLOTTED => \$DATE | 05-01-12 | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	266	423

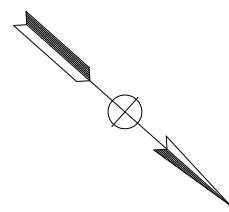
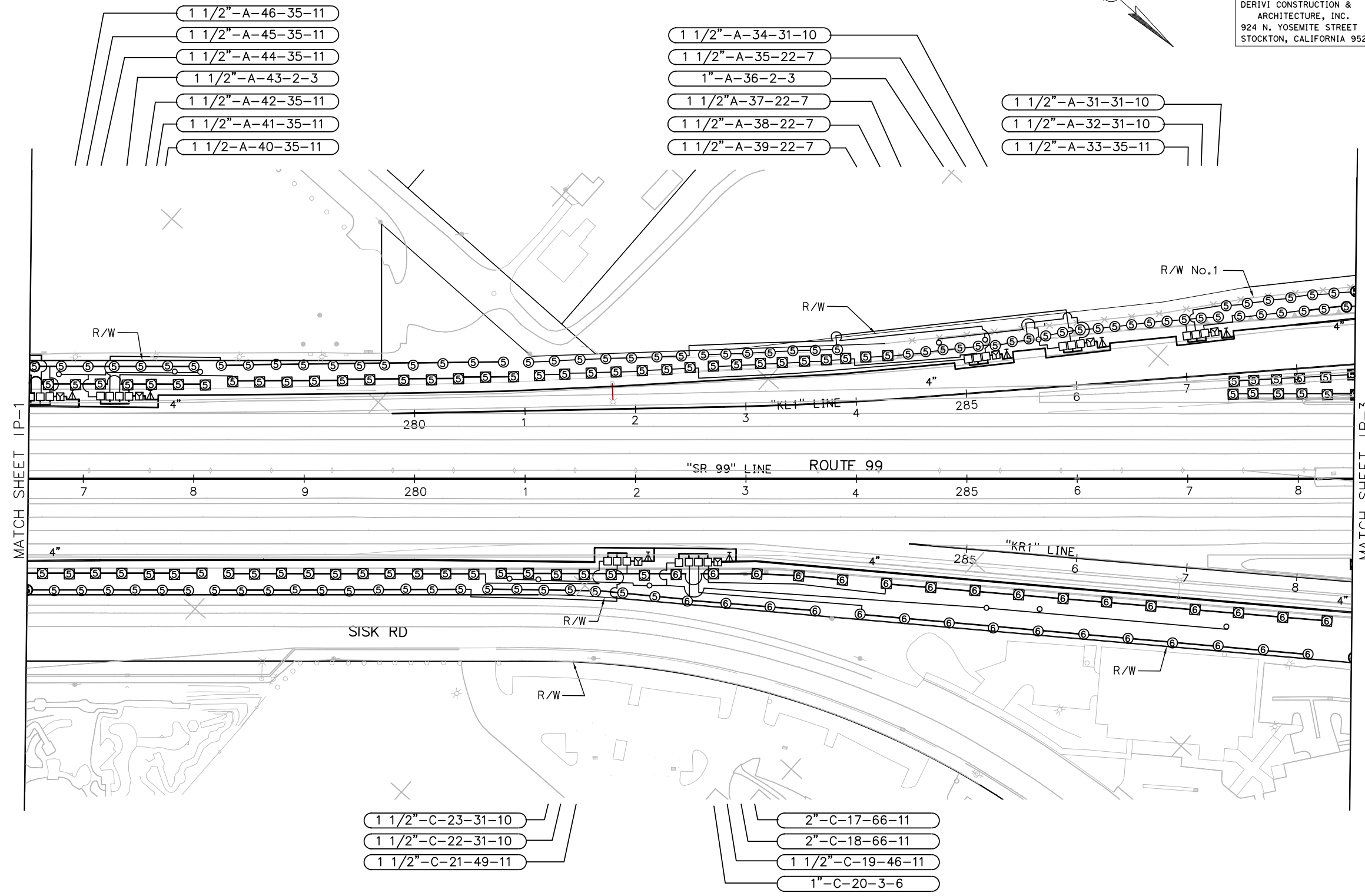
05-03-12
 LICENSED LANDSCAPE ARCHITECT DATE
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS
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DERIVI CONSTRUCTION &
 ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	REVISOR	DATE
	DAVID HARRINGTON	
	LINDA DERIVI	
CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	
BRAD COLE		




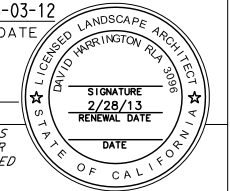
APPROVED FOR IRRIGATION WORK ONLY

IRRIGATION PLAN
SCALE 1" = 50'

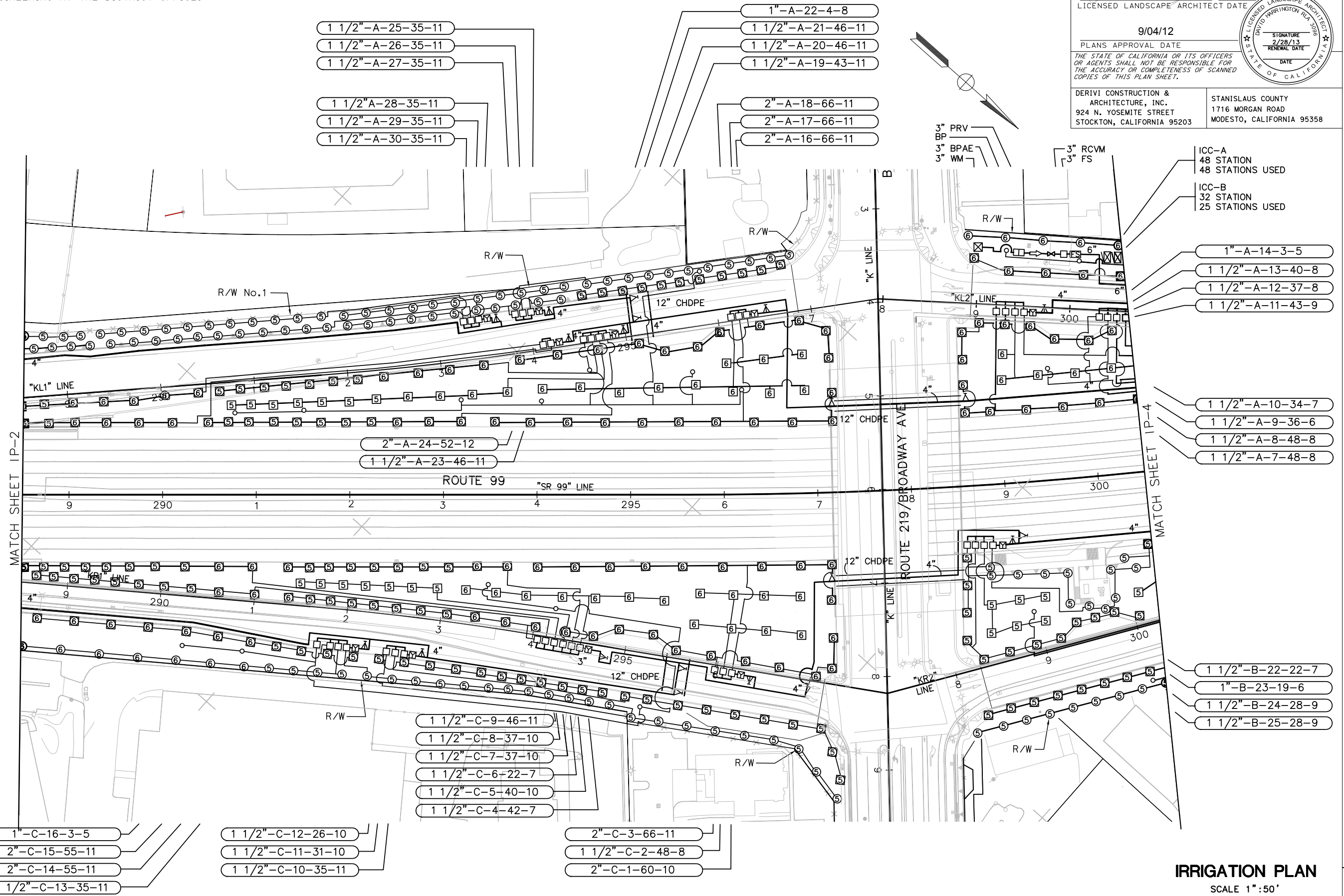
IP-2


LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	267	423

 05-03-12 LICENSED LANDSCAPE ARCHITECT DATE	
9/04/12 PLANS APPROVAL DATE	
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DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
	BRAD COLE	CHECKED BY	DAVID HARRINGTON
			LINDA DERIVI
			DATE REVISOR

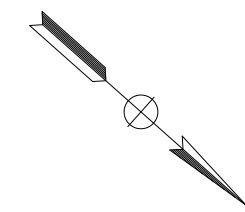
APPROVED FOR IRRIGATION WORK ONLY

IRRIGATION PLAN
SCALE 1"=50'
IP-3

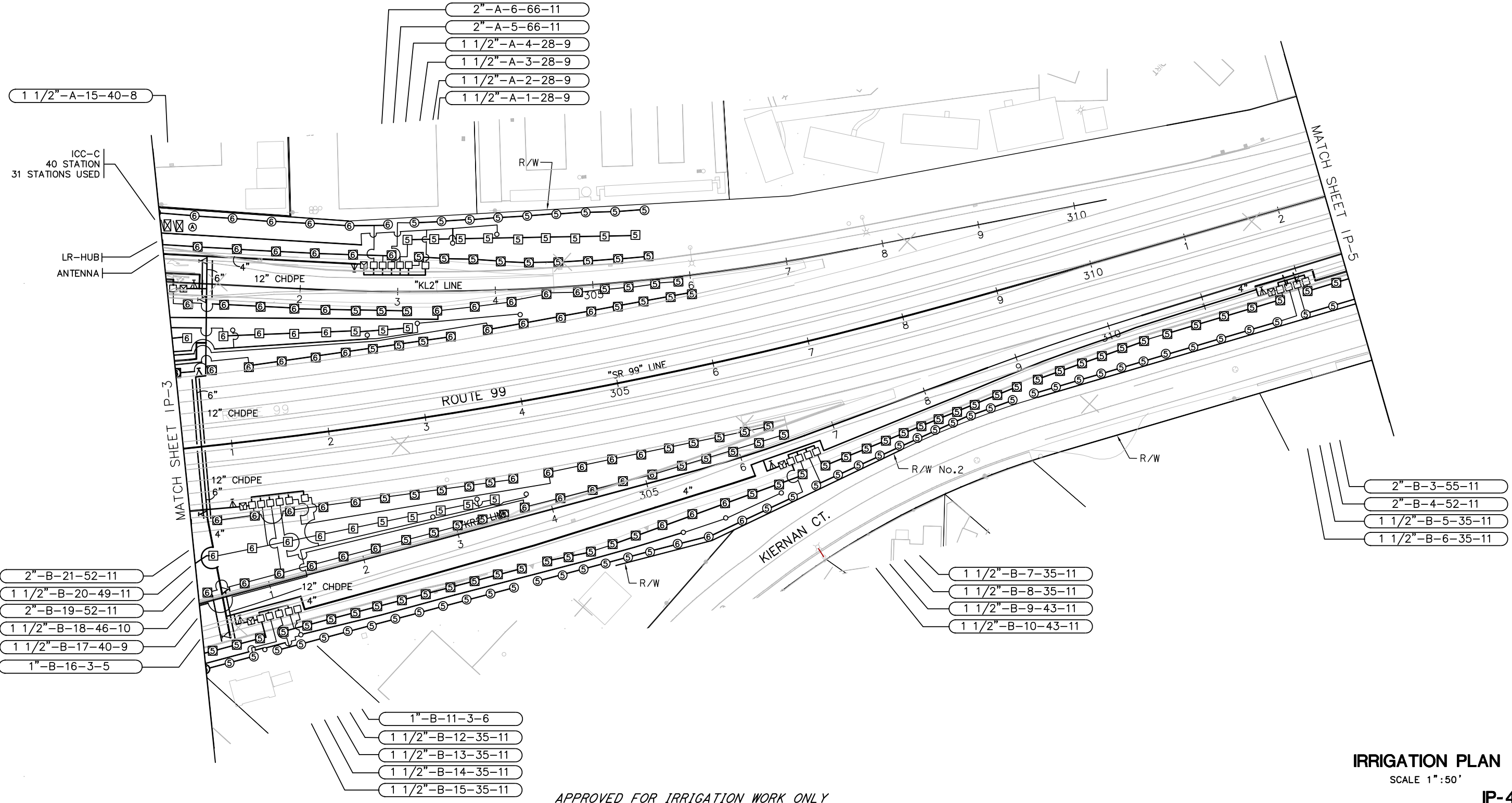
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	268	423

05-03-12
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 9/04/12
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 DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
BRAD COLE	BRAD COLE	DAVID HARRINGTON	DAVID HARRINGTON
	CHECKED BY	DATE	REVISOR
	LINDA DERIVI		LINDA DERIVI



- 2"-B-21-52-11
- 1 1/2"-B-20-49-11
- 2"-B-19-52-11
- 1 1/2"-B-18-46-10
- 1 1/2"-B-17-40-9
- 1"-B-16-3-5

- 1"-B-11-3-6
- 1 1/2"-B-12-35-11
- 1 1/2"-B-13-35-11
- 1 1/2"-B-14-35-11
- 1 1/2"-B-15-35-11


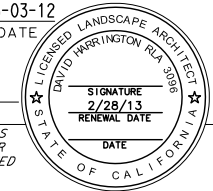
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- 1 1/2"-B-8-35-11
- 1 1/2"-B-9-43-11
- 1 1/2"-B-10-43-11

- 2"-B-3-55-11
- 2"-B-4-52-11
- 1 1/2"-B-5-35-11
- 1 1/2"-B-6-35-11

APPROVED FOR IRRIGATION WORK ONLY

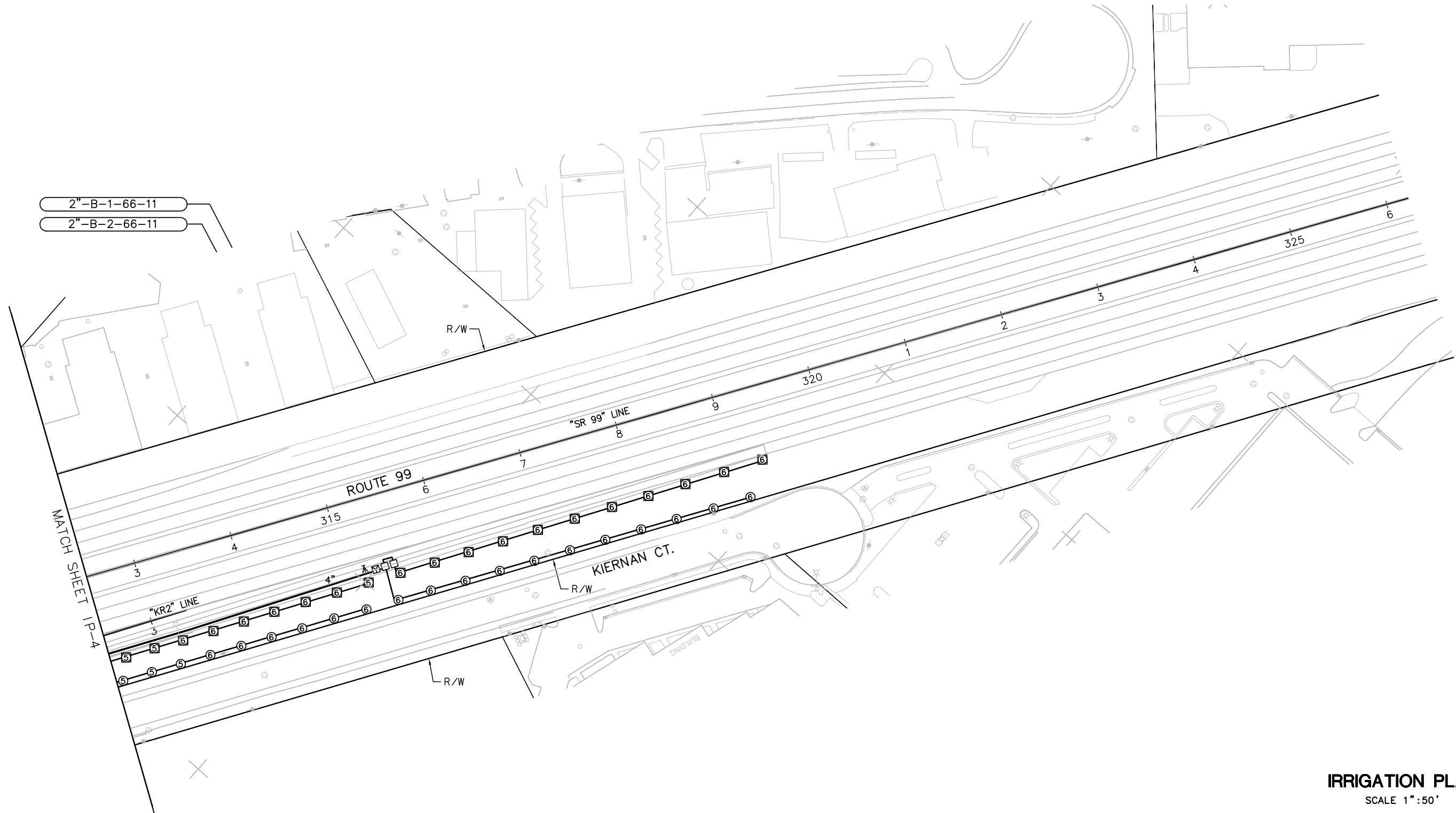
IRRIGATION PLAN
SCALE 1"=50'
IP-4

LAST REVISION DATE PLOTTED => \$DATE 05-01-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	269	423
 LICENSED LANDSCAPE ARCHITECT DATE 05-03-12					
PLANS APPROVAL DATE 9/04/12			<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
BRAD COLE	BRAD COLE	DAVID HARRINGTON	DAVID HARRINGTON
CH	CH	LINDA DERIVI	LINDA DERIVI



APPROVED FOR IRRIGATION WORK ONLY

IRRIGATION PLAN
SCALE 1" = 50'

IP-5

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	272	423

05-03-12
 LICENSED LANDSCAPE ARCHITECT DATE

9/04/12
 PLANS APPROVAL DATE

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DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

SPRINKLER SCHEDULE

SYMBOL	TYPE	DESCRIPTION	SPRAY PATTERN	OPERATING PRESSURE (PSI)	PRESSURE COMPENSATING	UP TO MINUS 30% OF RADIUS					MATERIAL	INLET CONNECTION (NPT INCH)	ADJ ARC	BACKSPASH PREVENTER	DIFFUSER PIN	DISTANCE CONTROL FLAP	ADJ DISCHARGE	RISER					SPRINKLER PROTECTOR (TYPE)	REMARKS			
						DISCHARGE			RADIUS (Ft)	WIDTH x LENGTH (Ft)								TYPE	MATERIAL		SIZE (IPS INCH)	HEIGHT (INCH)			FLOW SHUTOFF DEVICE	SWING JOINT (TYPE)	RISER SUPPORT
						GALLONS PER SECOND GPS	GALLONS PER MINUTE GPM	GALLONS PER HOUR GPH											PLASTIC	GALVANIZED							
⑤	A-5	GEAR DRIVEN POP-UP ROTOR	P	40	-	-	3.1	-	32'	-	PL	3/4"	-	-	-	-	-	-	3/4"	-	-	I	-	II	⑤ 12" POP-UP		
⑤	A-5	GEAR DRIVEN POP-UP ROTOR	P	40	-	-	3.1	-	32'	-	PL	3/4"	-	-	-	-	-	-	3/4"	-	-	I	-	-	12" POP-UP		
⑤	A-5	GEAR DRIVEN ROTOR	F	40	-	-	3.1	-	32'	-	PL	3/4"	-	-	-	-	IV	X	3/4"	12"	-	I	-	-	⑦		
⑥	A-6	GEAR DRIVEN POP-UP ROTOR	P	45	-	-	6.0	-	43'	-	PL	3/4"	-	-	-	-	-	-	3/4"	-	-	I	-	II	⑤ 12" POP-UP		
⑥	A-6	GEAR DRIVEN POP-UP ROTOR	P	45	-	-	6.0	-	43'	-	PL	3/4"	-	-	-	-	-	-	3/4"	-	-	I	-	-	12" POP-UP		
⑥	A-6	GEAR DRIVEN ROTOR	F	45	-	-	6.0	-	43'	-	PL	3/4"	-	-	-	-	IV	X	3/4"	12"	-	I	-	-	⑦		
○	C-2	FLOOD BUBBLER	-	30	-	-	.5	-	-	-	PL	1/2"	-	-	-	-	V	-	-	1/2"	-	-	II	-	.50 GPM for trees		

X IN BOX DENOTES REQUIREMENT

APPLICABLE WHEN CIRCLED BELOW:

- 1 - See Special Provisions.
- 2 - If a pressure compensating device is specified, the discharge and radii shown reflect its use.
- 3 - Arc Stop for impacts shall be fitted with a nut and bolt.
- 4 - Vinyl-coated cast iron housing.
- ⑤ - Required adjacent to shoulders, curbs, sidewalks, and dikes.
- 6 - Unless otherwise shown on plans.
- ⑦ - Install check valve below the head.

ABBREVIATIONS

ADJ — adjustable	GPH — gallons per hour
B/B — brass/bronze	GPM — gallons per minute
B/B/PL — brass/bronze/plastic	GPS — gallons per second
B/PL — brass/plastic	Ft — feet
CST — center strip	In — inch
DN — diameter nominal	NPT — national pipe thread
EST — end strip	P — part circle
F — full circle	PL — plastic
F/P — full/part circle	Q — quarter circle
H — half circle	SST — side strip
IPS — iron pipe size	T — third circle
PSI — pound per square inch	TQ — three quarter circle
L — length of coverage	TT — two thirds circle
	W — width of coverage

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: BRAD COLE
 CALCULATED/DESIGNED BY: LINDA DERIVI
 CHECKED BY: LINDA DERIVI
 DAVID HARRINGTON
 REVISED BY: LINDA DERIVI
 DATE REVISED:

SPRINKLER SCHEDULE

LD-1

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 05-01-12 | TIME PLOTTED => \$TIME

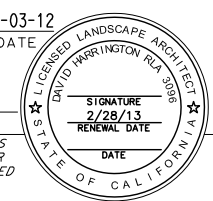
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10	Sta	99, 219	22.0/23.0, 0.0/0.5	273	423

05-03-12
 LICENSED LANDSCAPE ARCHITECT DATE
 9/04/12
 PLANS APPROVAL DATE

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DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CALIFORNIA 95203

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



IRRIGATION CROSSOVERS

LINE	STATION	SIDE		CONDUIT TYPE ③		(N) WATER LINE CROSSOVER SIZE (INCHES)	(N) SPRINKLER CONTROL CROSSOVER SIZE(INCHES)
		LT	RT	LENGTH (FT)			
				SIZE (INCH)			
8"	12"						
SR99	300+65	-	X	-	85	6"	3"
SR99	300+65	X	-	-	85	6"	4"
K	5+06	X	X	-	145	4"	3"
K	6+96	X	X	-	145	4"	3"
KL1	295+10	-	-	-	60	4"	2"
KL2	301+03	-	-	-	60	6"	4"
KR1	295+60	-	-	-	60	4"	2"
KR2	300+50	-	-	-	60	4"	2"
TOTAL				-	700		

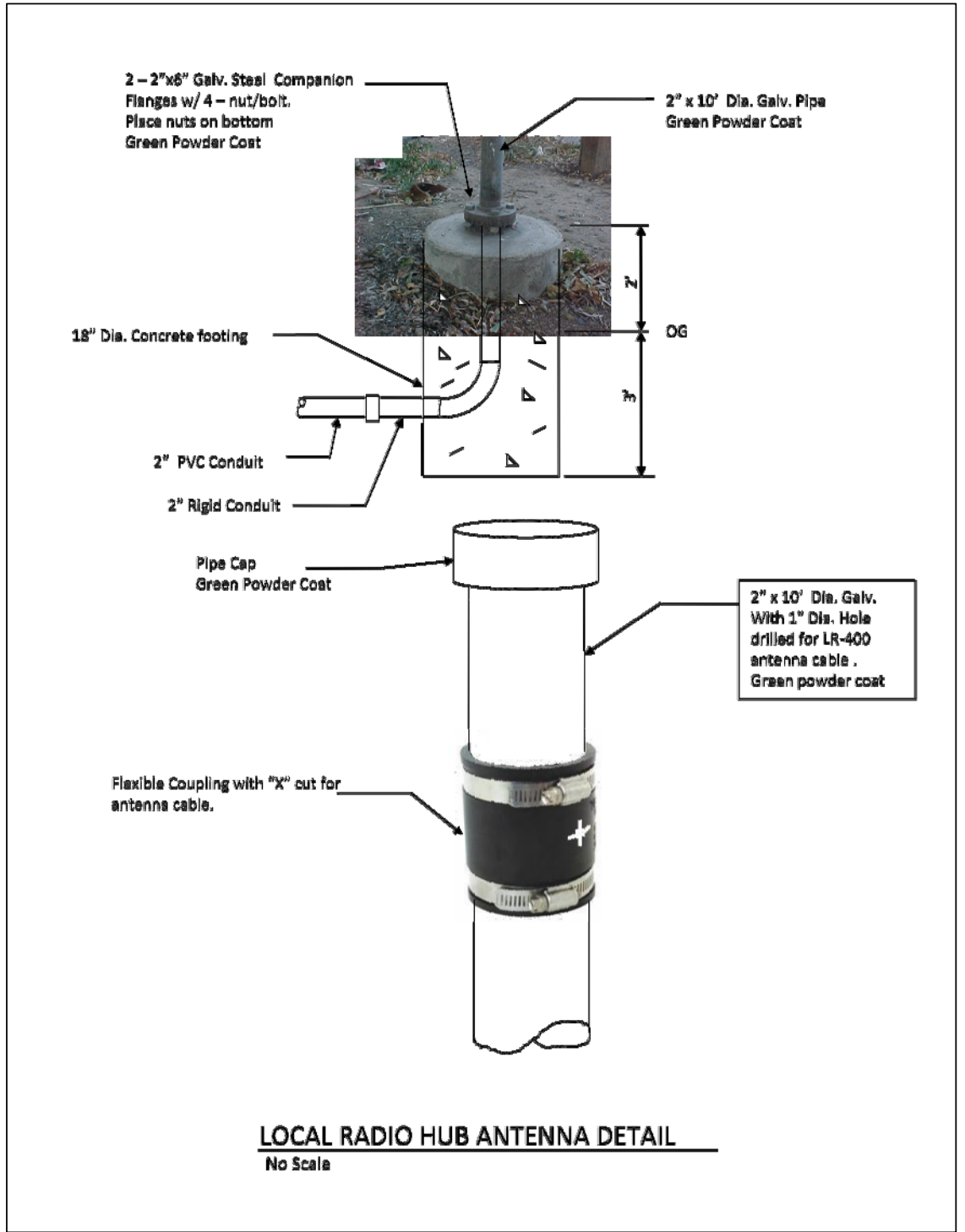
(N) Not a separate pay item, for information only.

X - Denotes Requirement

CONDUIT TYPE

(Applicable when circled below and shown under the 'CONDUIT TYPE' column heading)

- 1 - BITUMINOUS COATED CORRUGATED STEEL PIPE (1.626 mm THICK)
- 2 - CORRUGATED STEEL PIPE (1.625mm THICK)
- ③ - CORRUGATED HIGH DENSITY POLYEHTYLENE PIPE



LOCAL RADIO HUB ANTENNA DETAIL
No Scale

LANDSCAPE DETAILS

LD-2

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Galtrans®
 CONSULTANT SUPERVISOR: BRAD COLE
 CALCULATED/DESIGNED BY: DAVID HARRINGTON
 CHECKED BY: LINDA DERIVI
 REVISIONS: REVISOR: DATE: REVISION:

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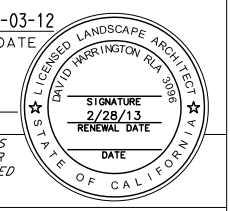
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	274	423

05-03-12
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9/04/12
 PLANS APPROVAL DATE

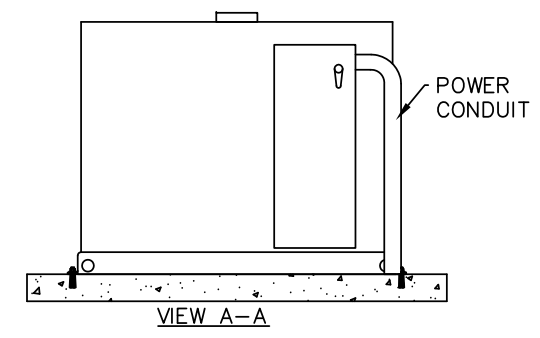
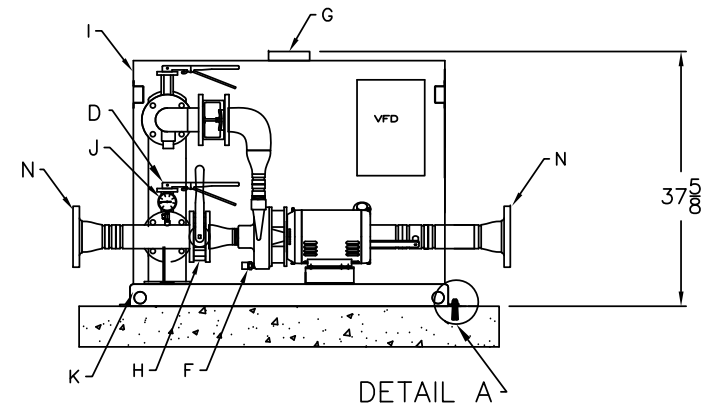
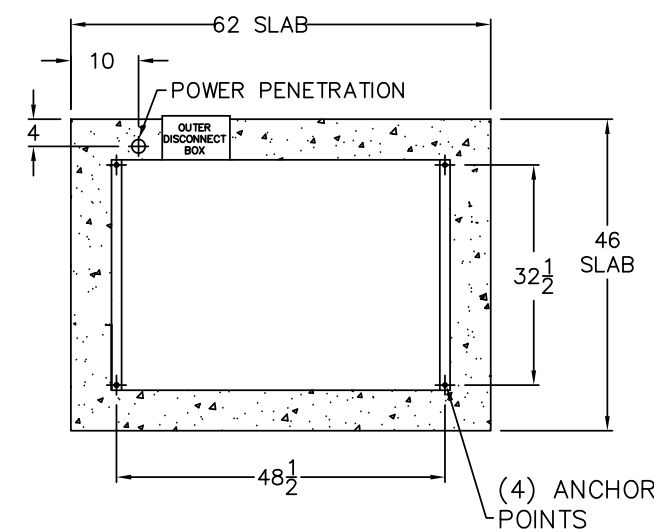
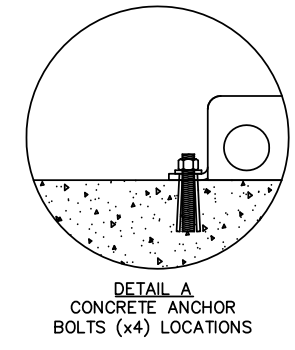
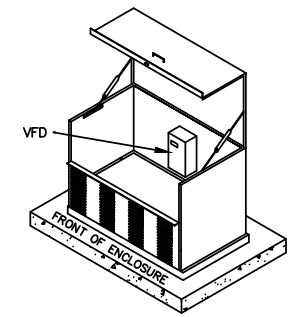
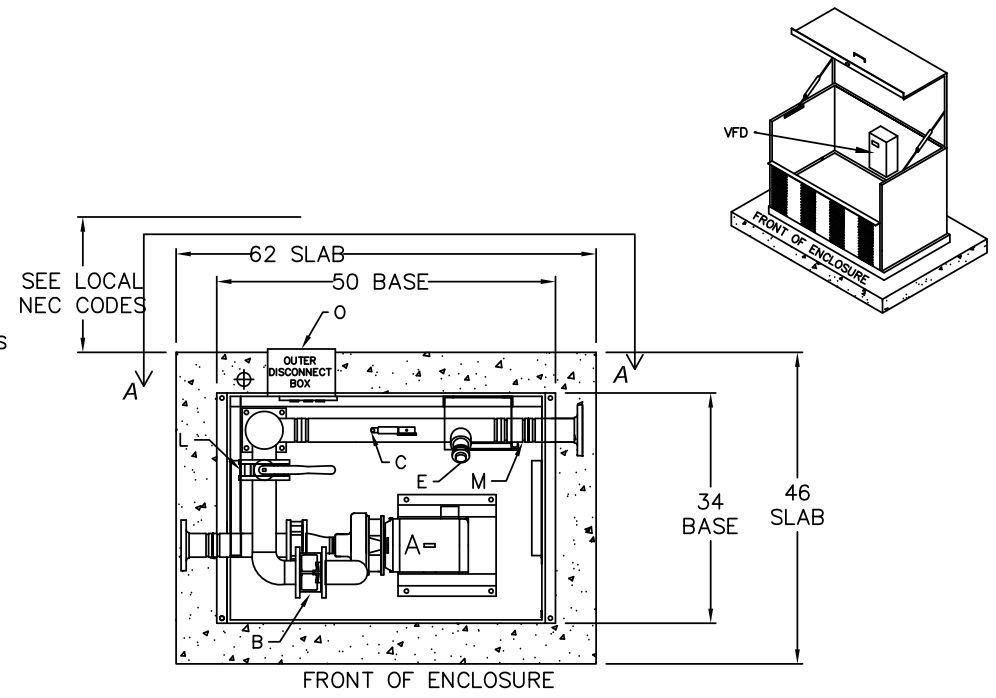
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DERIVI CONSTRUCTION & ARCHITECTURE, INC. 924 N. YOSEMITE STREET STOCKTON, CALIFORNIA 95203	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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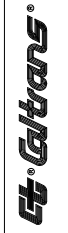
PUMP STATION SPECIFICATIONS:
NAME: CAL TRANS KIERNAN RT 219 HWY 99 - #10-0000-0100
STATION MODEL: WMBV-5000-2-10-230-3-150-56
STATION TOTAL PERFORMANCE: 150 GPM @ 56 PSI BOOST
REGULATE PRESSURE: 78 PSI
DYNAMIC INLET PRESSURE: 22 PSI
PUMP START - 3 CONTROLLERS
PUMP HORSEPOWER:
PUMP NO.1 10 HP (3600 RPM)
CHECK VALVE SIZES:
PUMP NO.1 3"
ISOLATION VALVE SIZES:
DISCHARGE ISOLATION VALVE SIZE: 3"
DISCHARGE MANIFOLD SIZE: 3"
EXHAUST FAN REQUIREMENTS: 240CFM
POWER REQUIREMENTS: 230 V, 60 HZ, 3 PHZ, 41 FULL LOAD AMPS

- STATION COMPONENTS:**
 A PUMP AND MOTOR
 B CHECK VALVE
 C PRESSURE TRANSDUCER WITH GAUGE
 D BYPASS VALVE
 E FLOW SENSOR
 F TEMP SENSOR
 G STATION FAN HOOD MOUNTED
 H PUMP INTAKE ISOLATION VALVE
 I MARINE GRADE ALUMINUM ENCLOSURE (UNPAINTED)
 J PSI/VAC GAUGE LIQUID FILLED
 K STAINLESS STEEL BASE (UNPAINTED)
 L PUMP DISCHARGE ISOLATION VALVE
 M STATION DISCHARGE ISOLATION VALVE
 N FLANGE CONNECTION
 O DEAD-FRONT DISCONNECT



CUSTOMER APPROVAL SIGNATURE <input type="checkbox"/> APPROVED AS SUBMITTED <input type="checkbox"/> MAKE INDICATED CHANGES AND RESUBMIT	7				SCALE: NONE DRWN BY: PJB THIS DRAWING AND DESIGN, IS THE PROPERTY OF WATERTRONICS AND IS NOT TO BE REPRODUCED IN WHOLE OR PART, NOR EMPLOYED FOR ANY PURPOSE OTHER THAN SPECIFICALLY PERMITTED IN WRITING BY WATERTRONICS. THIS DRAWING LOANED AND SUBJECT TO RETURN ON DEMAND	TITLE: CAL TRANS KIERNAN RT 219 HWY 99 IRRIGATION BOOSTER STATION WATERTRONICS ELECTRONICALLY CONTROLLED PUMPING SYSTEMS HARTLAND, WISCONSIN
	6					
	5					
	4					
	3					
	2					
	1	-	-	-		
	NO.	DATE	BY	DESCRIPTION	DATE: 03/05/2012 SHEET 1 OF 1 SHEETS	JOB NO.: (.5) DRAWING NO. PRST12074

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 BRAD COLE
 CALCULATED/DESIGNED BY
 CHECKED BY
 LINDA DERIVI
 DAVID HARRINGTON
 REVISOR
 DATE
 REVISED BY
 DATE
 REVISED



BOOSTER PUMP MECHANICAL

M-1

LAST REVISION DATE PLOTTED => \$DATE
 05-01-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEN NGAI
 REVISOR BY: HENRY ROBINO
 DATE REVISION: DAVE CLOW
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISOR BY: HENRY ROBINO
 DATE REVISION: DAVE CLOW

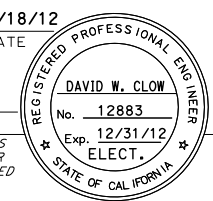
INDEX:

E-1	ELECTRICAL INDEX, NOTES, ABBREVIATIONS AND LEGEND	E-21	COUNT STATION (LOCATION 2)
E-2	PROJECT NOTES	E-22	SIGNAL AND LIGHTING (LOCATION 2) CONDUCTORS & POLE SCHEDULE
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E-5	LIGHTING AND SIGN ILLUMINATION LIGHTING (COUNTY) ELECTRIC SERVICE (IRRIGATION) INTERCONNECT SYSTEM	E-24	EXISTING SIGNAL AND LIGHTING (LOCATION 3)
E-6, E-7	LIGHTING AND SIGN ILLUMINATION	E-25	SIGNAL AND LIGHTING (LOCATION 3) CONDUCTORS & POLE SCHEDULE
E-8	LIGHTING AND SIGN ILLUMINATION SIGNAL AND LIGHTING (LOCATION 3) INTERCONNECT SYSTEM	E-26	CHANGEABLE MESSAGE SIGN SYSTEM CLOSED CIRCUIT TELEVISION SYSTEM HIGHWAY ADVISORY RADIO SYSTEM
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E-14	SIGNAL AND LIGHTING (COUNTY) CONDUCTORS & POLE SCHEDULE	E-37	ELECTRICAL NOTES AND LEGEND (STAGE CONSTRUCTION)
E-15	SIGNAL AND LIGHTING (LOCATION 1)	E-38 THRU E-44	LIGHTING AND SIGN ILLUMINATION (STAGE CONSTRUCTION)
E-16	EXISTING SIGNAL AND LIGHTING (LOCATION 1)	E-45 THRU E-52	SIGNAL AND LIGHTING (STAGE CONSTRUCTION)
E-17	COUNT STATION (LOCATION 1)	E-53 TO E-56	ELECTRICAL DETAILS (STAGE CONSTRUCTION)
E-18	SIGNAL AND LIGHTING (LOCATION 1) CONDUCTORS & POLE SCHEDULE		
E-19	SIGNAL AND LIGHTING (LOCATION 2)		
E-20	EXISTING SIGNAL AND LIGHTING (LOCATION 2)		

NOTES:

- ALL ELECTRICAL WORK SHALL CONFORM TO THE COUNTY OF STANISLAUS AND STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, LATEST EDITION AND PROJECT SPECIAL PROVISIONS.
- LOCATION OF ELECTROLIERS IS DIRECTLY RELATED TO THE GEOMETRIC LAYOUT. IF THE GEOMETRIC LAYOUT IS CHANGED FROM THAT SHOWN ON THESE PLANS, NEW ELECTROLIER LOCATIONS SHALL BE ESTABLISHED BY THE ENGINEER.
- THE LOCATIONS OF TRAFFIC SIGNAL STANDARDS, LOOP DETECTORS, LOOP WIRE RUNS AND DETECTOR HANDHOLES SHALL BE MARKED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- EQUIPMENT MOUNTED ON POLES OR STANDARDS SHALL BE INSTALLED ON THE BACKSIDE OF POLE PER ES-4A.
- THE CONTRACTOR IS RESPONSIBLE TO PROTECT EXISTING EQUIPMENT THAT WILL REMAIN IN PLACE. ANY DAMAGE TO EXISTING EQUIPMENT THAT WILL REMAIN SHALL BE REPAIRED AT NO COST TO THE COUNTY.
- FLASHING INDICATORS SHALL FLASH IN RED ON ALL PHASES.
- SIDEWALK REMOVAL SHALL BE TO THE NEAREST SCORE MARK OR AS DETERMINED BY THE ENGINEER. CONTRACTOR SHALL NEATLY SAW-CUT CONCRETE WHERE PULL BOXES ARE TO BE PLACED AND SHALL RESTORE THE SLAB TO MATCH THE EXISTING CONDITION.
- SERVICE CABINET FOUNDATIONS SHALL EXTEND 12" ABOVE PCC PAD OR SIDEWALK.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	275	423
David W. Clow REGISTERED ELECTRICAL ENGINEER DATE 5/18/12			9/04/12 PLANS APPROVAL DATE		
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



ABBREVIATIONS

LEGEND:

ACC	ANTENNA COAXIAL CABLE	<u>PROPOSED</u>	<u>EXISTING</u>
AFC	ANTENNA FEEDING CABLE		
CCTV	CLOSED CIRCUIT TELEVISION		COUNTY BRIDGE ELECTROLIER SEE COUNTY LIGHT FIXTURE SCHEDULE
CCU	CAMERA CONTROL UNIT		
EV	EMERGENCY VEHICLE PREEMPTION		AUDIBLE PEDESTRIAN SIGNAL
(G)	GROUND		TYPE I PEDESTRIAN BARRICADE
GF1	GROUND FAULT INTERRUPTER		
GRS	GALVANIZED RIGID STEEL CONDUIT		PTZ CCTV CAMERA
HPS	HIGH PRESSURE SODIUM		8" SIGNAL HEAD ON MA SEE DETAIL B ON SHEET E-32
LED	LIGHT EMITTING DIODE		
MID	MODESTO IRRIGATION DISTRICT		
OH	OVERHEAD		
PAS	PIEZO AXLE SENSOR		
PG&E	PACIFIC GAS AND ELECTRIC		
PTZ	PAN TILT ZOOM		
STC	SCREENED TRANSMISSION CABLE		
TS	TRAFFIC SIGNAL		
TSP	TELEPHONE SERVICE POINT TWISTED SHIELDED PAIR		
UHF	ULTRA HIGH FREQUENCY		
VCS	VEHICLE CLASSIFICATION STATION		
VEU	VIDEO ENCODER UNIT		

COUNTY LIGHT FIXTURE SCHEDULE						
LUMINAIRE	MANUFACTURERS MODEL NO.	LAMPS	TOTAL	VOLTS	MOUNTING	DESCRIPTION AND REMARKS
		TYPE	WATTS			
	<u>LUMINAIRE</u> GE LIGHTING SYSTEM MSCL20S0A12FMC3 OR APPROVED EQUAL	200 W HPS	240	240	SEE TRAFFIC SIGNAL PLANS	STANISLAUS COUNTY HPS STREET LIGHT
	<u>LUMINAIRE</u> STERNBERG A670TSRLED YALE 6ARC45T3-ML-240-BK OR APPROVED EQUAL <u>POLE</u> STERNBERG BARRINGTON 5200 SERIES 5218ETFP6 OR APPROVED EQUAL	96 W LED	115	240	SEE	BRIDGE ELECTROLIER - ACRYLIC REFRACTOR 4500 K - TYPE III LIGHT DISTRIBUTION - LM-79-08 TEST - BLACK POWDER FINISH EXTRUDED TAPERED FLUTED POLE

ELECTRICAL INDEX, NOTES, ABBREVIATIONS, AND LEGEND

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISIONS: REVISOR: DATE
 REVISOR: DATE

PROJECT NOTES

- 1 120/240 V, 1Ø, 3-WIRE, TYPE III-BF SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING CIRCUIT BREAKERS:
 LIGHTING AND ITS CTID No. 10380990022050T
- | AMPERES | VOLTS | POLES | LOAD | METER | PEU CONTROL TYPE |
|---------|-------|-------|------------------|-------|------------------|
| 100 | 240 | 2 | MAIN (M) | YES | - |
| 30 | 240 | 2 | HWY LTG | YES | - |
| 15 | 120 | 1 | LTG CONTROL | YES | V |
| 20 | 120 | 1 | SPARE | YES | - |
| 30 | 240 | 2 | SIGN LTG | YES | - |
| 15 | 120 | 1 | SIGN LTG CONTROL | YES | LC-3 |
| 30 | 120 | 1 | CCTV/HAR CABINET | YES | - |
| 30 | 240 | 2 | CMS | YES | - |
| 30 | 120 | 1 | CMS CABINET | YES | - |
| 20 | 120 | 1 | TDC | YES | - |
- 2 120/240 V, 1Ø, 3-WIRE, TYPE III-BF SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING CIRCUIT BREAKERS:
 LIGHTING CTID No. 10380990022320L
- | AMPERES | VOLTS | POLES | LOAD | METER | PEU CONTROL TYPE |
|---------|-------|-------|------------------|-------|------------------|
| 100 | 240 | 2 | MAIN | YES | - |
| 30 | 240 | 2 | SAFETY LTG | YES | - |
| 30 | 240 | 2 | SIGN LIGHTING | YES | - |
| 20 | 120 | 1 | SIGN LTG CONTROL | YES | LC-3 |
| 15 | 120 | 1 | LTG CONTROL | YES | V |
| 20 | 120 | 1 | SPARE | YES | - |
- 3 120/240 V, 1Ø, 3-WIRE, TYPE III-BF SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING CIRCUIT BREAKERS:
 LIGHTING AND ITS CTID No. 10380990022900T
- | AMPERES | VOLTS | POLES | LOAD | METER | PEU CONTROL TYPE |
|---------|-------|-------|-------------|-------|------------------|
| 100 | 240 | 2 | MAIN | YES | - |
| 30 | 240 | 2 | HWY LTG | YES | - |
| 30 | 120 | 1 | VCS CABINET | YES | - |
| 15 | 120 | 1 | LTG CONTROL | YES | V |
| 20 | 120 | 1 | SPARE | YES | - |
- 4 120/240 V, 1Ø, 3-WIRE, TYPE III-BF SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING CIRCUIT BREAKERS: (MID DELTA SERVICE)
 SB TRAFFIC SIGNAL, LIGHTING & ITS CTID No. 10382190000080T
- | AMPERES | VOLTS | POLES | LOAD | METER | PEU CONTROL TYPE |
|---------|-------|-------|----------------|-------|------------------|
| 100 | 240 | 2 | MAIN | YES | - |
| 30 | 240 | 2 | LTG | YES | - |
| 50 | 120 | 1 | TRAFFIC SIGNAL | YES | - |
| 15 | 120 | 1 | LTG CONTROL | YES | V |
| 30 | 120 | 1 | COUNT Sta | YES | - |
| 30 | 120 | 1 | VCS | YES | - |
| 30 | 120 | 1 | RM CABINET | YES | - |
| 30 | 120 | 1 | RWIS | YES | - |
| 20 | 120 | 1 | IISNS | YES | - |
| 15 | 120 | 1 | TDC | YES | - |

- 4A 120/240 V, 3Ø, 4-WIRE, TYPE III SERVICE EQUIPMENT ENCLOSURE PER ES-2D EXCEPT WITH DIMENSIONS ±10"x20"x63" AND EQUIPMENT AS SHOWN ON SHEET E-30. NAMEPLATE ON FRONT DOOR SHALL READ "IRRIGATION CONTROL CABINET, 120/240 V, 3Ø, 4-WIRE" CTID No. 10382190000080I.
- 5 120/240 V, 1Ø, 3-WIRE, TYPE III-BF SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING CIRCUIT BREAKERS:
 NB TRAFFIC SIGNAL & LIGHTING CTID No. 103821900000185
- | AMPERES | VOLTS | POLES | LOAD | METER | PEU CONTROL TYPE |
|---------|-------|-------|----------------|-------|------------------|
| 100 | 240 | 2 | MAIN (M) | YES | - |
| 30 | 240 | 2 | LTG | YES | - |
| 50 | 120 | 1 | TRAFFIC SIGNAL | YES | - |
| 15 | 120 | 1 | LTG CONTROL | YES | V |
| 30 | 120 | 1 | RM CABINET | YES | - |
| 20 | 120 | 1 | COUNT STA | YES | - |
| 20 | 120 | 1 | SPARE | YES | - |
| 20 | 120 | 1 | IISNS | YES | - |
- 6 120/240 V, 1Ø, 3-WIRE, TYPE III-BF SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING CIRCUIT BREAKERS:
 TRAFFIC SIGNAL & LIGHTING CTID No. 103821900000360S
- | AMPERES | VOLTS | POLES | LOAD | METER | PEU CONTROL TYPE |
|---------|-------|-------|----------------|-------|------------------|
| 100 | 240 | 2 | MAIN (M) | YES | - |
| 30 | 240 | 2 | LTG | YES | - |
| 50 | 120 | 1 | TRAFFIC SIGNAL | YES | - |
| 15 | 120 | 1 | LTG CONTROL | YES | V |
| 20 | 120 | 1 | SPARE | YES | - |
| 20 | 120 | 1 | IISNS | YES | - |
- 7 INSTALL TYPE III-BF SERVICE EQUIPMENT ENCLOSURE PROVIDE EQUIPMENT ITEMS AS SHOWN ON SHEET E-30 SEE ES-2D FOR FOUNDATION DETAIL.
- 8 Exist ELECTROLIER WITH BELL BASE
- 9 2" C, 3#2 (SERVICE, 120/240 V)
- 9A 2" C, 4#2 (SERVICE 120/240 V, 3Ø, 4-WIRE)
- 10 3" C, MT (CONDUCTORS BY MID) COORDINATE WITH MID
- 11 INSTALL PULL BOX WITHIN 5'-0" OF SERVICE EQUIPMENT ENCLOSURE. COVER SHALL BE MARKED "CALTRANS/MID". [SC] BY MID.
- 11A INSTALL PULL BOX PER SERVICE UTILITY REQUIREMENTS. COVER SHALL BE MARKED "MID"
- 11B [RC] Exist PULL BOX AND INSTALL NEW PULL BOX PER NOTE 11A.
- 12 3" C, 2#6 (LIGHTING, 240 V)
 2#8 (SIGN LIGHTING, 240 V)
 3#6 (CMS SERVICE, 240 V)
 2#6 (CMS CABINET, 120 V)
 2#6 (CCTV/HAR, 120 V)
- 13 2" C, 2#6 (LIGHTING, 240 V)
- 14 3" C, 2#6 (LIGHTING, 240 V)
 2#8 (SIGN LIGHTING, 240 V)
 3#6 (CMS SERVICE, 240 V)
 2#6 (CMS CABINET, 120 V)
 2#6 (CCTV/HAR, 120 V)
 2#10 (TDC, 120 V)
- 15 1 1/2" C, 2#8 (SIGN LIGHTING, 240 V)
- 16 3" C, 2#6 (LIGHTING, 240 V)
 2#8 (SIGN LIGHTING, 240 V)
- 17 COORDINATE WITH MID
- 17A COORDINATE WITH AT&T
- 17B INSTALL 2" C, MT (CABLE BY AT&T)
- 18 2" C, 2#6 (RM CABINET, 120 V)
 2#8 (LIGHTING, 240 V)

- 19 SEE VCS DETAILS ON SHEET E-34.
- 20 INSTALL CONDUIT EXPANSION FITTING IN ALL CONDUITS PASSING THROUGH EXPANSION JOINT PER DETAIL XY ON ES-9B
- 21 2" C, MT
- 22 INSTALL 170E CONTROLLER IN TYPE 332 CABINET WITH EXTERNAL BBS/UPS SYSTEM. SEE ES-3C FOR FOUNDATION DETAIL.
- 23 INSTALL IRRIGATION CONTROL CABINET AND IRRIGATION CABINET ON PCC PAD BESIDE PAD ON SHEET E-18.
- 24 2" C, SIC CABLE
- 25 2" C, 2#6 (LIGHTING, 240 V)
 2#8 (SIGN LIGHTING, 240 V)
- 26 LOOP DETECTORS SHALL BE SPACED 20' LEADING EDGE TO LEADING EDGE
- 27 TYPE 1 PEDESTRIAN BARRICADE. SEE SHEET ES-7Q.
- 28 3" C, 2#6 (LIGHTING, 240 V)
 2#6 (VCS CABINET, 120 V)
- 29 2" C, 2#6 (LIGHTING, 240 V)
 2#6 (VCS CABINET, 120 V)
- 30 INSTALL 2" C, 2 SIC CABLES
- 31 INSTALL METER-ON SIGN AS SHOWN ON DETAIL 2 ON SHEET E-33.
- 32 INSTALL RAMP METERING SIGNAL AS SHOWN IN DETAIL 1 ON SHEET E-33.
- 33 INSTALL RAMP METERING SIGNAL AS SHOWN IN DETAIL 3 ON SHEET E-33.
- 34 INSTALL NEW LOOP DETECTORS AS SHOWN ON DETAIL 1 ON SHEET E-32.
- 35 INSTALL NEW LOOP DETECTORS AS SHOWN ON DETAIL 2 ON SHEET E-32.
- 36 INSTALL MODEL 334 CABINET WITH AUTOMATIC TRAFFIC COUNTER
- 37 INSTALL HIGH SPEED WIRELESS MODEM ASSEMBLY IN VCS CABINET
- 38 Exist 2" C, [RC] sic CABLE
 INSTALL 1 SIC CABLE
- 39 1 1/2" C, 2#10 (TDC, 120 V)
- 40 3" C, 3#6 (CMS SERVICE, 240 V)
 2#6 (CMS CABINET, 120 V)
 2#6 (CCTV/HAR, 120 V)
- 41 2" C, 2-TELEPHONE CABLE
- 42 2" C, 1-TELEPHONE CABLE
- 43 STATE-FURNISHED MODEL 170 CONTROLLER ASSEMBLY FOR CMS IN TYPE 334 CABINET. SEE ES-3C FOR FOUNDATION DETAIL.
- 44 INSTALL MODEL 334 CCTV CABINET ASSEMBLY WITH VEU, CCU, HAR TRANSMITTER AND STATE-FURNISHED ROUTER. SEE ES-3C FOR FOUNDATION DETAIL.
- 45 1/2" C, 1#6 (G)
- 46 PROTECTIVE PULL BOX WITH GROUND ELECTRODE, 5 FT Min FROM THE POLE. SEE SHEET E-35.
- 47 INSTALL STATE-FURNISHED MODEL 170 CONTROLLER ASSEMBLY IN TYPE 334 CABINET WITH CONTRACTOR FURNISHED HIGH SPEED WIRELESS MODEM ON FOUNDATION PER ES-3C.
- 48 2" C, 4 DLC, 2 STC
- 49 2" C, 6 DLC, 3 STC
- 50 2" C, 10 DLC, 5 STC
- 51 3" C, 12 DLC, 6 STC
- 52 2" C, 2#6 (VCS CABINET, 120 V)
- 53 CCTV CAMERA WITH INTEGRAL, PAN AND TILT UNIT ON TOP OF CCTV 10 POLE. SEE ES-16B.
- 54 STATE FURNISHED MODEL 500 CMS SIGN

- 55 INSTALL HAR ANTENNA, LIGHTING ARRESTOR AND ARRESTOR ENCLOSURE ON POLE ON NEW FOUNDATION. SEE DETAIL ON SHEET E-35.
- 56 INSTALL TYPE C TELEPHONE DEMARCATION CABINET ON NEW FOUNDATION PER ES-3E.
- 57 INSTALL 2-2" C, 2#6 (TRAFFIC SIGNAL, 120 V)
 2#6 (COUNT STATION, 120 V)
 2#6 (RM, 120 V)
 2#8 (SAFETY LIGHTING, 240 V)
 2#6 (TMS, 120 V)
 2#10 (WEATHER STATION, 120 V)
 2#10 (TDC, 120 V)
 2#10 (IISNS, 120 V)
- 58 INSTALL 2" C, 2#6 (TRAFFIC SIGNAL, 120 V)
 2#8 (LIGHTING, 240 V)
 2#6 (RM, 120 V)
 2#10 (IISNS, 120 V)
- 59 INSTALL STATE-FURNISHED MODEL 2070 CONTROLLER ASSEMBLY AND CONTRACTOR FURNISHED 334 CABINET AND HIGH SPEED WIRELESS MODEM.
- 60 INSTALL PREFORMED DETECTOR LOOPS IN BRIDGE DECK AND APPROACH SLAB.
- 61 INSTALL STATE-FURNISHED MODEL 2070 CONTROLLER ASSEMBLY, 332 CABINET, BATTERY BACK UP SYSTEM, CONTRACTOR FURNISHED EXTERNAL BATTERY BACKUP CABINET AND BATTERIES.
- 62 2" C, 2#6 (TRAFFIC SIGNAL, 120 V)
 2#8 (INTERSECTION LTG, 240 V)
 2#10 (IISNS, 120 V)
- 63 2" C, 2#6 (TRAFFIC SIGNAL, 120 V)
 2#8 (INTERSECTION LTG, 240 V)
 2#6 (RM, 120 V)
 2#6 (COUNT STATION, 120 V)
 2#10 (IISNS, 120 V)
- 64 2" C, 2#6 (COUNTY LIGHTING, 240 V)
 1#6 (G)
- 65 2" C, 3#8 (IRRIGATION PUMP, 240 V, 3Ø)
 2#10 (IRRIGATION CABINETS, 120 V)
 1#10 (G)
 2" C, 2-#22 TSP (FLOW AND PRESSURE SENSORS)
 6#14 (PUMP CONTROL RELAYS, 24 VAC)
- 66 1 1/4" C, 3#8 (IRRIGATION PUMP, 240 V, 3Ø)
 1#10 (G)
 2#14 (PUMP THERMAL SWITCH)
 1" C, 2-#22 TSP (FLOW AND PRESSURE SENSORS)
- 67 DISCONNECT AND REMOVE Exist WELL PUMP SERVICE EQUIPMENT AND INSTALL NEW SERVICE. SEE SHEETS E-5 AND E-18.
- 68 1" C, 1-TC TO PULL BOX IN FRONT OF TDC. SEE SHEET E-18.
- 69 INSTALL RAMP METERING SIGNAL AS SHOWN IN DETAIL 4 ON SHEET E-33.
- 70 INSTALL 1" C, 2#10 (IRRIGATION CONTROLLER, 120 V)
 2#14 (PUMP SWITCH RELAY, 24 VAC)

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	276	423

David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE

9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

ALLIANCE ENGINEERING CONSULTING, INC.
 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054

STANISLAUS COUNTY
 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

REG. NO. 12883
 Exp. 12/31/12
 ELECT.

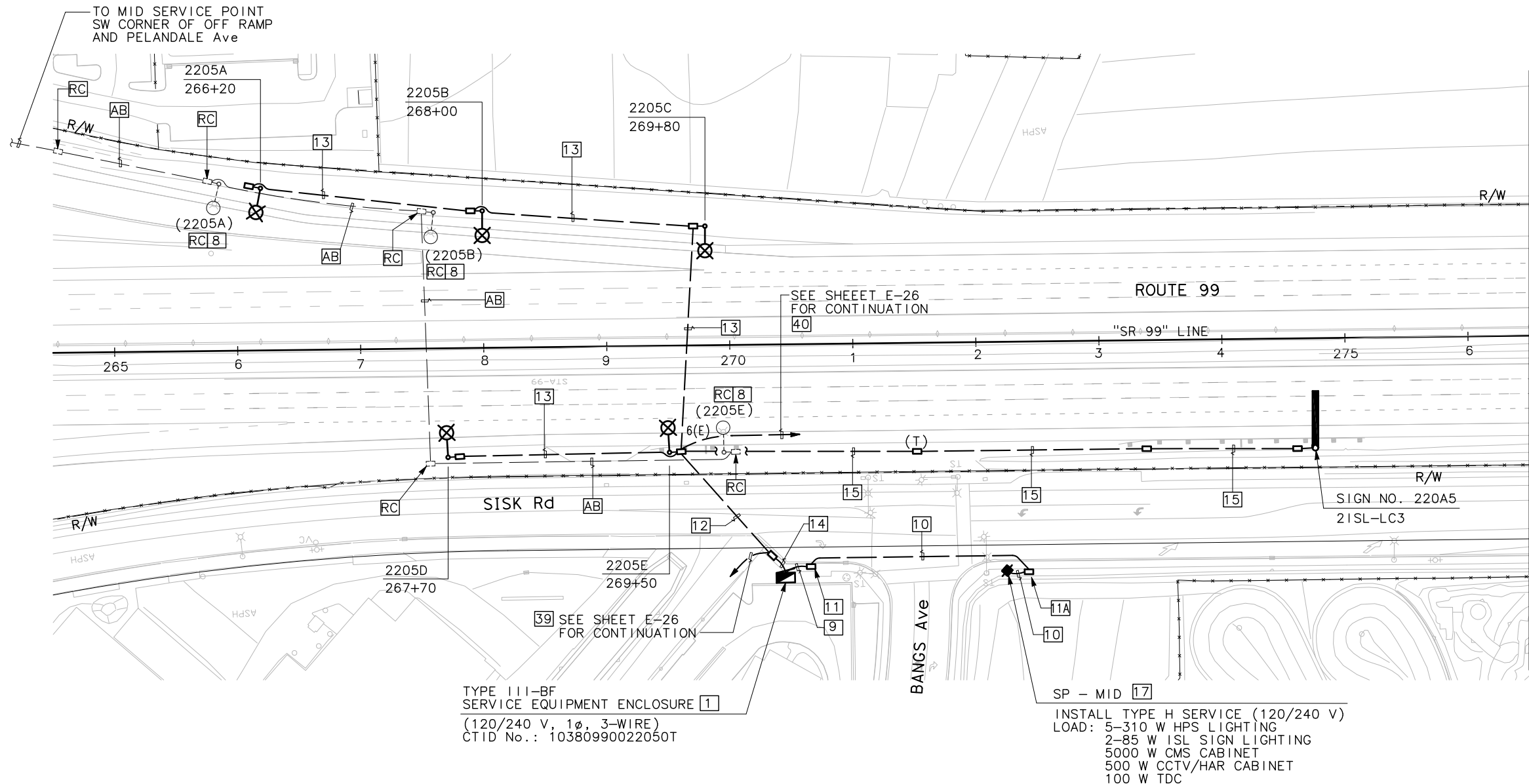
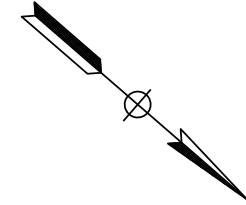
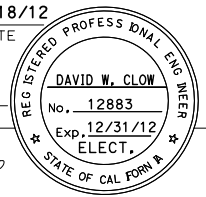
PROJECT NOTES
E-2

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	277	423

David W. Clow	5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISIONS: REVISOR: HENRY ROBINO, DATE: 7/2/2010
 REVISOR: DAVE CLOW, DATE: 9/04/12

LIGHTING AND SIGN ILLUMINATION

SCALE: 1" = 50'

E-3

APPROVED FOR ELECTRICAL WORK ONLY.

NOTES:

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- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	278	423

David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE

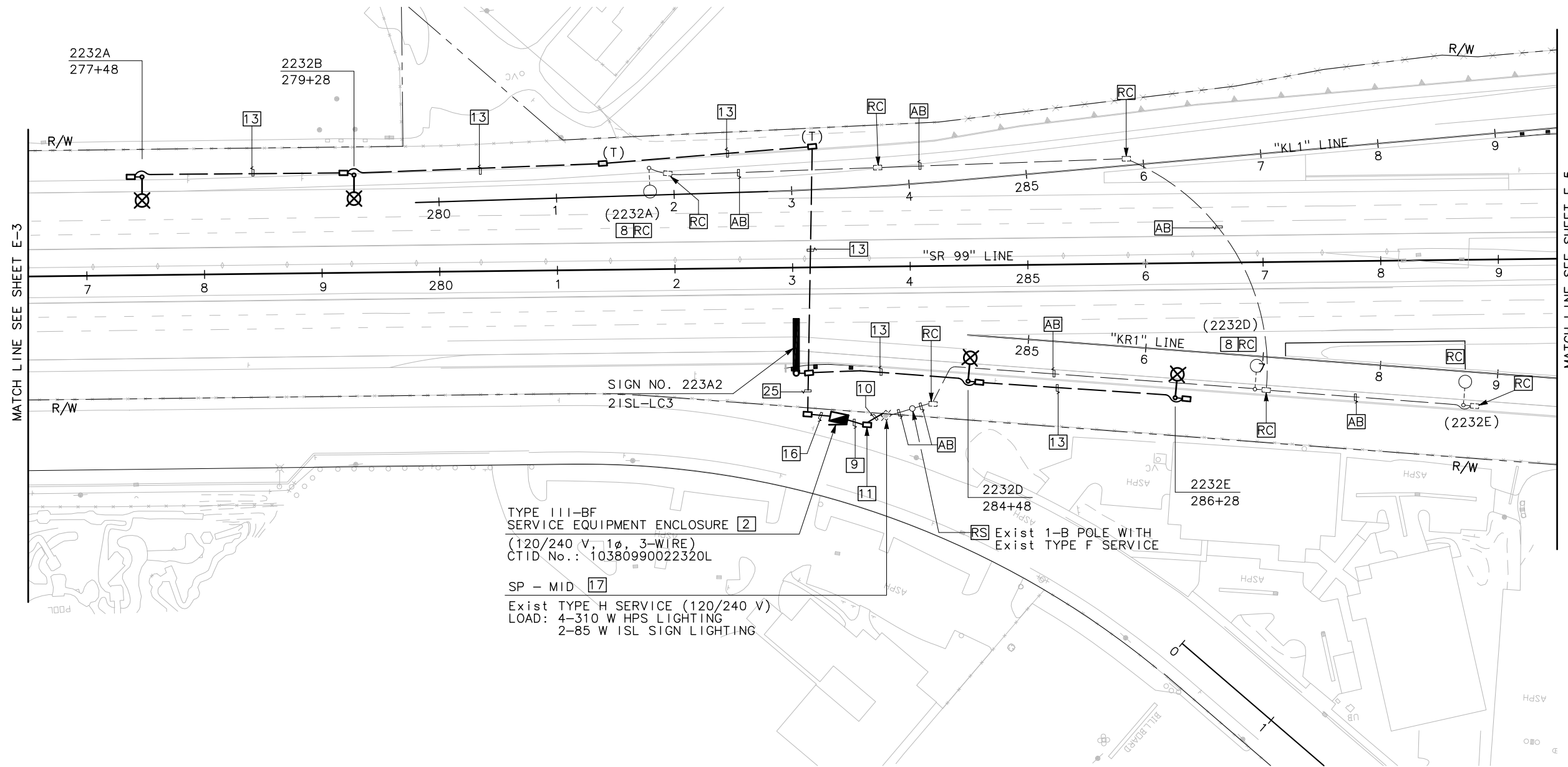
9/04/12
 PLANS APPROVAL DATE

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ALLIANCE ENGINEERING CONSULTING, INC.
 4701 PATRICK HENERY DR., #10
 SANTA CLARA, CALIFORNIA 95054

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 DAVID W. CLOW
 No. 12883
 Exp. 12/31/12
 ELECT.
 STATE OF CAL FORM #



TYPE III-BF
 SERVICE EQUIPMENT ENCLOSURE [2]
 (120/240 V, 1 ϕ , 3-WIRE)
 CTID No.: 10380990022320L
 SP - MID [17]
 Exist TYPE H SERVICE (120/240 V)
 LOAD: 4-310 W HPS LIGHTING
 2-85 W ISL SIGN LIGHTING

LIGHTING AND SIGN ILLUMINATION
 SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-4

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Ken Ngai
 CONSULTANT FUNCTIONAL SUPERVISOR
 Checked by
 Dave Clow
 HENRY ROBINO
 REVISOR
 DATE REVISOR

BORDER LAST REVISED 7/2/2010

USERNAME => #USER
 DGN FILE => #REQUEST

RELATIVE BORDER SCALE
 IS IN INCHES



UNIT 1457

PROJECT NUMBER & PHASE 100000100

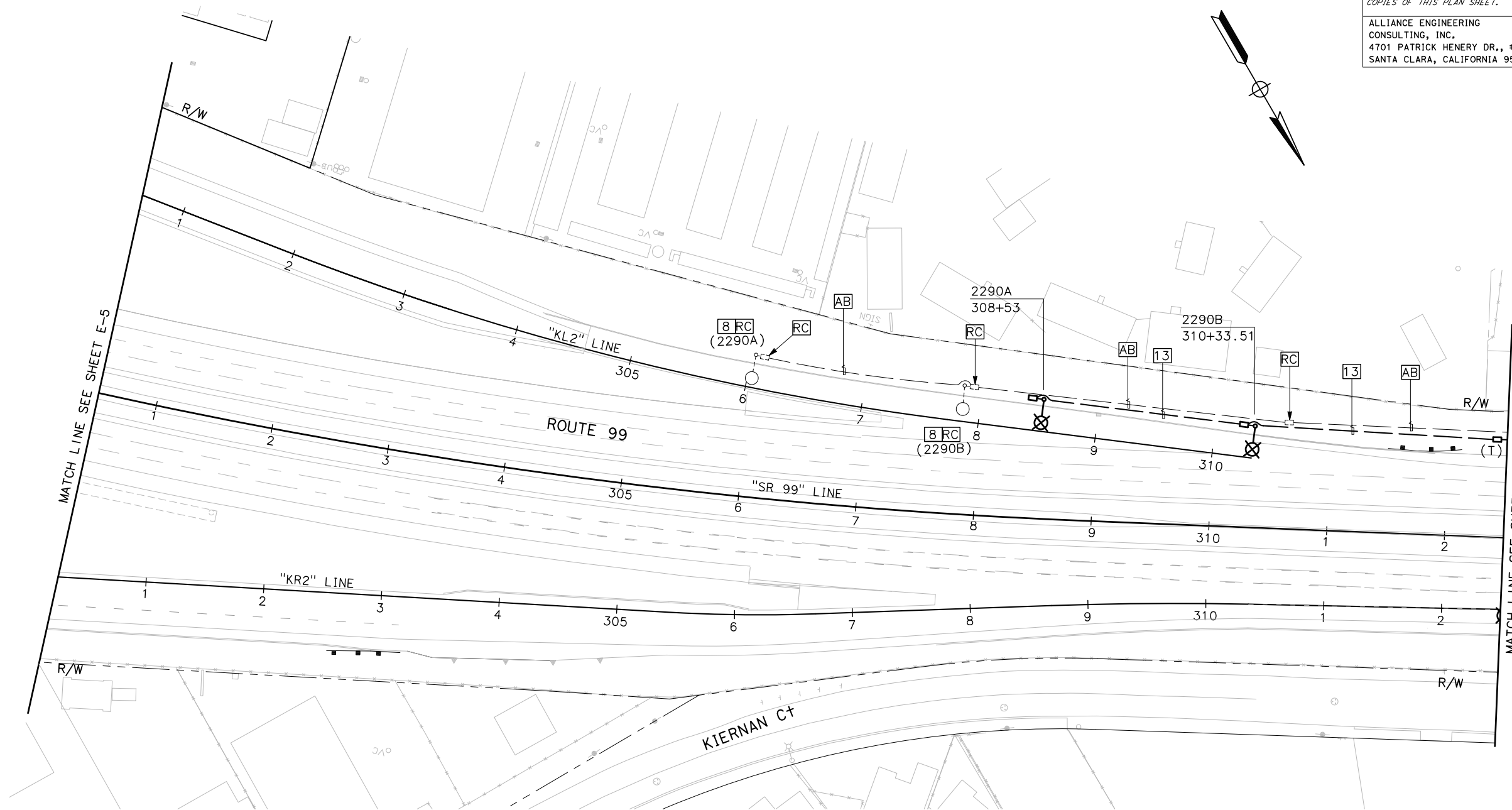
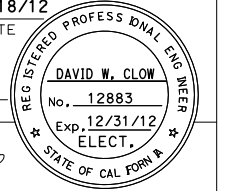
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 05-18-12 | TIME PLOTTED => \$TIME

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	280	423

<i>David W. Clow</i>		5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



LIGHTING AND SIGN ILLUMINATION
SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-6

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
Caltrans	KEN NGAI	CHECKED BY	DAVE CLOW
		DESIGNED BY	HENRY ROBINO
		DATE REVISED	

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	281	423

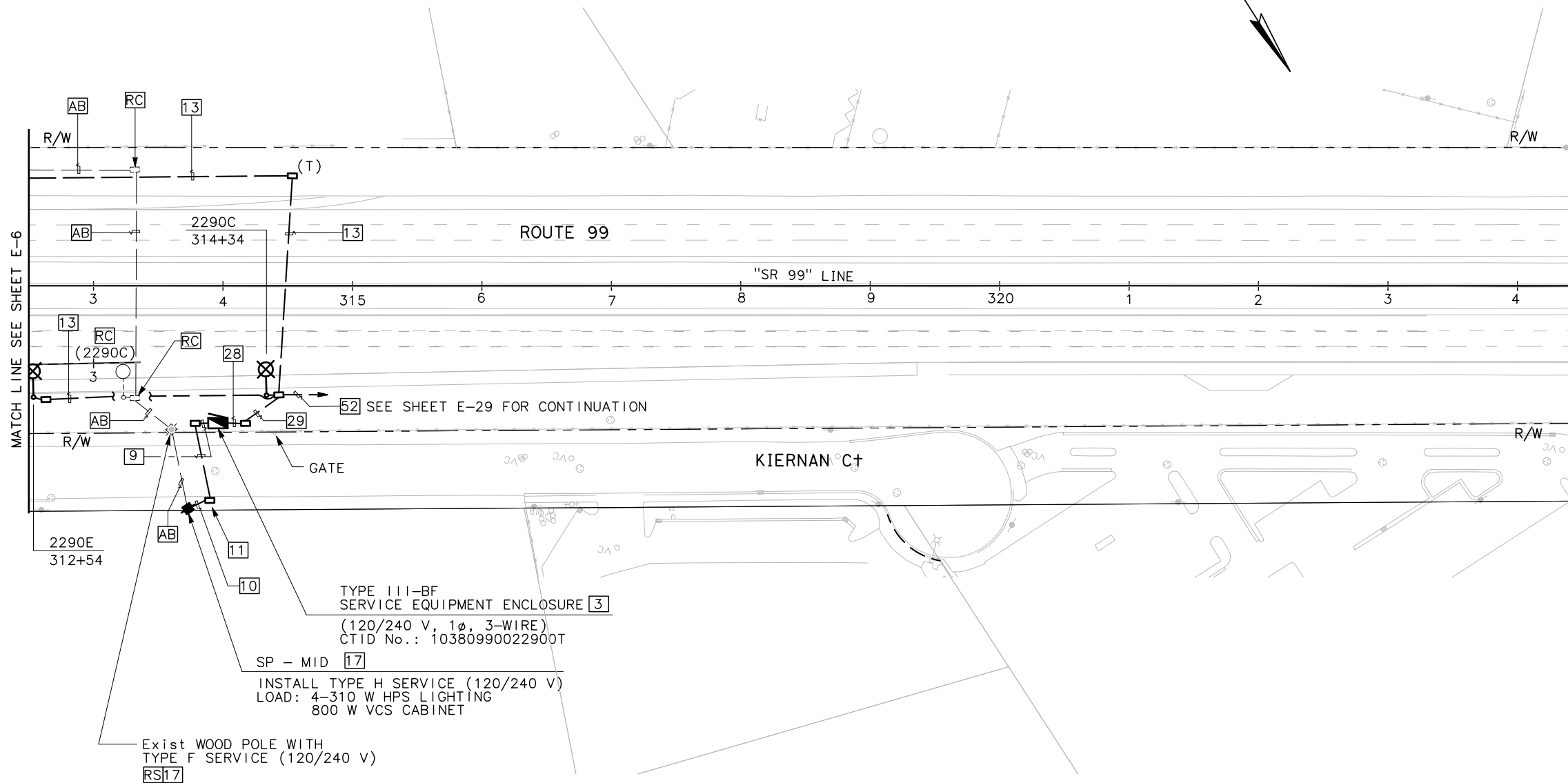
David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE

9/04/12
 PLANS APPROVAL DATE

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ALLIANCE ENGINEERING CONSULTING, INC.
 4701 PATRICK HENERY DR., #10
 SANTA CLARA, CALIFORNIA 95054

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



REVISOR: HENRY ROBINO
 DATE: DAVE CLOW
 CHECKED BY: KEN NGAI
 DESIGNED BY: KEN NGAI
 SUPERVISOR: KEN NGAI
 TRANSPORTATION: DEPARTMENT OF TRANSPORTATION
 CALIFORNIA: STATE OF CALIFORNIA
 GILTRANS

LIGHTING AND SIGN ILLUMINATION
 SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-7

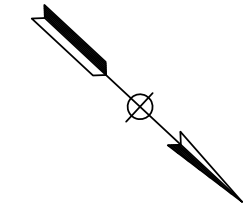
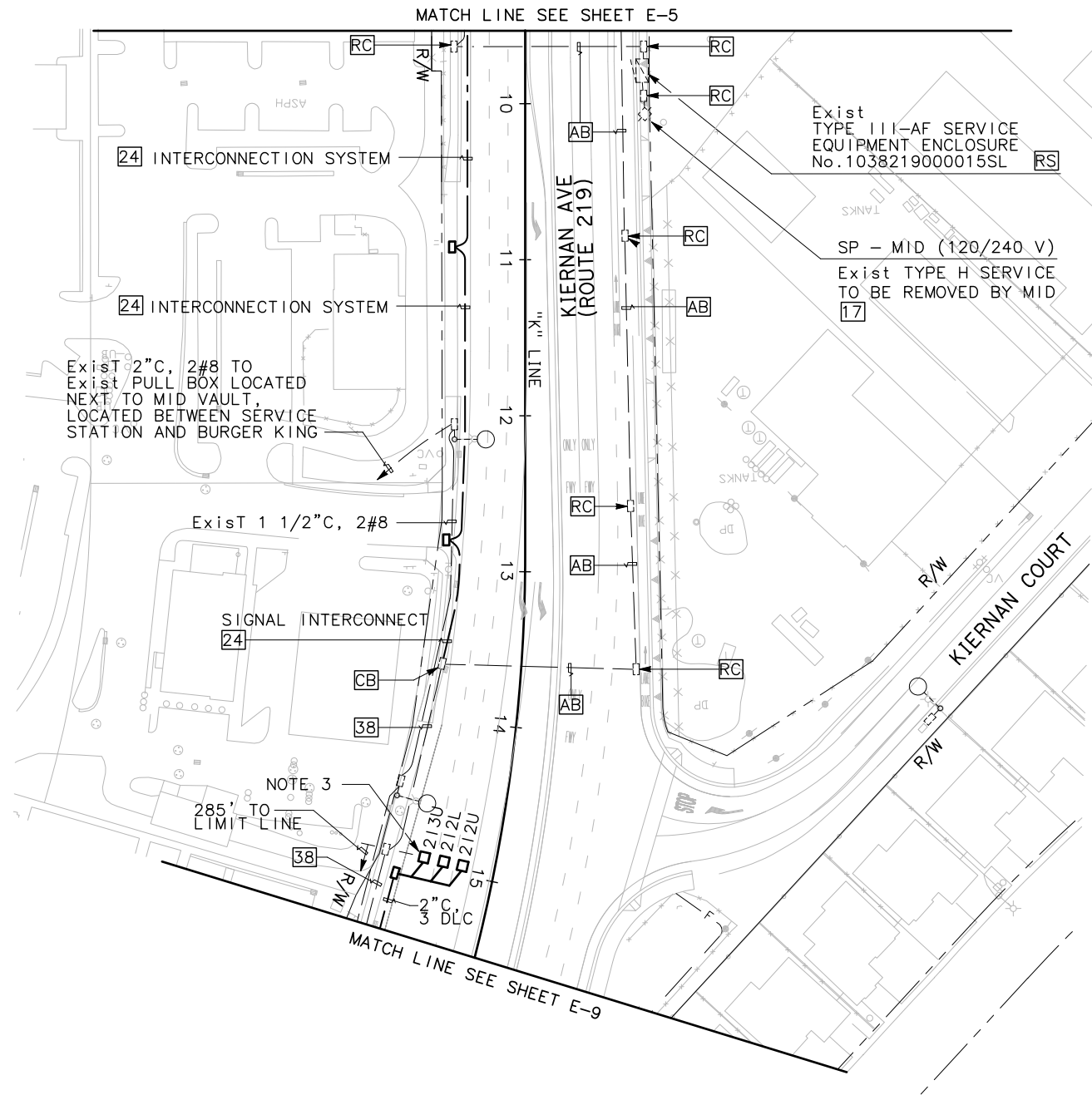
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: CHECKED BY:
 HENRY ROBINO DAVE CLOW
 REVISED BY: DATE REVISED:

NOTES:

1. FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
2. FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.
3. SIGNAL AND LIGHTING (LOCATION 3) WORK.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	282	423

David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



**LIGHTING AND SIGN ILLUMINATION
 SIGNAL AND LIGHTING (LOCATION 3)
 INTERCONNECT SYSTEM**

SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

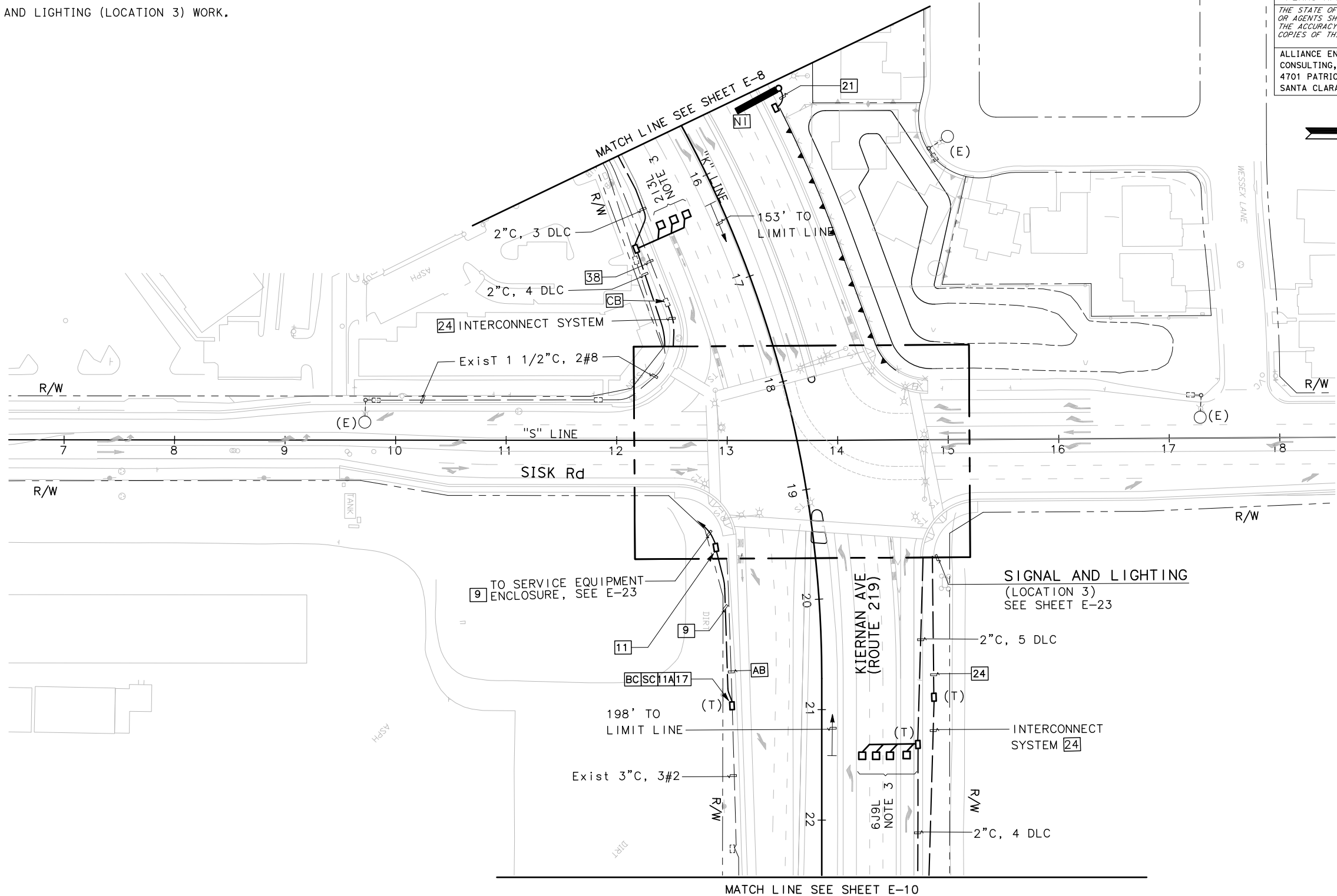
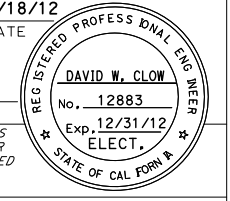
E-8

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISIONS: REVISOR: DATE
 REVISOR: DATE

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.
- SIGNAL AND LIGHTING (LOCATION 3) WORK.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	283	423
David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12					
PLANS APPROVAL DATE					
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



**SIGNAL AND LIGHTING (LOCATION 3)
 INTERCONNECT SYSTEM**
 SCALE: 1" = 50'

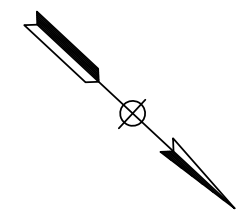
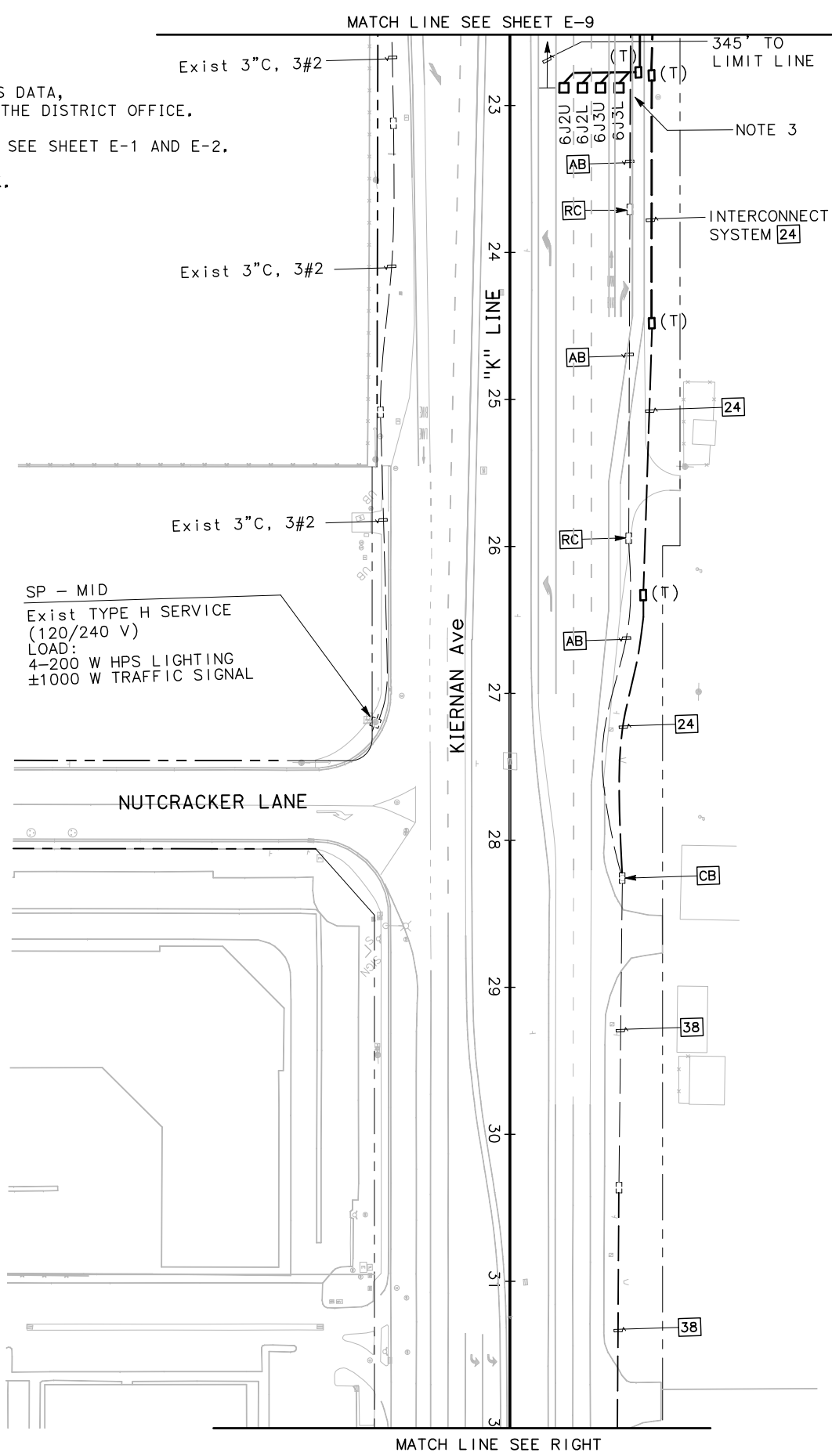
APPROVED FOR ELECTRICAL WORK ONLY.

LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

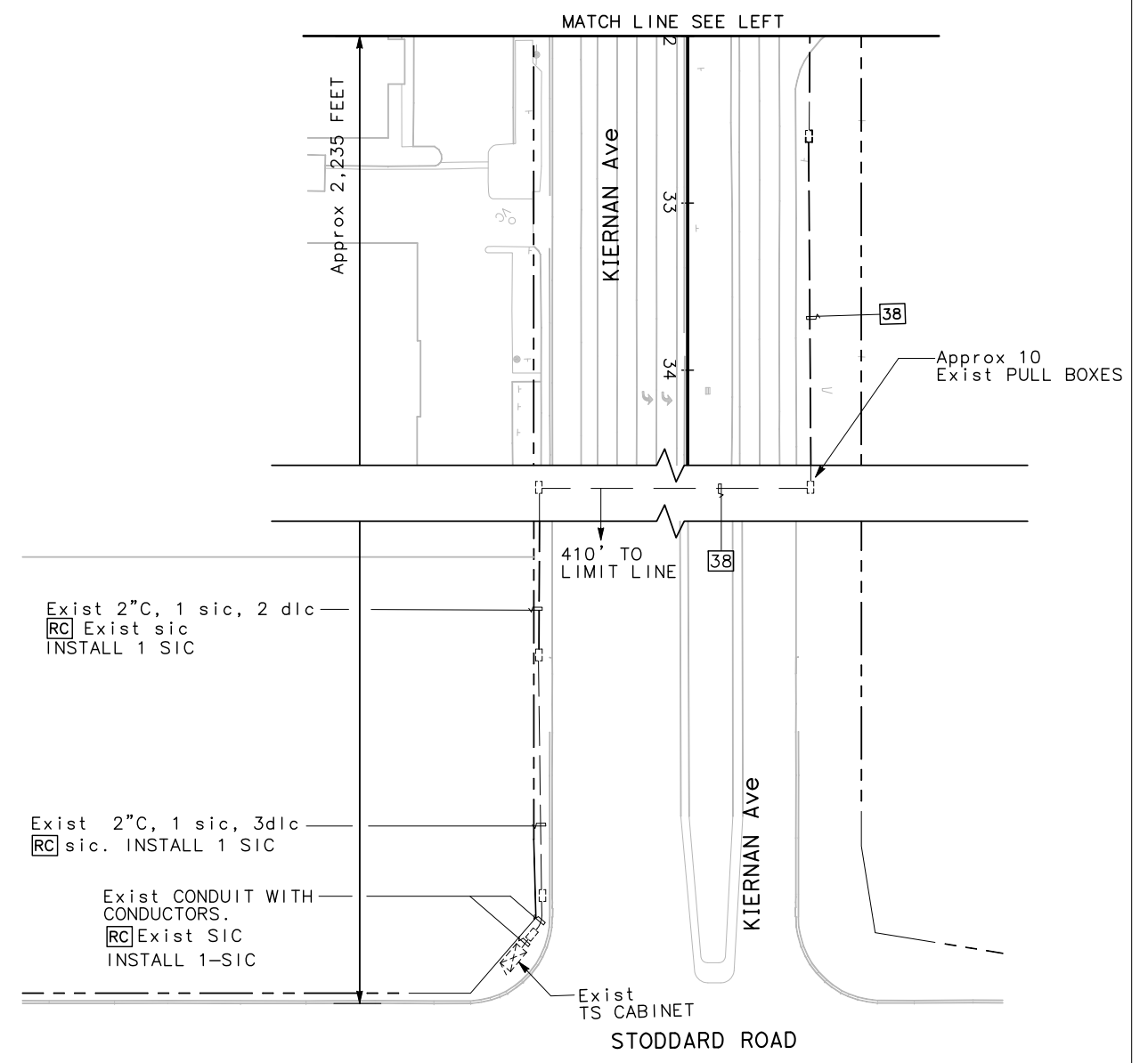
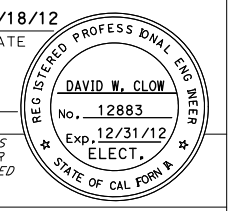
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	HENRY ROBINO	REVISOR BY	DAVE CLOW
GT Caltrans	KEN NGAI	CHECKED BY	DAVE CLOW	DATE REVISED	

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.
- SIGNAL AND LIGHTING (LOCATION 3) WORK.



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	284	423
David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12		PLANS APPROVAL DATE			
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



**SIGNAL AND LIGHTING (LOCATION 3)
INTERCONNECT SYSTEM**

SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-10

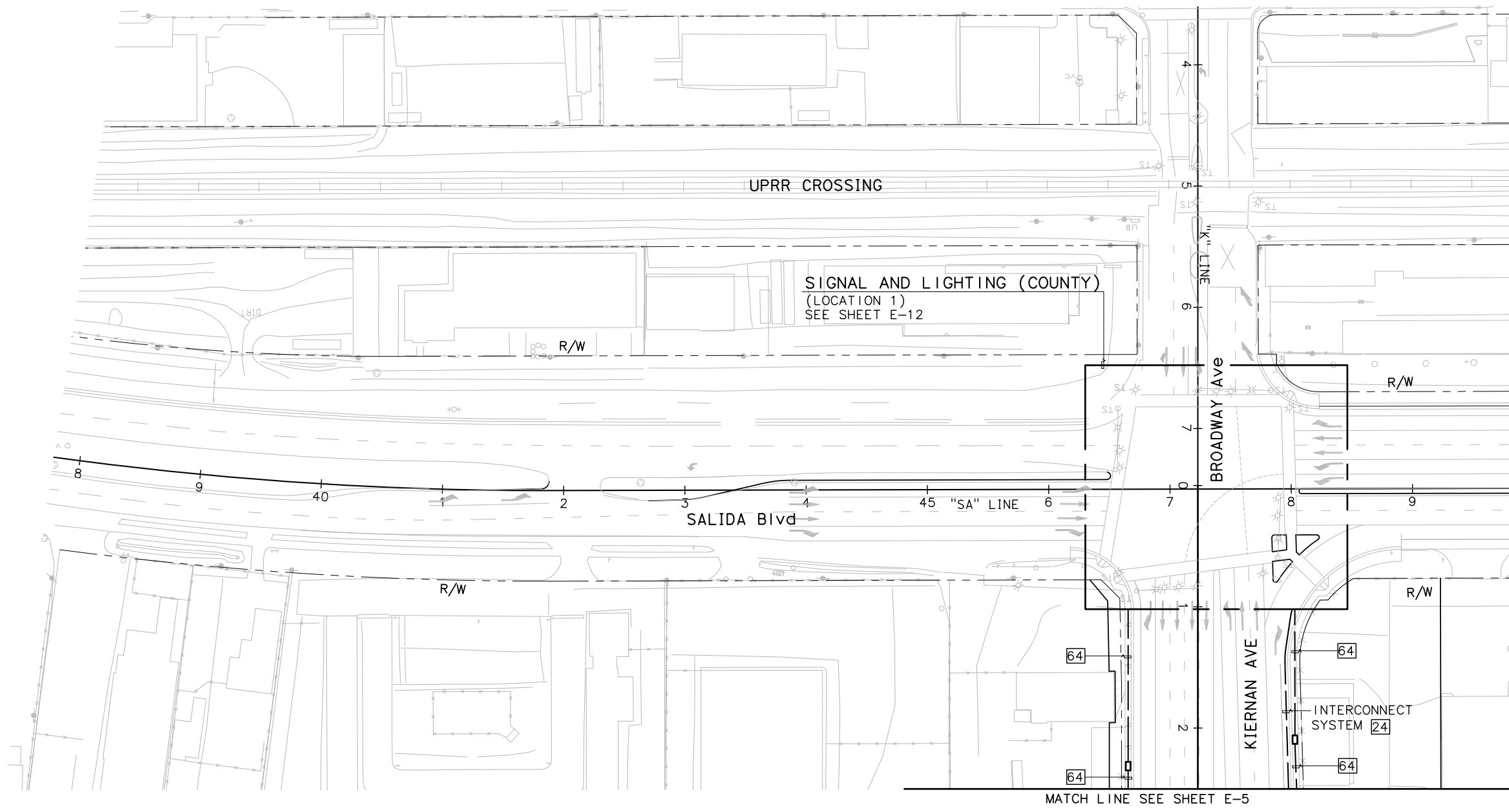
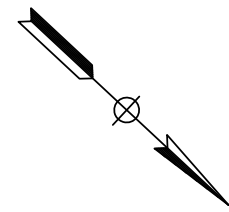
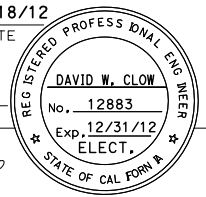
LAST REVISION | DATE PLOTTED => \$DATE
05-18-12 | TIME PLOTTED => \$TIME

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-1 AND E-2.
- SIGNAL AND LIGHTING (LOCATION 3) WORK.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	285	423

<i>David W. Clow</i>	5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISED BY: [blank]
 DATE REVISED: [blank]

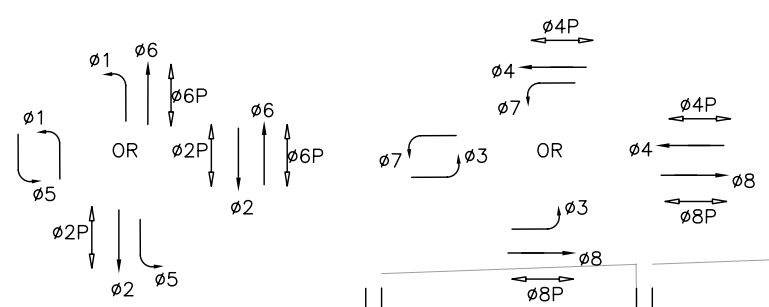
INTERCONNECT SYSTEM
SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

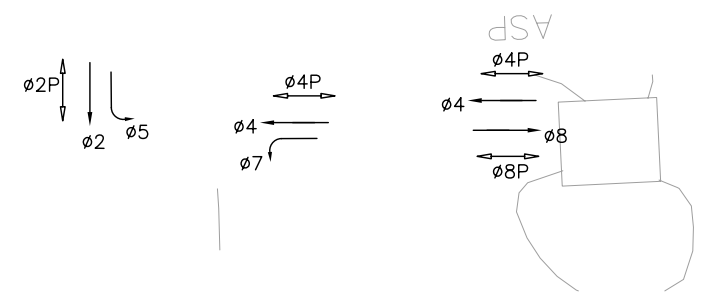
LAST REVISION | DATE PLOTTED => \$DATE
 05-18-12 | TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	286	423
<i>P. Sakshmi</i> REGISTERED CIVIL ENGINEER			05-18-12 DATE		
9/04/12 PLANS APPROVAL DATE					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

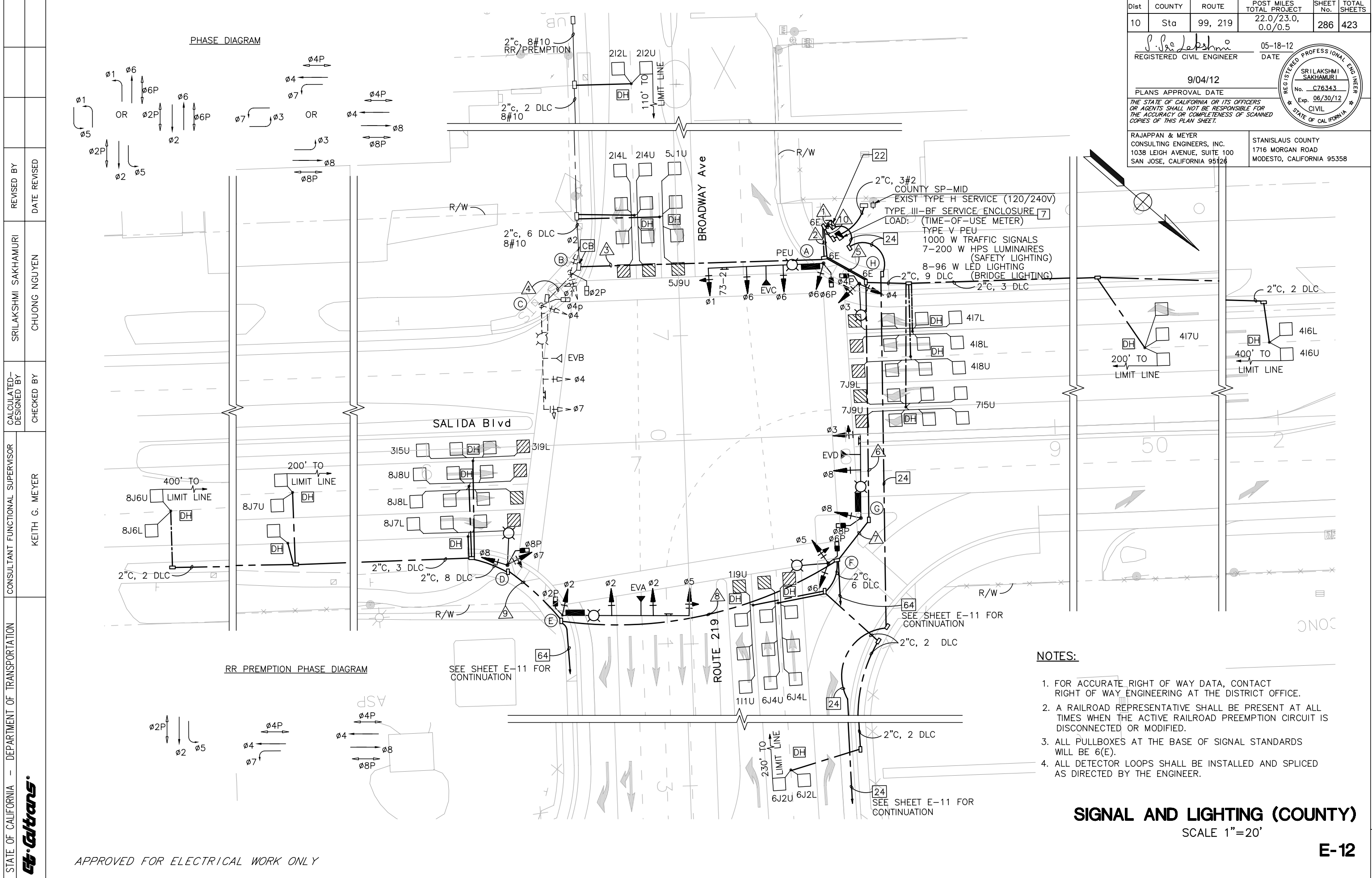
PHASE DIAGRAM



RR PREMPTION PHASE DIAGRAM



SEE SHEET E-11 FOR CONTINUATION



NOTES:

1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
2. A RAILROAD REPRESENTATIVE SHALL BE PRESENT AT ALL TIMES WHEN THE ACTIVE RAILROAD PREEMPTION CIRCUIT IS DISCONNECTED OR MODIFIED.
3. ALL PULLBOXES AT THE BASE OF SIGNAL STANDARDS WILL BE 6(E).
4. ALL DETECTOR LOOPS SHALL BE INSTALLED AND SPLICED AS DIRECTED BY THE ENGINEER.

SIGNAL AND LIGHTING (COUNTY)

SCALE 1"=20'

E-12

APPROVED FOR ELECTRICAL WORK ONLY



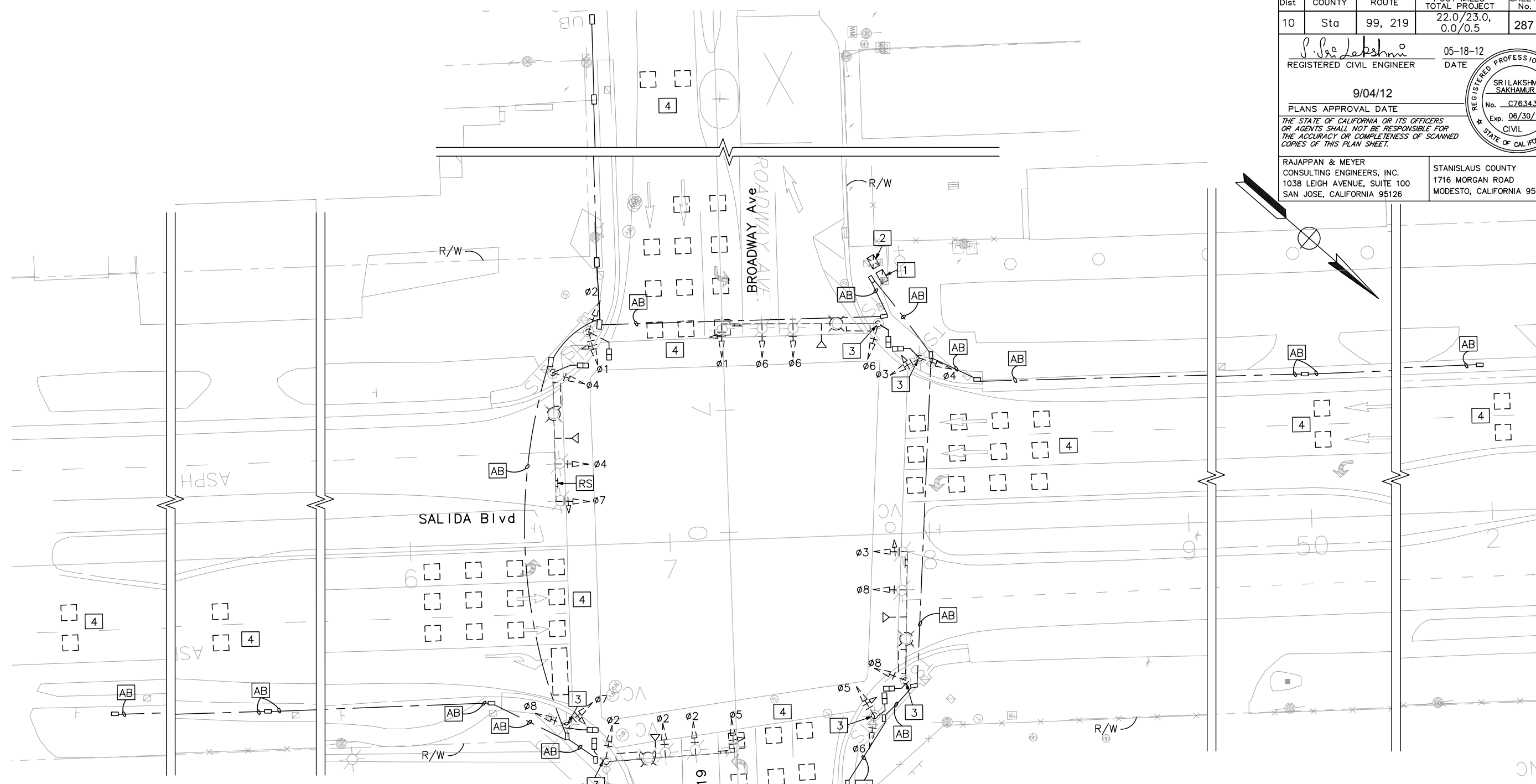
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: SRI LAKSHMI SAKHAMURI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: SRI LAKSHMI SAKHAMURI
 DATE REVISED: CHUONG NGUYEN

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	287	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
 No. C76343
 Exp. 06/30/12
 CIVIL
 STATE OF CALIFORNIA

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



CONSTRUCTION NOTES (This sheet only)

- 1 [RC] EXISTING SERVICE CABINET.
[RC] CONDUIT AND PULL BOX, WHEN NO LONGER NEEDED.
 - 2 [RS] EXISTING CONTROLLER CABINET. [RC] FOUNDATION.
 - 3 [RS] EXISTING POLE AND EQUIPMENT. [RC] ENTIRE FOUNDATION AND PULL BOX.
- RETURN ALL SALVAGED EQUIPMENT TO COUNTY YARD.
 1716 MORGAN ROAD
 MODESTO, CA 95358.
- 4 [AB] EXISTING LOOP DETECTORS. [RC] CONNECTED PULL BOXES.

EXISTING SIGNAL AND LIGHTING (COUNTY)

SCALE 1"=20'

E-13

APPROVED FOR ELECTRICAL WORK ONLY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	288	423

P. Sakshmi
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

CONDUCTOR SCHEDULE

CABLE SIZE	Ø PPB	POLE	No. OF CONDUCTORS/ RUN No.															
			△	△	△	△	△	△	△	△	△	△	△	△				
12/3	Ø1, Ø6, Ø6P	4PPB	POLE A	1-1	1-1													
	Ø1, Ø2, Ø2P	4PPB	POLE B	1-1	1-1	1-1												
	Ø4, Ø7, Ø4P	2PPB	POLE C	1-1	1-1	1-1	1-1											
	Ø7, Ø8, Ø8P	2PPB	POLE D	1-1	1-1			1-1	1-1	1-1	1-1	1-1						
	Ø2, Ø5, Ø2P	8PPB	POLE E	1-1	1-1			1-1	1-1	1-1	1-1							
	Ø5, Ø6, Ø6P	8PPB	POLE F	1-1	1-1			1-1	1-1	1-1								
	Ø3, Ø8, Ø8P	6PPB	POLE G	1-1	1-1			1-1	1-1									
	Ø3, Ø4, Ø4P	6PPB	POLE H	1-1	1-1			1-1										
TOTAL SIGNAL CABLES						8-8	8-8	2-2	1-1	5-5	4-4	3-3	2-2	1-1				
#10			IISNS			4	2	2	2	2	2	2	2	2				4
#10			LUMINAIRE			4	2	2	2	2	2	2	2	2				4
#10			RR PREEMPTION SERVICE	8	8	8												
#6			GROUND	2														2
#14			PEU	1														1
DETECTOR CABLES	Ø1			2	2			2	2	2								
	Ø2			4	4	4												
	Ø3			2	2			2	2	2	2	2						
	Ø4			6	6			6										
	Ø5			2	2	2												
	Ø6			4	4			4	4	4								
	Ø7			3	3			3										
	Ø8			6	6			6	6	6	6	6						
TOTAL DETECTION CABLES						29	29	6		23	14	14	8	8				
EMERGENCY VEHICLE PREEMPTION	EV A			1	1			1	1	1	1							
	EV B			1	1	1												
	EV C			1	1													
	EV D			1	1			1	1									
TOTAL EVP CABLES						4	4	1		2	2	1	1					
NEW OR EXISTING						N	N	N	E	N	N	N	N	N	N			
CONDUIT SIZE (INCHES)						2-4"	2-4"	4"	4"	2-3"	4"	4"	3"	3"	2"			

EQUIPMENT AND POLE SCHEDULE

SIGNAL POLE	NEW OR EXISTING	TYPE	MAST ARM LENGTH		SIGNAL MOUNTING		PPB		PED SIGNAL MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
			SIGNAL MAST ARM	LUMINAIRE MAST ARM	MAST ARM	POLE	Ø	ARROW			
(A)	N	29-5-100	55'	15'	MAS, MAS, MAS	S-1V-T	4	←	S-1P-T	200	INSTALL PEU. INSTALL IISNS "SALIDA BOULEVARD". INSTALL R73-2 SIGN.
(B)	E	1-A	-	-	-	T-2V-T	4	→	S-1P-T	-	
(C)	E	29-5-100	50'	15'	MAS, MAS	S-1V-T	2	←	S-1P-T	200	REMOVE R73-2 AND INSTALL R73-6
(D)	N	15TS	-	15'	-	S-2V-T	2	→	S-1P-T	200	
(E)	N	61-5-100	60'	15'	MAS, MAS, MAS	S-1V-T	8	←	S-1P-T	200	INSTALL R73-3. INSTALL LED IISNS "SALIDA BOULEVARD".
(F)	N	15TS	-	15'	-	S-2V-T	8	→	S-1P-T	200	
(G)	N	26-4-100	40'	15'	MAS, MAS	S-1V-T	6	←	S-1P-T	200	INSTALL R73-2. INSTALL LED IISNS "BROADWAY AVE".
(H)	N	15TS	-	15'	-	S-2V-T	6	→	S-1P-T	200	

SIGNAL AND LIGHTING (COUNTY) CONDUCTORS & POLE SCHEDULE

E-14

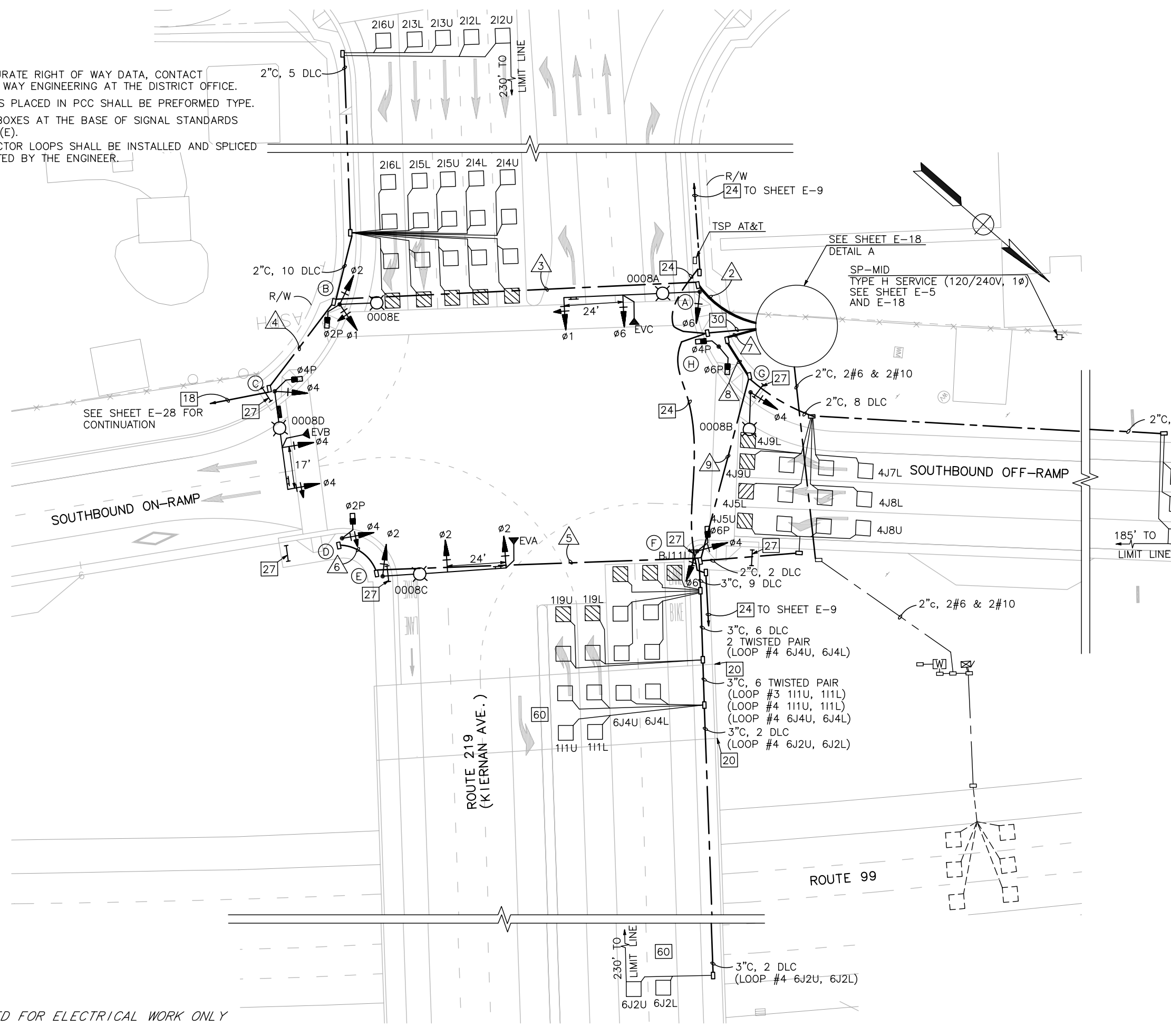
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 KEITH G. MEYER
 CALTRANS

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-18-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: [Blank]
 CHECKED BY: [Blank]
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY: [Blank]
 DATE REVISED: [Blank]

NOTES:

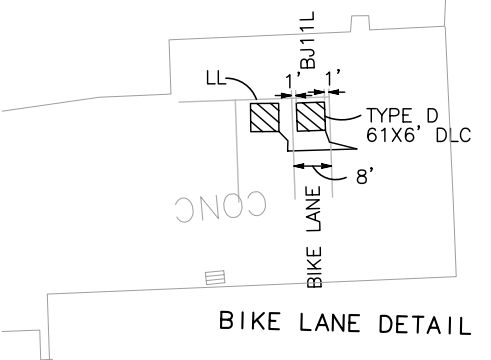
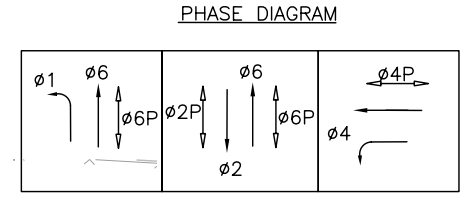
1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
2. ALL LOOPS PLACED IN PCC SHALL BE PREFORMED TYPE.
3. ALL PULLBOXES AT THE BASE OF SIGNAL STANDARDS WILL BE 6(E).
4. ALL DETECTOR LOOPS SHALL BE INSTALLED AND SPliced AS DIRECTED BY THE ENGINEER.



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	289	423

REGISTERED CIVIL ENGINEER: *P. Lakshmi* DATE: 05-18-12
 PLANS APPROVAL DATE: 9/04/12
 No. C76343 Exp. 06/30/12
 REGISTERED PROFESSIONAL ENGINEER: SRILAKSHMI SAKHAMURI
 CIVIL STATE OF CALIFORNIA

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



SIGNAL AND LIGHTING (LOCATION 1)
 SCALE 1"=20'

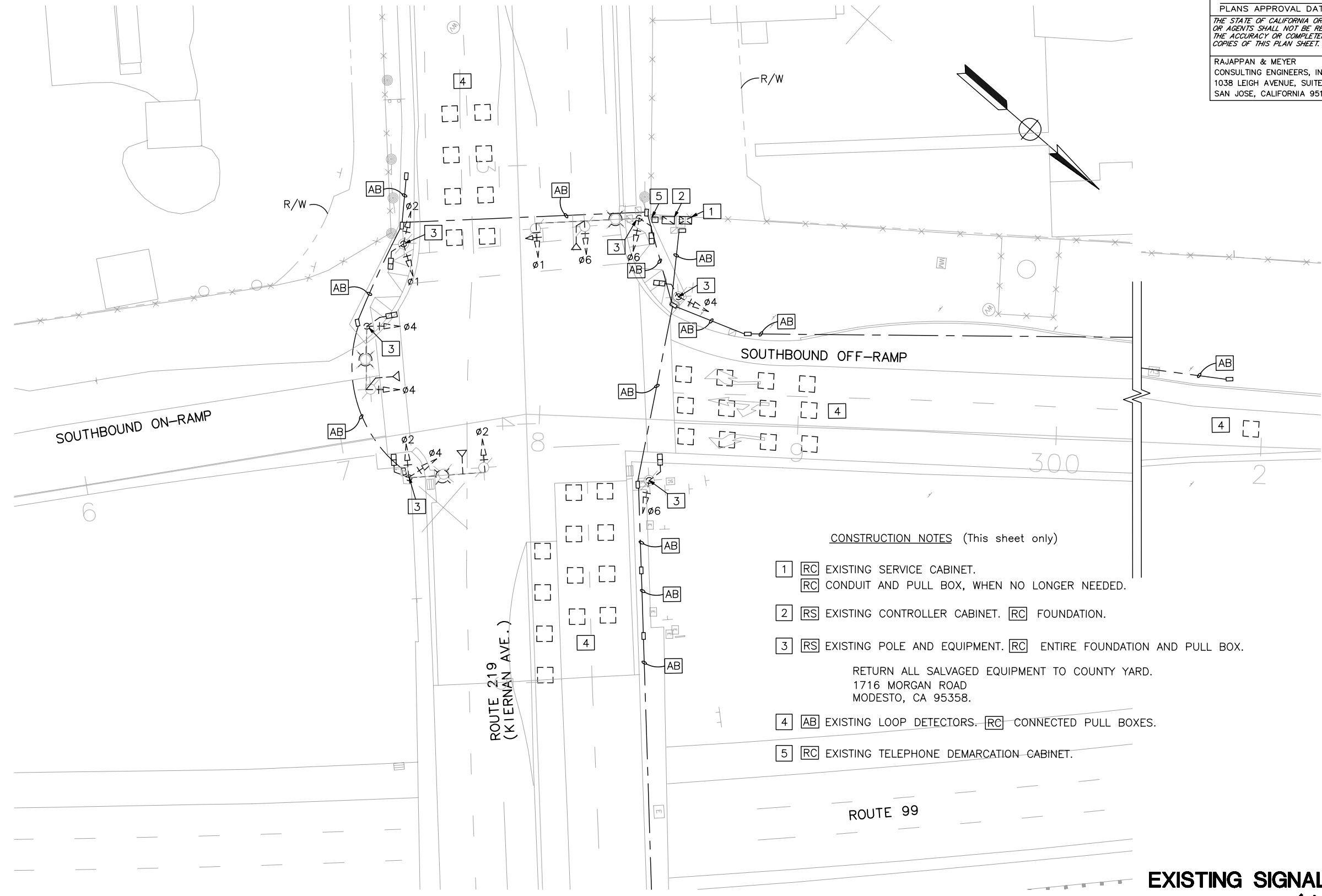
E-15

APPROVED FOR ELECTRICAL WORK ONLY

STATE OF CALIFORNIA — DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHECKED BY:
 SRILAKSHMI SAKHAMURI CHUONG NGUYEN
 REVISED BY: DATE REVISED:

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	290	423

P. Sakhamuri
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



- CONSTRUCTION NOTES** (This sheet only)
- 1 [RC] EXISTING SERVICE CABINET.
[RC] CONDUIT AND PULL BOX, WHEN NO LONGER NEEDED.
 - 2 [RS] EXISTING CONTROLLER CABINET. [RC] FOUNDATION.
 - 3 [RS] EXISTING POLE AND EQUIPMENT. [RC] ENTIRE FOUNDATION AND PULL BOX.
RETURN ALL SALVAGED EQUIPMENT TO COUNTY YARD.
1716 MORGAN ROAD
MODESTO, CA 95358.
 - 4 [AB] EXISTING LOOP DETECTORS. [RC] CONNECTED PULL BOXES.
 - 5 [RC] EXISTING TELEPHONE DEMARCATION CABINET.

**EXISTING SIGNAL AND LIGHTING
 (LOCATION 1)
 SCALE 1"=20'**

E-16

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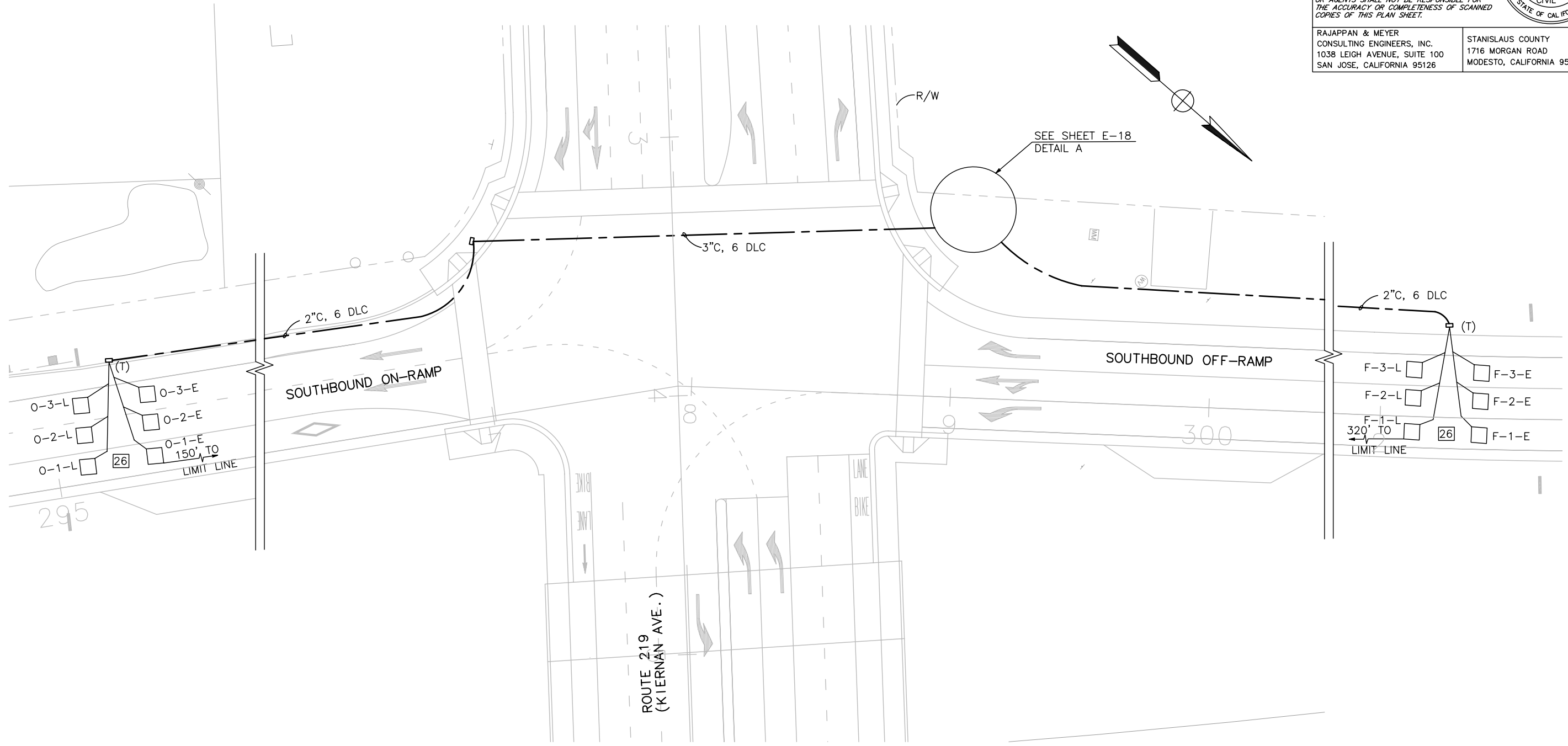
LAST REVISION: DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-18-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 SRI LAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	291	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
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RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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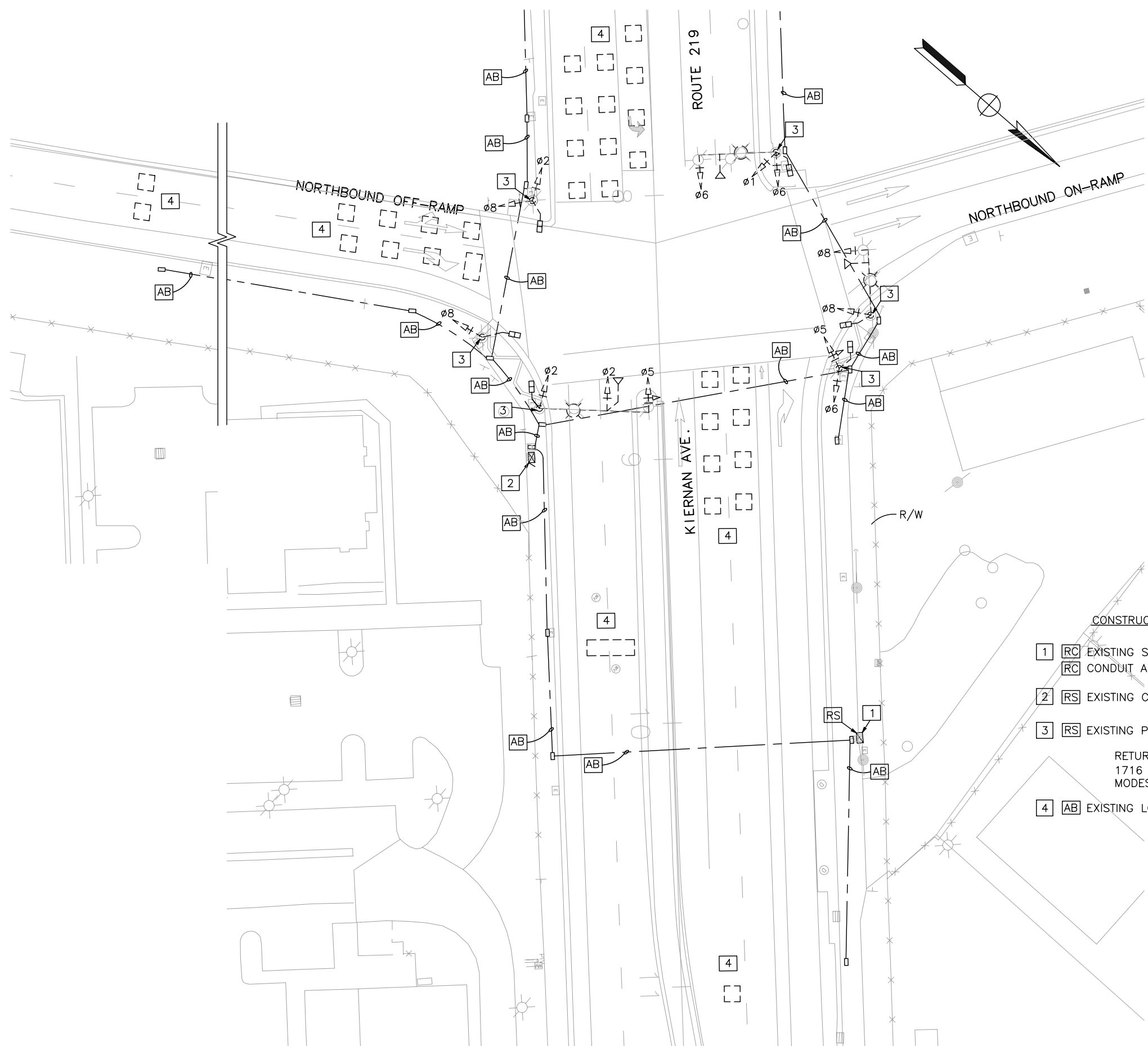


**COUNT STATION
 (LOCATION 1)**
 SCALE 1"=20'
E-17

APPROVED FOR ELECTRICAL WORK ONLY

DATE PLOTTED => \$DATE
 TIME PLOTTED => \$TIME
 LAST REVISION
 05-18-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: CHECKED BY:
 SRILAKSHMI SAKHAMURI: CHUONG NGUYEN
 REVISED BY: DATE REVISED:



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	294	423

P. Sakhamuri 05-18-12
 REGISTERED CIVIL ENGINEER DATE
 9/04/12
 PLANS APPROVAL DATE
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 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 SRILAKSHMI SAKHAMURI
 No. C76343
 Exp. 06/30/12
 CIVIL
 STATE OF CALIFORNIA

- CONSTRUCTION NOTES (This sheet only)**
- 1 [RC] EXISTING SERVICE CABINET. [RC] CONDUIT AND PULL BOX, WHEN NO LONGER NEEDED.
 - 2 [RS] EXISTING CONTROLLER CABINET. [RC] FOUNDATION.
 - 3 [RS] EXISTING POLE AND EQUIPMENT. [RC] ENTIRE FOUNDATION AND PULL BOX.
- RETURN ALL SALVAGED EQUIPMENT TO COUNTY YARD.
 1716 MORGAN ROAD
 MODESTO, CA 95358.
- 4 [AB] EXISTING LOOP DETECTORS. [RC] CONNECTED PULL BOXES.

**EXISTING SIGNAL AND LIGHTING
 (LOCATION 2)
 SCALE 1"=20'**

E-20

APPROVED FOR ELECTRICAL WORK ONLY

LAST REVISION DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME
 05-18-12

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans

CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: [Blank]
 CHECKED BY: [Blank]
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY: [Blank]
 DATE REVISED: [Blank]

NOTES:
 1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	295	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
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 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



COUNT STATION (LOCATION 2)
 SCALE 1"=20'

E-21

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	296	423

P. S. Lakshmi
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

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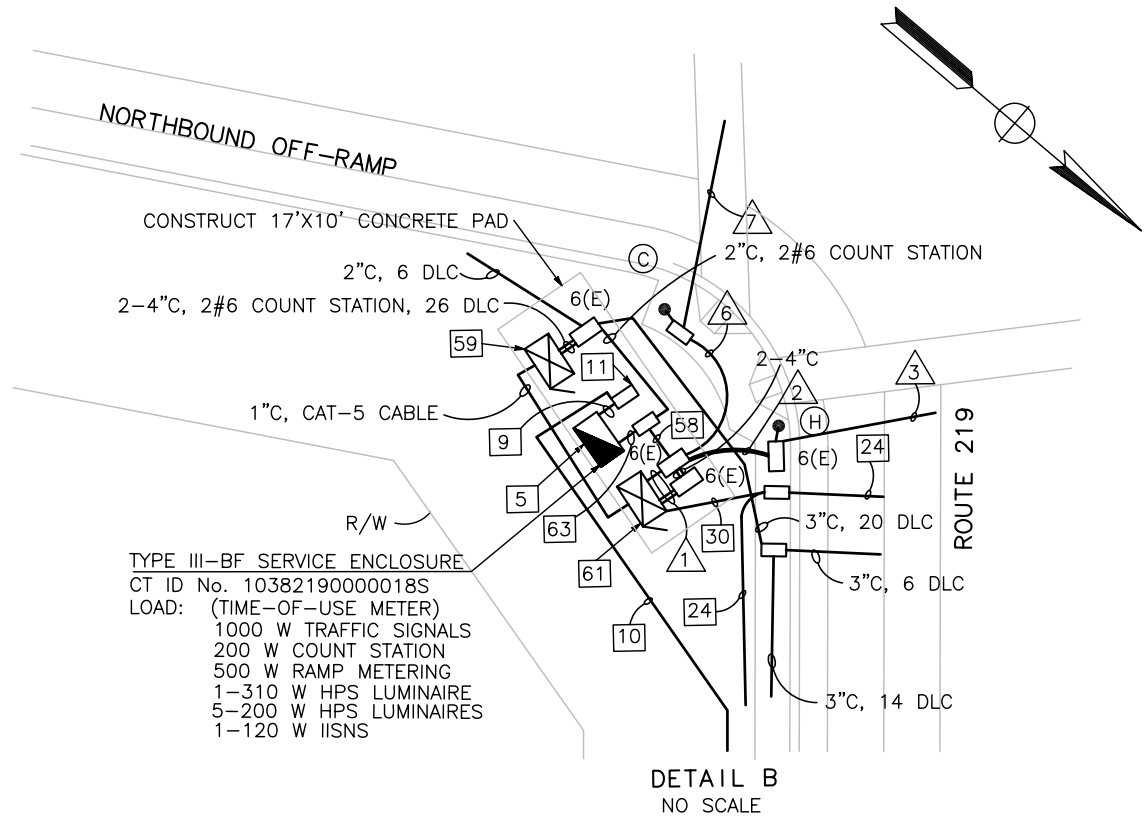
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



EQUIPMENT AND POLE SCHEDULE										
SIGNAL POLE	TYPE	MAST ARM LENGTH		SIGNAL MOUNTING		PPB		PED MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
		SIGNAL MAST ARM	LUMINAIRE MAST ARM	MAST ARM	POLE	ø	ARROW			
(A)	29A-5-100	55'	15'	MAS, MAS	SV-2-TB	6	←	SP-1-T	310	INSTALL R3-18 SIGN.
(B)	1-A	-	-	-	TV-2-T	2	→	SP-1-T	-	
(C)	15TS	-	15'	-	SV-1-T	2	→	SP-1-T	200	
(D)	29-5-100	55'	15'	MAS, MAS	SV-1-T	8	←	SP-1-T	200	INSTALL R73-2
(E)	15TS	-	15'	-	SV-3-TB	8	→	SP-1-T	200	
(F)	24-4-100	35'	12'	MAS, MAS	SV-1-T	6	←	SP-1-T	200	INSTALL IISNS "KIERNAN AVE".

CONDUCTOR SCHEDULE												
CABLE SIZE	ø	POLE	No. OF CONDUCTORS/ RUN No.									
			△1	△2	△3	△4	△5	△6	△7	△8	△9	
3-12 CONDUCTOR CABLE	ø6, ø8, ø6P	6PPB	POLE A	1-1	1-1	1-1	1-1	1-1				
	ø2, ø8, ø2P	2PPB	POLE B	1-1					1-1	1-1		
	ø8, ø8P	2PPB	POLE C	1-1					1-1			
	ø2, ø5, ø2P	8PPB	POLE D	1-1	1-1							
	ø5, ø6, ø8, ø6P	8PPB	POLE E	1-2	1-2	1-2						
	ø8, ø8P	6PPB	POLE F	1-1	1-1	1-1	1-1					
TOTAL SIGNAL CABLES				6-7	4-5	3-4	2-2	1-1	2-2	1-1		
#8		LUMINAIRE		2	2	2	2	2				
#10		IISNS		2	2	2						
#6		SERVICE		2								
#6		RAMP METERING		2	2	2						
DETECTOR CABLES	ø2			6					6	6		
	ø5			2					2	2		
	ø6			9	9	9						
	ø8			9					9			
	EB BIKE			1					1	1		
WB BIKE			1	1	1							
TOTAL DETECTION CABLES				28	10	10			18	9		
EMERGENCY VEHICLE PREEMPTION	EVA			1	1							
	EVC			1	1	1	1	1				
	EVD			1	1	1	1					
TOTAL EVP CABLES				3	3	2	2	1				
CONDUIT SIZE (INCHES)				3-4"	2-4"	2-3"	4"	3"	4"	3"		
% CONDUIT FILL				17%	17%	24%	13%	11%	21%	17%		



**SIGNAL AND LIGHTING (LOCATION 2)
CONDUCTORS & POLE SCHEDULE**

E-22

REVISIONS:
 REVISED BY: SRI LAKSHMI SAKHAMURI
 DATE REVISED: CHUONG NGUYEN
 CALCULATED/DESIGNED BY: KEITH G. MEYER
 CHECKED BY:

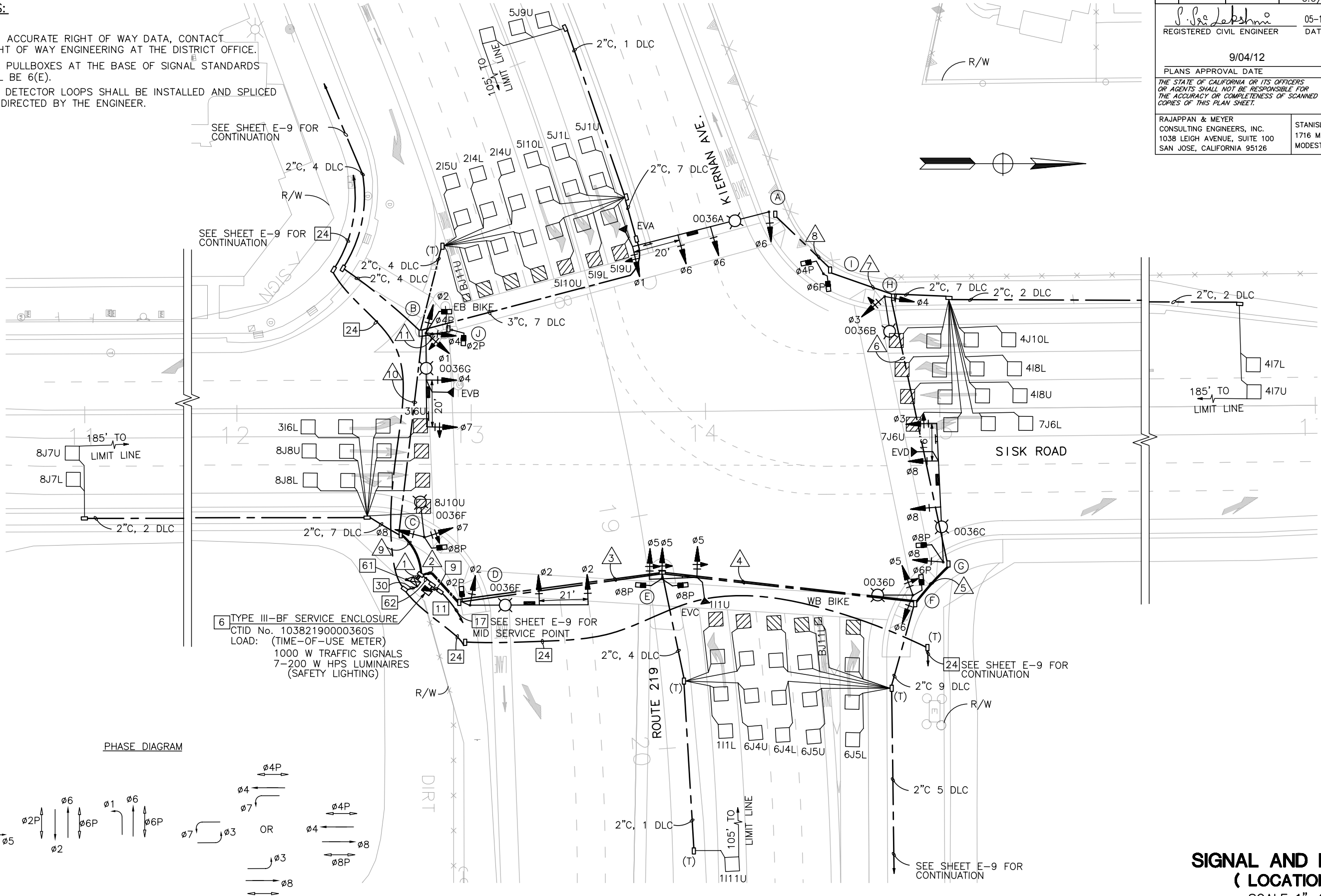
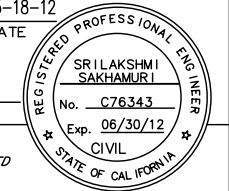
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: SRI LAKSHMI SAKHAMURI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: DATE REVISED

NOTES:

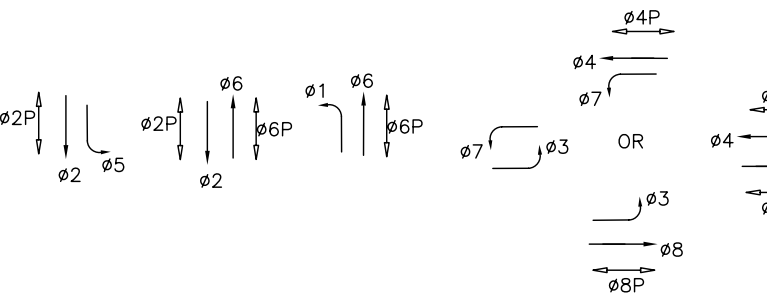
1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
2. ALL PULLBOXES AT THE BASE OF SIGNAL STANDARDS WILL BE 6(E).
3. ALL DETECTOR LOOPS SHALL BE INSTALLED AND SPLICED AS DIRECTED BY THE ENGINEER.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	297	423

P. S. Lakshmi
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



PHASE DIAGRAM




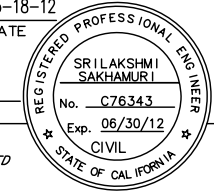
6 TYPE III-BF SERVICE ENCLOSURE
 CTID No. 10382190000360S
 LOAD: (TIME-OF-USE METER)
 1000 W TRAFFIC SIGNALS
 7-200 W HPS LUMINAIRES
 (SAFETY LIGHTING)

DIRT

APPROVED FOR ELECTRICAL WORK ONLY

**SIGNAL AND LIGHTING
 (LOCATION 3)**
 SCALE 1"=20'

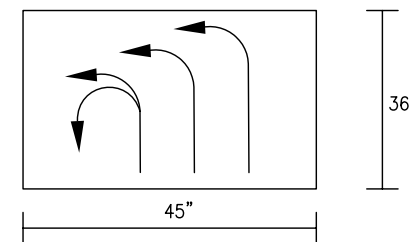
E-23

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	299	423
 REGISTERED CIVIL ENGINEER			05-18-12 DATE		
PLANS APPROVAL DATE 9/04/12					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

CONDUCTOR SCHEDULE

CABLE SIZE	Ø PPB	POLE	No. OF CONDUCTORS/ RUN No.																			
			△1	△2	△3	△4	△5	△6	△7	△8	△9	△10	△11									
3-12 CONDUCTOR CABLE	Ø1, Ø6	4PPB	POLE A	1-1	1-1	1-1	1-1	1-1	1-1	1-1	1-1											
	Ø1, Ø2, Ø4, Ø7, Ø4P	2PPB	POLE B	1-2											1-2	1-2						
	Ø7, Ø8, Ø8P	2PPB	POLE C	1-1											1-1							
	Ø2, Ø2P	8PPB	POLE D	1-1	1-1																	
	Ø5, Ø8P	8PPB	POLE E	1-1	1-1	1-1																
	Ø5, Ø6, Ø6P	8PPB	POLE F	1-1	1-1	1-1	1-1															
	Ø3, Ø8, Ø8P	6PPB	POLE G	1-1	1-1	1-1	1-1	1-1														
	Ø3, Ø4	6PPB	POLE H	1-1	1-1	1-1	1-1	1-1	1-1													
	Ø4P, Ø6P		POLE I	0-1	0-1	0-1	0-1	0-1	0-1	0-1												
	Ø2P	4PPB	POLE J	1-1											1-1	1-1	1-1					
TOTAL SIGNAL CABLES					9-11	6-7	5-6	4-5	3-4	2-3	1-2	1-1	3-4	2-3	1-1							
#8			LUMINAIRE		2	2	2	2	2	2	2	2	2	2								
#10			IISNS		2	2	2	2	2	2	2	2	2	2								
#6			SERVICE	2																		
DETECTOR CABLES																						
	Ø1			3	3	3																
	Ø2			7									7	7								
	Ø3			2									2									
	Ø4			5	5	5	5	5	5													
	Ø5			7									7	7								
	Ø6			9	9	9	8															
	Ø7			2	2	2	2	2	2													
	Ø8			5									5									
	EB BIKE			1									1	1								
	WB BIKE			1	1	1	1															
TOTAL DETECTION CABLES					42	20	20	16	7	7				22	15							
EMERGENCY VEHICLE PREEMPTION																						
	EVA			1	1	1	1	1	1	1												
	EVB			1									1	1								
	EVC			1	1	1																
	EVD			1	1	1	1	1														
TOTAL EVP CABLES					4	3	3	2	2	1	1	1	1	1	1							
CONDUIT SIZE (INCHES)					3-4"	2-4"	2-4"	2-4"	2-3"	4"	3"	3"	2-4"	4"	2"							
% CONDUIT FILL					26%	23%	21%	17%	21%	19%	16%	12%	16%	23%	18%							

EQUIPMENT AND POLE SCHEDULE										
SIGNAL POLE	TYPE	MAST ARM LENGTH		SIGNAL MOUNTING		PPB		PED MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
		SIGNAL MAST ARM	LUMINAIRE MAST ARM	MAST ARM	POLE	Ø	ARROW			
(A)	61A-5-100	60'	15'	MAS, MAS, MAS	SV-1-T	4	←	-	310	INSTALL R73-2 SIGN. INSTALL LED IISNS "SISK ROAD".
(B)	26A-4-100	40'	15'	MAS, MAS	SV-3-TB	2	←	SP-1-T	310	INSTALL R73-2. INSTALL LED IISNS "KIERNAN AVE". INSTALL R13A ON THE POLE
(C)	15TS	-	15'	-	SV-2-T	2	→	SP-1-T	200	
(D)	29-5-100	50'	15'	MAS, MAS	SV-1-T	8	←	SP-1-T	200	INSTALL LED IISNS "SISK ROAD".
(E)	TYPE 33	LEFT 15' RIGHT 5'	-	MAS, MAS	SV-1-T	8	↔	SP-2-T	-	INSTALL DETAIL C 45 x 36.
(F)	15TS	-	15'	-	SV-2-T	8	←	SP-1-T	200	
(G)	61-5-100	60'	15'	MAS, MAS, MAS	SV-1-T	6	←	SP-1-T	200	INSTALL R73-3. INSTALL LED IISNS "KIERNAN AVE".
(H)	15TS	-	15'	-	SV-2-T	6	→	-	200	INSTALL R13A
(I)	1-A (7')	-	-	-	-	-	-	TP-2-T	-	
(J)	1-A (7')	-	-	-	-	4	←	TP-1-T	-	



DETAIL C

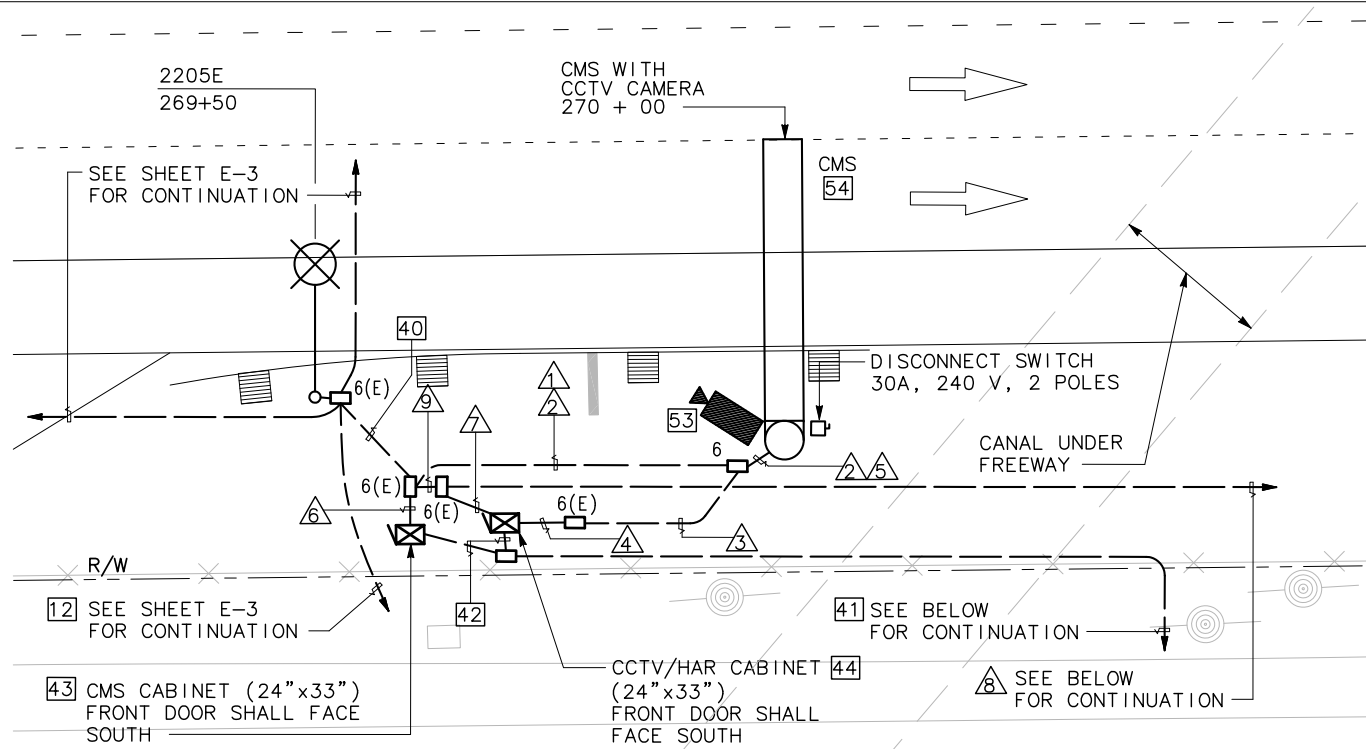
SIGNAL AND LIGHTING (LOCATION 3) CONDUCTORS & POLE SCHEDULE

E-25

NOTES:

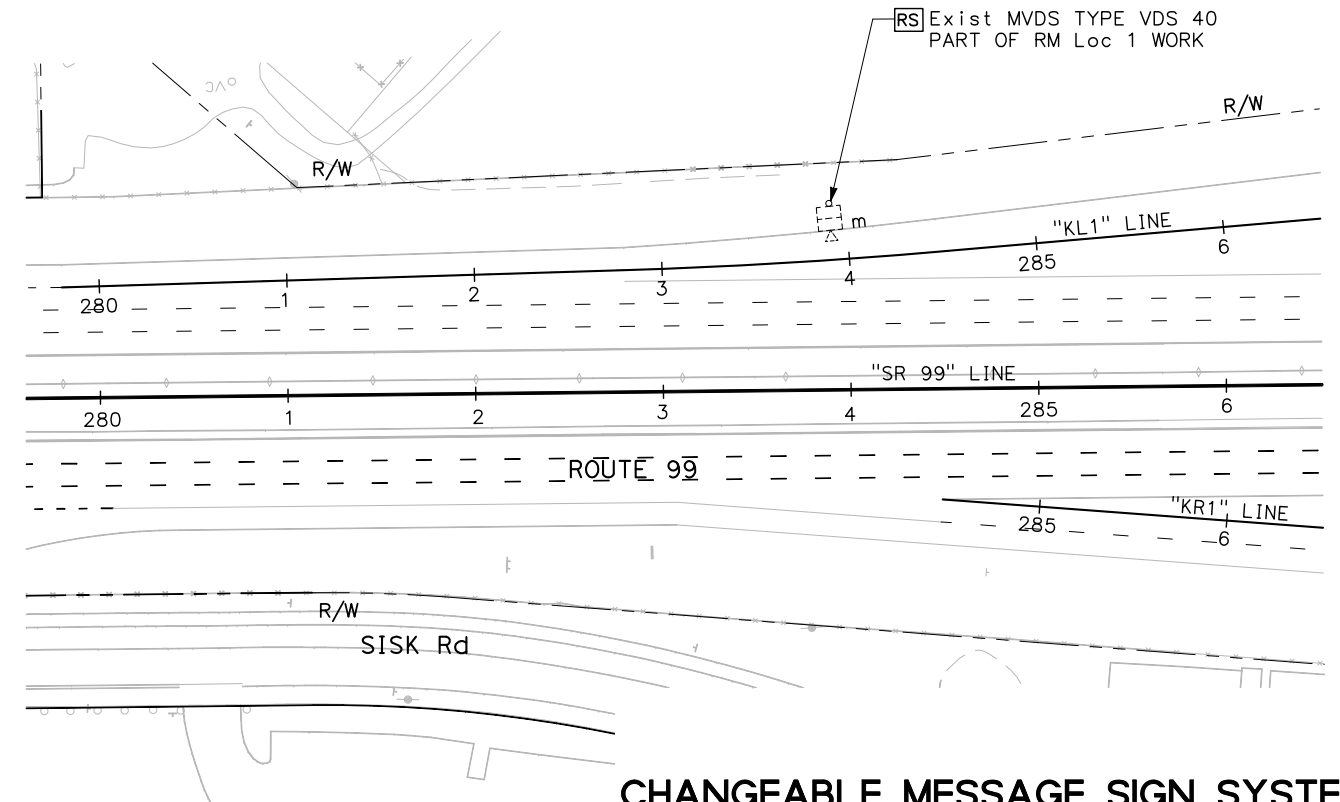
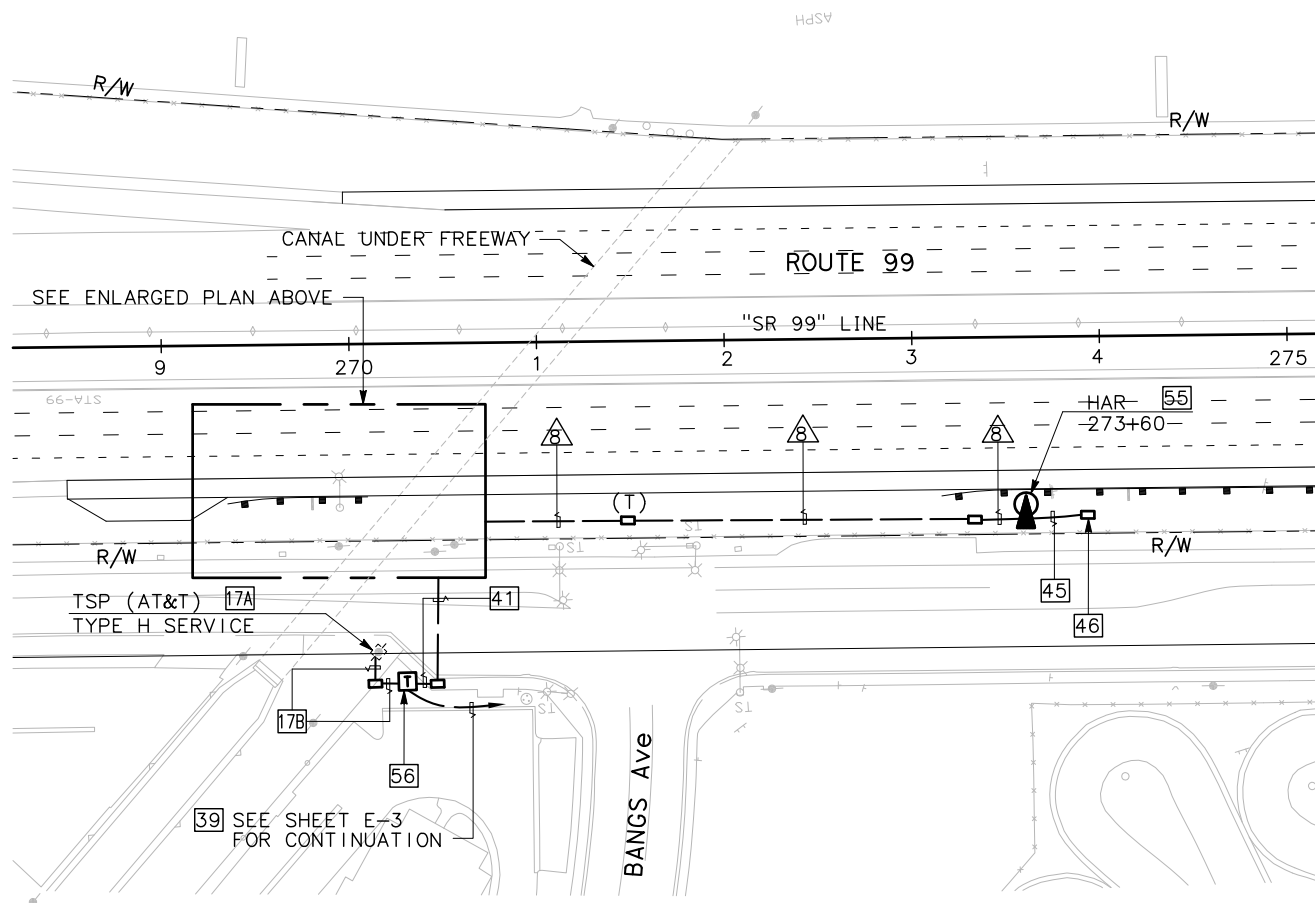
- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR LEGEND, SYMBOLS AND ABBREVIATIONS, SEE SHEET E-1 AND E-2.

CABLE SCHEDULE		CONDUCTOR RUN & SIZE								
AWG	CIRCUIT	△1	△2	△3	△4	△5	△6	△7	△8	△9
#6	334 CABINET CMS						2			
#6	334 CABINET CCTV							2		2
#6	CMS LAMPS		2							
#6	CMS LAMP NEUTRAL		1							
#6	CMS GROUND		1							
ACC								1	1	
	SF HARNESS #4	1				1	1			
	SF HARNESS #5	1				1	1			
	CAMERA COMPOSITE CABLE			1	1					
	CONDUIT SIZE	3"C	2"C	3"C	3"C	3"C	2"C	2"C	2"	3"C



ENLARGED PLAN

SCALE: 1" = 10'



**CHANGEABLE MESSAGE SIGN SYSTEM
CLOSED CIRCUIT TELEVISION SYSTEM
HIGHWAY ADVISORY RADIO SYSTEM**

SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	300	423
David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12		PLANS APPROVAL DATE			
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
G4 Caltrans

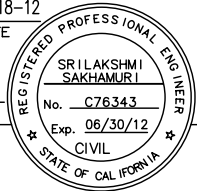
REVISOR: HENRY ROBINO
DATE: DAVE CLOW
CALCULATED/DESIGNED BY: HENRY ROBINO
CHECKED BY: DAVE CLOW

CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI

REVISOR: HENRY ROBINO
DATE: DAVE CLOW
CALCULATED/DESIGNED BY: HENRY ROBINO
CHECKED BY: DAVE CLOW

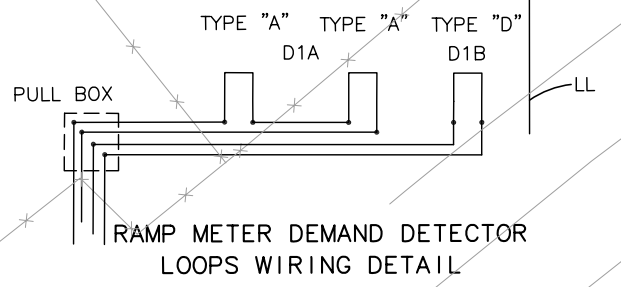
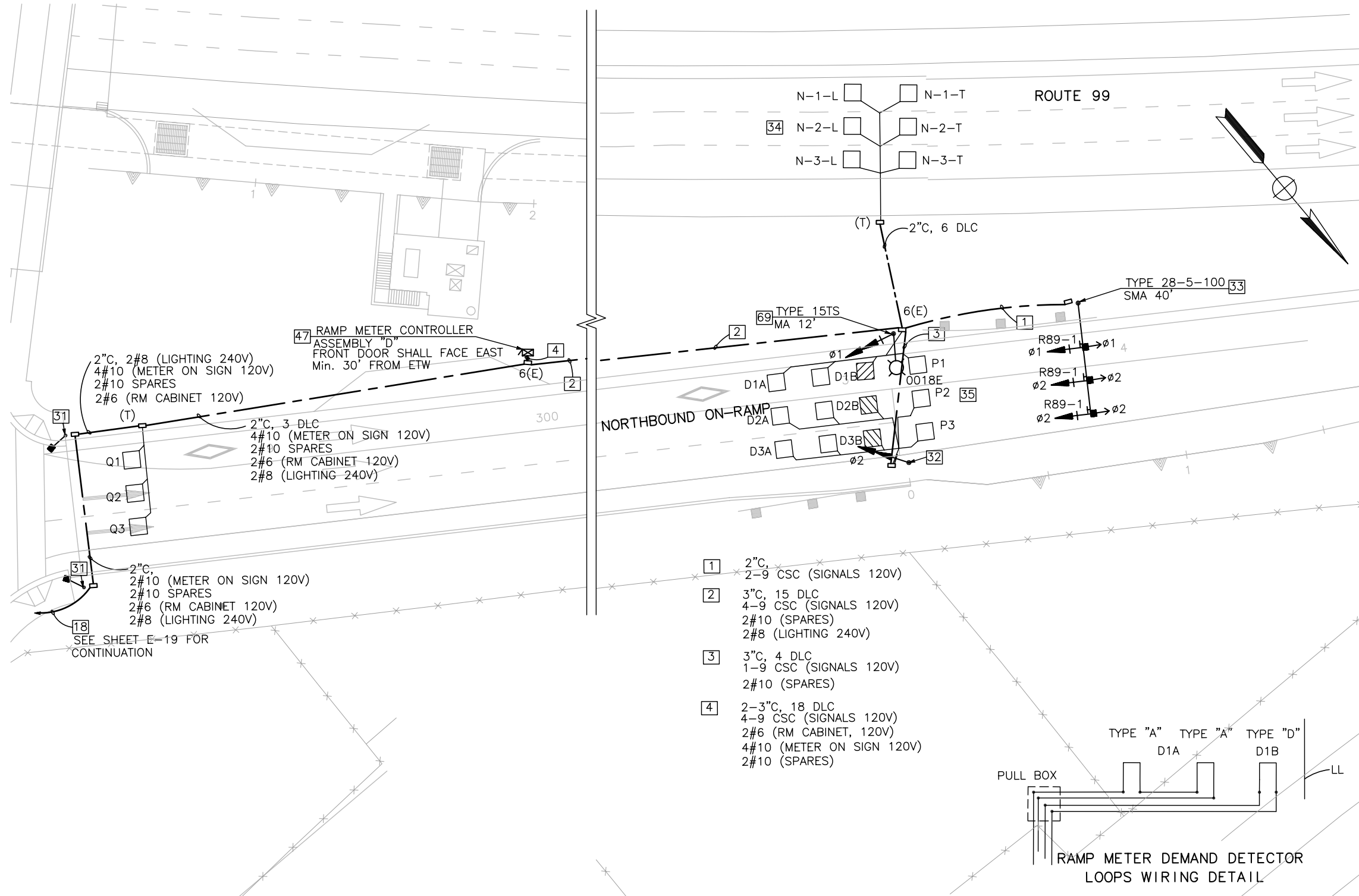
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	301	423

<i>P. S. Lakshmi</i>		05-18-12
REGISTERED CIVIL ENGINEER		DATE
9/04/12		
PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



NOTES:

- TYPE 28-5-100, SIGNAL MASTARM SHOULD BE MOUNTED ON THE LEFT SIDE OF THE POLE. TERMINAL COMPARTMENT TO BE USED TO TERMINATE THE SIGNAL CABLES.



- 1 2" C, 2-9 CSC (SIGNALS 120V)
- 2 3" C, 15 DLC
4-9 CSC (SIGNALS 120V)
2#10 (SPARES)
2#8 (LIGHTING 240V)
- 3 3" C, 4 DLC
1-9 CSC (SIGNALS 120V)
2#10 (SPARES)
- 4 2-3" C, 18 DLC
4-9 CSC (SIGNALS 120V)
2#6 (RM CABINET, 120V)
4#10 (METER ON SIGN 120V)
2#10 (SPARES)

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DRAWN BY
 CHECKED BY
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED



APPROVED FOR ELECTRICAL WORK ONLY

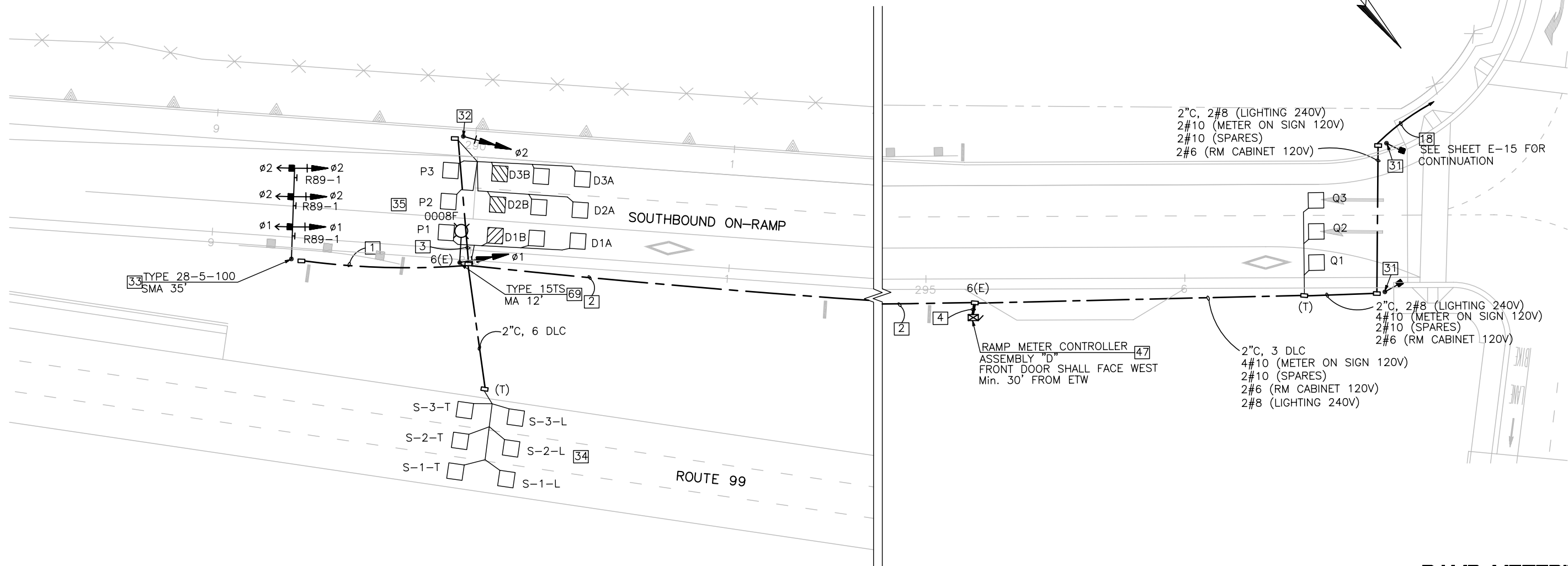
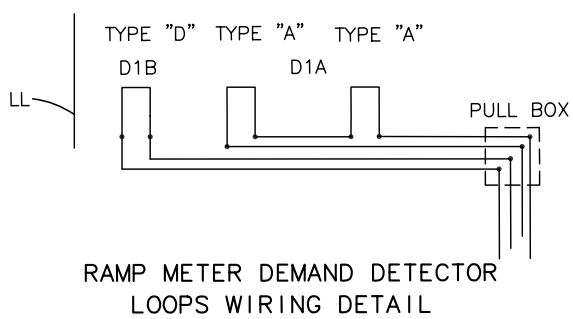
**RAMP METERING
(LOCATION 1)**
SCALE 1"=20'
E-27

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-D
 DESIGNED BY
 CHECKED BY
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTES:

1. TYPE 28-5-100, SIGNAL MASTARM SHOULD BE MOUNTED ON THE LEFT SIDE OF THE POLE. TERMINAL COMPARTMENT TO BE USED TO TERMINATE THE SIGNAL CABLES.

- 1 2"C, 2-9 CSC (SIGNALS 120V)
- 2 3"C, 15 DLC
4-9 CSC (SIGNALS 120V)
2#10 (SPARES)
2#8 (LIGHTING 240V)
- 3 3"C, 4 DLC
1-9 CSC (SIGNALS 120V)
2#10 (SPARES)
- 4 2-3"C, 18 DLC
4-9 CSC (SIGNALS 120V)
2#6 (RM CABINET, 120V)
4#10 (METER ON SIGN 120V)
2#10 (SPARES)



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	302	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER
 SRILAKSHMI SAKHAMURI
 No. C76343
 Exp. 06/30/12
 CIVIL
 STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
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**RAMP METERING
 (LOCATION 2)
 SCALE 1"=20'
 E-28**

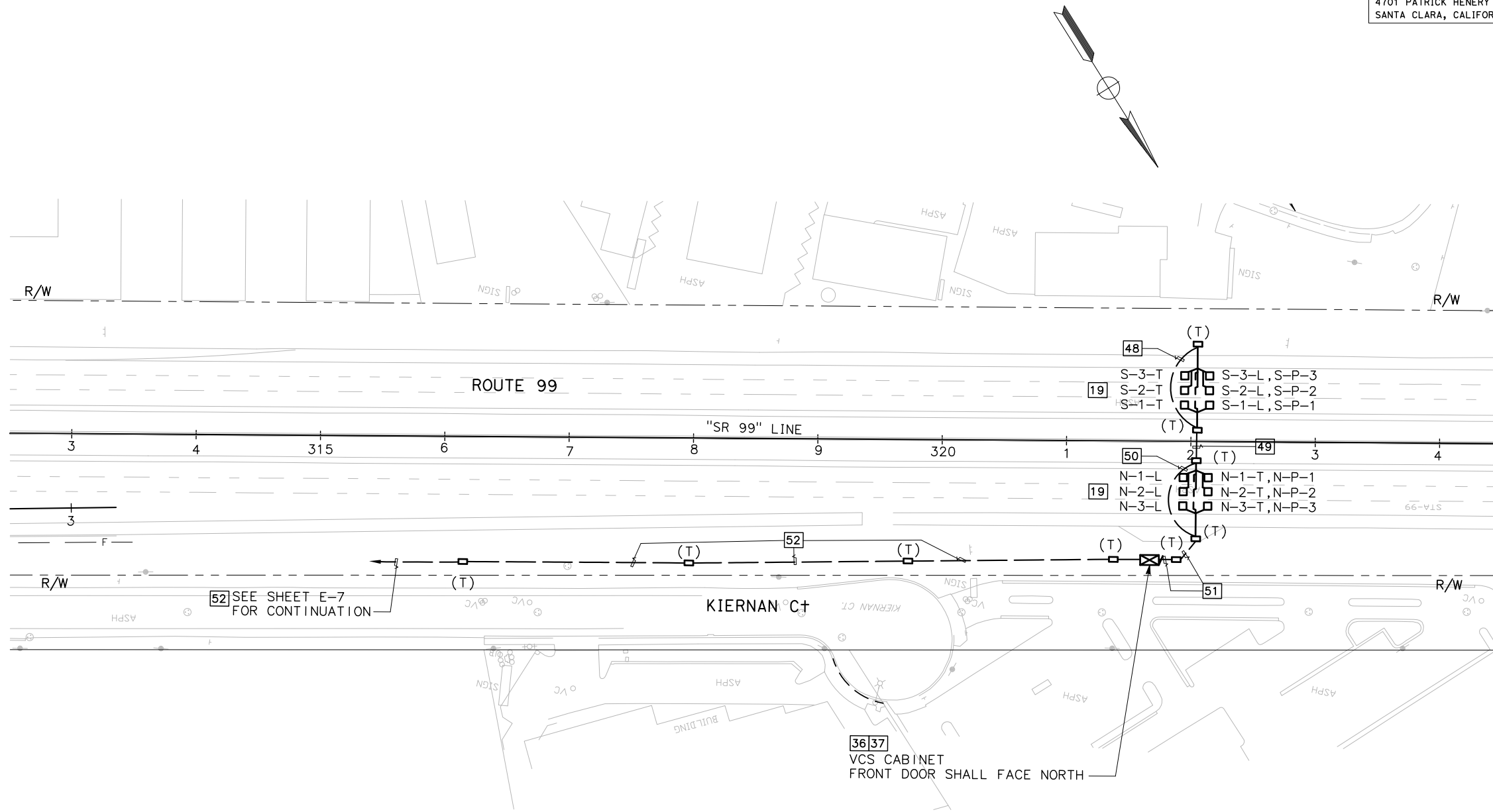
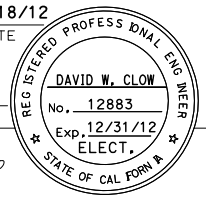
APPROVED FOR ELECTRICAL WORK ONLY

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR LEGEND, SYMBOLS AND ABBREVIATIONS, SEE SHEET E-1 AND E-2.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	303	423

<i>David W. Clow</i>	5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



VEHICLE CLASSIFICATION STATION

SCALE: 1" = 50'

E-29

APPROVED FOR ELECTRICAL WORK ONLY.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	REVISOR	DATE
Caltrans	HENRY ROBINO	DAVE CLOW
CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	DATE
KEN NGAI		
	DESIGNED BY	DATE
	REVISOR	DATE
	DESIGNED BY	DATE

LAST REVISION DATE PLOTTED => \$DATE
05-18-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	304	423

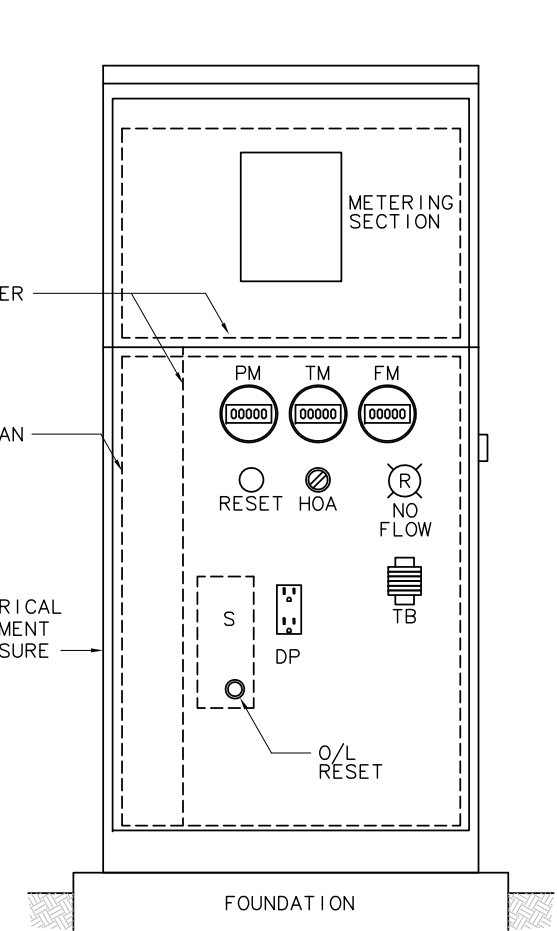
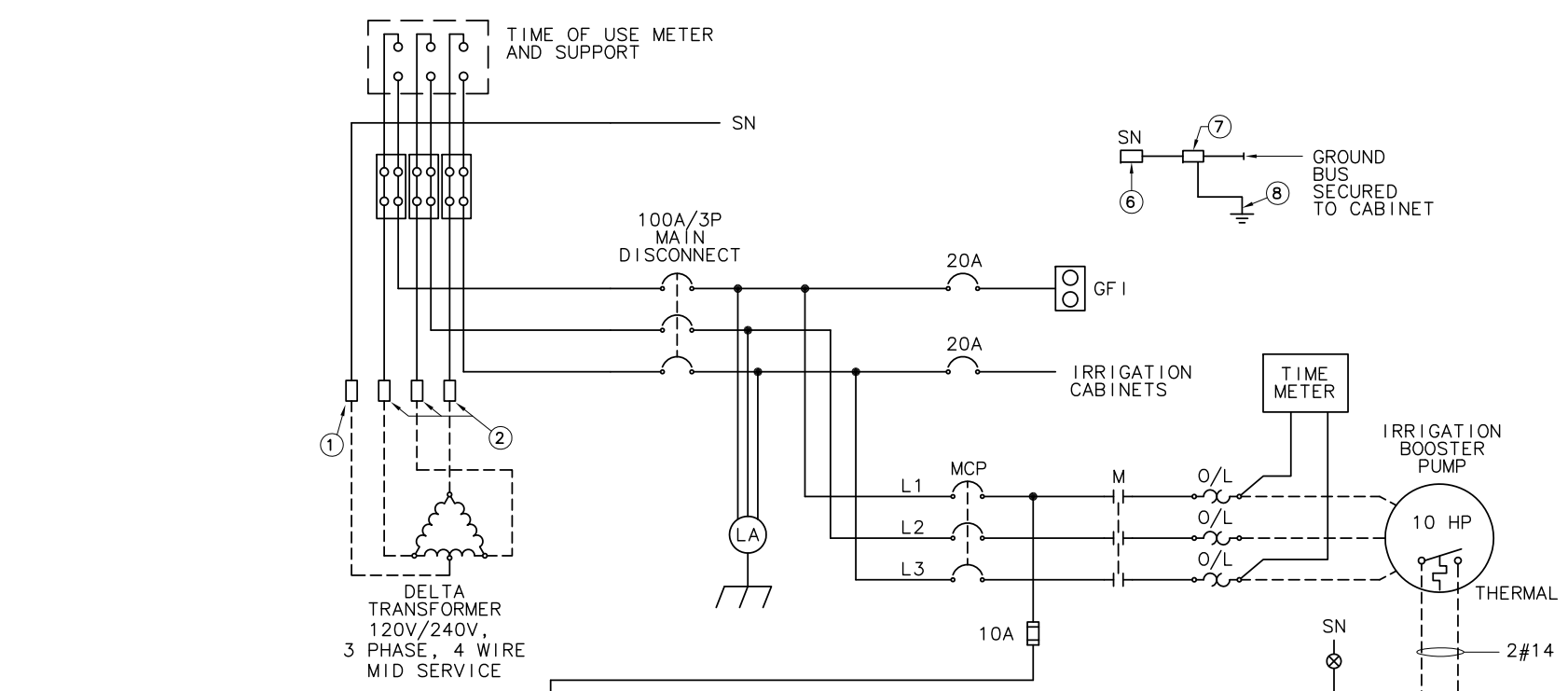
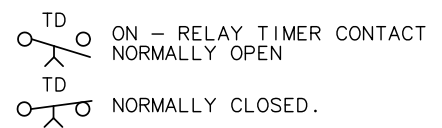
<i>David W. Clow</i> 5/18/12	
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

GENERAL NOTES:

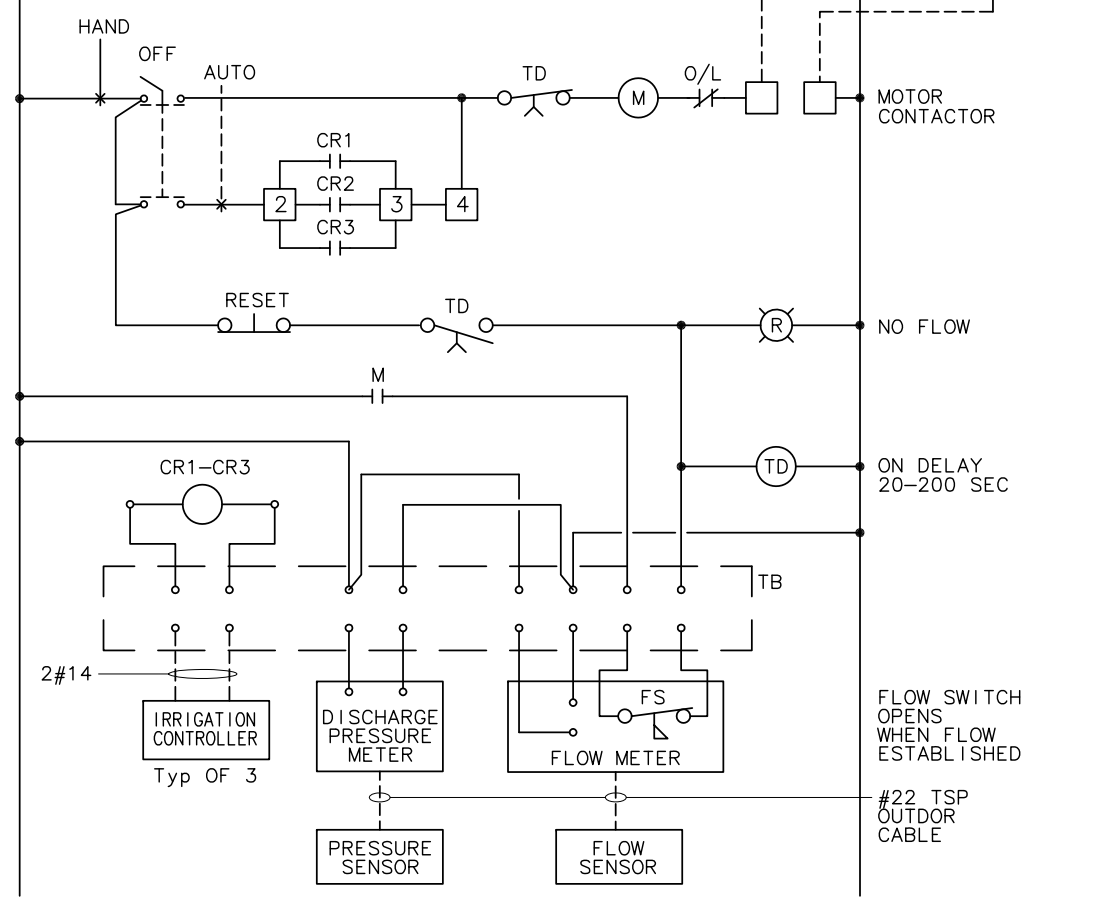
- SEE CALTRANS STANDARD PLANS ES-2C AND ES-2D FOR EQUIPMENT ITEMS AND OTHER DETAILS.

LEGEND:

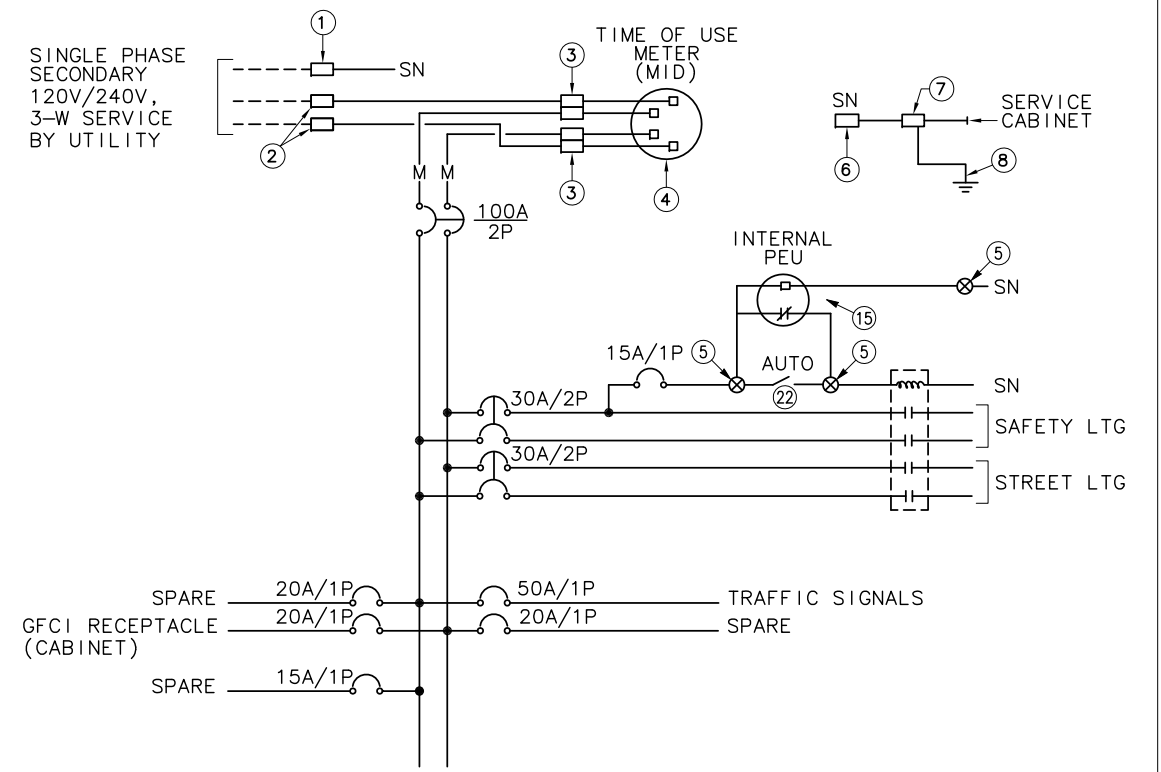
- CR CONTROL RELAY
- DP DUPLEX RECEPTACLE
- FM FLOW METER
- FS FLOW SWITCH
- HOA HAND OFF AUTO
- LA LIGHTNING ARRESTOR
- M MOTOR STARTER
- MCP MOTOR CIRCUIT PROTECTOR
- O/L OVERLOAD
- PM PRESSURE METER
- S STARTER
- TB TERMINAL BLOCK
- TD TIME DELAY
- TM TIME METER
- TSP TWISTED SHIELDED PAIR



IRRIGATION PUMP CONTROL CABINET



120/240 V, 3 PHASE, 4-WIRE CALTRANS IRRIGATION CONTROL CABINET



120/240 V SERVICE WIRING DIAGRAM

COUNTY SYSTEM
(SALIDA BLVD AND BROADWAY Ave)

SERVICE EQUIPMENT WIRING DIAGRAM TYPE III-A SERIES

E-30

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISOR: HENRY ROBINO
 DATE: 7/2/2010

BORDER LAST REVISED 7/2/2010

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DGN FILE => #REQUEST

RELATIVE BORDER SCALE IS IN INCHES

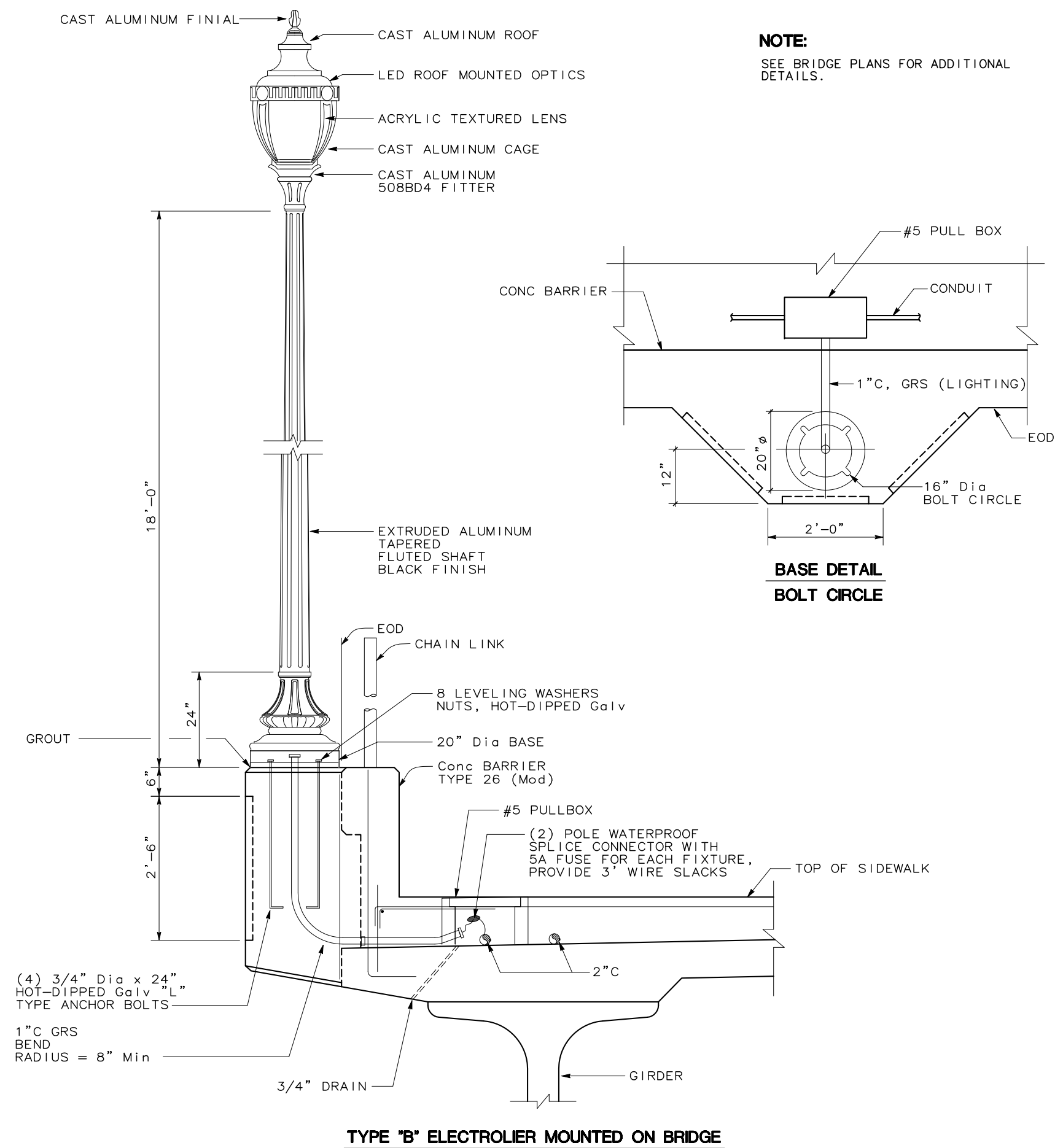
UNIT 1457

PROJECT NUMBER & PHASE 100000100

CTID No. 10382190000080L
 (SB OFF-RAMP AT KIERNAN)(SHEET E-5)
 APPROVED FOR ELECTRICAL WORK ONLY.

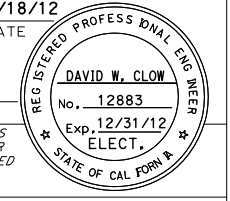
LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	REVISOR	DATE
	KEN NGAI	CHECKED BY	DAVE CLOW	
GILTRANS	DESIGNED BY	DATE	REVISOR	DATE
	HENRY ROBINO	DAVE CLOW		



NOTE:
SEE BRIDGE PLANS FOR ADDITIONAL DETAILS.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	305	423
David W. Clow			5/18/12		
REGISTERED ELECTRICAL ENGINEER			DATE		
9/04/12					
PLANS APPROVAL DATE					
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



TYPE "B" ELECTROLIER MOUNTED ON BRIDGE

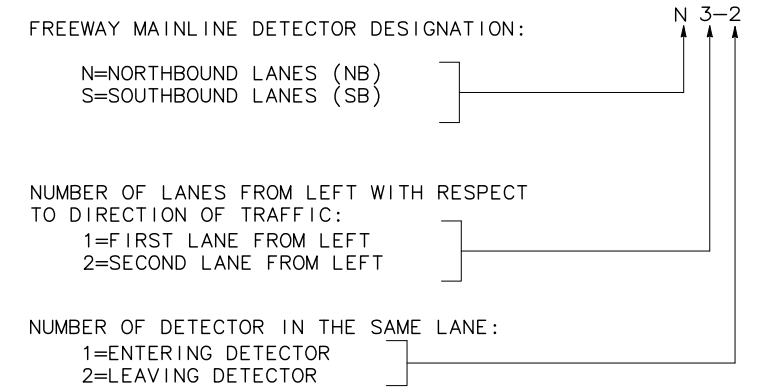
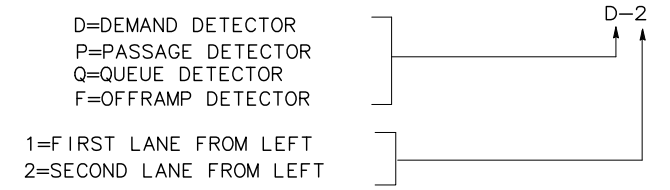
APPROVED FOR ELECTRICAL WORK ONLY.

ELECTRICAL DETAILS
(LIGHTING - COUNTY)
NO SCALE

E-31

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	306	423
David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12					
PLANS APPROVAL DATE					
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

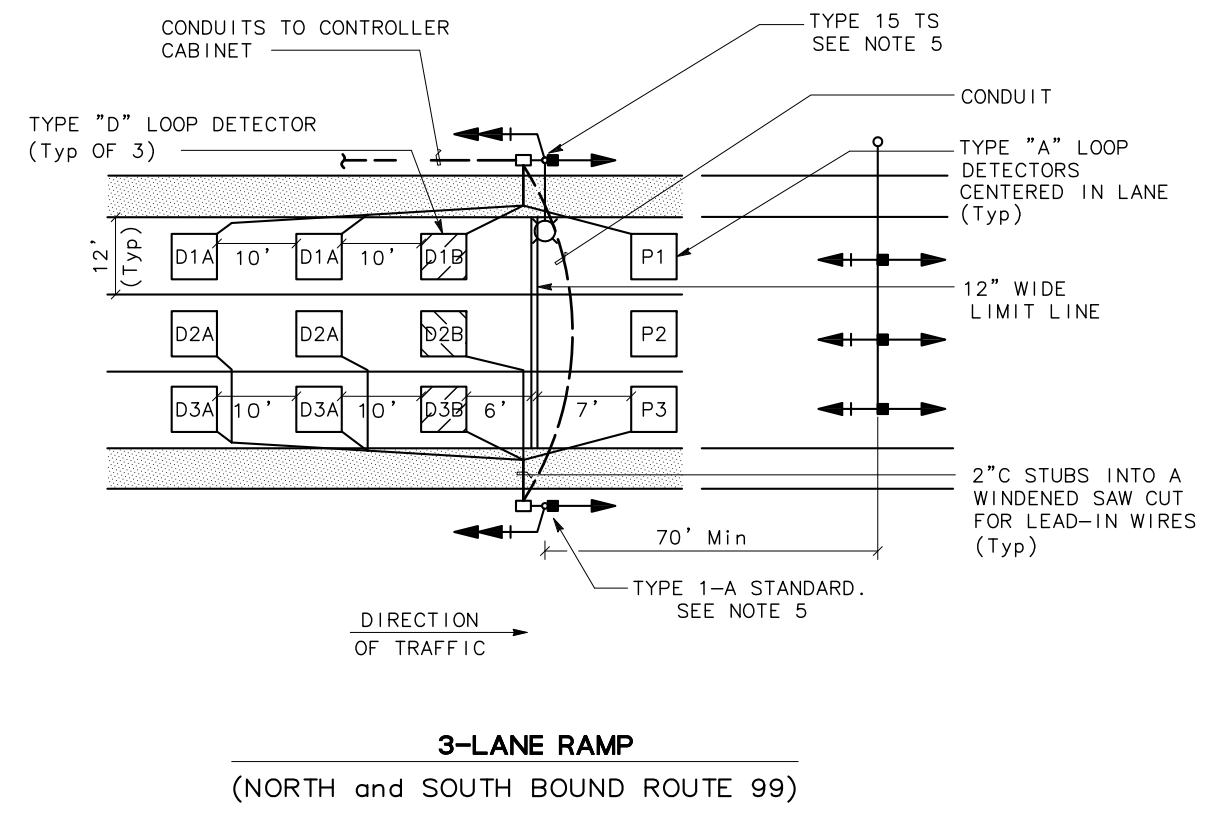
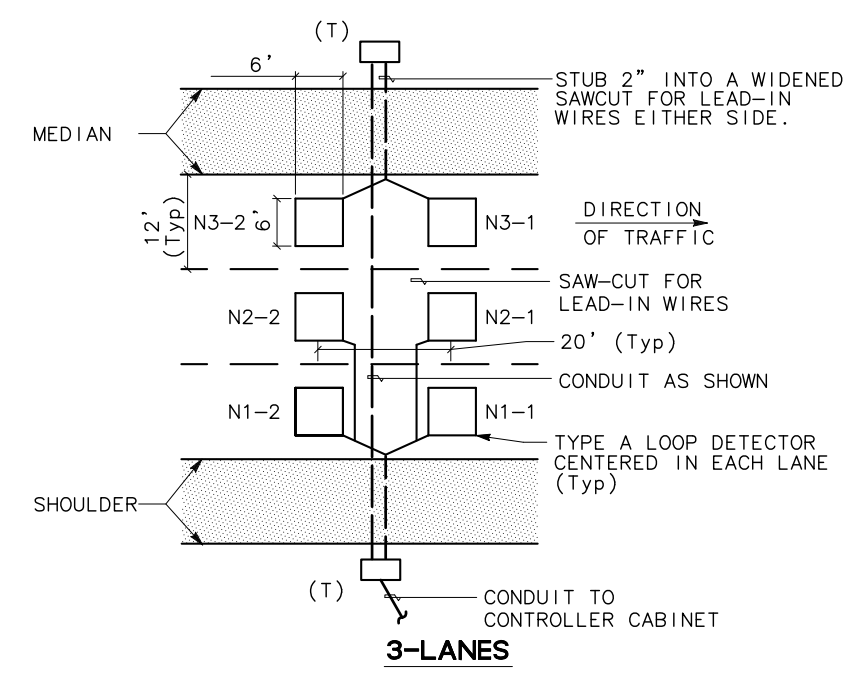
RAMP DETECTOR DESIGNATION:



NOTES

- SEE ES-5A AND 5B FOR ADDITIONAL DETAILS.
- LEAD-IN WIRES SHALL BE SPLICED IN NEAREST PULL BOX.
- COORDINATE EXACT LOCATION OF LOOP DETECTORS WITH THE ENGINEER.
- SEE PD PLANS FOR EXACT LOCATION OF LIMIT LINES.
- INSTALL STANDARD 12 INCHES DOWN STREAM OF THE LIMIT LINE AND 3.28 Ft FROM THE EDGE OF SHOULDER. WITH SIGNAL HEAD AND SIGNAL MOUNT AS SHOWN ON DETAIL 1 ON SHEET E-33.

DETECTOR IDENTIFICATION



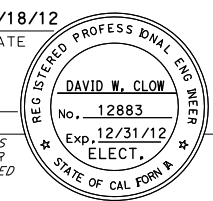
ELECTRICAL DETAILS
(RAMP METERING SYSTEM)
NO SCALE

APPROVED FOR ELECTRICAL WORK ONLY.

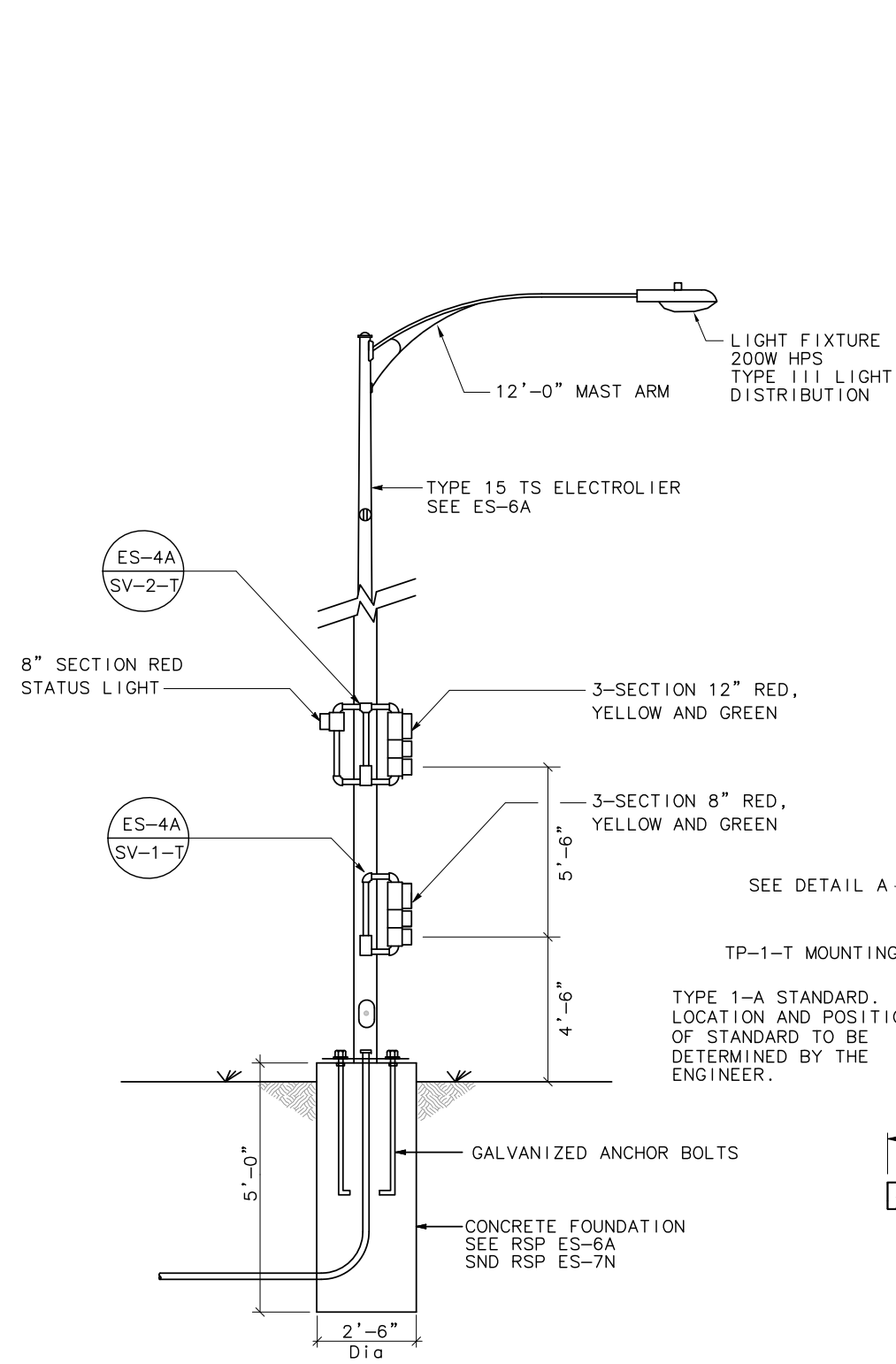
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Ken Ngai
Henry Robino
Dave Clow
Alliance Engineering Consulting, Inc.

LAST REVISION DATE PLOTTED => \$DATE 05-18-12 TIME PLOTTED => \$TIME

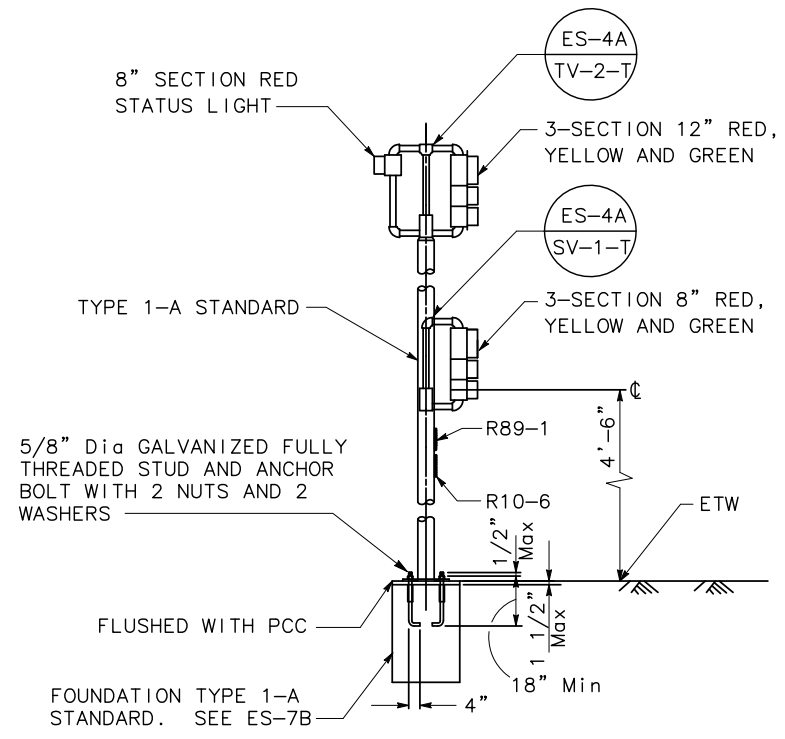
Dist	COUNTY	ROUTE	POST MILES	SHEET	TOTAL
10	Sta	99, 219	22.0/23.0, 0.0/0.5	307	423
David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12					
PLANS APPROVAL DATE					
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



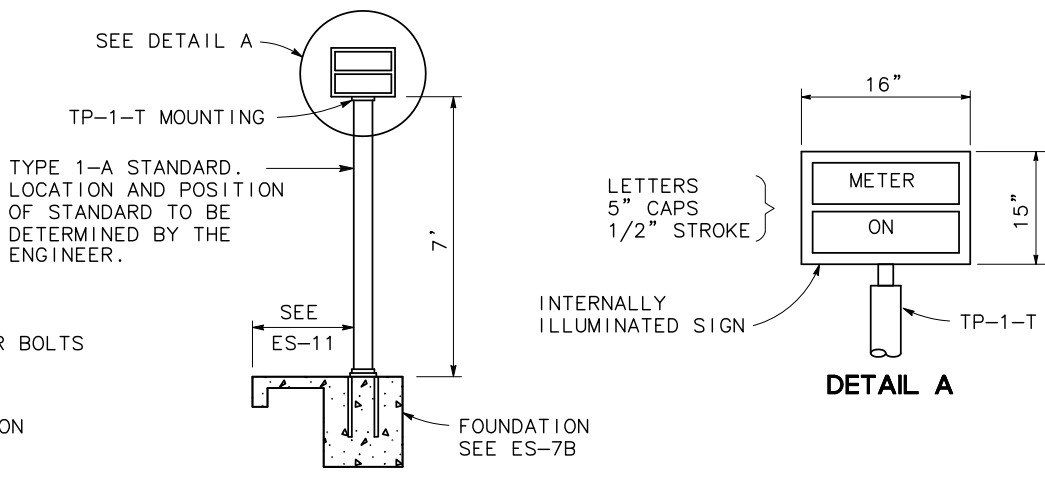
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISOR: HENRY ROBINO
 DATE: DAVE CLOW



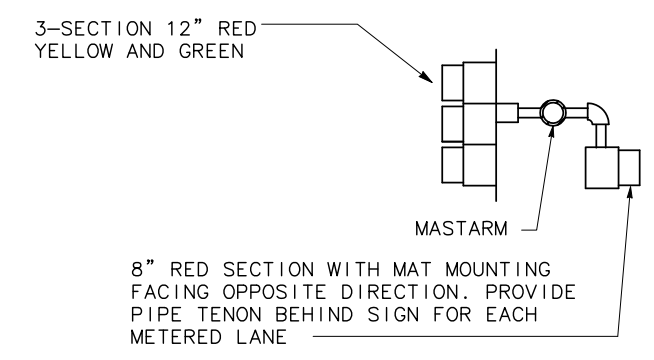
4
-
RAMP METER SIGNAL,
TYPE 15 TS POLE MOUNTED



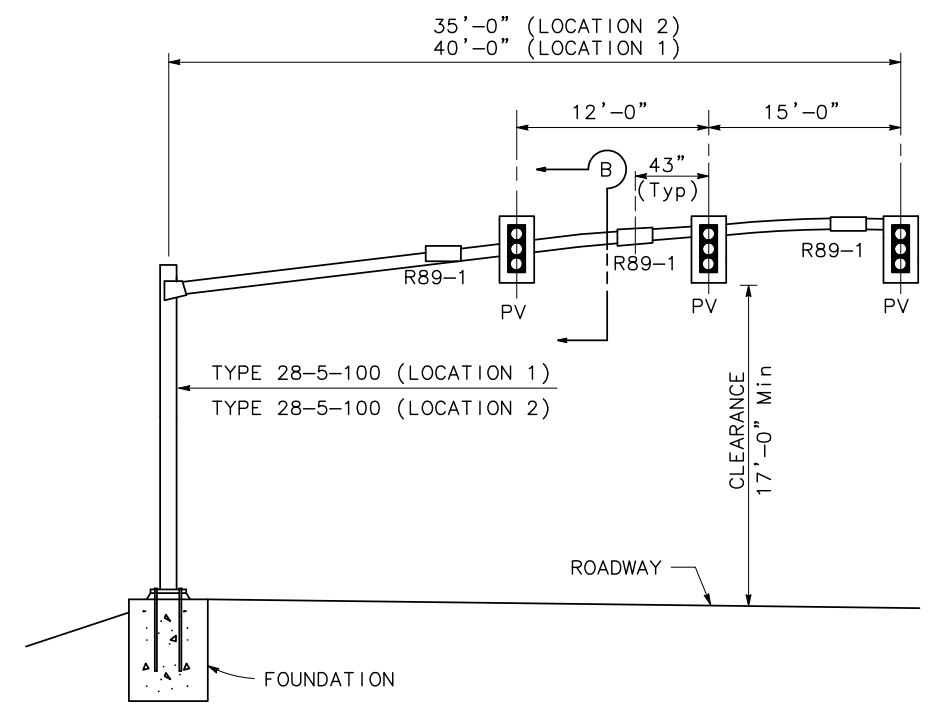
1
-
RAMP METER SIGNAL,
TYPE 1-A POLE MOUNTED



2
-
METER-ON SIGN DETAIL



SECTION B



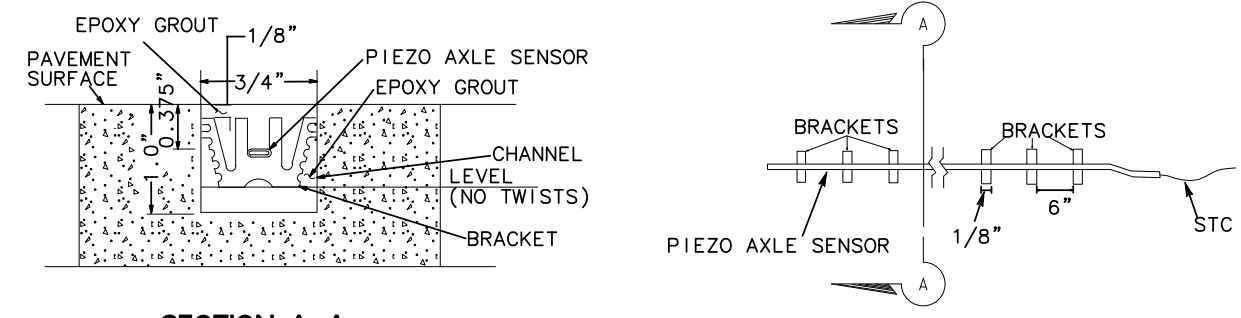
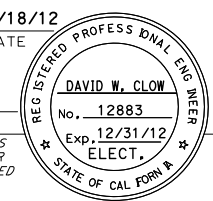
3
-
RAMP METER SIGNAL,
MAST ARM MOUNTED
SEE SHEET E-27 (LOCATION 1) and SHEET E-28 (LOCATION 2)

**ELECTRICAL DETAILS
 (RAMP METERING SYSTEM)**
 NO SCALE

APPROVED FOR ELECTRICAL WORK ONLY.

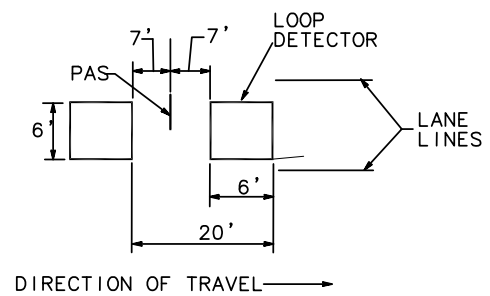
LAST REVISION DATE PLOTTED => \$DATE 05-18-12 TIME PLOTTED => \$TIME

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
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David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12		PLANS APPROVAL DATE			
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		

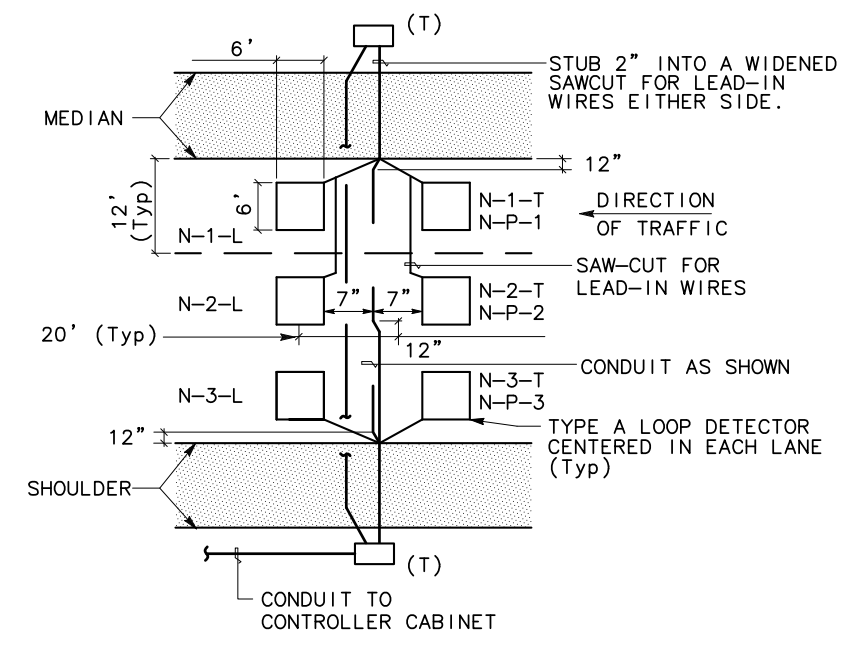


SECTION A-A

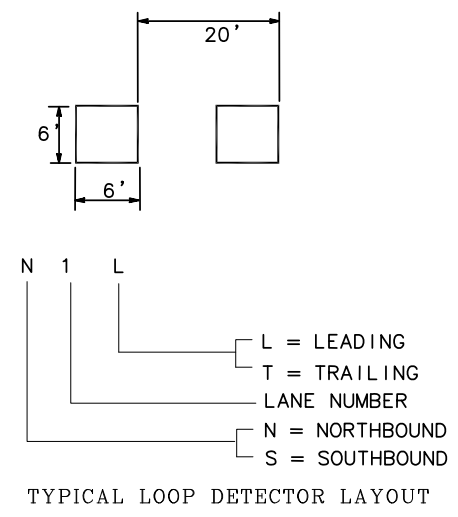
PIEZO AXLE SENSOR INSTALLATION



TYPICAL LANE LAYOUT FOR PAS AND LOOP DETECTOR



3-LANES



TYPICAL LOOP DETECTOR LAYOUT
LOOP DETECTOR IDENTIFICATION

ELECTRICAL DETAILS
(VEHICLE CLASSIFICATION STATION)
NO SCALE

APPROVED FOR ELECTRICAL WORK ONLY.

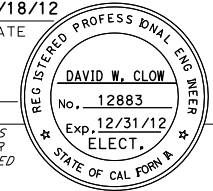
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Ken Ngai
Henry Robino
Dave Clow
Alliance Engineering Consulting, Inc.

LAST REVISION DATE PLOTTED => \$DATE 05-18-12 TIME PLOTTED => \$TIME

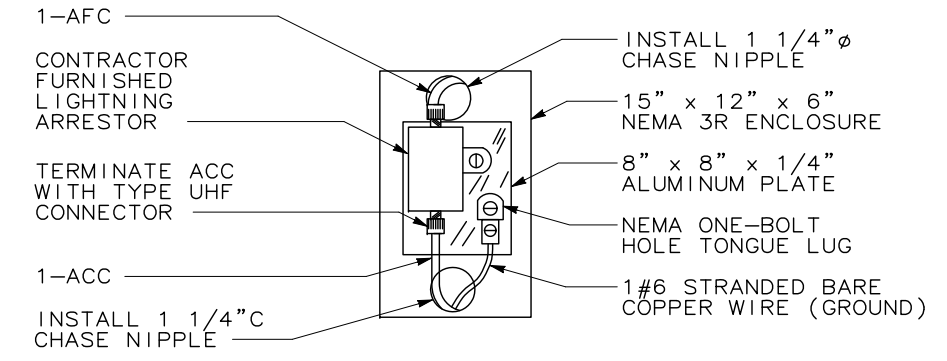
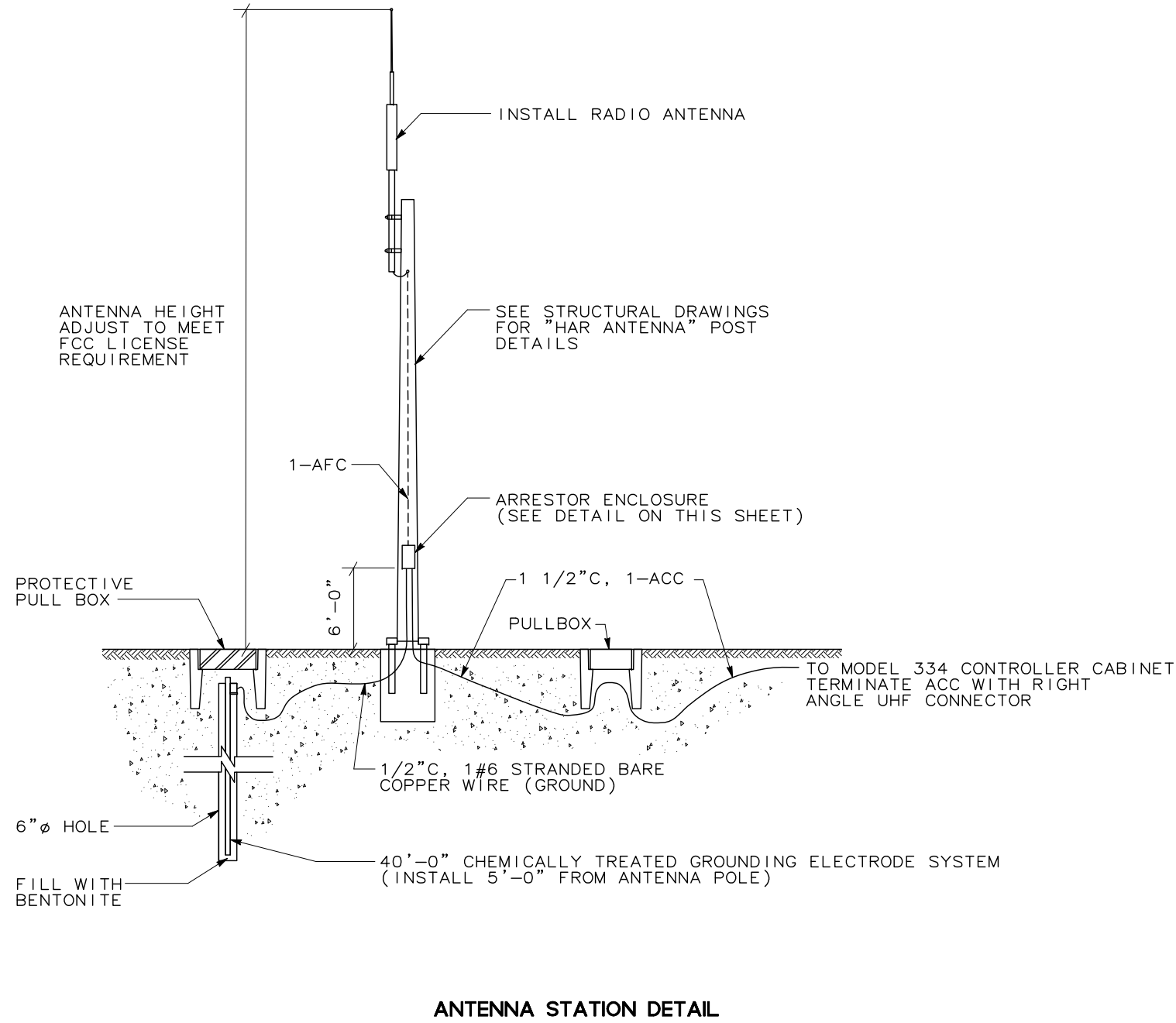
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	309	423

<i>David W. Clow</i>		5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 Ken Ngai
 CONSULTANT FUNCTIONAL SUPERVISOR
 Ken Ngai
 CHECKED BY
 Ken Ngai
 DESIGNED BY
 Ken Ngai
 HENRY ROBINO
 DAVE CLOW
 REVISOR
 HENRY ROBINO
 DAVE CLOW
 DATE REVISION
 DATE REVISION



ARRESTOR ENCLOSURE DETAIL

**ELECTRICAL DETAILS
(HAR ANTENNA, ARRESTOR ENCLOSURE)**
NO SCALE

APPROVED FOR ELECTRICAL WORK ONLY.

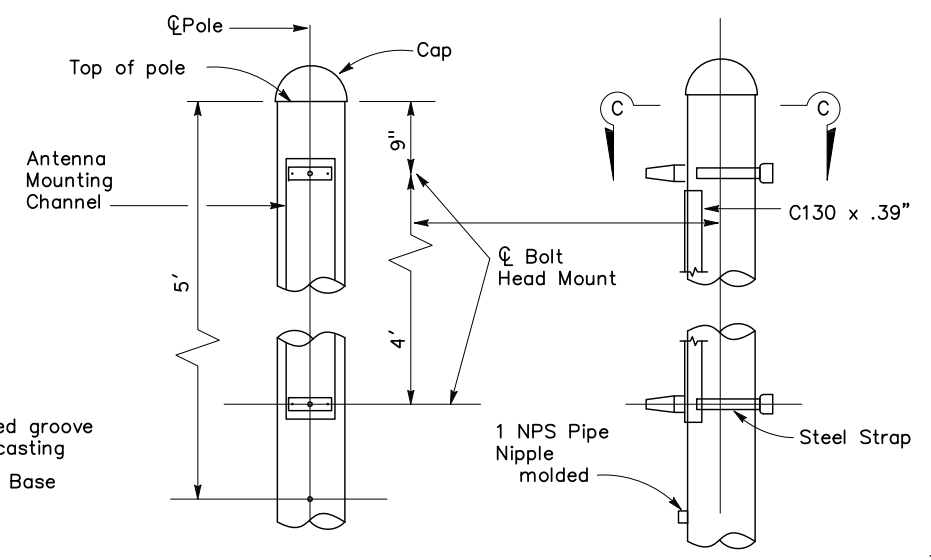
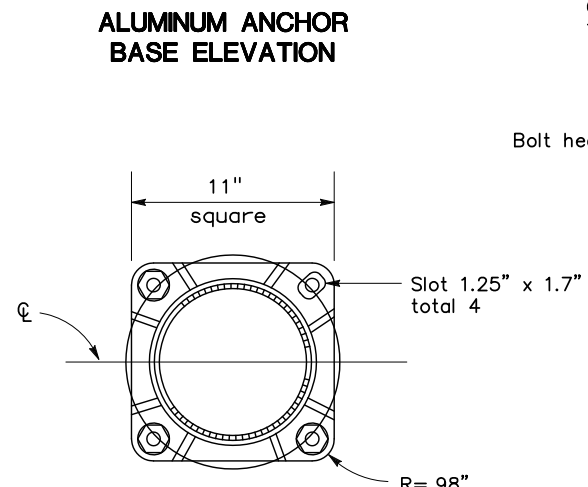
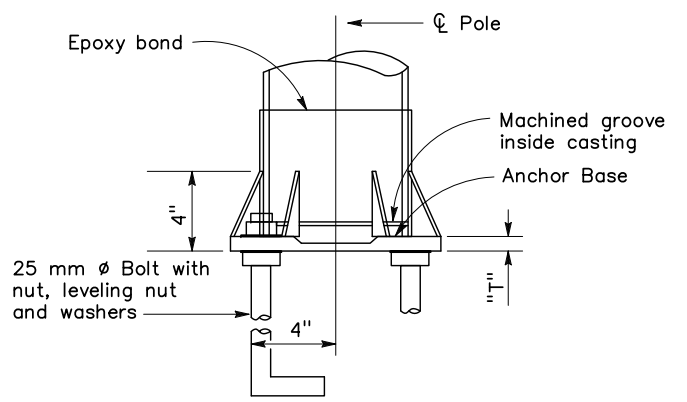
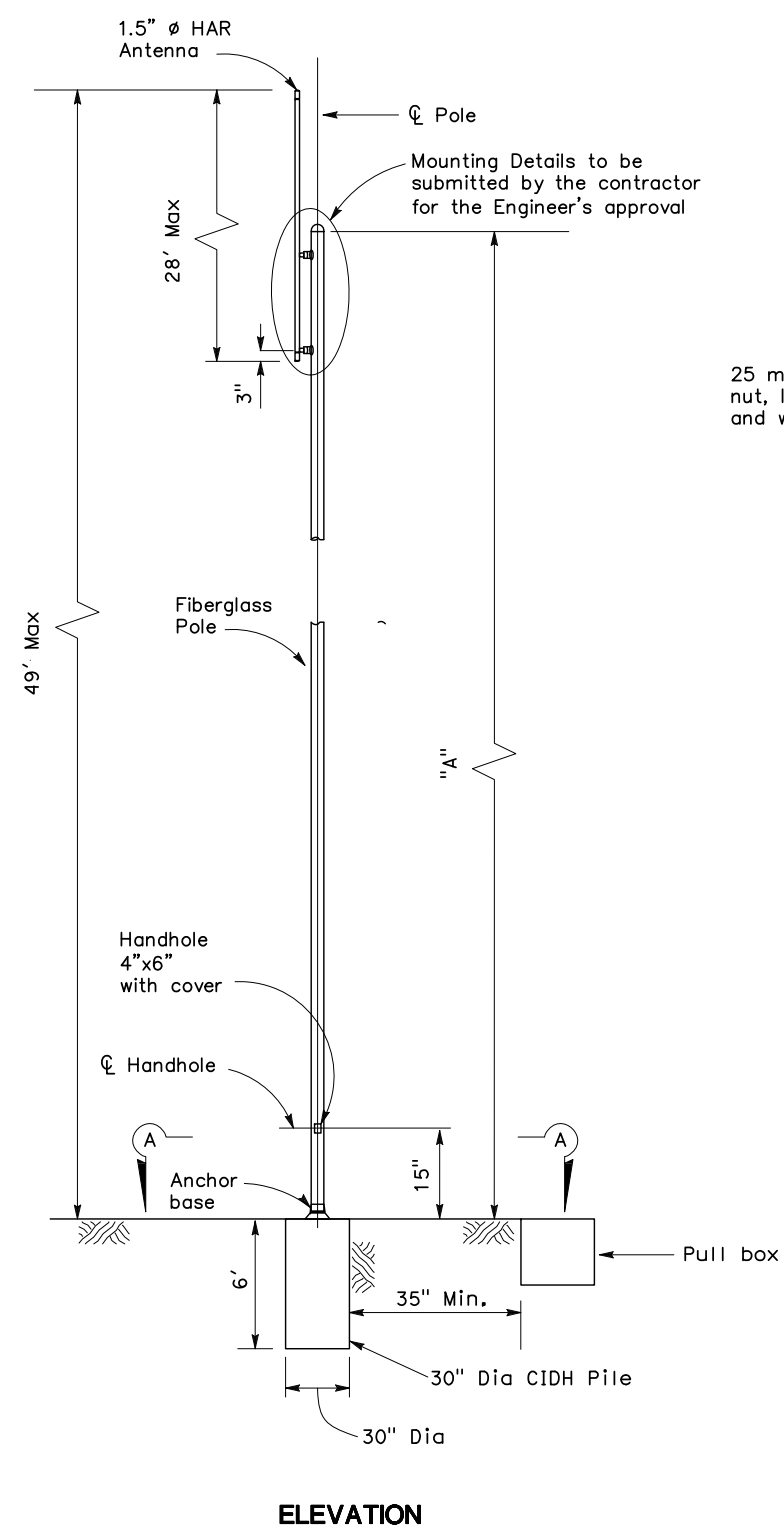
E-35

LAST REVISION | DATE PLOTTED => \$DATE
 05-18-12 | TIME PLOTTED => \$TIME

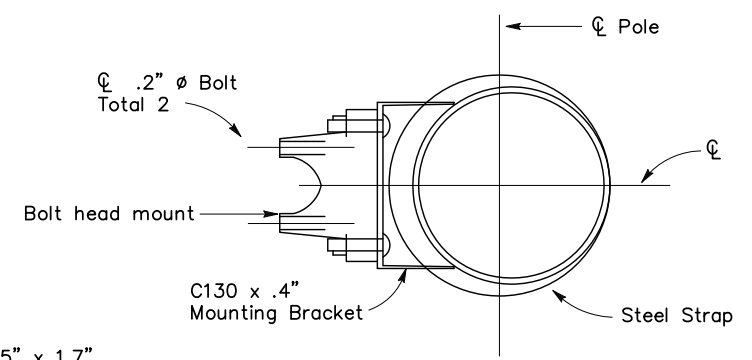
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	310	423

<i>David W. Clow</i> 5/18/12 REGISTERED ELECTRICAL ENGINEER DATE	
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

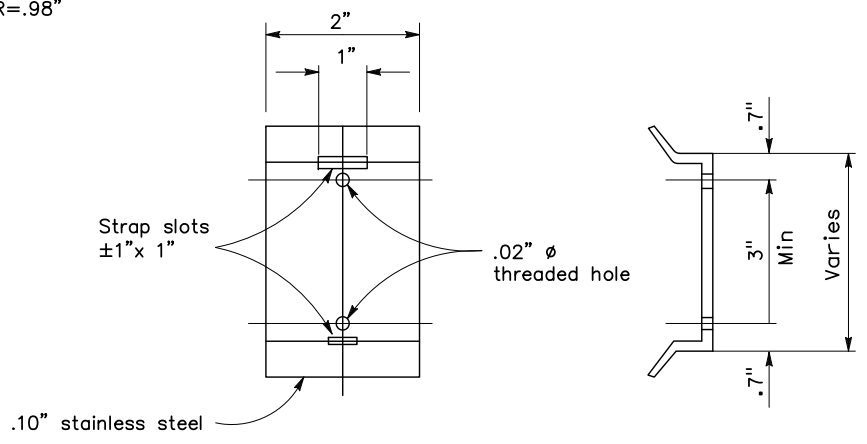
Fiberglass Pole Type	Pole Data				Anchor Base		
	"A" Height	Min. OD (in)		Weight lbs	Thickness "t"	Anchor Bolts	
		Base	Top			Size (in x in x in)	Bolt Circle (in)
21F	35'	9	5	185	74"	1 x 36 x 4	11



MOUNTING BRACKET DETAILS



SECTION C-C



SADDLE BRACKET

GENERAL NOTES:
Specifications

Design: AASHTO specifications for the design and construction of structural supports for highway signs, dated 1994.

Construction: Standard specifications and the special provisions.

Notes:

1. The Contractor shall verify all controlling field dimensions before ordering or fabricating any material.
2. During pole erection, the post shall be raked as necessary with the use of leveling nuts to provide a plumb pole.
3. For location, see Sheet E-XX
4. HAR antenna, shall be placed plumb.
5. All connections to fiberglass pole to be epoxy bond except when noted otherwise.

**ELECTRICAL DETAILS
(FIBERGLASS HAR STANDARD
CIDH/ANCHOR BASE)
(BREAKAWAY)
NO SCALE**

APPROVED FOR ELECTRICAL WORK ONLY.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISOR: HENRY ROBINO
 DATE: 5/18/12

LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEN NGAI
 CALCULATED/DESIGNED BY
 CHECKED BY
 HENRY ROBINO
 DAVE CLOW
 REVISOR BY
 DATE REVISOR
 HENRY ROBINO
 DAVE CLOW

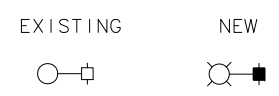
GENERAL NOTES FOR STAGE CONSTRUCTION

1. EXISTING EQUIPMENT SHOWN WITHOUT PROJECT NOTES AND EXISTING EQUIPMENT NOT SHOWN SHALL BE MAINTAINED IN CONTINUOUS OPERATION UNLESS OTHERWISE NOTED.
2. TEMPORARY ELECTRICAL EQUIPMENT NOT TO BECOME PART OF PERMANENT SYSTEM SHALL BE REMOVED AND DISPOSED OF WHEN NO LONGER REQUIRED.
3. EXACT LOCATION OF TEMPORARY ELECTRICAL EQUIPMENT WILL BE DETERMINED BY THE ENGINEER.
4. ALL TEMPORARY ELECTRICAL EQUIPMENT NO LONGER REQUIRED AT THE END OF A STAGE SHALL BE REMOVED OR REUSED IN STAGES TO FOLLOW. PULL BOXES SHALL BE REMOVED AND BECOME PROPERTY OF CONTRACTOR.
5. OTHER PERMANENT ELECTRICAL EQUIPMENT NOT SHOWN ON THE STAGE CONSTRUCTION PLANS CAN BE INSTALLED AT ANY STAGE IF NOT INTERFERING WITH CONSTRUCTION AND TRAFFIC AND AS DIRECTED BY THE ENGINEER.
6. ALL TEMPORARY WOOD POLES FOR HIGHWAY LIGHTING SHALL BE PLACED BEHIND CONCRETE BARRIER OR GUARD RAIL.
7. MESSENGER WIRE SHALL BE 3/8" 7 STRAND GALVANIZED STEEL WIRE. IT SHALL BE UTILITIES GRADE #1 WITH 6,000 POUND BREAKING STRENGTH OR STRONGER GRADE.

PROJECT NOTES FOR STAGE CONSTRUCTION

- 1A PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
- 2A ELECTRICAL EQUIPMENT TO REMAIN IN EFFECTIVE OPERATION FOR THE STAGE AS SHOWN ON THE PLAN AND TO BE REMOVED AND DISPOSED OF AS NOTED ON OTHER PARTS OF THIS CONTRACT PLAN.
- 3A ELECTRICAL EQUIPMENT TO BE INSTALLED FOR TEMPORARY LIGHTING OR RAMP METERING AND IN OPERATION FOR THE STAGE CONSTRUCTION AS SHOWN ON THE PLAN OR AS NOTED. WHEN NO LONGER REQUIRED ALL ABOVE GROUND ELECTRICAL EQUIPMENT SHALL BE REMOVED. PULL BOXES AND FOUNDATIONS SHALL BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR AND CONDUIT SHALL BE ABANDONED.
- 4A COORDINATE NEW 120/240 V SERVICE CONNECTION WITH PG&E.
- 5A RS Exist 240/480 V SERVICE CABINET AND INSTALL THE NEW 120/240 V SERVICE CABINET.
- 6A TURN OFF ELECTROLIER AT THIS STAGE.
- 7A INSTALL 2 #8 (LIGHTING, 240 V)
- 8A INSTALL 1 1/2" C, 2#8
- 9A CONTRACTOR SHALL COORDINATE WITH MID BEFORE CONNECTING TEMPORARY LIGHTING TO AN UNMETERED SERVICE.
- 10A CONNECT NEW TEMPORARY LIGHTING SERVICE TO LOAD SIDE OF Exist DISCONNECT SWITCH

LEGEND



TEMPORARY 200 W HPS LUMINAIRE ON 50'-0" WOOD POLE. SEE DETAIL ON SHEET E-53.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	311	423

David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE

9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

ELECTRICAL NOTES AND LEGEND
(STAGE CONSTRUCTION)
 NO SCALE

E-37

LAST REVISION DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

NOTES:

- 1. FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- 2. FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-37

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	313	423

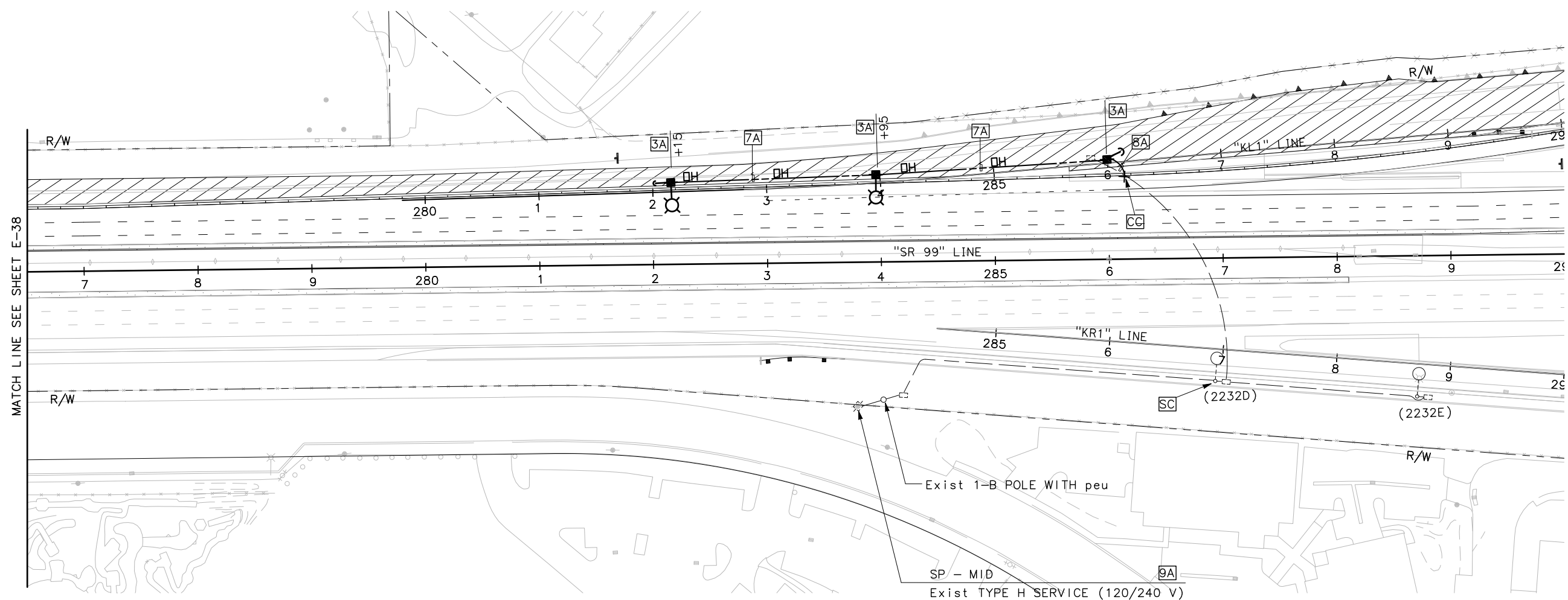
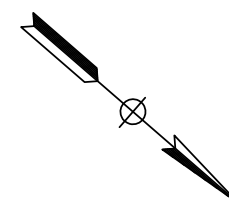
David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE

9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

ALLIANCE ENGINEERING CONSULTING, INC.
 4701 PATRICK HENRY DR., #10
 SANTA CLARA, CALIFORNIA 95054

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



STAGE 1

LIGHTING AND SIGN ILLUMINATION
(STAGE CONSTRUCTION)
 SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

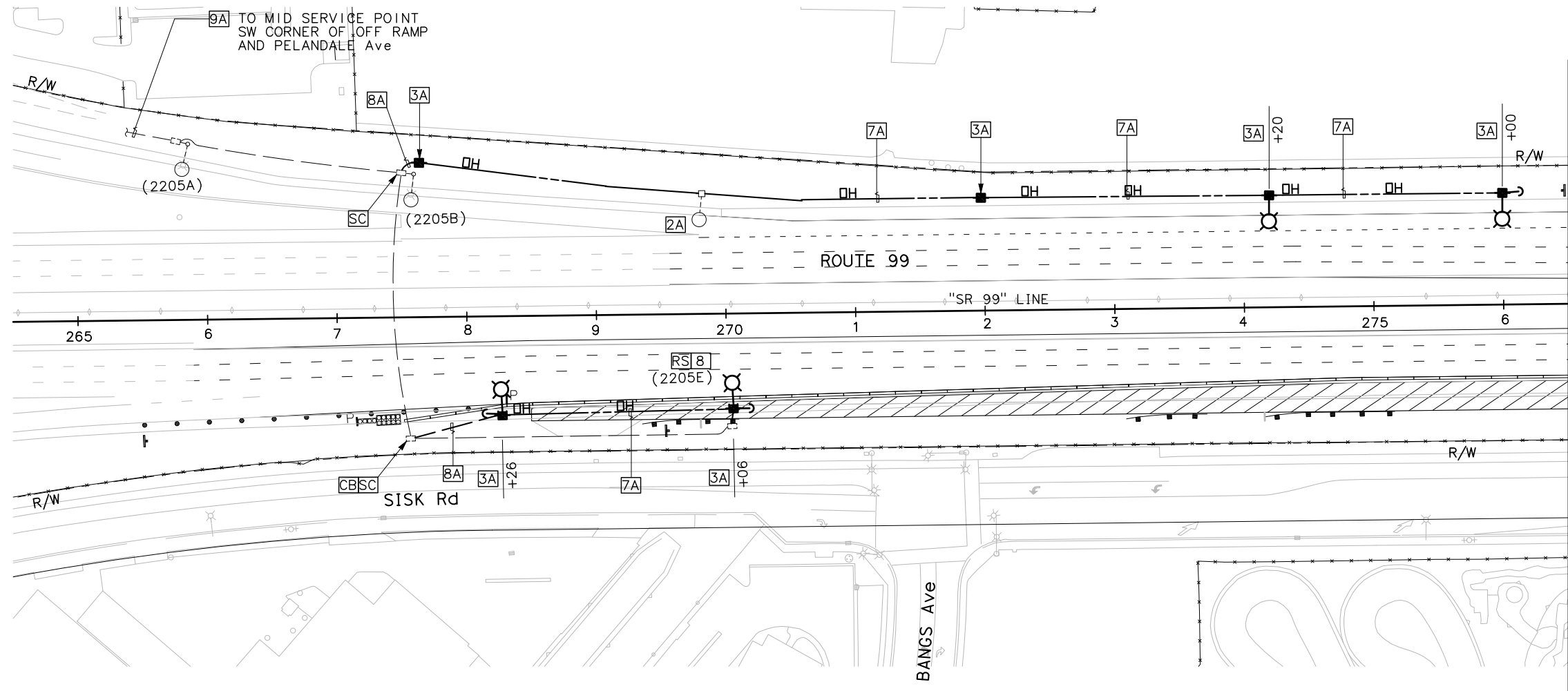
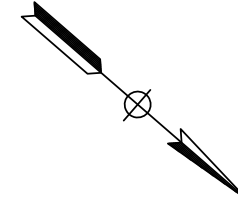
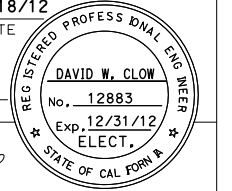
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISED BY: [blank]
 DATE REVISED: [blank]

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-37

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	315	423

<i>David W. Clow</i>	5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



STAGE 2

**LIGHTING AND SIGN ILLUMINATION
(STAGE CONSTRUCTION)**

SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-41

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	REVISOR	DATE
Caltrans	HENRY ROBINO	DAVE CLOW
CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	DESIGNED BY
KEN NGAI		

BORDER LAST REVISED 7/2/2010

USERNAME => #USER
DGN FILE => #REQUEST

RELATIVE BORDER SCALE
IS IN INCHES

UNIT 1457

PROJECT NUMBER & PHASE 100000100

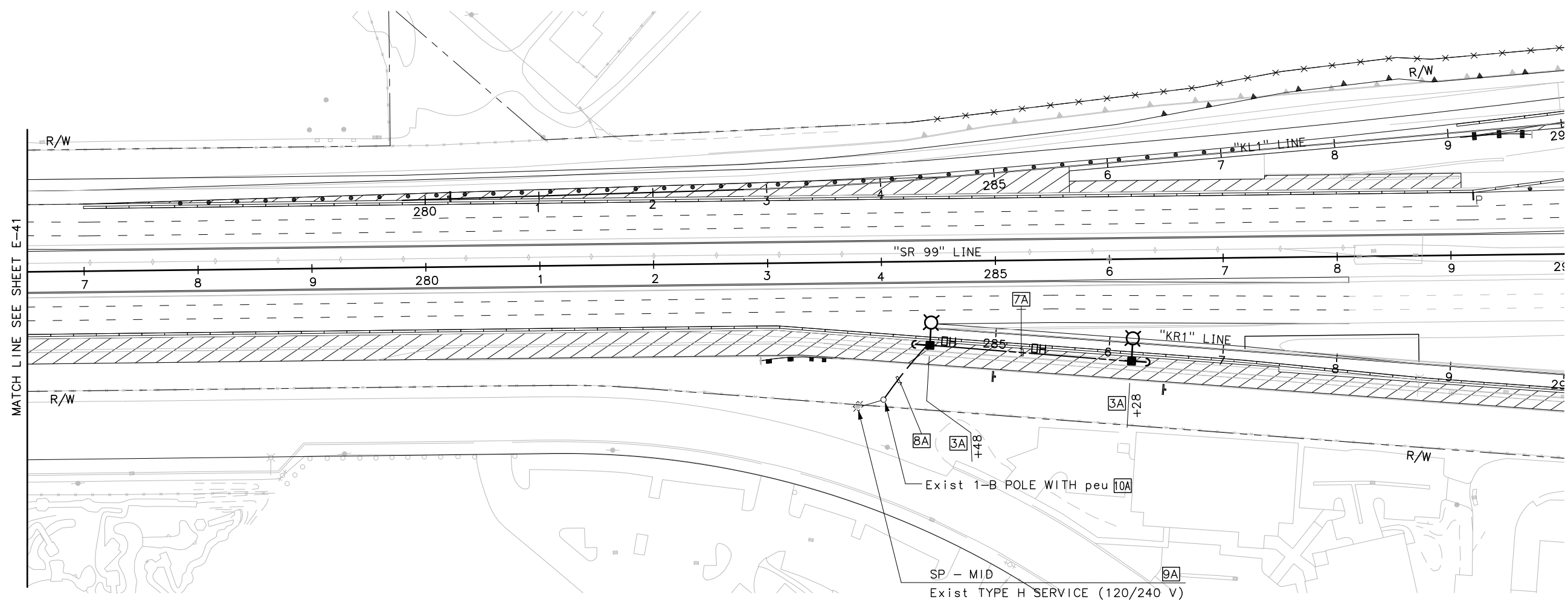
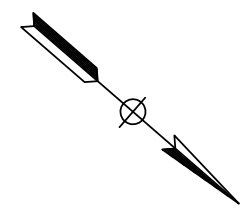
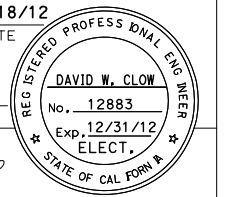
LAST REVISION | DATE PLOTTED => \$DATE
05-18-12 | TIME PLOTTED => \$TIME

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-37

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	316	423

<i>David W. Clow</i>		5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE	
9/04/12		
PLANS APPROVAL DATE		
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>		
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054		STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358



MATCH LINE SEE SHEET E-41

STAGE 2

**LIGHTING AND SIGN ILLUMINATION
(STAGE CONSTRUCTION)**

SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-42

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	REVISOR	DATE
Caltrans	HENRY ROBINO	DAVE CLOW
CONSULTANT FUNCTIONAL SUPERVISOR	CHECKED BY	DATE
KEN NGAI		
DESIGNED BY	REVISOR	DATE

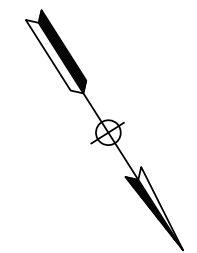
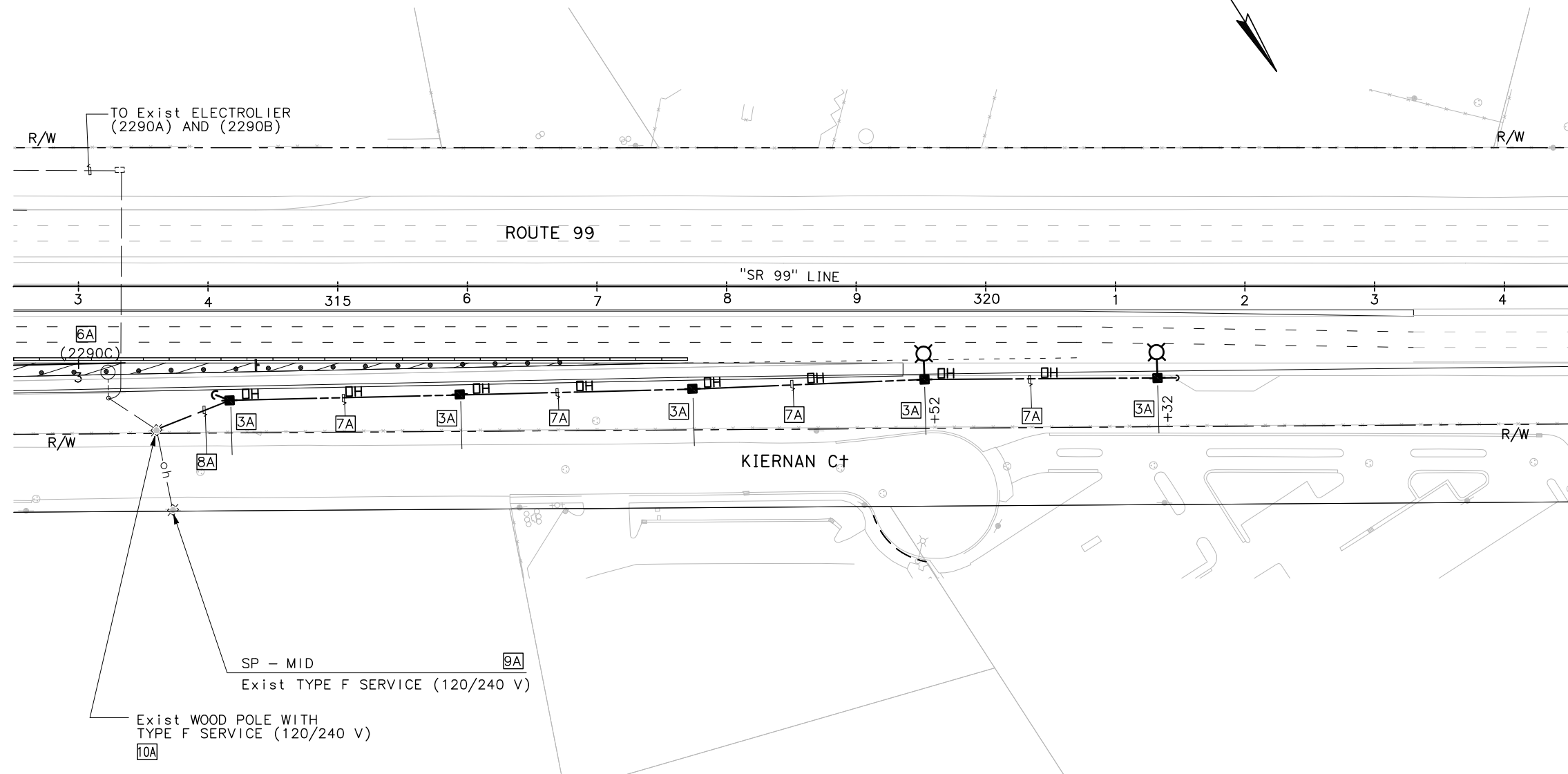
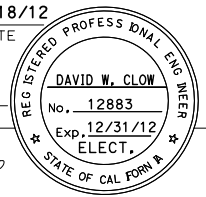
NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-37

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	317	423

David W. Clow	5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	

ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: CHECKED BY:
 HENRY ROBINO: DAVE CLOW
 REVISOR BY: DATE REVISOR BY: DATE REVISOR BY: DATE REVISOR BY: DATE

STAGE 2

**LIGHTING AND SIGN ILLUMINATION
(STAGE CONSTRUCTION)**
SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

LAST REVISION | DATE PLOTTED => \$DATE | 05-18-12 | TIME PLOTTED => \$TIME

NOTES:

- FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET E-37

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	318	423

David W. Clow 5/18/12
 REGISTERED ELECTRICAL ENGINEER DATE

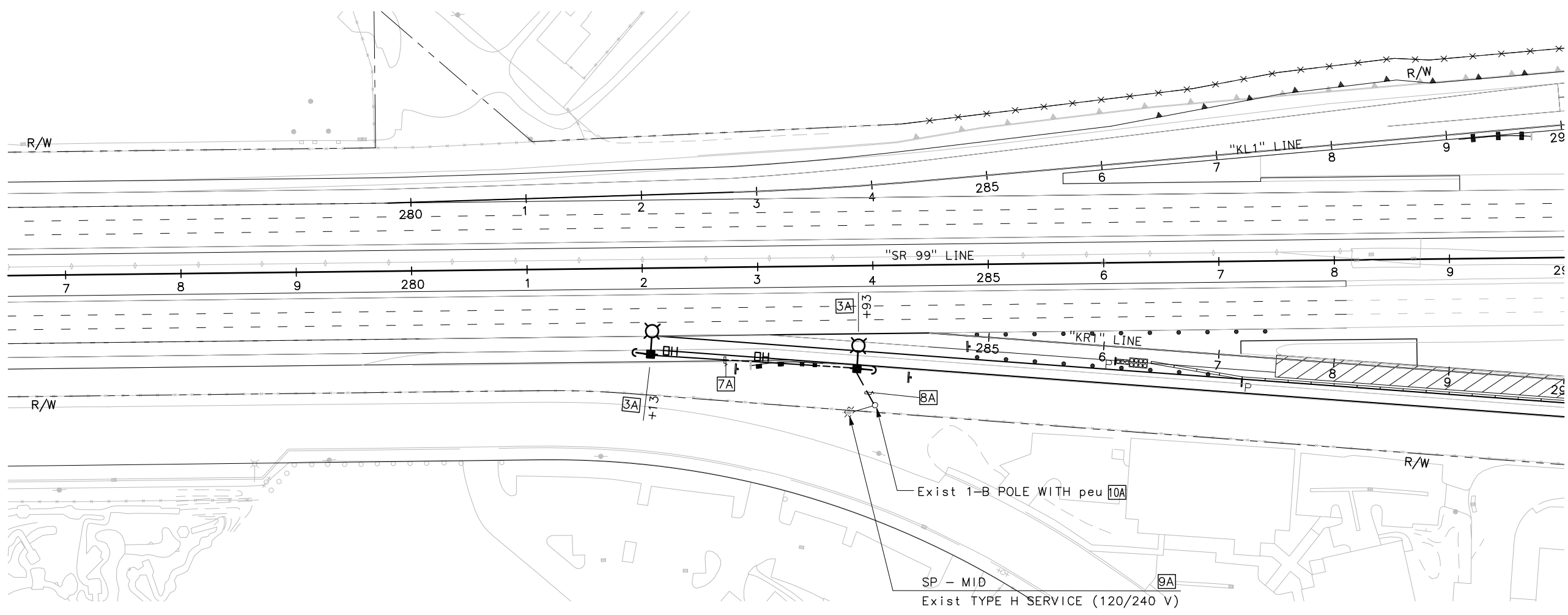
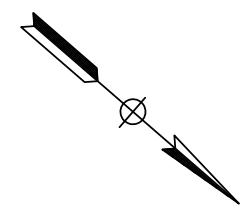
9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

ALLIANCE ENGINEERING CONSULTING, INC.
 4701 PATRICK HENERY DR., #10
 SANTA CLARA, CALIFORNIA 95054

STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

REGISTERED PROFESSIONAL ENGINEER
 DAVID W. CLOW
 No. 12883
 Exp. 12/31/12
 ELECT.
 STATE OF CAL FORM #



STAGE 3

**LIGHTING AND SIGN ILLUMINATION
(STAGE CONSTRUCTION)**
SCALE: 1" = 50'

APPROVED FOR ELECTRICAL WORK ONLY.

E-44

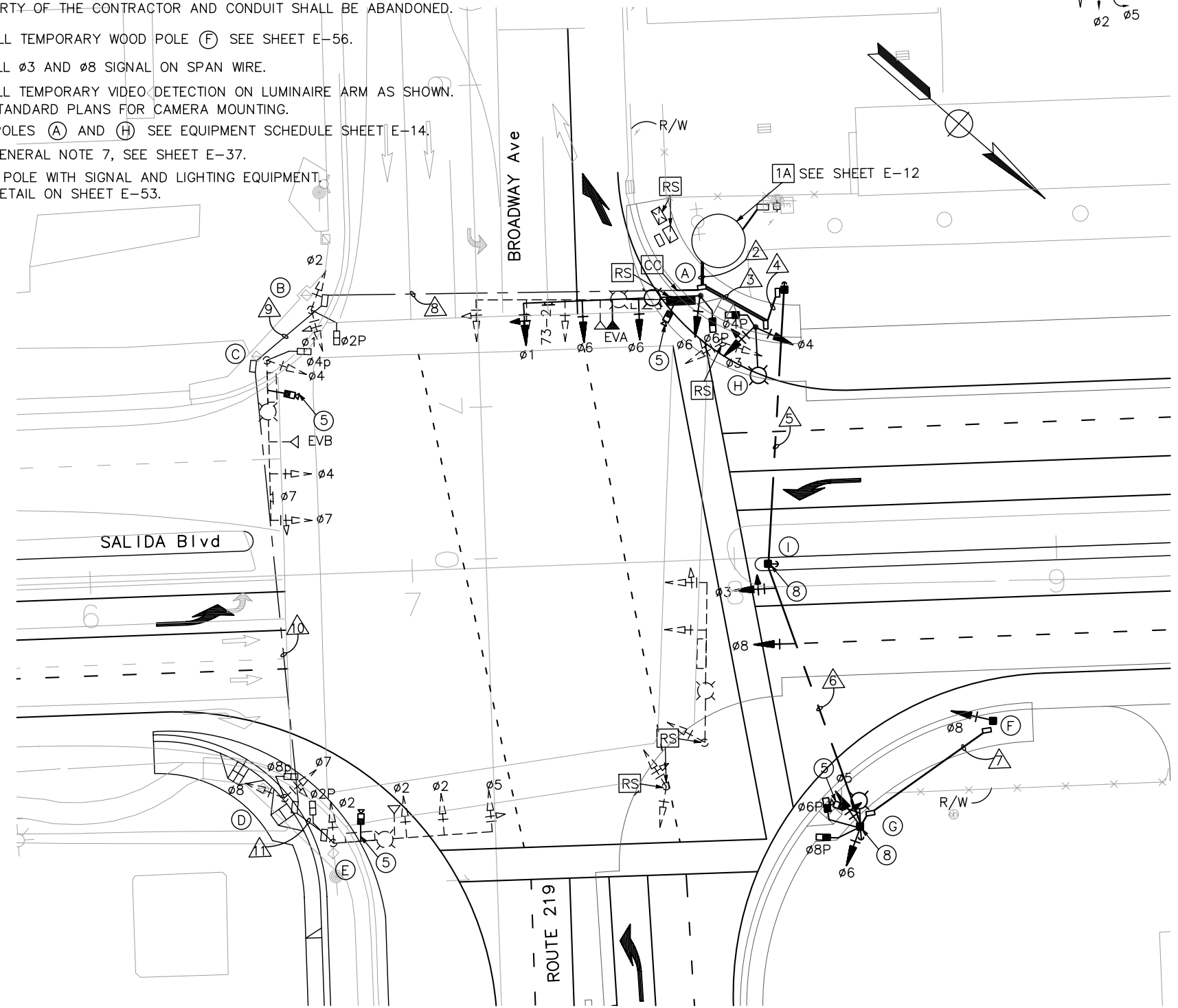
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISIONS: REVISOR: DATE: REVISIONS: DATE: REVISIONS: DATE: REVISIONS: DATE:

LAST REVISION: DATE PLOTTED => \$DATE
 05-18-12 TIME PLOTTED => \$TIME

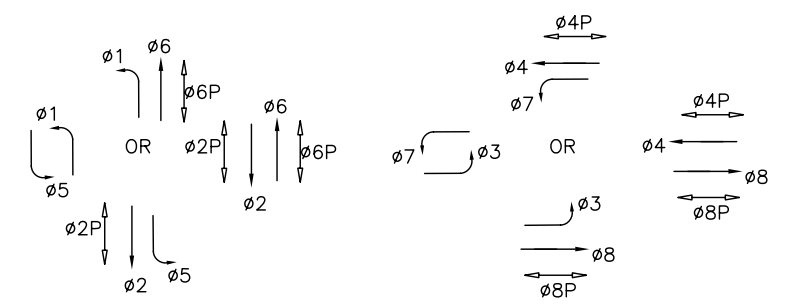
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTES (FOR THIS SHEET ONLY):

- ① PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
- ② ELECTRICAL EQUIPMENT TO BE INSTALLED FOR TEMPORARY SIGNAL AND IN EFFECTIVE OPERATION FOR THE STAGE CONSTRUCTION AS SHOWN ON THE PLAN OR AS NOTED. WHEN NO LONGER REQUIRED ALL ABOVE GROUND ELECTRICAL EQUIPMENT SHALL BE REMOVED. PULL BOXES AND FOUNDATIONS SHALL BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR AND CONDUIT SHALL BE ABANDONED.
- ③ INSTALL TEMPORARY WOOD POLE (F) SEE SHEET E-56.
- ④ INSTALL $\phi 3$ AND $\phi 8$ SIGNAL ON SPAN WIRE.
- ⑤ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ⑥ FOR POLES (A) AND (H) SEE EQUIPMENT SCHEDULE SHEET E-14.
- ⑦ SEE GENERAL NOTE 7, SEE SHEET E-37.
- ⑧ WOOD POLE WITH SIGNAL AND LIGHTING EQUIPMENT. SEE DETAIL ON SHEET E-53.



PHASE DIAGRAM



CONDUCTOR SCHEDULE

CABLE SIZE	PPB	POLE	No. OF CONDUCTORS/ RUN No.																		
			△	△	△	△	△	△	△	△	△	△	△								
$\phi 1, \phi 6, \phi 6P$	4PPB	POLE A	1-1																		
$\phi 1, \phi 2, \phi 2P$	4PPB	POLE B	1-1																		
$\phi 4, \phi 7, \phi 4P$	2PPB	POLE C	1-1																		
$\phi 7, \phi 8, \phi 8P$	2PPB	POLE D	1-1																		
$\phi 2, \phi 5, \phi 2P$	8PPB	POLE E	1-1																		
$\phi 8$		POLE F	1-0	1-0	1-0	1-0	1-0	1-0													
$\phi 5, \phi 6, \phi 6P, \phi 8P$	6PPB 8PPB	POLE G	1-2	1-2	1-2	1-2	1-2														
$\phi 3, \phi 4, \phi 4P$	6PPB	POLE H	1-1	1-1																	
	6PPB	POLE I	0-1	0-1	0-1	0-1															
TOTAL SIGNAL CABLES			8-9	3-4	2-3	2-3	2-2	1-0	4-4	3-3	2-2	1-1									
#10		LUMINAIRE	2	2	2	2	2		2	2	2	2									
		VIDEO CABLE	4	1	1	1	1		2	2	1	1									
CONDUIT SIZE (INCHES)			2-4"	2-3"	4"	*	*	2"	4"	4"	4"	2"									

* △ △ AND △ ARE OVERHEAD MESSENGER CABLES, SEE DETAIL ON SHEET E-53

EQUIPMENT AND POLE SCHEDULE

SIGNAL POLE	STANDARD TYPE	LMA	VEH SIG MTG		PPB		PED SIGNAL MOUNTING	H.P.S. LUMINARE WATTAGE	REMARKS
			ϕ	SPAN WIRE MOUNT	POLE MOUNT	ϕ			
(F)	WOOD POLE	8			SV-1-T				
(G)	WOOD POLE	8'	5		SV-2-T	6	← →	S-2P-T	200
(I)	WOOD POLE	3	8		SW-1	6	← →		

SW - SPAN WIRE

**SIGNAL AND LIGHTING (COUNTY)
 (STAGE CONSTRUCTION)
 STAGE 2 LOCATION 1**

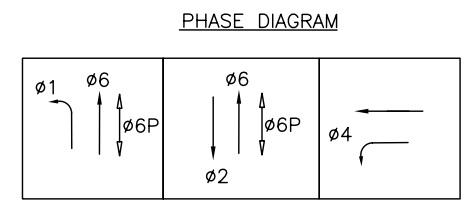
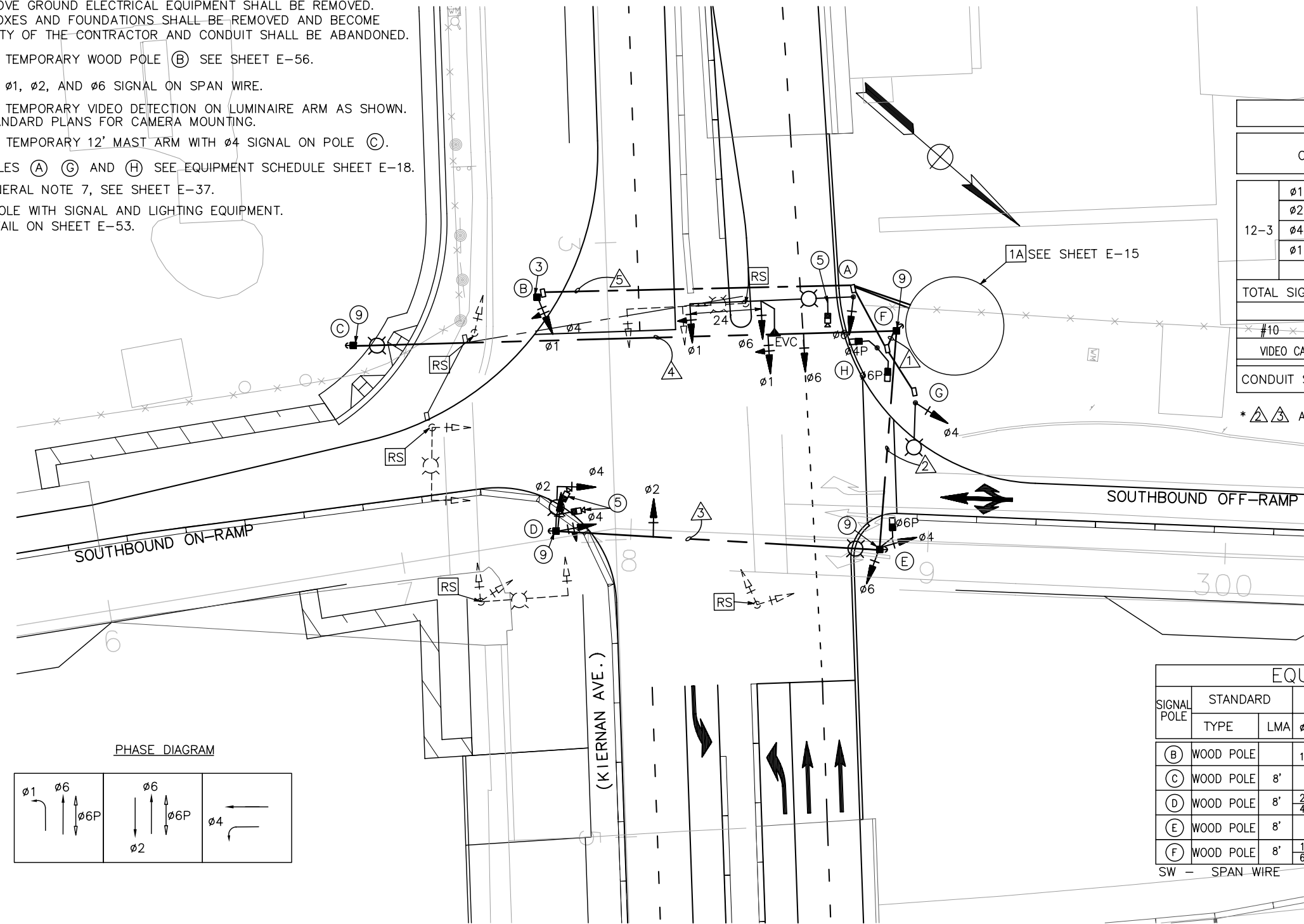
SCALE 1"=20'

E-45

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTES (FOR THIS SHEET ONLY):

- ① PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
- ② ELECTRICAL EQUIPMENT TO BE INSTALLED FOR TEMPORARY SIGNAL AND IN EFFECTIVE OPERATION FOR THE STAGE CONSTRUCTION AS SHOWN ON THE PLAN OR AS NOTED. WHEN NO LONGER REQUIRED ALL ABOVE GROUND ELECTRICAL EQUIPMENT SHALL BE REMOVED. PULL BOXES AND FOUNDATIONS SHALL BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR AND CONDUIT SHALL BE ABANDONED.
- ③ INSTALL TEMPORARY WOOD POLE (B) SEE SHEET E-56.
- ④ INSTALL $\phi 1$, $\phi 2$, AND $\phi 6$ SIGNAL ON SPAN WIRE.
- ⑤ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ⑥ INSTALL TEMPORARY 12' MAST ARM WITH $\phi 4$ SIGNAL ON POLE (C).
- ⑦ FOR POLES (A) (G) AND (H) SEE EQUIPMENT SCHEDULE SHEET E-18.
- ⑧ SEE GENERAL NOTE 7, SEE SHEET E-37.
- ⑨ WOOD POLE WITH SIGNAL AND LIGHTING EQUIPMENT. SEE DETAIL ON SHEET E-53.



CONDUCTOR SCHEDULE						
CABLE SIZE	PPB	POLE	No. OF CONDUCTORS/ RUN No.			
			▲	▲	▲	▲
$\phi 1$		POLE B				1-0
$\phi 2, \phi 4$		POLE D	1-0	1-0	1-0	
$\phi 4, \phi 6, \phi 6P$	6PPB	POLE E	1-1	1-1		
$\phi 1, \phi 6$		POLE F	1-0			
TOTAL SIGNAL CABLES			3-1	2-1	1-0	1-0
* #10 *		LUMINAIRE	2	2	2	2
VIDEO CABLE		VIDEO DETECTION	2	2	2	
CONDUIT SIZE (INCHES)			4"	*	*	2"

* ▲ ▲ AND ▲ ARE OVERHEAD MESSENGER CABLES, SEE DETAIL ON SHEET E-53

EQUIPMENT AND POLE SCHEDULE								
SIGNAL POLE	STANDARD TYPE	LMA	VEH SIG MTG		PPB	PED SIGNAL MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
			SPAN WIRE MOUNT	POLE MOUNT				
(B)	WOOD POLE		1		SV-1-T			15' WOOD POLE
(C)	WOOD POLE	8'					200	
(D)	WOOD POLE	8'	2/4	SW-1 MAS	SV-2-T		200	12' MAST ARM
(E)	WOOD POLE	8'			SV-2-T	6	S-1P-T	200
(F)	WOOD POLE	8'	1/6	SW-1				

SW - SPAN WIRE

**SIGNAL AND LIGHTING
 (STAGE CONSTRUCTION)
 STAGE 2 LOCATION 1**

SCALE 1"=20'

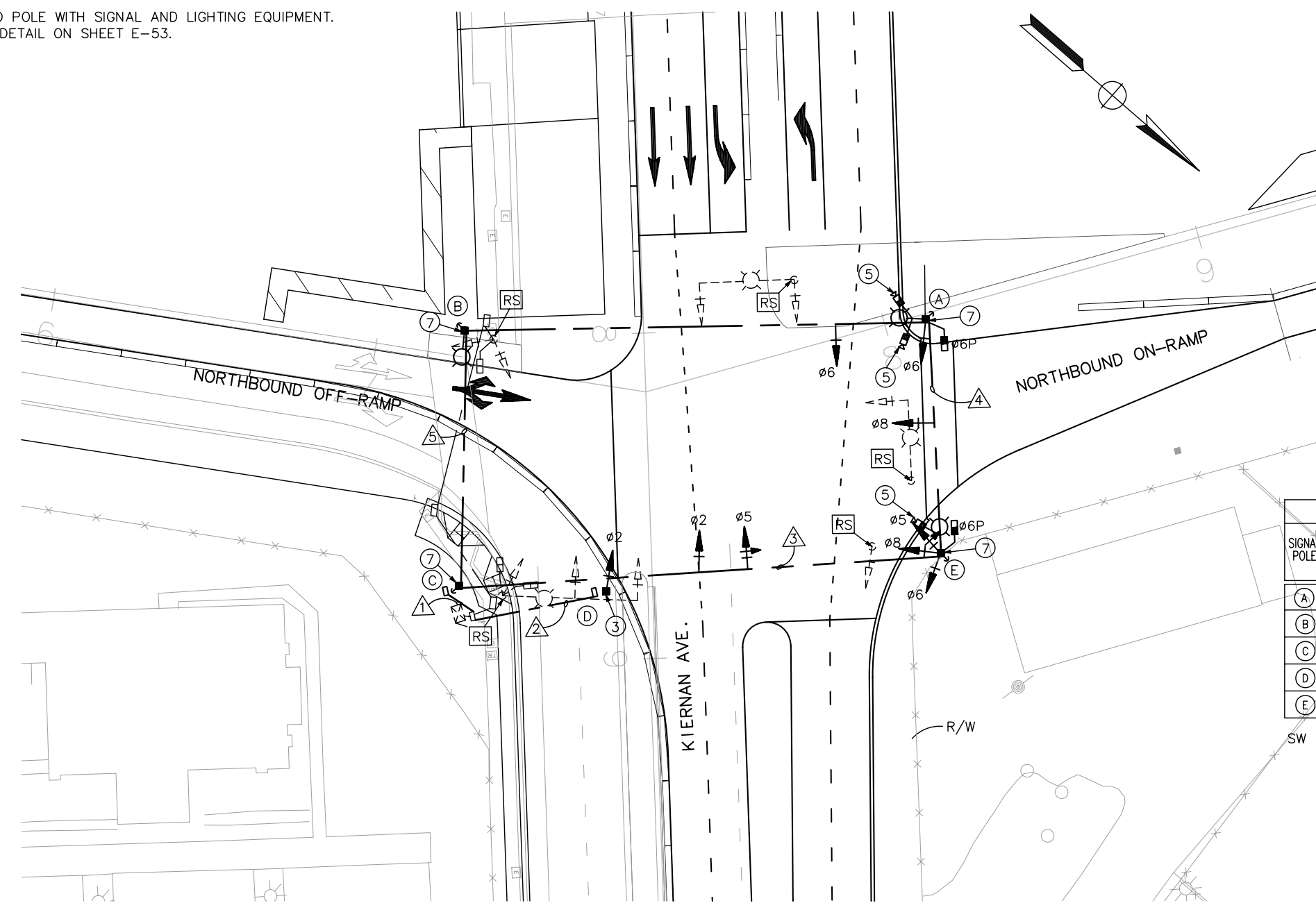
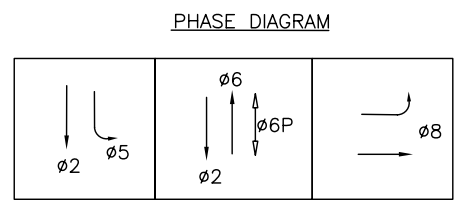
E-46

APPROVED FOR ELECTRICAL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CALCULATED-DESIGNED BY
 CHECKED BY
 SRILAKSHMI SAKHAMURI
 CHUONG NGUYEN
 REVISED BY
 DATE REVISED

NOTES (FOR THIS SHEET ONLY):

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- ② ELECTRICAL EQUIPMENT TO BE INSTALLED FOR TEMPORARY SIGNAL AND IN EFFECTIVE OPERATION FOR THE STAGE CONSTRUCTION AS SHOWN ON THE PLAN OR AS NOTED. WHEN NO LONGER REQUIRED ALL ABOVE GROUND ELECTRICAL EQUIPMENT SHALL BE REMOVED. PULL BOXES AND FOUNDATIONS SHALL BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR AND CONDUIT SHALL BE ABANDONED.
- ③ INSTALL TEMPORARY WOOD POLE ④ SEE SHEET E-56.
- ④ INSTALL $\phi 2$, $\phi 5$, $\phi 6$ AND $\phi 8$ SIGNAL ON SPAN WIRE.
- ⑤ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ⑥ SEE GENERAL NOTE 7, SEE SHEET E-37.
- ⑦ WOOD POLE WITH SIGNAL AND LIGHTING EQUIPMENT. SEE DETAIL ON SHEET E-53.



CONDUCTOR SCHEDULE

CABLE SIZE	PPB	POLE	No. OF CONDUCTORS/ RUN No.						
			△	△	△	△	△	△	
$\phi 6$, $\phi 6P$	6PPB	POLE A	1-1		1-1	1-1			
$\phi 2$, $\phi 5$		POLE C	1-0						
$\phi 2$		POLE D	1-0	1-0					
$\phi 5$, $\phi 6$, $\phi 8$, $\phi 6P$	6PPB	POLE E	2-1		2-1				
TOTAL SIGNAL CABLES			5-2	1-0	3-2	1-1			
#10		LUMINAIRE	2		2	2	2		
		VIDEO CABLE			3	2			
CONDUIT SIZE (INCHES)			2-4"	2"	*	*	*		

*△△ AND △△ ARE OVERHEAD MESSENGER CABLES, SEE DETAIL ON SHEET E-53

EQUIPMENT AND POLE SCHEDULE

SIGNAL POLE	STANDARD		VEH SIG MTG		PPB		PED SIGNAL MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
	TYPE	LMA	ϕ SPAN WIRE MOUNT	POLE MOUNT	ϕ ARROW				
(A)	WOOD POLE	8'	6	SW-1	SV-1-T	6	←	S-1P-T	200
(B)	WOOD POLE	8'							200
(C)	WOOD POLE		2/5	SW-1					
(D)	WOOD POLE			SW-1	SV-1-T				
(E)	WOOD POLE	8'	8	SW-1	SV-3-T	6	←	S-1P-T	200

SW - SPAN WIRE

**SIGNAL AND LIGHTING
 (STAGE CONSTRUCTION)
 STAGE 2 LOCATION 2**

SCALE 1"=20'

E-47

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	322	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER
 DATE 05-18-12
 9/04/12
 PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126

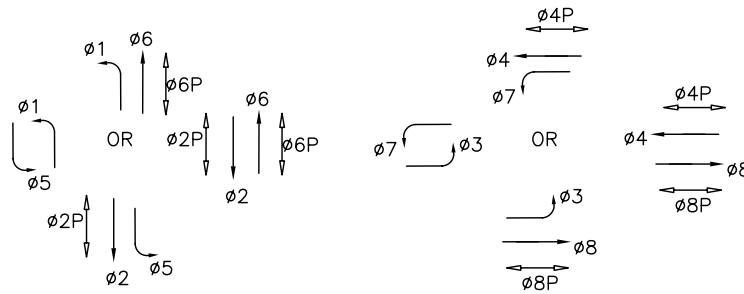
STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358



NOTES (FOR THIS SHEET ONLY):

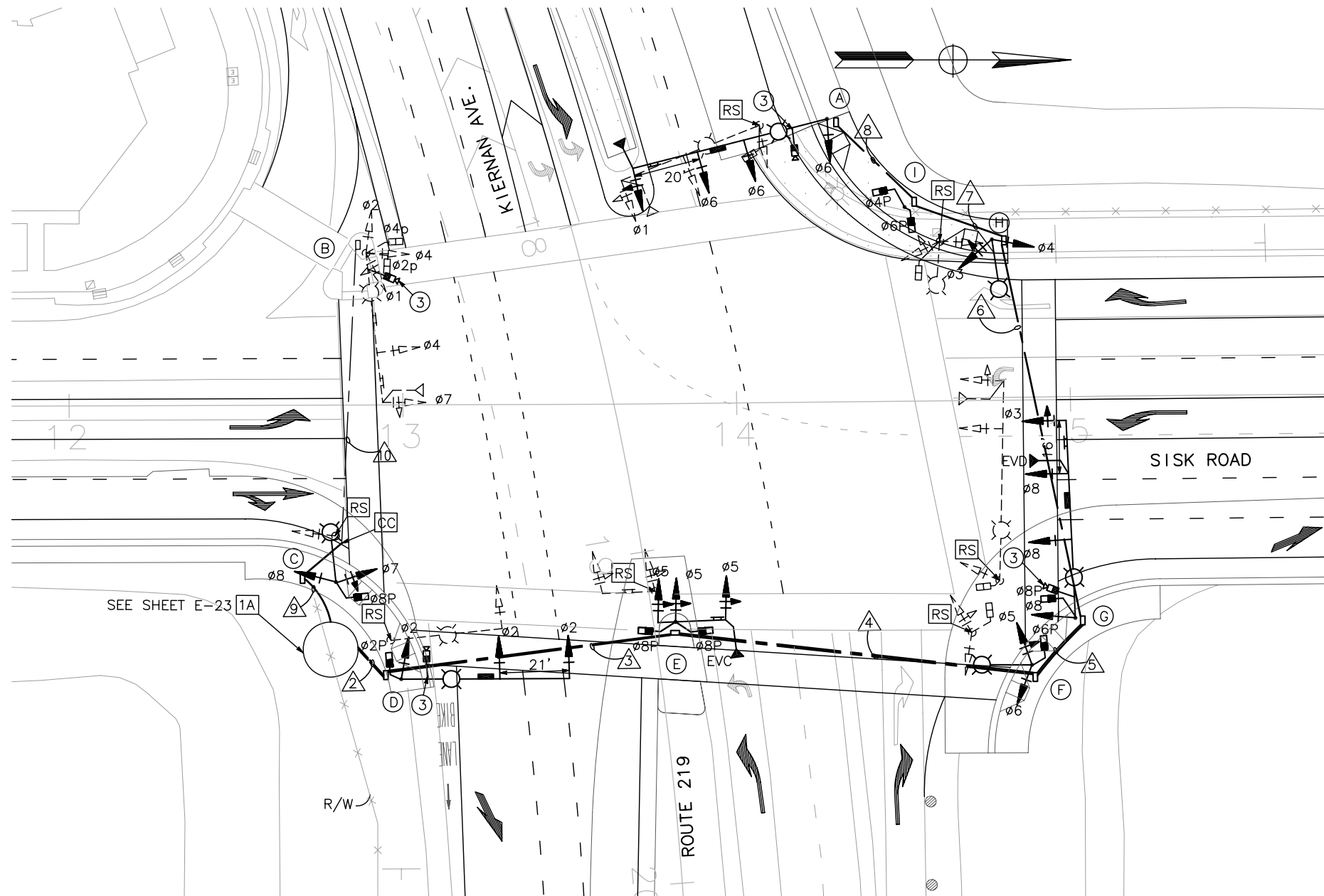
- PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
- ELETRICAL EQUIPMENT TO BE INSTALLED FOR TEMPORARY SIGNAL AND IN EFFECTIVE OPERATION FOR THE STAGE CONSTRUCTION AS SHOWN ON THE PLAN OR AS NOTED. WHEN NO LONGER REQUIRED ALL ABOVE GROUND ELECTRICAL EQUIPMENT SHALL BE REMOVED. PULL BOXES AND FOUNDATIONS SHALL BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR AND CONDUIT SHALL BE ABANDONED.
- INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- FOR ALL POLES EXCEPT FOR (B) SEE EQUIPMENT SCHEDULE SHEET E-25.
- POLE (B) EXISTING TO REMAIN FOR THIS STAGE.

PHASE DIAGRAM



CONDUCTOR SCHEDULE

CABLE SIZE	PPB	POLE	No. OF CONDUCTORS/ RUN No.																	
			1	2	3	4	5	6	7	8	9	10								
12/3		POLE A	1-1	1-1	1-1	1-1	1-1	1-1	1-1	1-1	1-1									
12/3	2PPB, 4PPB	POLE B																2-2	2-2	
12/3	2PPB	POLE C																	1-1	
12/3		POLE D	1-1																	
12/3		POLE E	1-1	1-1																
12/3		POLE F	1-1	1-1	1-1															
12/3		POLE G	1-1	1-1	1-1	1-1														
12/3		POLE H	1-1	1-1	1-1	1-1	1-1													
12/3		POLE I	0-1	0-1	0-1	0-1	0-1	0-1	0-1											
TOTAL SIGNAL CABLES			6-7	5-6	4-5	3-4	2-3	1-2	1-1	3-3	2-2									
#10		LUMINAIRE	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	
		VIDEO CABLE	3	2	2	2	1	1	1	1	1	1	1	1	1	1	1	1	1	
CONDUIT SIZE (INCHES)			2-4"	2-4"	2-4"	2-3"	4"	3"	3"	3"	2-4"	4"								



**SIGNAL AND LIGHTING
(STAGE CONSTRUCTION)
STAGE 2 LOCATION 3**
SCALE 1"=20'

E-48

APPROVED FOR ELECTRICAL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER
 CALCULATED/DESIGNED BY SRI LAKSHMI SAKHAMURI
 CHECKED BY CHUONG NGUYEN
 REVISED BY DATE REVISED

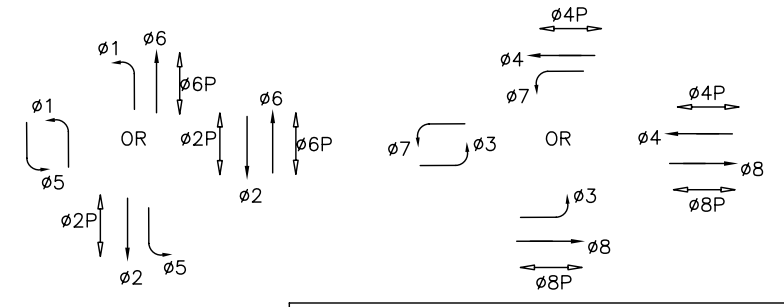
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED/DESIGNED BY: SRI LAKSHMI SAKHAMURI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: DATE REVISED

NOTES (FOR THIS SHEET ONLY):

- ① PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
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- ③ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ④ COVER Ø6 HEAD THIS STAGE.
- ⑤ INSTALL TEMPORARY WOOD POLE (F) SEE SHEET E-56.1
- ⑥ INSTALL Ø3 AND Ø8 SIGNAL ON SPAN WIRE.

- ⑦ FOR POLES (A) (D) (E) AND (H) SEE EQUIPMENT SCHEDULE SHEET E-14.
- ⑧ SEE GENERAL NOTE 7, SEE SHEET E-37.
- ⑨ WOOD POLE WITH SIGNAL AND LIGHTING EQUIPMENT. SEE DETAIL ON SHEET E-53.

PHASE DIAGRAM



CONDUCTOR SCHEDULE

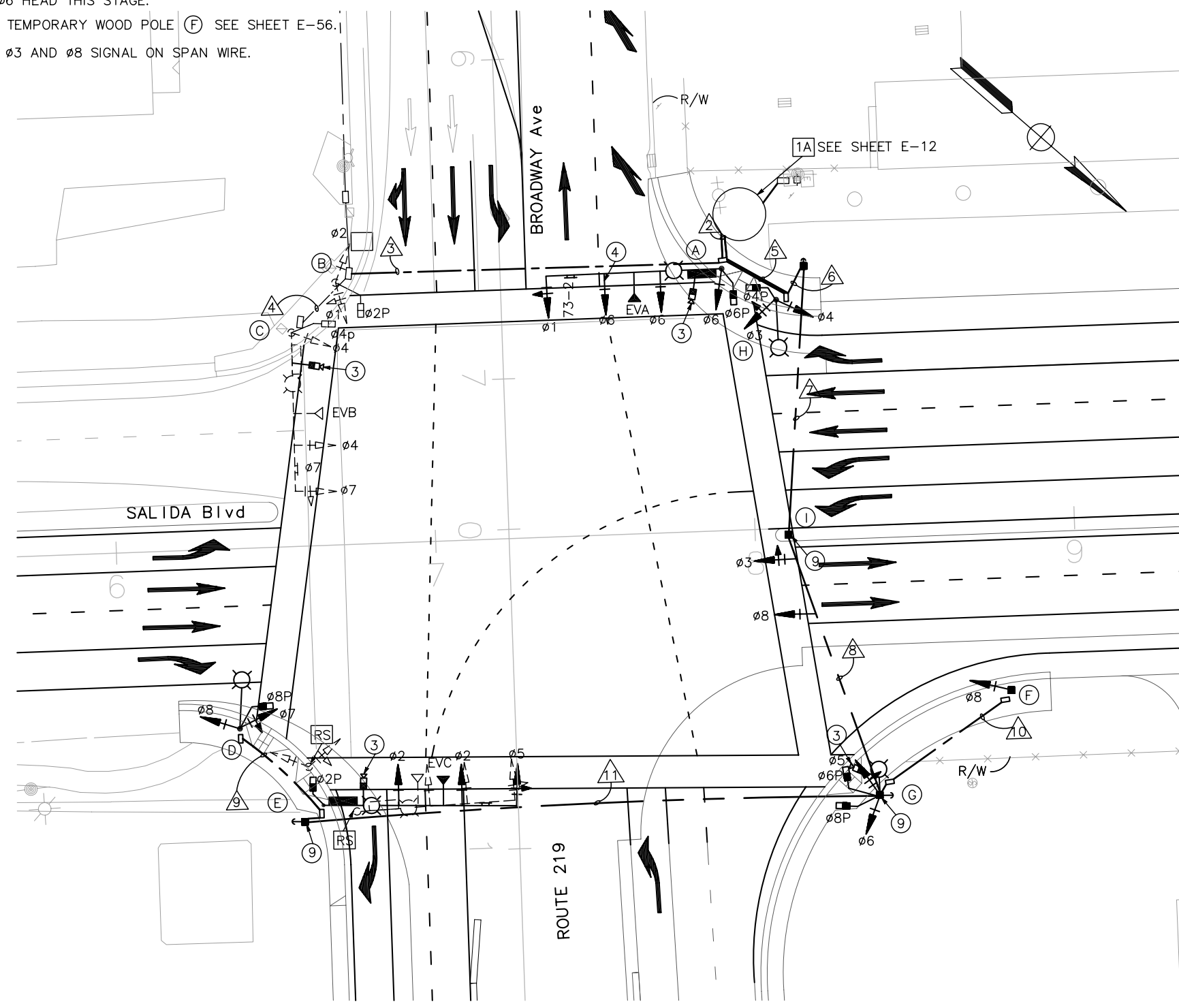
CABLE SIZE	PPB	POLE	No. OF CONDUCTORS/ RUN No.																	
			△	△	△	△	△	△	△	△	△	△								
Ø1, Ø6, Ø6P	4PPB	POLE A	1-1																	
Ø1, Ø2, Ø2P	4PPB	POLE B	1-1	1-1																
Ø4, Ø7, Ø4P	2PPB	POLE C	1-1	1-1	1-1															
Ø7, Ø8, Ø8P	2PPB	POLE D	1-1			1-1	1-1	1-1	1-1	1-1										
Ø2, Ø5, Ø2P	8PPB	POLE E	1-1			1-1	1-1	1-1	1-1											1-1
Ø8		POLE F	1-0			1-0	1-0	1-0	1-0											1-0
Ø5, Ø6, Ø6P, Ø8P	6PPB 8PPB	POLE G	1-2			1-2	1-2	1-2	1-2											
Ø3, Ø4, Ø4P	6PPB	POLE H	1-1			1-1														
	6PPB	POLE I	0-1			0-1	0-1	0-1												
TOTAL SIGNAL CABLES			8-9	2-2	1-1	5-6	4-5	4-5	4-4	1-1	1-0	2-2								
#10		LUMINAIRE	2	2	2	2	2	2	2			2								
		VIDEO DETECTION	1			1	1	1				1								
CONDUIT SIZE (INCHES)			2-4"	4"	4"	2-3"	4"	*	*	3"	2"	*								

* △ △ AND △ ARE OVERHEAD MESSENGER CABLES, SEE DETAIL ON SHEET E-53

EQUIPMENT AND POLE SCHEDULE

SIGNAL POLE	STANDARD	VEH SIG MTG			PPB	PED SIGNAL MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
		TYPE	LMA	Ø				
(F)	WOOD POLE		8					SV-1-T
(G)	WOOD POLE	8'	5/6		SW-1	SV-2-T	6/8	S-2P-T 200
(I)	WOOD POLE				SW-1		6	

SW - SPAN WIRE



APPROVED FOR ELECTRICAL WORK ONLY

**SIGNAL AND LIGHTING (COUNTY)
 (STAGE CONSTRUCTION)
 STAGE 3 LOCATION 1**

SCALE 1"=20'

E-49

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEITH G. MEYER
 CALCULATED-DESIGNED BY: SRI LAKSHMI SAKHAMURI
 CHECKED BY: CHUONG NGUYEN
 REVISED BY: DATE REVISED

NOTES (FOR THIS SHEET ONLY):

- ① PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
- ② ELECTRICAL EQUIPMENT TO BE INSTALLED FOR TEMPORARY SIGNAL AND IN EFFECTIVE OPERATION FOR THE STAGE CONSTRUCTION AS SHOWN ON THE PLAN OR AS NOTED. WHEN NO LONGER REQUIRED ALL ABOVE GROUND ELECTRICAL EQUIPMENT SHALL BE REMOVED. PULL BOXES AND FOUNDATIONS SHALL BE REMOVED AND BECOME PROPERTY OF THE CONTRACTOR AND CONDUIT SHALL BE ABANDONED.
- ③ INSTALL $\phi 1$, $\phi 2$ AND $\phi 6$ SIGNAL ON SPAN WIRE.
- ④ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ⑤ FOR POLES (A) (B) (C) (D) (G) AND (H) SEE EQUIPMENT SCHEDULE SHEET E-18.
- ⑥ SEE GENERAL NOTE 7, SEE SHEET E-37.
- ⑦ WOOD POLE WITH SIGNAL AND LIGHTING EQUIPMENT. SEE DETAIL ON SHEET E-53.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	324	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVENUE, SUITE 100
 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CALIFORNIA 95358

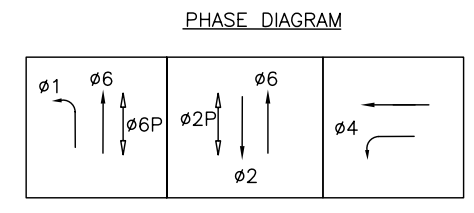
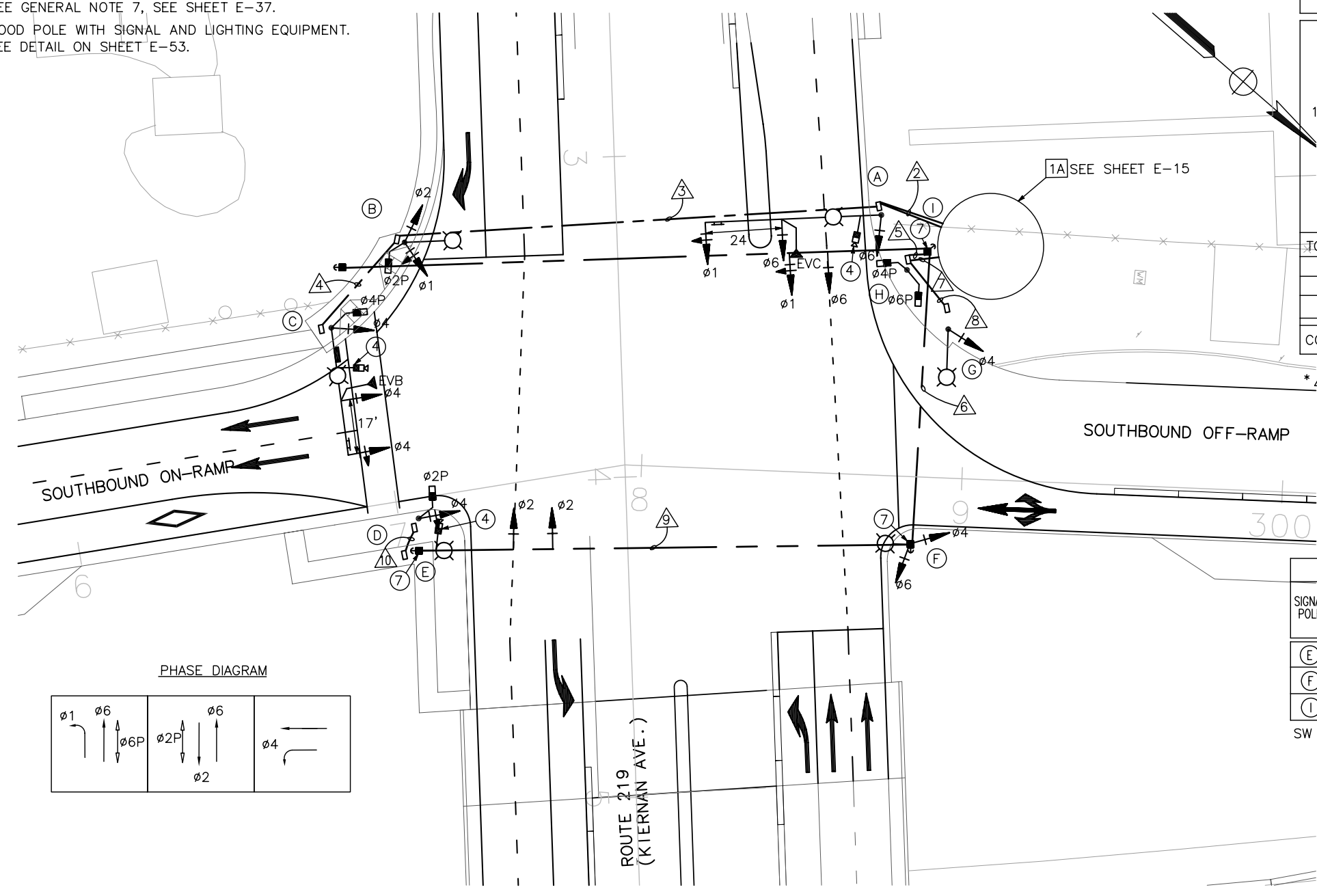


CONDUCTOR SCHEDULE										
CABLE SIZE	ϕ PPB	POLE	No. OF CONDUCTORS/ RUN No.							
			1	2	3	4	5	6	7	8
$\phi 1, \phi 6$		POLE A	1-0							
$\phi 1, \phi 2, \phi 2P$		POLE B	1-0	1-0						
$\phi 4$	2PPB	POLE C	1-1	1-1	1-1					
$\phi 4, \phi 2P$	2PPB	POLE D				1-1	1-1	1-1		
$\phi 2$		POLE E				1-0	1-0	1-0		
$\phi 4, \phi 6$		POLE F				1-0	1-0	1-0		
$\phi 4$		POLE G							1-0	1-0
		POLE H							1-0	
$\phi 1, \phi 6$		POLE I				1-0		1-0		
TOTAL SIGNAL CABLES			3-1	2-1	1-1	4-1	3-1	6-1	1-0	2-1
#10		LUMINAIRE	2	2	2	2	2	2	2	2
VIDEO CABLE		VIDEO DETECTION	2	1	1	1	1	1		1
CONDUIT SIZE (INCHES)			2-4"	4"	3"	2"	*	2-4"	2-4"	2"

* \triangle AND ∇ ARE OVERHEAD MESSENGER CABLES, SEE DETAIL ON SHEET E-53

EQUIPMENT AND POLE SCHEDULE									
SIGNAL POLE	STANDARD		VEH SIG MTG		PPB	PED SIGNAL MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS	
	TYPE	LMA	ϕ	SPAN WIRE MOUNT					POLE MOUNT
(E)	WOOD POLE	8'	2	SW-2			200		
(F)	WOOD POLE	8'	4		SV-2-T		200		
(I)	WOOD POLE		1	SW-2					

SW - SPAN WIRE



SIGNAL AND LIGHTING (STAGE CONSTRUCTION) STAGE 3 LOCATION 1

SCALE 1"=20'

E-50

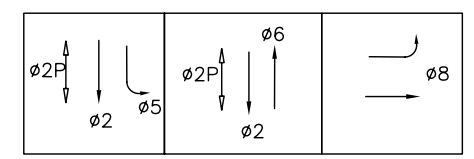
APPROVED FOR ELECTRICAL WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR KEITH G. MEYER
 CALCULATED-BY DESIGNED BY CHECKED BY
 SRILAKSHMI SAKHAMURI CHUONG NGUYEN
 REVISED BY DATE REVISED

NOTES (FOR THIS SHEET ONLY):

- ① PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
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- ③ INSTALL $\phi 2$, AND $\phi 5$ SIGNAL ON SPAN WIRE.
- ④ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ⑤ FOR POLES (A) (B) (C) (D) (E) AND (F) SEE EQUIPMENT SCHEDULE SHEET E-22.
- ⑥ SEE GENERAL NOTE 7, SEE SHEET E-37.
- ⑦ WOOD POLE WITH SIGNAL AND LIGHTING EQUIPMENT. SEE DETAIL ON SHEET E-53.

PHASE DIAGRAM



Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	325	423

P. Sri Lakshmi
 REGISTERED CIVIL ENGINEER
 05-18-12
 DATE
 9/04/12
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVENUE, SUITE 100 SAN JOSE, CALIFORNIA 95126
 STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

CONDUCTOR SCHEDULE

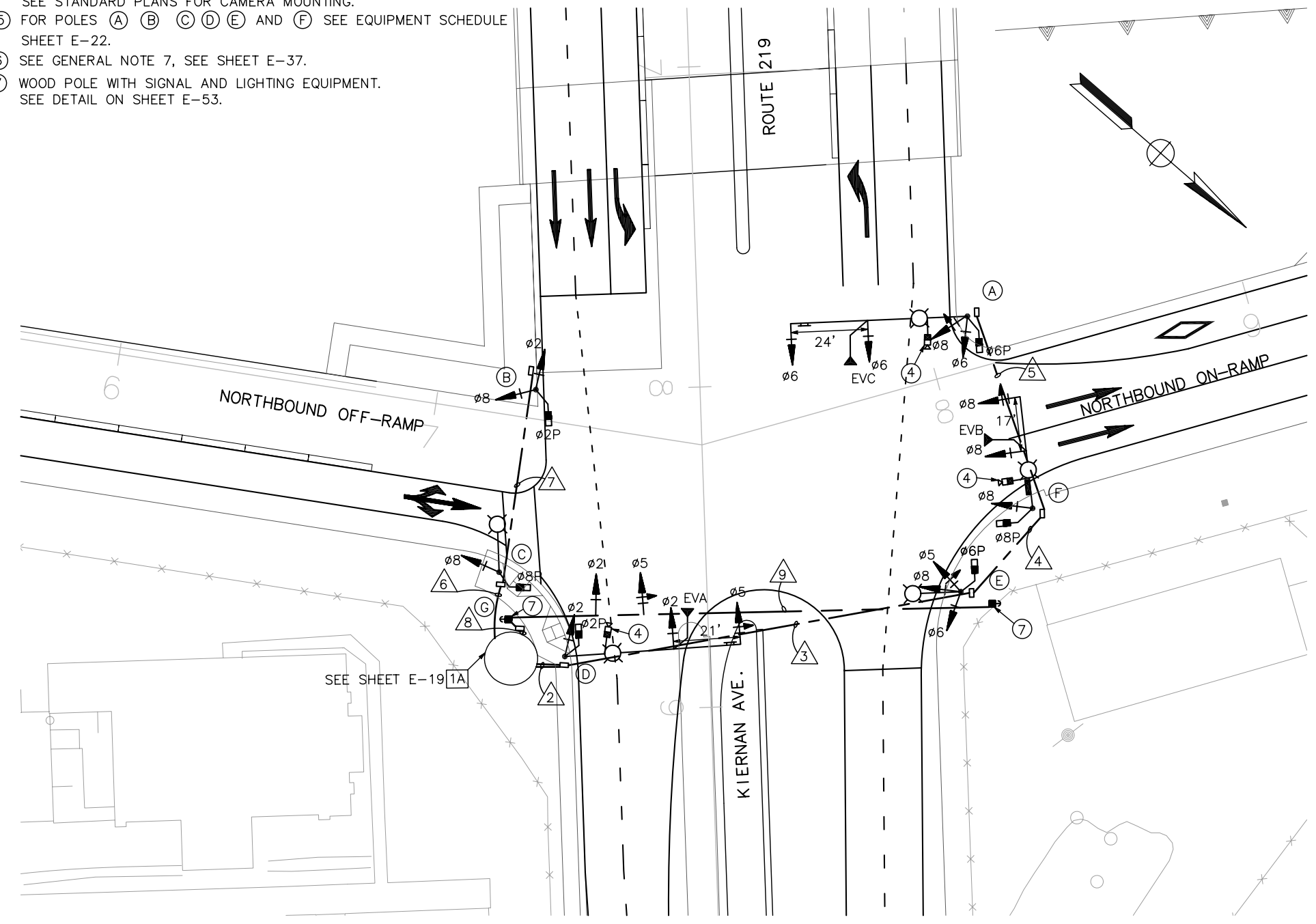
CABLE SIZE	ϕ PPB	POLE	No. OF CONDUCTORS/ RUN No.										
			Δ	Δ	Δ	Δ	Δ	Δ	Δ	Δ			
$\phi 6, \phi 8$		POLE A	1-0	1-0	1-0	1-0							
$\phi 2, \phi 8, \phi 2P$	2PPB	POLE B							1-1	1-1			
$\phi 8$	2PPB	POLE C							1-1				
$\phi 2, \phi 5, \phi 2P$		POLE D	1-0										
$\phi 5, \phi 6, \phi 8$		POLE E	1-0	1-0									
$\phi 8$		POLE F	1-0	1-0	1-0								
$\phi 2, \phi 5$		POLE G									1-0	1-0	
TOTAL SIGNAL CABLES			4-0	3-0	2-0	1-0	2-2	1-1	1-0	1-0			
#10		LUMINAIRE	2	2	2	2	2	2					
VIDEO CABLE		VIDEO DETECTION	3	2	2	1							2
CONDUIT SIZE (INCHES)			2-4"	2-3"	4"	3"	4"	3"	2"	*			

* Δ IS OVERHEAD MESSENGER CABLE, SEE DETAIL ON SHEET E-53

EQUIPMENT AND POLE SCHEDULE

SIGNAL POLE	STANDARD TYPE	LMA	VEH SIG MTG		PPB	PED SIGNAL MOUNTING	H.P.S. LUMINAIRE WATTAGE	REMARKS
			ϕ SPAN WIRE MOUNT	ϕ POLE MOUNT				
(G)	WOOD POLE	2	5	SW-2				

SW - SPAN WIRE



SIGNAL AND LIGHTING (STAGE CONSTRUCTION) STAGE 3 LOCATION 2

SCALE 1"=20'

E-51

APPROVED FOR ELECTRICAL WORK ONLY

DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR
 KEITH G. MEYER
 CHECKED BY
 SRI LAKSHMI SAKHAMURI
 CHUONG NGUYEN
 DESIGNED BY
 REVISIONS
 REVISED BY
 DATE REVISED

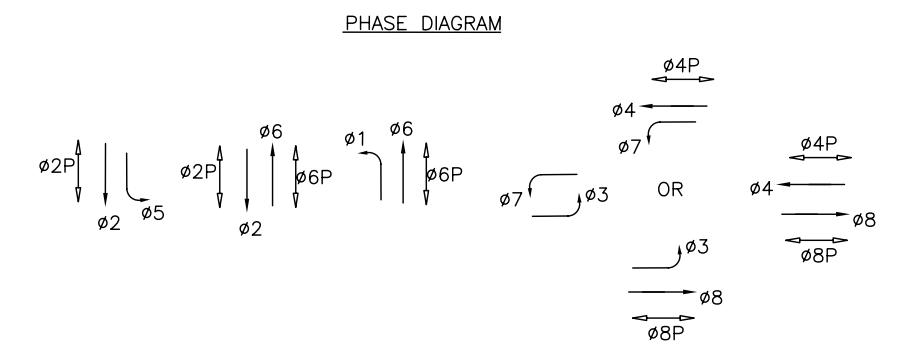
NOTES (FOR THIS HSEET ONLY):

- ① PERMANENT ELECTRICAL EQUIPMENT TO BE INSTALLED AND IN EFFECTIVE OPERATION AT THE BEGINNING OF THE STAGE AS SHOWN ON THE PLAN OR AS NOTED.
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- ③ INSTALL TEMPORARY VIDEO DETECTION ON LUMINAIRE ARM AS SHOWN. SEE STANDARD PLANS FOR CAMERA MOUNTING.
- ④ FOR ALL POLES EXCEPT FOR (B) SEE EQUIPMENT SCHEDULE SHEET E-25.
- ⑤ POLE (B) EXISTING TO REMAIN FOR THIS STAGE.



CONDUCTOR SCHEDULE

CABLE SIZE	PPB	POLE	No. OF CONDUCTORS/ RUN No.																	
			1	2	3	4	5	6	7	8	9	10								
ø1, ø6	4PPB	POLE A	1-1	1-1	1-1	1-1	1-1	1-1	1-1	1-1										
ø1, ø2, ø4, ø7, ø2P, ø4P	4PPB 2PPB	POLE B															2-2	2-2		
ø7, ø8, ø8P	2PPB	POLE C																	1-1	
ø7, ø8, ø2P	8PPB	POLE D	1-1																	
ø5, ø8P	8PPB	POLE E	1-1	1-1																
ø5, ø6, ø6P	8PPB	POLE F	1-1	1-1	1-1															
ø3, ø8, ø8P	6PPB	POLE G	1-1	1-1	1-1	1-1														
ø3, ø4	6PPB	POLE H	1-1	1-1	1-1	1-1	1-1													
ø4P, ø6P		POLE I	1-0	1-0	1-0	1-0	1-0	1-0	1-0											
TOTAL SIGNAL CABLES			7-6	6-5	5-4	4-3	3-2	2-1	1-1	3-3	2-2									
#10		LUMINAIRE	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
VIDEO CABLE		VIDEO DETECTION	3	2	2	2	1	1	1	1	1	1	1	1	1	1	1	1	1	1
CONDUIT SIZE (INCHES)			2-4"	2-4"	2-4"	2-3"	4"	3"	3"	2-4"	4"									



**SIGNAL AND LIGHTING
 (STAGE CONSTRUCTION)
 STAGE 3 LOCATION 3**

SCALE 1"=20'

E-52

APPROVED FOR ELECTRICAL WORK ONLY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	327	423

<i>David W. Clow</i> 5/18/12 REGISTERED ELECTRICAL ENGINEER DATE	
9/04/12 PLANS APPROVAL DATE	
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

GENERAL NOTES:

SPECIFICATIONS

DESIGN: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS DATED 2001.

LOADING

WIND LOADINGS: 85 MPH.

UNIT STRESSES

TIMBER POLES: Fb = 1850 psi TAPERED TREATED ROUND POLE
 Fv = 110 psi ASTM D2899 STANDARD
 E = 1500 x 10³ psi

TREATMENT

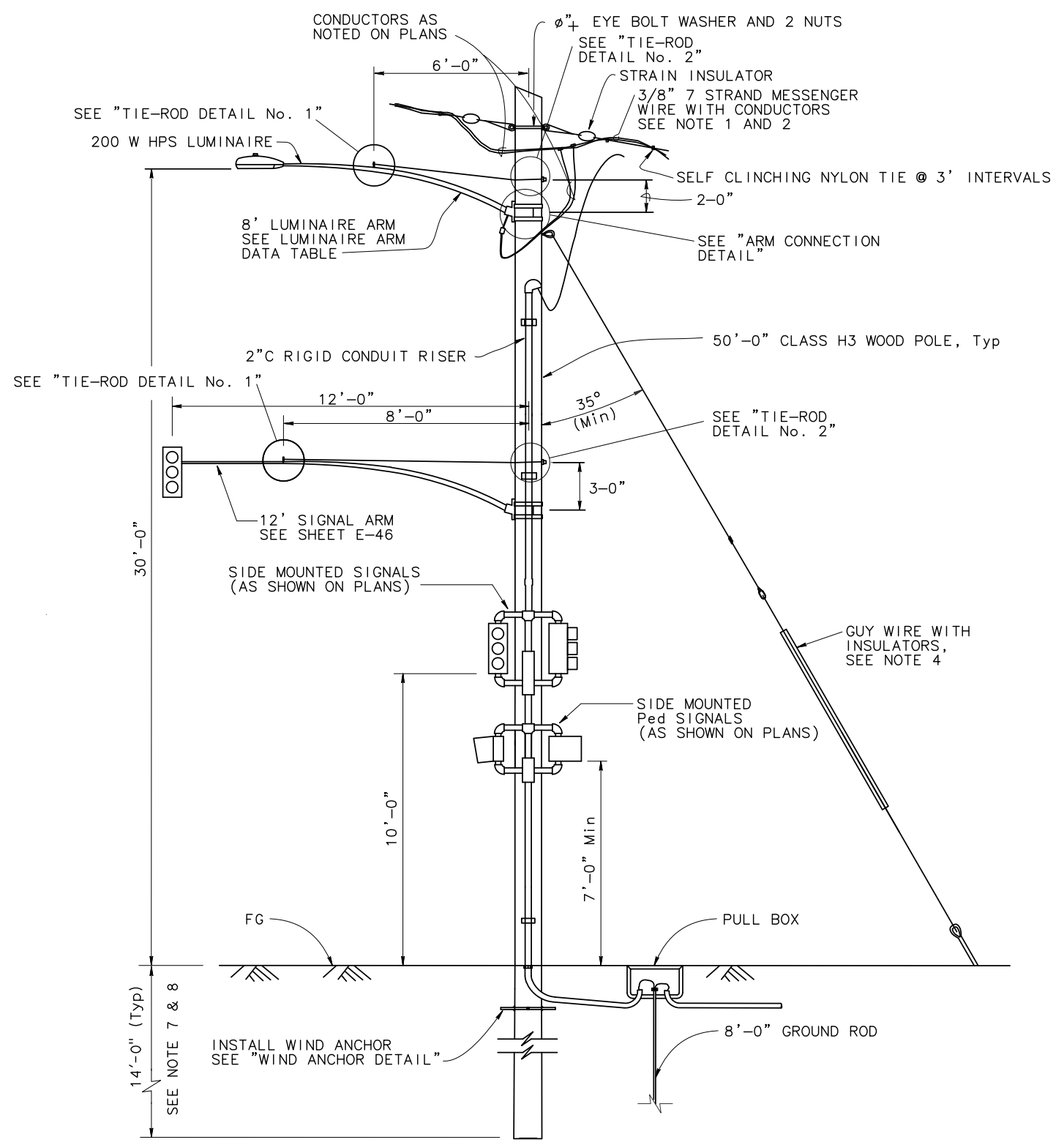
TO CONFORM WITH SECTION 86 STANDARD SPECIFICATIONS.

SPECIFICATION

CALTRANS STANDARD SPECIFICATIONS MAY 2010
 ANSI WOOD POLES
 UTILITY GRADE WIRES

NOTES:

1. ALL OVERHEAD CABLES SHALL BE INSTALLED WITH 25'-0" MINIMUM OVERHEAD CLEARANCE.
2. CONDUCTORS SHALL BE SUSPENDED FROM SPAN-WIRE AS FOLLOWS:
 A) MAIN RUN 3/8" SPAN-WIRE WITH A MAXIMUM OF 4.5% SAG.
 NO SPARE CONDUCTORS ALLOWED EXCEPT AS NOTED.
3. OVERHEAD LINE CONSTRUCTION NOT SPECIFICALLY COVERED HERE SHALL CONFORM WITH THE PROVISIONS OF GENERAL ORDER No. 95 OF PUBLIC UTILITIES COMMISSION.
4. WOOD POLES SHALL BE STABILIZED USING GUY WIRES, BREAST BLOCKS OR RAKES AT EACH DEAD END, CORNER, OH DROP OR LINE DEVIATION MORE THAN 15° FROM STRAIGHT LINE. THE DIRECTION OF THE GUY SHALL COUNTERACT THE RESULTANT OF UNBALANCED FORCE APPLIED TO POLE. WHERE SPACE OR CONFLICT PREVENT GUY INSTALLATION, A DIAGONAL BRACE SHALL BE USED. THE BRACE SHALL BE WOOD AND SHALL BE CONNECTED TO THE POLE BY MEANS TO SATISFY STRUCTURAL AND ELECTRICAL REQUIREMENTS. THE DIRECTION OF THE BRACE SHALL COUNTERACT THE RESULTANT OF UNBALANCED HORIZONTAL FORCE OF 4000 POUNDS (Min) APPLIED TO THE POLE.
5. GUY SHALL BE ATTACHED TO POLE AS NEARLY AS PRACTICAL TO THE CENTER OF CONDUCTORS LOAD, OR 4'-0" Max OTHERWISE, SEE NOTE 4.
6. ALL ATTACHMENTS SHALL BE MOUNTED WITH STAINLESS STEEL STRAPS OR OTHER MANUFACTURERS METHODS WITHOUT DRILLING HOLES IN POLE, EXCEPT AS SHOWN. DRILLING THROUGH POLE WILL REQUIRE THE ENGINEER'S APPROVAL.
7. FOUNDATION DESIGN IS BASED ON AASHTO 2001 ARTICLE 13.6 BROMS' APPROXIMATE PROCEDURE ASSUMING A COHESIONLESS MATERIAL. THE ANGLE OF INTERNAL FRICTION USED IS 30° AND UNIT WEIGHT OF SOIL USED IS 120 Lb/Ft³. THE CONTRACTOR TO VERIFY ACTUAL SOIL CONDITION.
8. IF POLE IS LOCATED ON A STEEP SLOPE ADD 2 FEET EXTRA FOR EMBEDMENT.
9. SEE SHEETS E-54 AND E-55 FOR DETAILS.
10. FOR DETAILS NOT SHOWN, SEE "2006 STANDARD PLANS" AND "2006 REVISED STANDARD PLANS".
11. ALL TEMPORARY POLES SUPPORT OH CONDUCTORS. ATTACH LUMINAIRE ARM AND/OR COMBINATION OF ATTACHMENTS AS SPECIFIED AT LOCATIONS WHERE INDICATED ON ELECTRICAL SHEETS.
12. WOOD POLES SHALL BE PROTECTED FROM TRAFFIC BY MEANS ACCORDING TO CALIFORNIA DEPARTMENT OF TRANSPORTATION HIGHWAY DESIGN MANUAL REQUIREMENTS. SEE ROADWAY PLANS.



TYPICAL WOOD POLE SUPPORT WITH LUMINAIRE

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL.

APPROVED FOR ELECTRICAL WORK ONLY.

**ELECTRICAL DETAILS
 (STAGE CONSTRUCTION)**
 NO SCALE

E-53

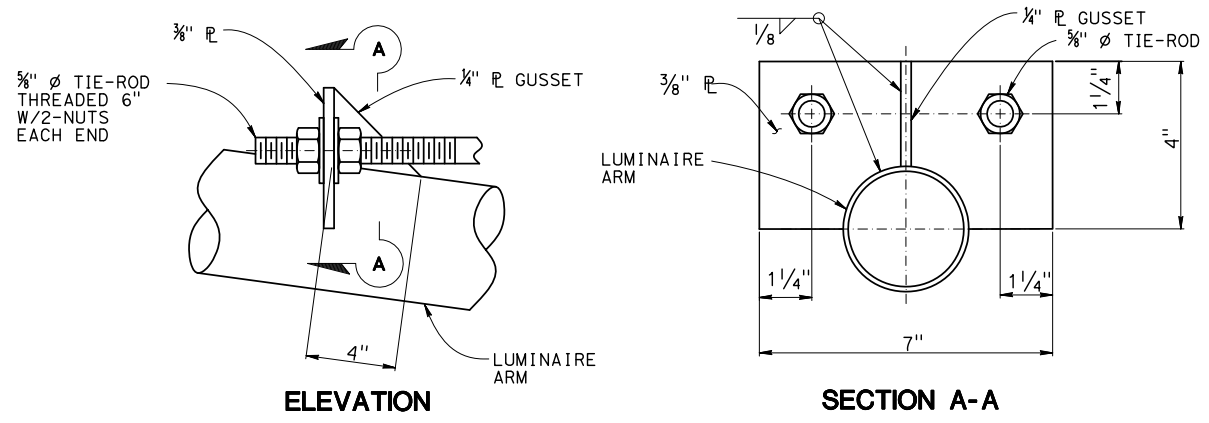
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
Caltrans	KEN NGAI	HENRY ROBINO	DAVE CLOW
	CHECKED BY	DATE REVISOR	DATE REVISOR

LAST REVISION DATE PLOTTED => \$DATE 05-18-12 TIME PLOTTED => \$TIME

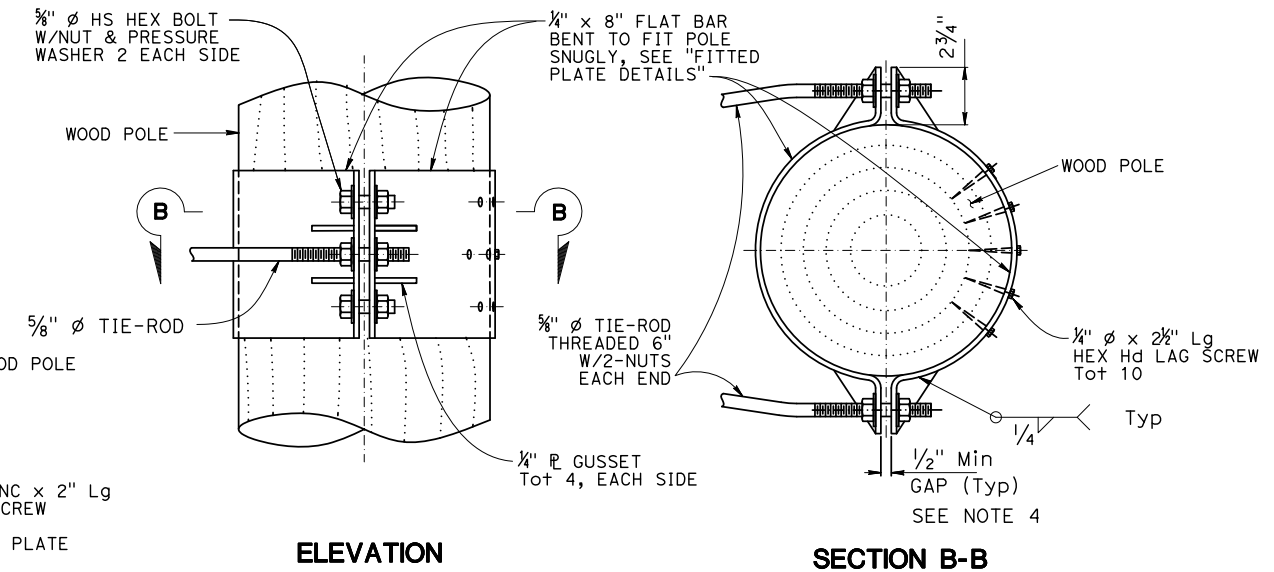
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	328	423

<i>David W. Clow</i> 5/18/12	
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.	
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358

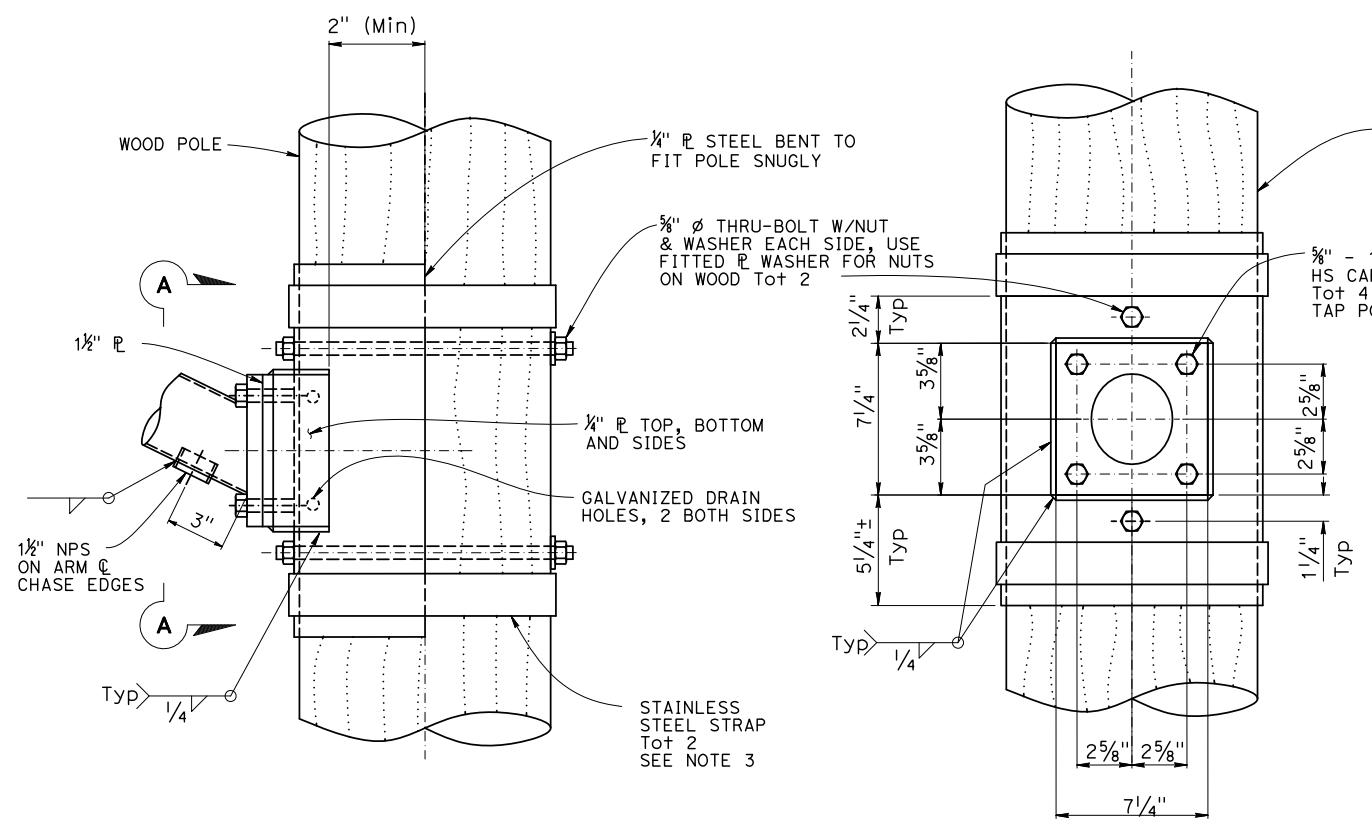
- NOTES:**
1. ALL HARDWARE AND STEEL SHALL BE GALVANIZED AFTER FABRICATION.
 2. ARM BASE CONNECTION DETAILS SHALL BE IN COMPLIANCE WITH STANDARD PLANS DETAIL SHEET ES-6D WITH NOTED MODIFICATIONS.
 3. 2000 LB Min CAPACITY STRAP SYSTEM SHALL BE USED FOR TOP AND BOTTOM OF PLATE.
 4. THE CONTRACTOR TO VERIFY POLE DIMENSIONS AT TIE-ROD ATTACHMENT HEIGHT. FABRICATE 8" FLAT BAR WITH "L" DIMENSION TO MAINTAIN AN OPEN GAP BETWEEN ENCASEMENT IN FINISHED INSTALLATION.



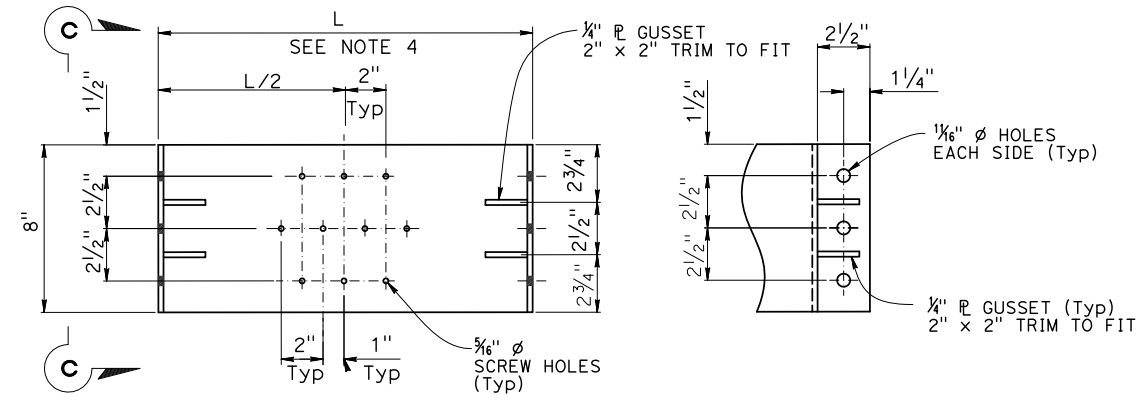
TIE-ROD DETAIL No. 1



TIE-ROD DETAIL No. 2



ARM CONNECTION DETAILS



FITTED PLATE DETAILS

NOTE: 2 REQUIRED (1 W/SCREW HOLES, 1 WITHOUT)

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL.

APPROVED FOR ELECTRICAL WORK ONLY.

ELECTRICAL DETAILS
(STAGE CONSTRUCTION)
NO SCALE

E-54

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBINO
 CHECKED BY: DAVE CLOW
 REVISIONS: REVISOR: HENRY ROBINO, DATE: 7/2/2010
 REVISOR: DAVE CLOW, DATE: 7/2/2010

USERNAME => #USER
DGN FILE => #REQUEST

RELATIVE BORDER SCALE IS IN INCHES
 0 1 2 3

UNIT 1457 PROJECT NUMBER & PHASE 100000100

LAST REVISION DATE PLOTTED => \$DATE
05-18-12 TIME PLOTTED => \$TIME

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
Caltrans
 CONSULTANT FUNCTIONAL SUPERVISOR: KEN NGAI
 CALCULATED/DESIGNED BY: HENRY ROBIÑO
 CHECKED BY: DAVE CLOW
 REVISIONS:
 REVISION NO. | DATE | BY | DESCRIPTION

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	329	423

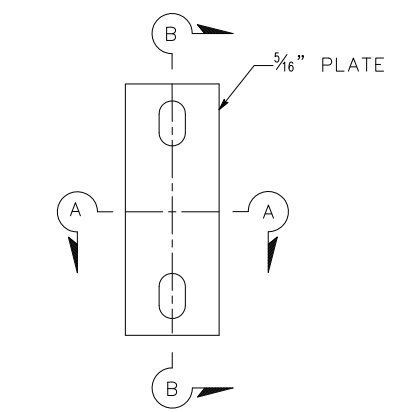
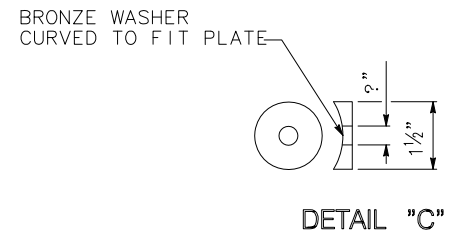
David W. Clow	5/18/12
REGISTERED ELECTRICAL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	

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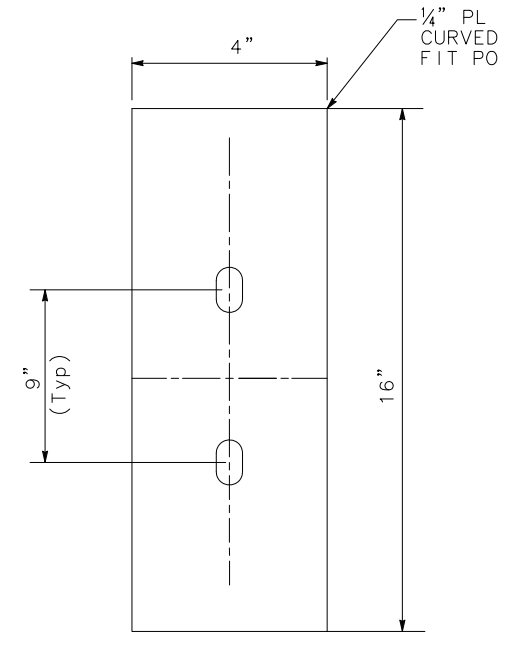
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENERY DR., #10 SANTA CLARA, CALIFORNIA 95054	STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358
--	--

NOTES:

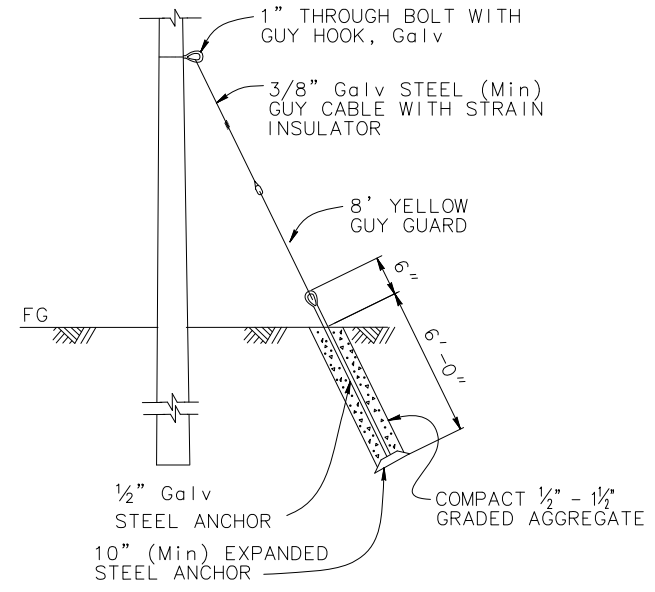
1. THE CONTRACTOR TO VERIFY SOIL CONDITIONS, SLOPE, AND ADJUST ANCHORING TO SATISFY BASIC DESIGN REQUIREMENTS. SEE NOTE 7 ON SHEET E-53.
2. THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL.



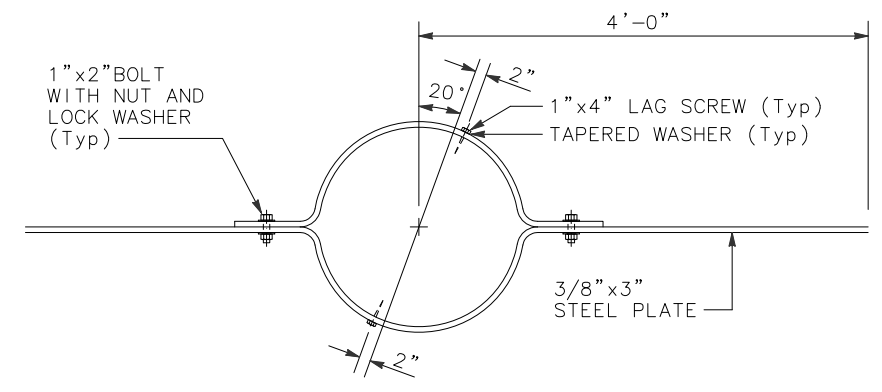
COMPARTMENT PLATE
 MODIFIED POLE PLATE SEE Std DETAIL "POLE PLATE" FOR SIDE MOUNTINGS



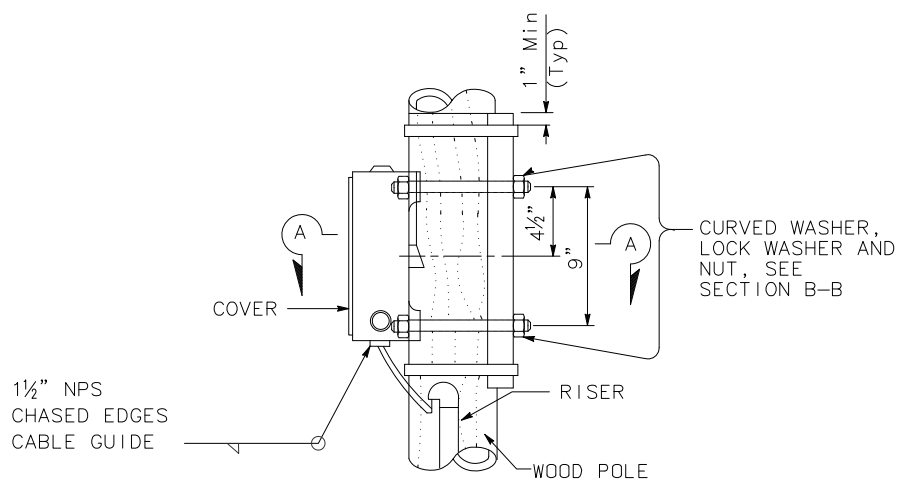
BACK PLATE



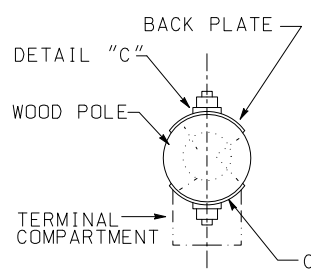
GUY WIRE INSTALLATION DETAIL



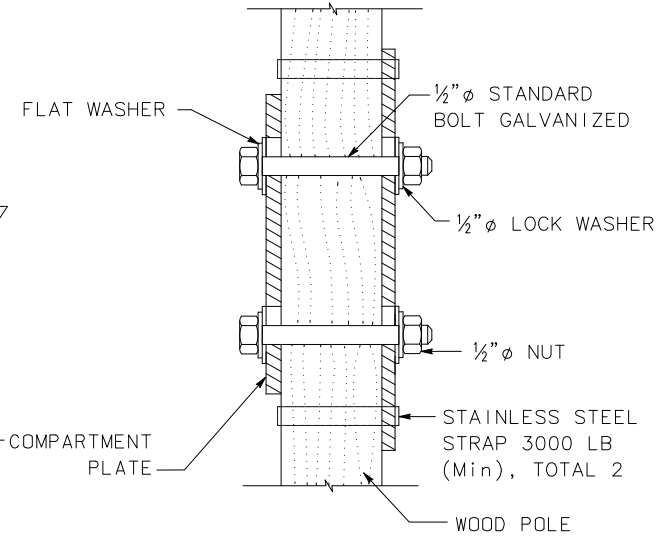
WIND ANCHOR
 TO BE INSTALLED PERPENDICULAR TO MAST ARMS AND 2'-0" Min BELOW GRADE



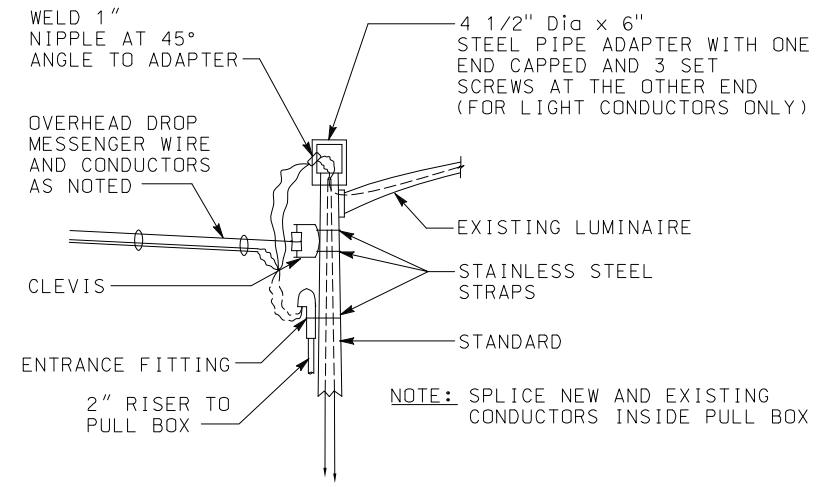
SIDE MOUNTING TERMINAL COMPARTMENTS



SECTION A-A



SECTION B-B



TYPICAL OVERHEAD DROP INSTALLATION DETAIL
 (FOR FIXED BASE ELECTROLIER ONLY)

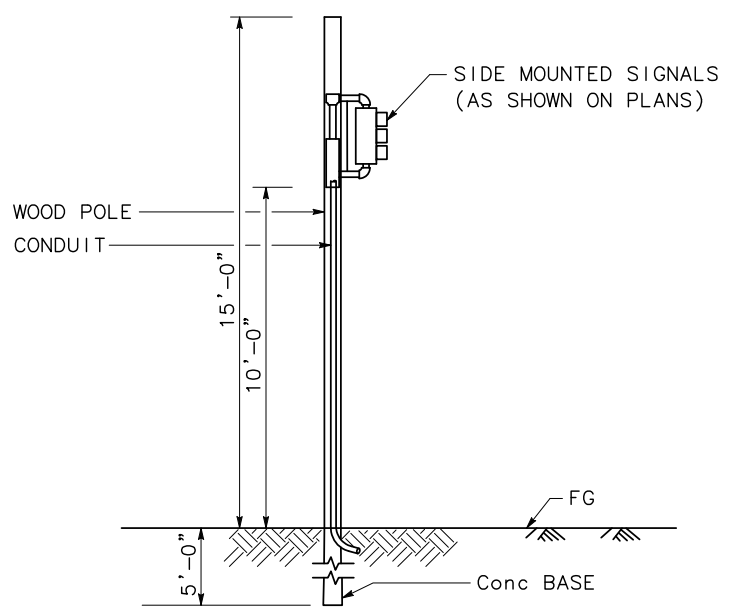
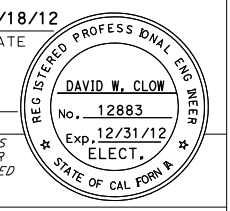
SIGNAL HEADS MOUNTING
 (FOR DETAILS NOT SHOWN SEE RSP ES-4D)

ELECTRICAL DETAILS
(STAGE CONSTRUCTION)
 NO SCALE

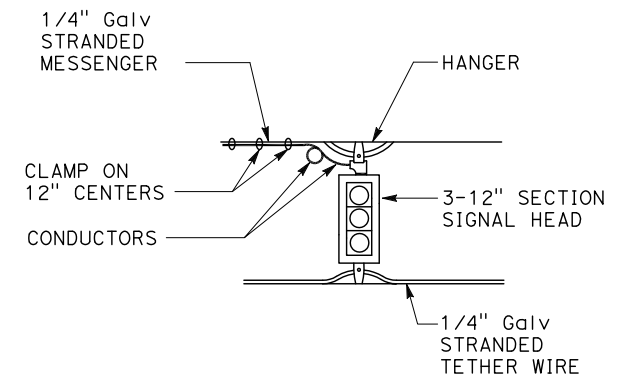
E-55

APPROVED FOR ELECTRICAL WORK ONLY.

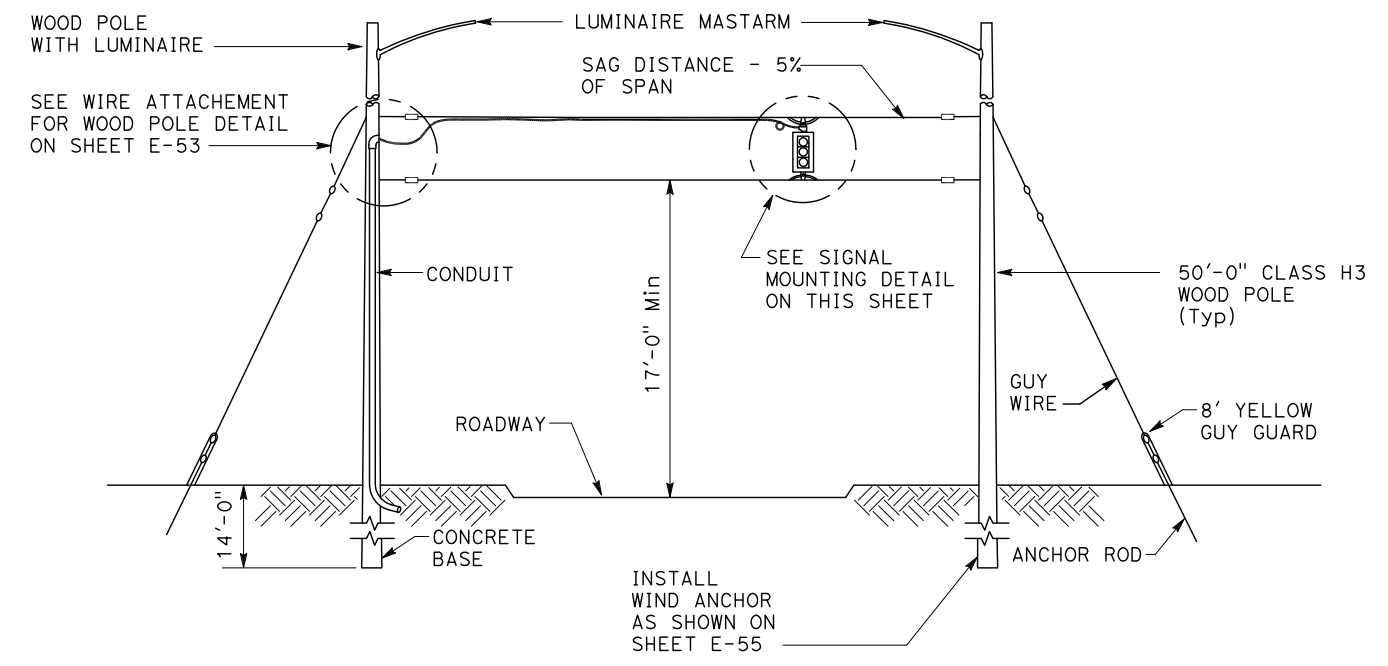
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	330	423
David W. Clow		5/18/12			
REGISTERED ELECTRICAL ENGINEER		DATE			
9/04/12					
PLANS APPROVAL DATE					
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
ALLIANCE ENGINEERING CONSULTING, INC. 4701 PATRICK HENRY DR., #10 SANTA CLARA, CALIFORNIA 95054			STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CALIFORNIA 95358		



TYPICAL TRAFFIC SIGNAL ON TEMPORARY WOOD POLE



SIGNAL MOUNTING DETAIL



TYPICAL WOOD POLE SPAN WIRE DETAIL

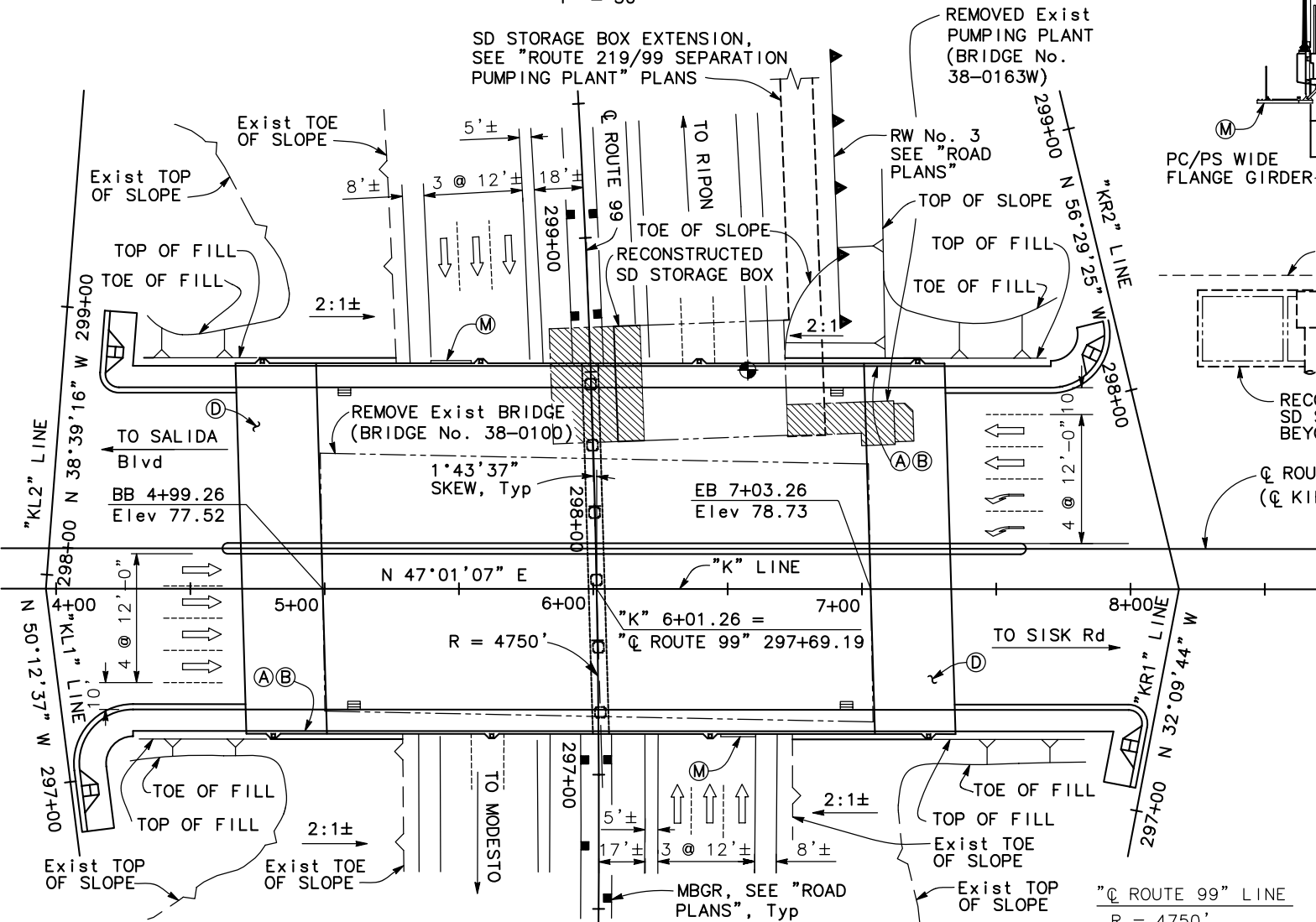
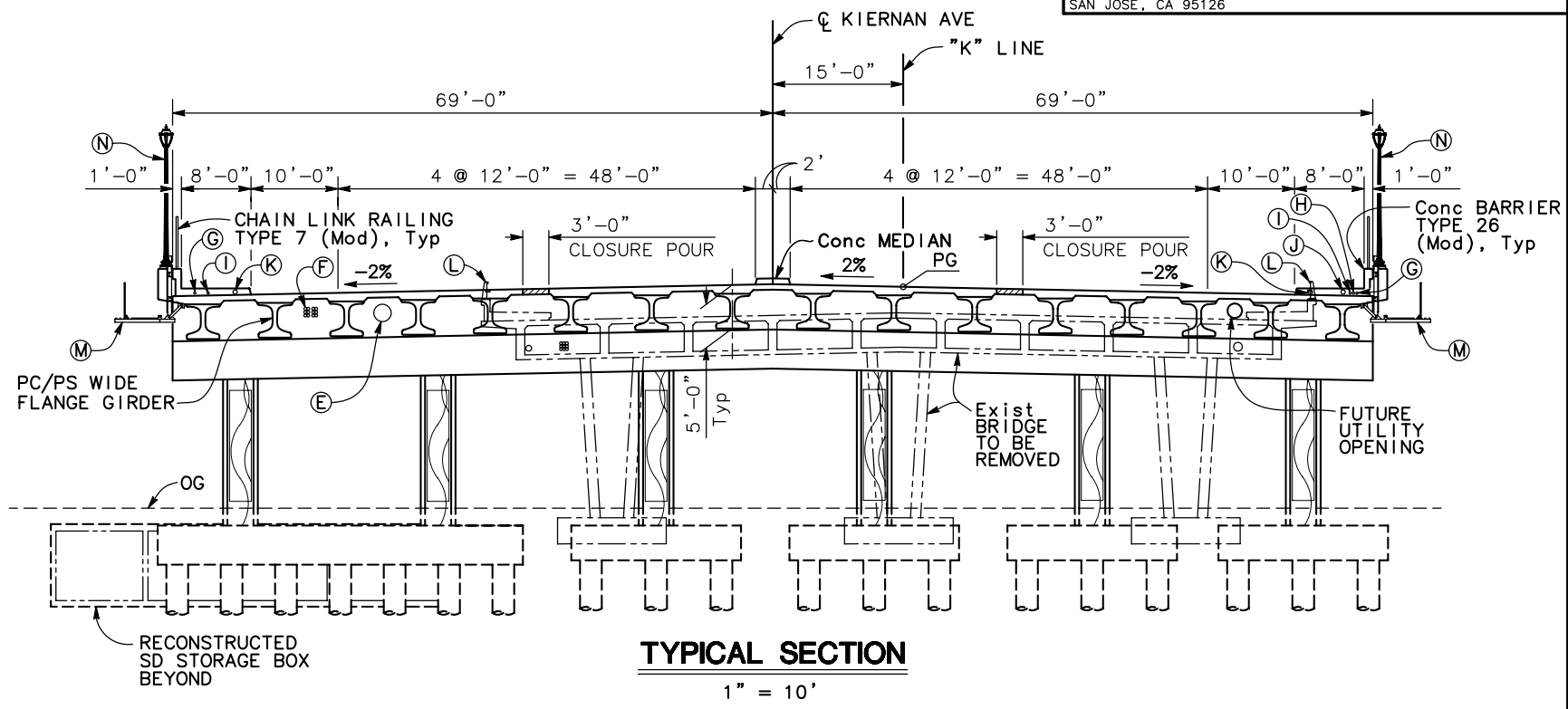
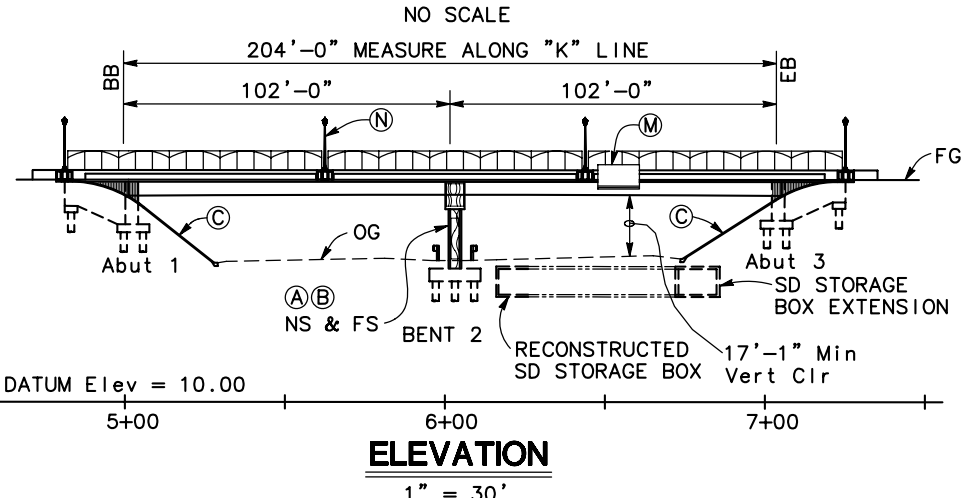
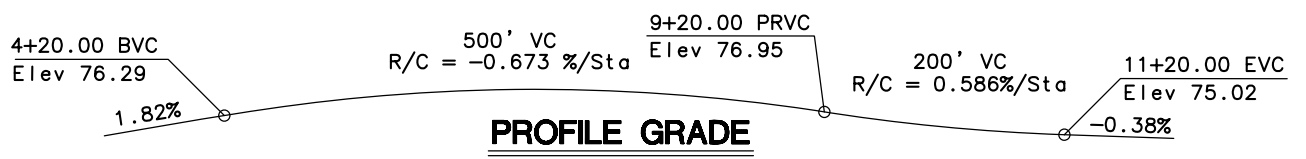
**ELECTRICAL DETAILS
(STAGE CONSTRUCTION)
NO SCALE**

E-56

APPROVED FOR ELECTRICAL WORK ONLY.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED/DESIGNED BY	REVISOR
Caltrans	KEN NGAI	HENRY ROBINO	HENRY ROBINO
		CHECKED BY	DATE REVISOR
		DAVE CLOW	DAVE CLOW

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	331	423
REGISTERED CIVIL ENGINEER			DATE	5/6/12	
PLANS APPROVAL DATE			9/04/12		
The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126					



NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

SEE "INDEX TO PLANS" SHEET FOR QUANTITIES

LEGEND:

- (A) Paint 'ROUTE 219/99 SEPARATION' and year completed
- (B) Paint 'Bridge No. 38-0163'
- (C) Concrete Slope Paving - Full Slope as last order of work after all staged construction finished.
- (D) Structure Approach Slab Type N(30S)
- (E) 16" Dia Water Line in 24" Dia Casing
- (F) 12-4" AT&T Conduits for Tel & Fiber Optic Lines
- (G) 2" Dia Conduit (County Lighting), see "Road Plans"
- (H) 2" Dia Conduit (Signal Interconnect), see "Road Plans"
- (I) 2" Dia Conduit (Spare), see "Road Plans"
- (J) 4" Dia Schedule 40 PVC Conduit (Charter Communication)
- (K) 4" Dia Future Utility Opening
- (L) Remove and salvage existing Type 11 Barrier Railing
- (M) Bridge Mounted Sign, see "Road Plans"

- (N) Electrolier on Barrier, see B11-54 ES-9A ES-9B ES-9C ES-9D for Anchorage Details and "Road Plans" for Type, location, Bolt Circle Diameter, and details.
- ⊙ Indicates point of minimum vertical clearance
- ≡ Indicates Deck Drain Type D-1 (Mod)
- ▲▲ Indicates Retaining Wall
- Indicates existing structure
- ▨ Pumping Plant Removal
- ▨ Closure Pour

DESIGN OVERSIGHT	BY A. WANG	CHECKED C. SHENOY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE
DETAILS	BY S. DESALEGN	CHECKED K. HARIRSAZ	LAYOUT	BY S. DESALEGN
SIGN OFF DATE	BY S. DESALEGN	CHECKED B. SWARUP	SPECIFICATIONS	BY K. HARIRSAZ

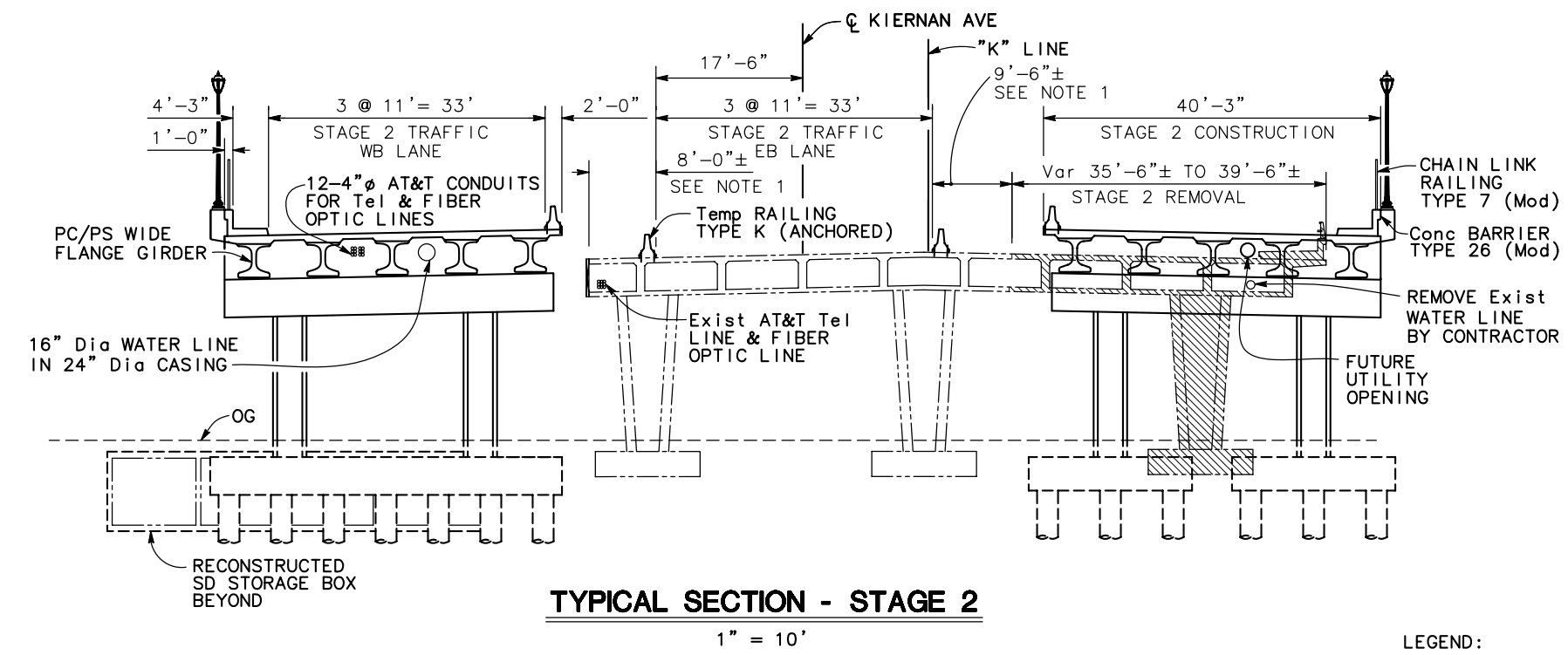
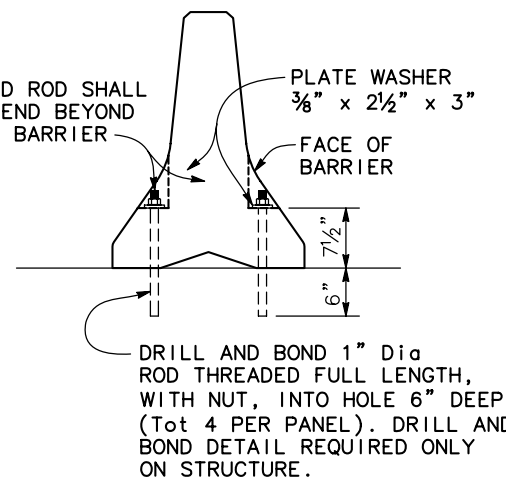
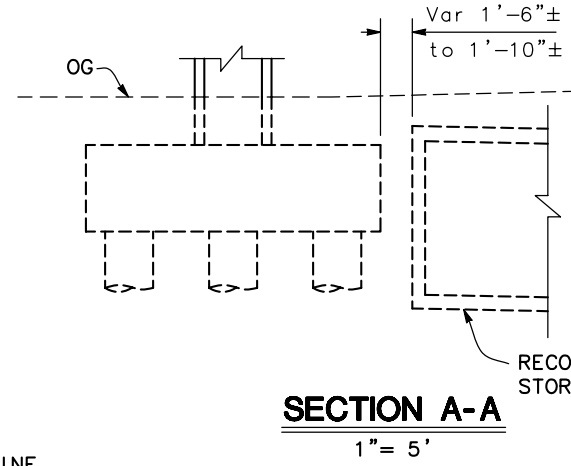
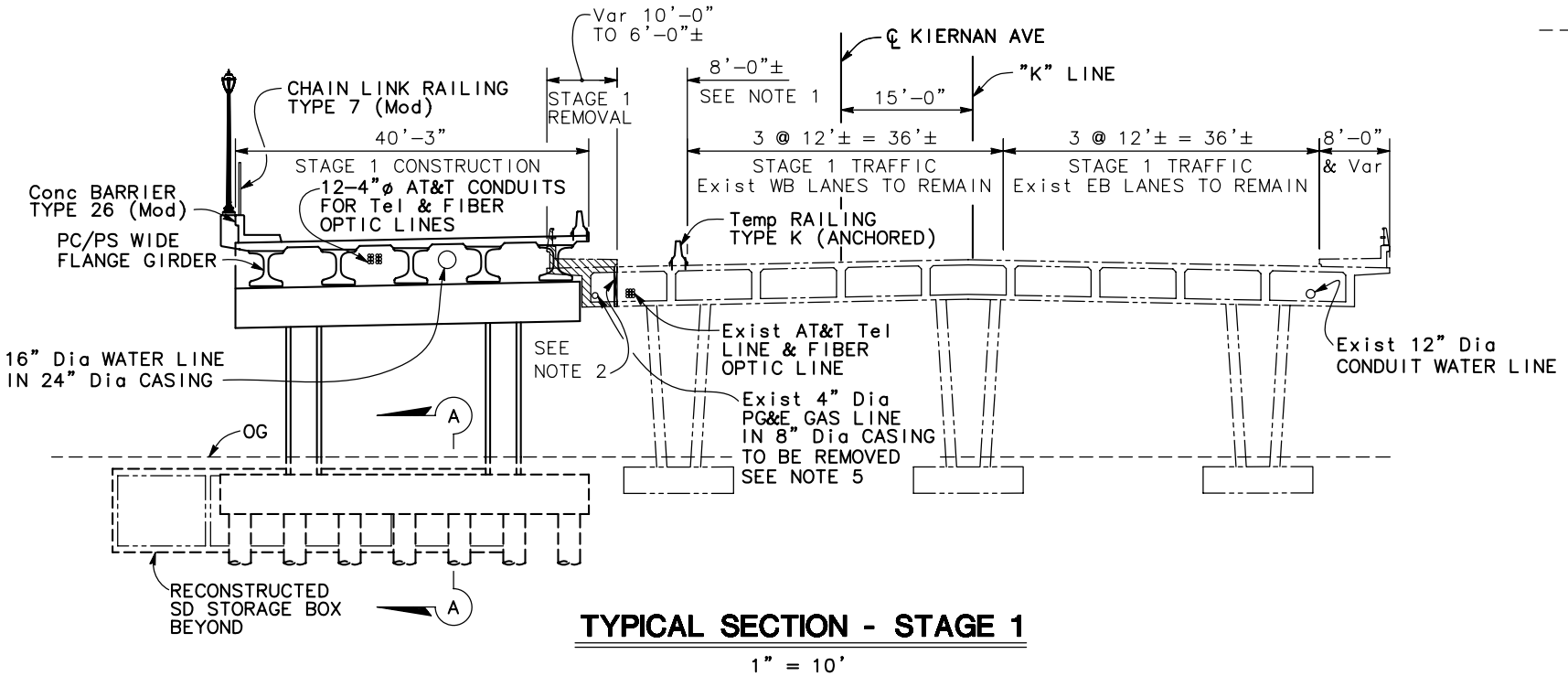
BRIDGE NO.	38-0163
POST MILES	R22.56
PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	
A. WANG	PROJECT ENGINEER

ROUTE 219/99 SEPARATION (REPLACE) GENERAL PLAN No. 1	
UNIT:	1457
PROJECT NUMBER & PHASE:	1000000100
CONTRACT NO.:	10-0L3304

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	332	423

REGISTERED CIVIL ENGINEER
 K. Harirsa
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



LEGEND:
 Bridge Removal

- NOTES:
- No construction loading allowed over the space between Temporary Railing K (Anchored) and edge of the partially demolished existing bridge.
 - Existing utilities shall be protected in place within the bridge cell to avoid potential damage from construction activities, debris and water.
 - Main cap reinforcement extending during Stage 1 Construction and reinforcement across construction joint shall be mechanically spliced. See "BENT LAYOUT" sheet and "BENT DETAILS No. 1" sheet for details.
 - For roadway closures, traffic handling and detour operations during demolition of the existing bridge, erection of the precast-prestressed wide flange girder, deck form placement and deck form removal, see "Road Plans"
 - Contractor is responsible for removing the 4" diameter gas line and 8" diameter casing after PG&E capped the gas line at both ends of bridge.

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) GENERAL PLAN No. 2
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	LAYOUT BY S. DESALEGN	CHECKED K. HARIRSAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	SPECIFICATIONS BY K. HARIRSAZ	PLANS AND SPECS COMPARED K. HARIRSAZ	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 2 OF 46

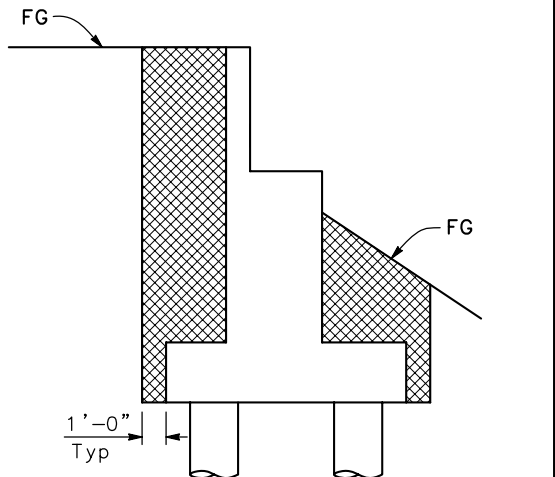
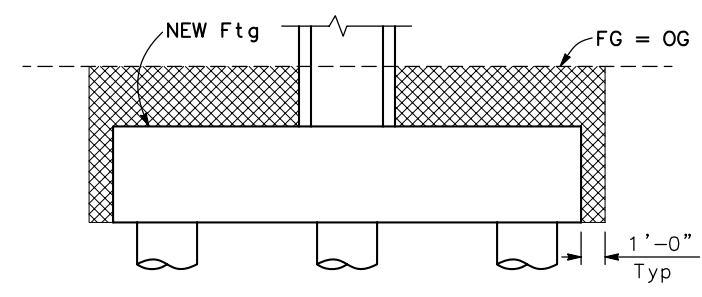
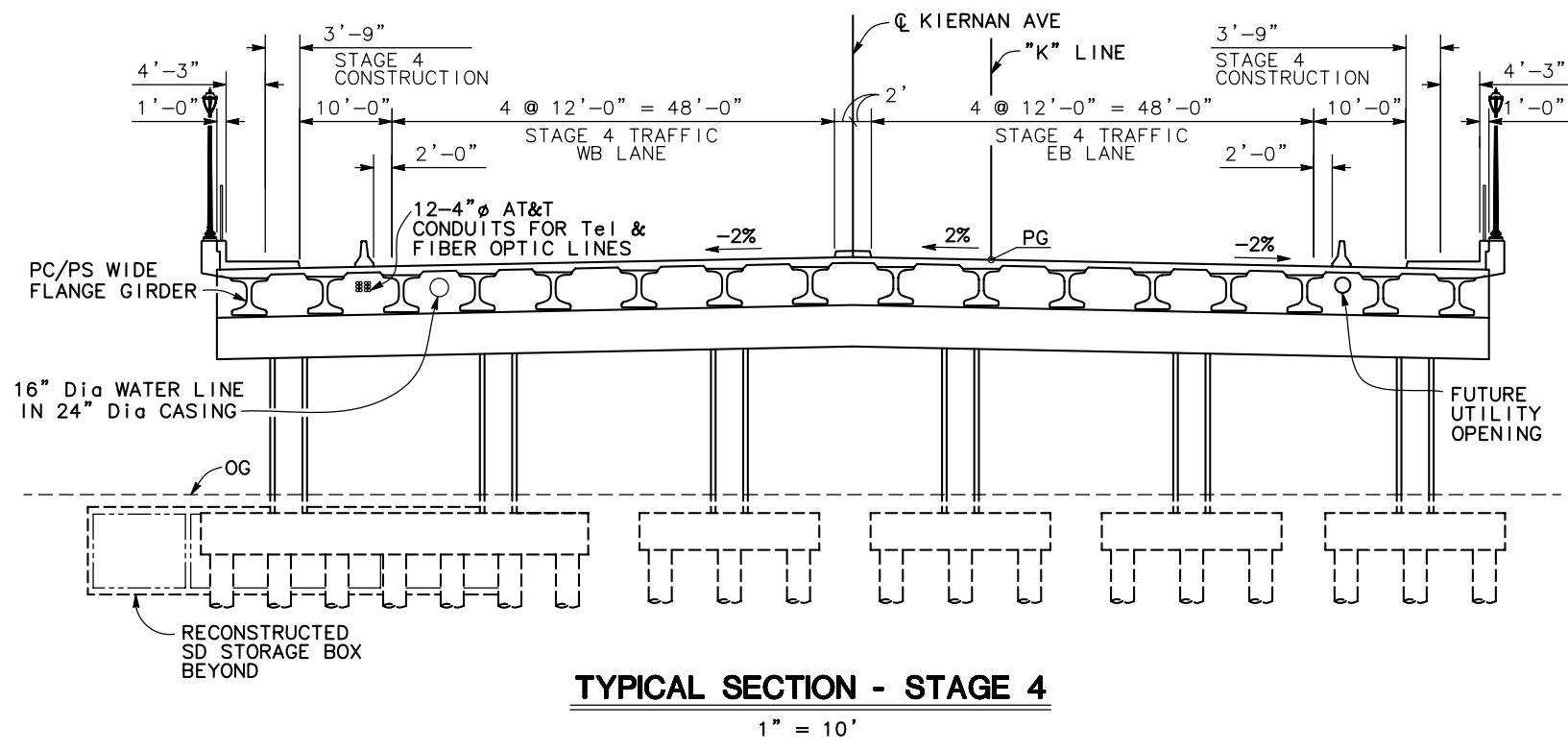
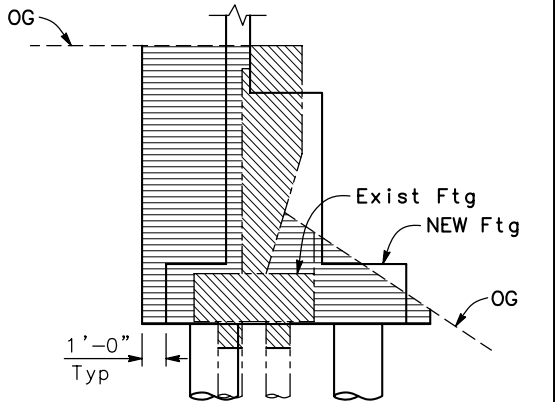
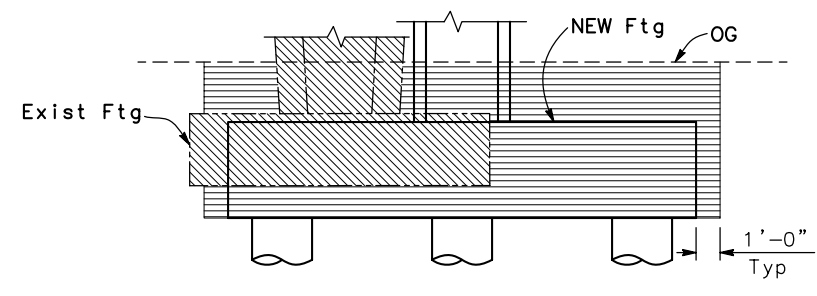
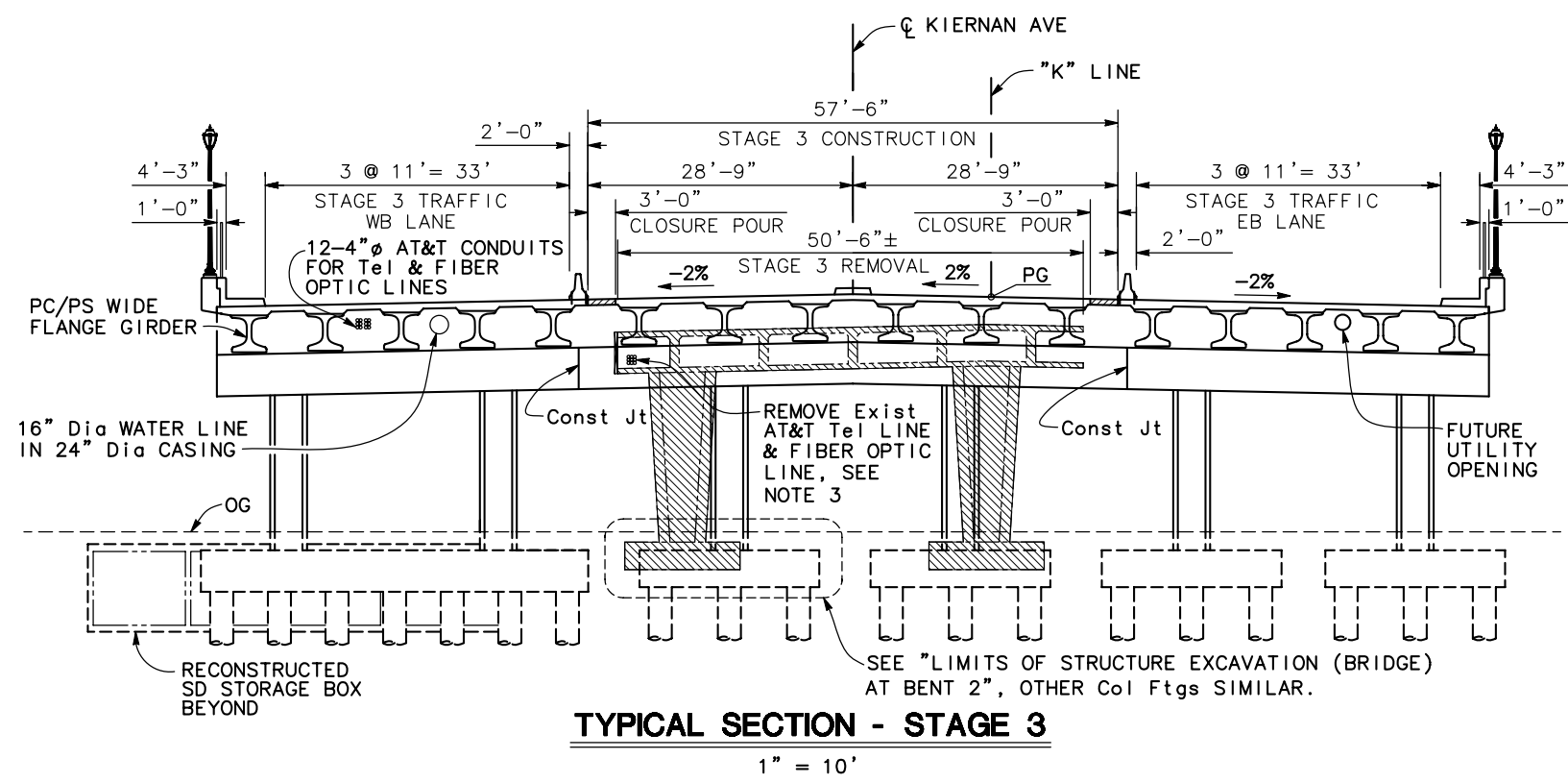
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	333	423

REGISTERED CIVIL ENGINEER
 K. Harisaz
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12
 REGISTERED PROFESSIONAL ENGINEER
 KIANOUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

NOTES:

1. Main cap reinforcement extending during Stage 1 Construction and reinforcement across construction joint shall be mechanically spliced. See "BENT LAYOUT" sheet and "BENT DETAILS No. 1" sheet for details.
2. For roadway closures, traffic handling and detour operations during demolition of the existing bridge, erection of the precast-prestressed wide flange girder, deck form placement and deck form removal, see "Road Plans"
3. Contractor is responsible for removing the empty conduits after AT&T removed all the Telephone and Fiber Optic cables.



- LEGEND:
- Bridge Removal
 - Closure Pour
 - Structure Excavation (Bridge)
 - Structure Backfill (Bridge)

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) GENERAL PLAN No. 3
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	LAYOUT BY S. DESALEGN	CHECKED K. HARIRSAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	SPECIFICATIONS BY K. HARIRSAZ	PLANS AND SPECS COMPARED K. HARIRSAZ	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 3 OF 46

GENERAL NOTES
LOAD AND RESISTANCE FACTOR DESIGN

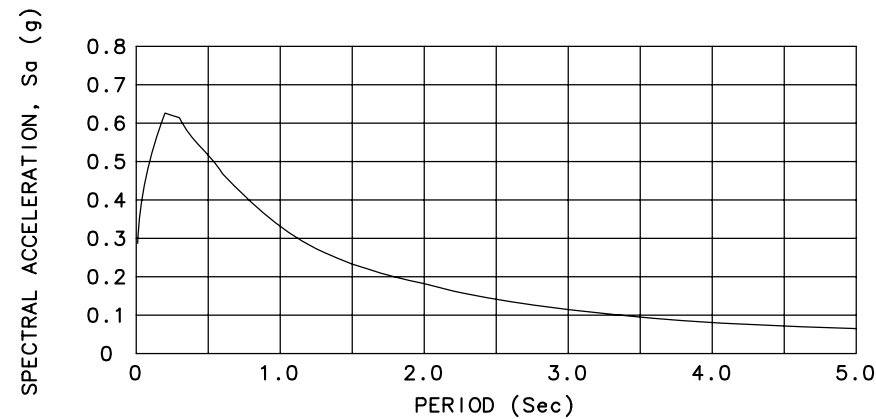
DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION AND THE CALTRANS AMENDMENTS, PREFACE DATED SEPTEMBER 2010; EXCEPT THAT BRIDGE (Incl. BARRIER AND RAILING) DETAILS TAKEN FROM STANDARD PLANS 2010 AND 2011, STANDARD BRIDGE DETAILS XS SHEETS, BRIDGE ABUTMENTS ARE DESIGNED USING BRIDGE DESIGN SPECIFICATIONS ('96 AASHTO WITH REVISION BY CALTRANS)

SEISMIC DESIGN: CALTRANS SEISMIC DESIGN CRITERIA (SDC), VERSION 1.6 DATED NOV, 2010

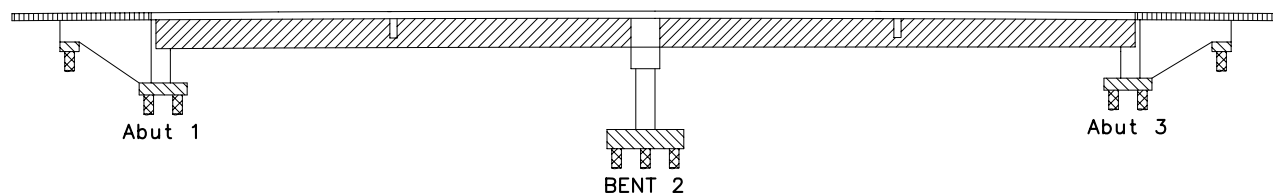
DEAD LOAD: INCLUDES 35 psf FOR FUTURE WEARING SURFACE THE DECK LOAD BETWEEN THE GIRDERS HAS BEEN INCREASED BY A FACTOR OF 10% TO ALLOW FOR THE USE OF STEEL DECK FORMS

LIVE LOADING: HL93 w/ "LOW-BOY" AND PERMIT DESIGN VEHICLE

SEISMIC LOAD: SOIL PROFILE $V_{330} = 300$ m/s
 $M_{max} = 6.7$, SEE CURVE BELOW
PEAK ROCK ACCELERATION = 0.29 g



CONCRETE: $f_y = 60$ ksi
 $f'_c = 3.6$ ksi (EXCEPT AS SHOWN ON 'CONCRETE STRENGTH AND TYPE LIMITS' DIAGRAM
 $n = 8$
SEE 'PRESTRESSING NOTES' ON "PC/PS CONCRETE GIRDER DETAILS No. 1" SHEET



	Structural Concrete, Bridge ($f'_c = 4.0$ ksi @ 28 days)		Structural Concrete, Bridge Footing
	PC/PS Concrete Girder, see "PC/PS CONCRETE GIRDER DETAILS No. 1" sheet for strength		Cast-in-Drilled-Hole Concrete Pile ($f'_c = 4.0$ ksi @ 28 days)
	Structural Concrete, Approach Slab		

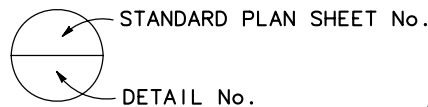
CONCRETE STRENGTH AND TYPE LIMITS

NO SCALE

NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

STANDARD PLANS DATED 2010

- A10A ABBREVIATIONS (SHEET 1 OF 2)
- A10B ABBREVIATIONS (SHEET 2 OF 2)
- A10C LINES AND SYMBOLS (SHEET 1 OF 3)
- A10D LINES AND SYMBOLS (SHEET 2 OF 3)
- A10E LINES AND SYMBOLS (SHEET 3 OF 3)
- A62C LIMITS OF PAYMENT FOR EXCAVATION AND BACKFILL - BRIDGE
- B0-1 BRIDGE DETAILS
- B0-3 BRIDGE DETAILS
- B0-5 BRIDGE DETAILS
- B0-13 BRIDGE DETAILS
- B2-3 16" AND 24" CAST-IN-DRILLED-HOLE CONCRETE PILE
- B6-10 UTILITY OPENINGS T-BEAM
- B6-21 JOINT SEALS (MAXIMUM MOVEMENT RATING = 2")
- B7-6 DECK DRAINS TYPE D-1 AND D-2
- B7-8 DECK DRAINAGE DETAILS
- B7-10 UTILITY OPENING BOX GIRDER
- B7-11 UTILITY DETAILS
- B11-52 CHAIN LINK RAILING TYPE 7
- B11-54 CONCRETE BARRIER TYPE 26
- ES-9A ELECTRICAL SYSTEMS (STRUCTURAL PULL BOX INSTALLATIONS)
- ES-9B ELECTRICAL SYSTEMS (CONDUIT RISER AND EXPANSION FITTING, STRUCTURE INSTALLATIONS)
- ES-9C ELECTRICAL SYSTEMS (STRUCTURAL PULL BOX)
- ES-9D ELECTRICAL SYSTEMS (STRUCTURAL PULL BOX INSTALLATIONS)
- T3A TEMPORARY RAILING (TYPE K)



QUANTITIES

BRIDGE REMOVAL	LUMP SUM
SALVAGE METAL BRIDGE RAILING	480 LF
STRUCTURE EXCAVATION (BRIDGE)	1,319 CY
STRUCTURE BACKFILL (BRIDGE)	1,036 CY
4" Dia (CHARTER COMMUNICATION) CONDUIT (BRIDGE)	304 LF
24" CAST-IN-DRILLED-HOLE CONCRETE PILING	4,510 LF
30" CAST-IN-DRILLED-HOLE CONCRETE PILING	4,056 LF
STRUCTURAL CONCRETE, BRIDGE FOOTING	596 CY
STRUCTURAL CONCRETE, BRIDGE	1,652 CY
STRUCTURAL CONCRETE, APPROACH SLAB	307 CY
MINOR CONCRETE (MINOR STRUCTURE)	26 CY
ARCHITECTURAL TEXTURE (BENT CAP AND COLUMN)	943 SQFT
FRACTURED RIB TEXTURE	1,104 SQFT
DRILL AND BOND DOWEL	1,142 LF
FURNISH PRECAST PRESTRESSED CONCRETE WIDE FLANGE GIRDER (100' - 110')	32 EA
ERECT PRECAST PRESTRESSED CONCRETE GIRDER JOINT SEAL (MR 1")	32 EA
BAR REINFORCING STEEL (BRIDGE)	276 LF
SLOPE PAVING (PATTERNED CONCRETE)	849,258 LB
MISCELLANEOUS METAL (BRIDGE)	129 CY
CHAIN LINK RAILING TYPE 7 (MODIFIED)	72,000 LB
CONCRETE BARRIER TYPE 26 (MODIFIED)	488 LF
	528 LF

INDEX TO BRIDGE PLANS

SHEET No.	TITLE
1	GENERAL PLAN No. 1
2	GENERAL PLAN No. 2
3	GENERAL PLAN No. 3
4	INDEX TO PLANS
5	DECK CONTOURS
6	DEMOLITION PLAN No. 1
7	DEMOLITION PLAN No. 2
8	FOUNDATION PLAN
9	ABUTMENT 1 LAYOUT
10	ABUTMENT 3 LAYOUT
11	ABUTMENT DETAILS No. 1
12	ABUTMENT DETAILS No. 2
13	ABUTMENT DETAILS No. 3
14	ABUTMENT DETAILS No. 4
15	BENT LAYOUT
16	BENT DETAILS No. 1
17	BENT DETAILS No. 2
18	BENT DETAILS No. 3
19	BENT DETAILS No. 4
20	TYPICAL SECTION
21	GIRDER LAYOUT
22	DECK REINFORCEMENT No. 1
23	DECK REINFORCEMENT No. 2
24	PC/PS CONCRETE GIRDER DETAILS No. 1
25	PC/PS CONCRETE GIRDER DETAILS No. 2
26	PC/PS CONCRETE GIRDER DETAILS No. 3
27	PC/PS CONCRETE GIRDER DETAILS No. 4
28	DECK DRAIN DETAILS
29	CHAIN LINK RAILING TYPE 7 (MOD)
30	STRUCTURE APPROACH TYPE N (30S)
31	STRUCTURE APPROACH DRAINAGE DETAILS
32	SLOPE PAVING (PATTERNED CONCRETE) - FULL SLOPE
33	SLOPE PAVING (PATTERNED CONCRETE) DETAILS No. 1
34	SLOPE PAVING (PATTERNED CONCRETE) DETAILS No. 2
35	CONCRETE BARRIER TYPE 26 (MOD) DETAILS No. 1
36	CONCRETE BARRIER TYPE 26 (MOD) DETAILS No. 2
37	ARCHITECTURAL DETAILS
38	UTILITY DETAILS No. 1
39	UTILITY DETAILS No. 2
40	LOG OF TEST BORINGS 1 OF 7
41	LOG OF TEST BORINGS 2 OF 7
42	LOG OF TEST BORINGS 3 OF 7
43	LOG OF TEST BORINGS 4 OF 7
44	LOG OF TEST BORINGS 5 OF 7
45	LOG OF TEST BORINGS 6 OF 7
46	LOG OF TEST BORINGS 7 OF 7

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	334	423

REGISTERED CIVIL ENGINEER *K. Haney* DATE 5/6/12

PLANS APPROVAL DATE 9/04/12

REGISTERED PROFESSIONAL ENGINEER
KAMDOUSH HARRISAZ
No. C39591
Exp. 12/31/13
CIVIL
STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

RAJAPPAN & MEYER
CONSULTING ENGINEERS, INC.
1038 LEIGH AVE., SUITE 100
SAN JOSE, CA 95126

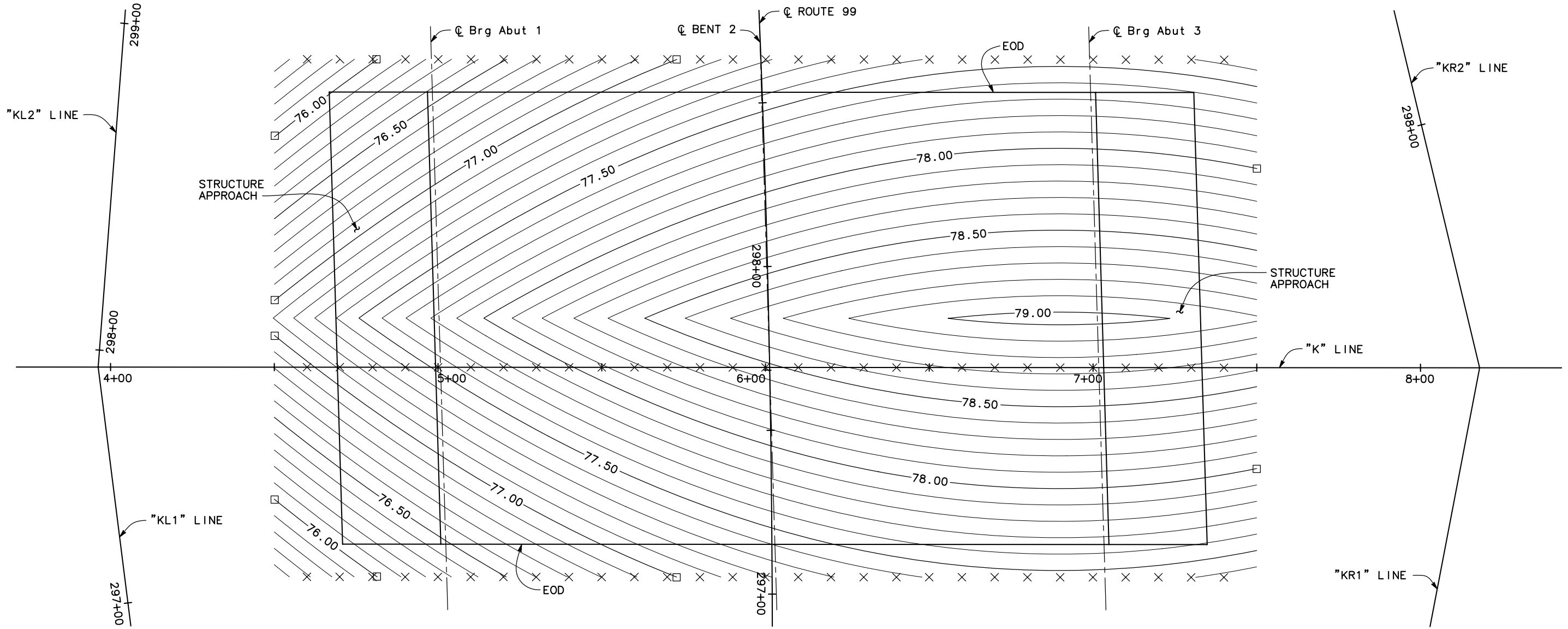
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE No. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARRISAZ			POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	INDEX TO PLANS
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS			DISREGARD PRINTS BEARING EARLIER REVISION DATES		REVISION DATES	SHEET 4 OF 46
			FILE => \$REQUEST		2/24/12 4/9/12 5/8/12 7/24/12	

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	335	423

K. Hariri
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KAMRUSH HARIRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

- NOTES:
- Indicates even foot contour.
 - X Indicates 10 ft intervals measured along "K" Line.
 - Contour interval = 0.1 ft.
 - Contours do not include camber



PLAN
 1/16" = 1'-0"

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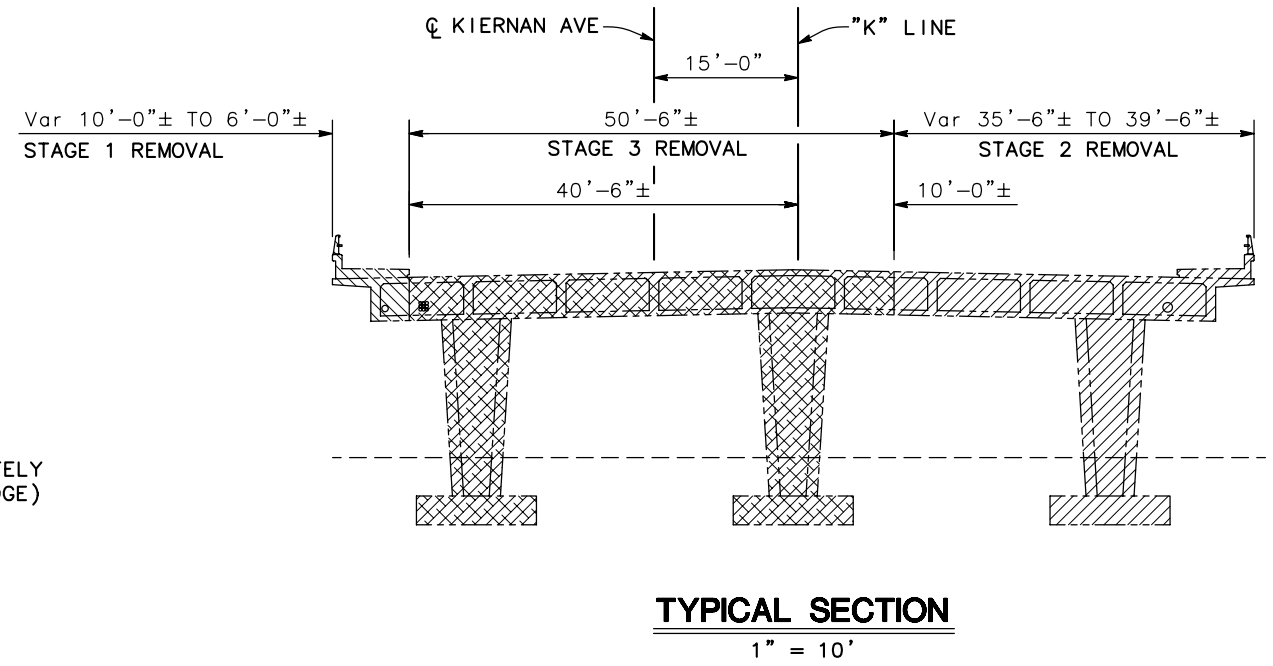
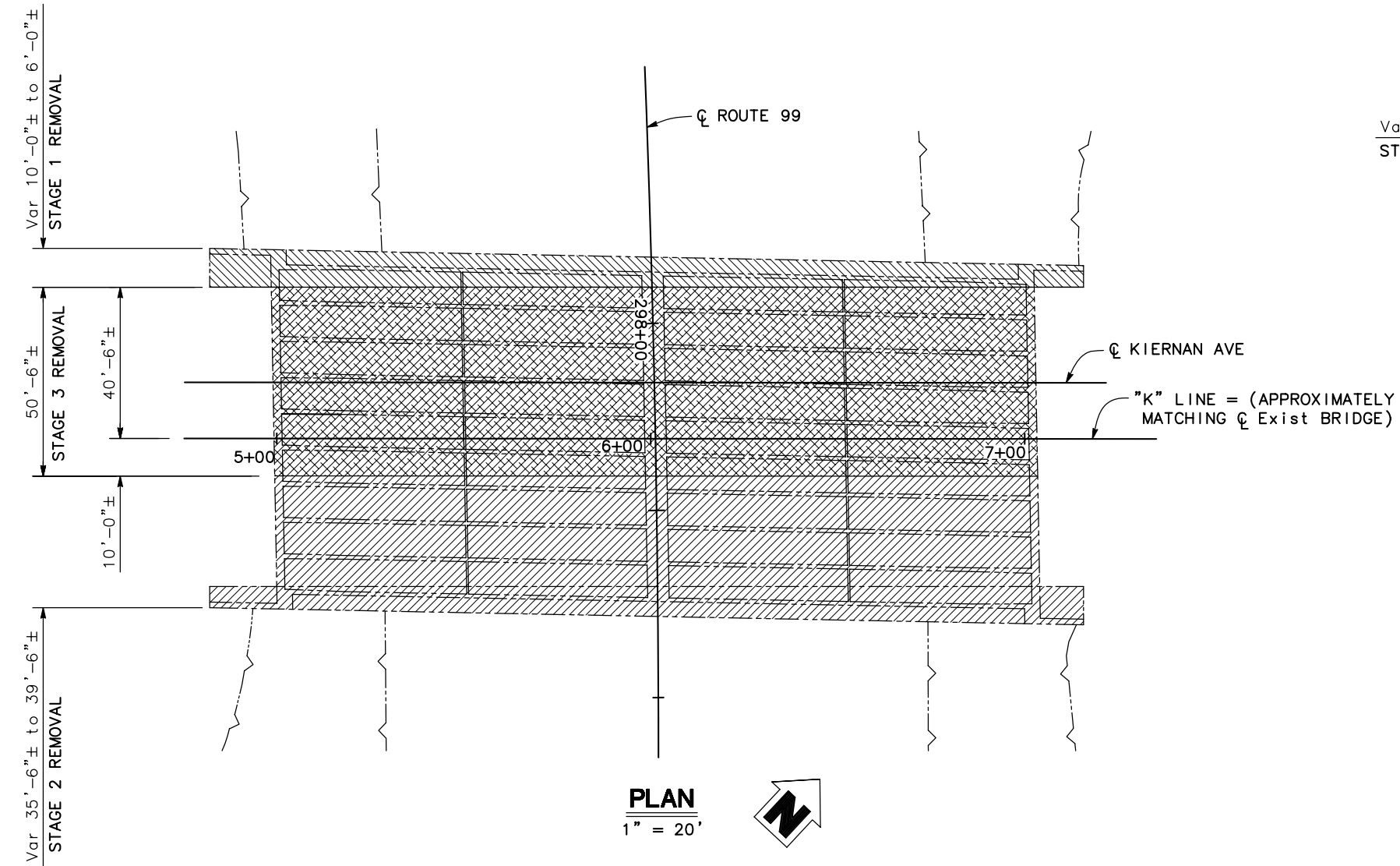
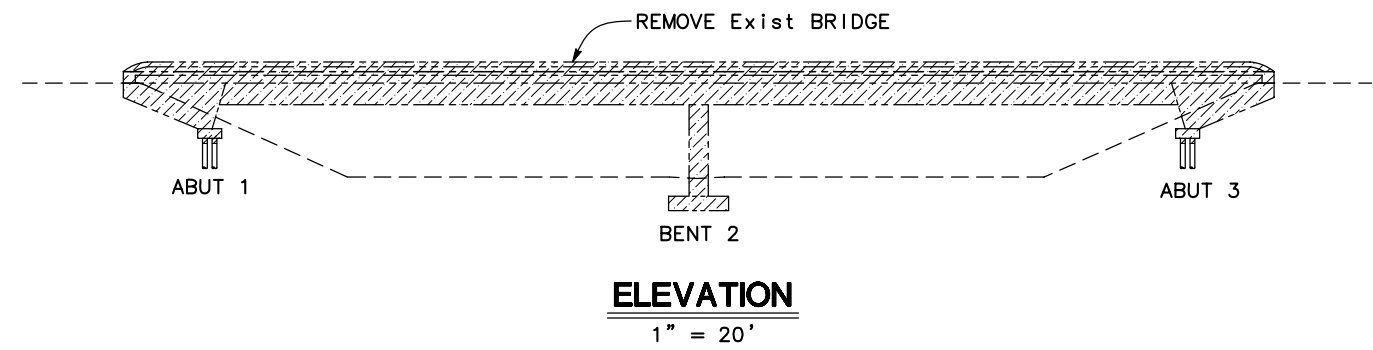
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) DECK CONTOURS			
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56				
	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP							
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 5 OF 46

USERNAME => \$USER DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	336	423

K. Hanu
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KANUDSH HARRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



- LEGEND:**
- Denotes Stage 1 Removal
 - Denotes Stage 2 Removal
 - Denotes Stage 3 Removal
 - Denotes Removal at all Stages

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

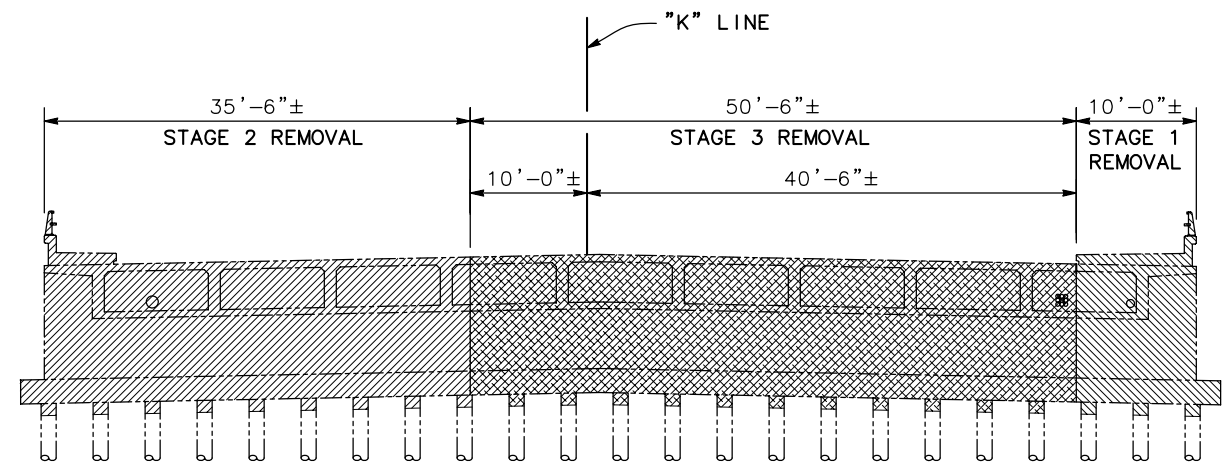
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) DEMOLITION PLAN No. 1
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARRISAZ			POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS			0 1 2 3	FILE => \$REQUEST	DISREGARD PRINTS BEARING EARLIER REVISION DATES	SHEET 6 OF 46

TIME PLOTTED => \$TIME
DATE PLOTTED => \$DATE
USERNAME => \$USER

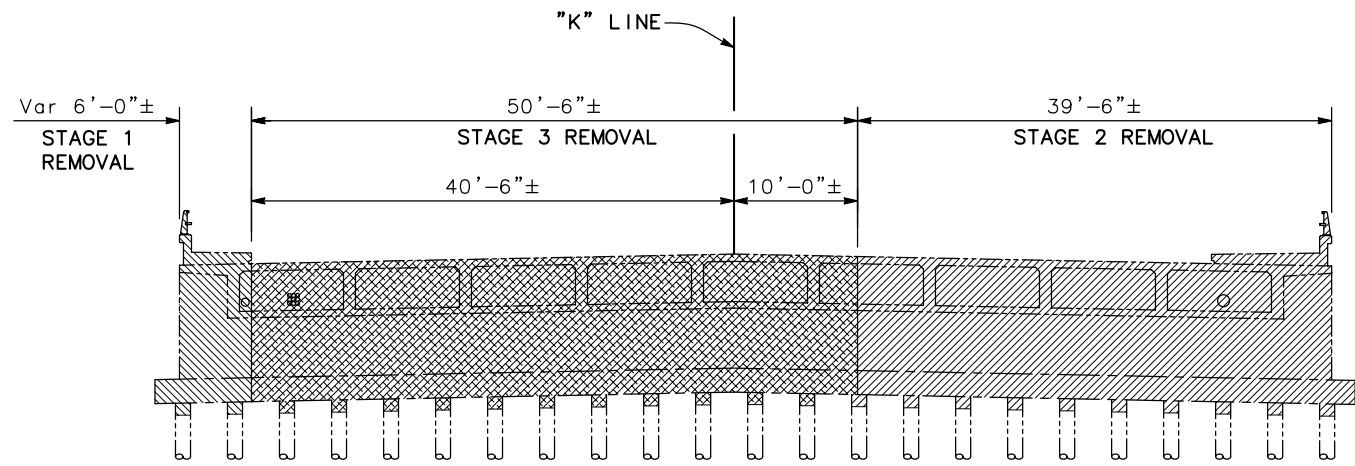
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	337	423

K. Harisaz
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12
 REGISTERED PROFESSIONAL ENGINEER
 KANADUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



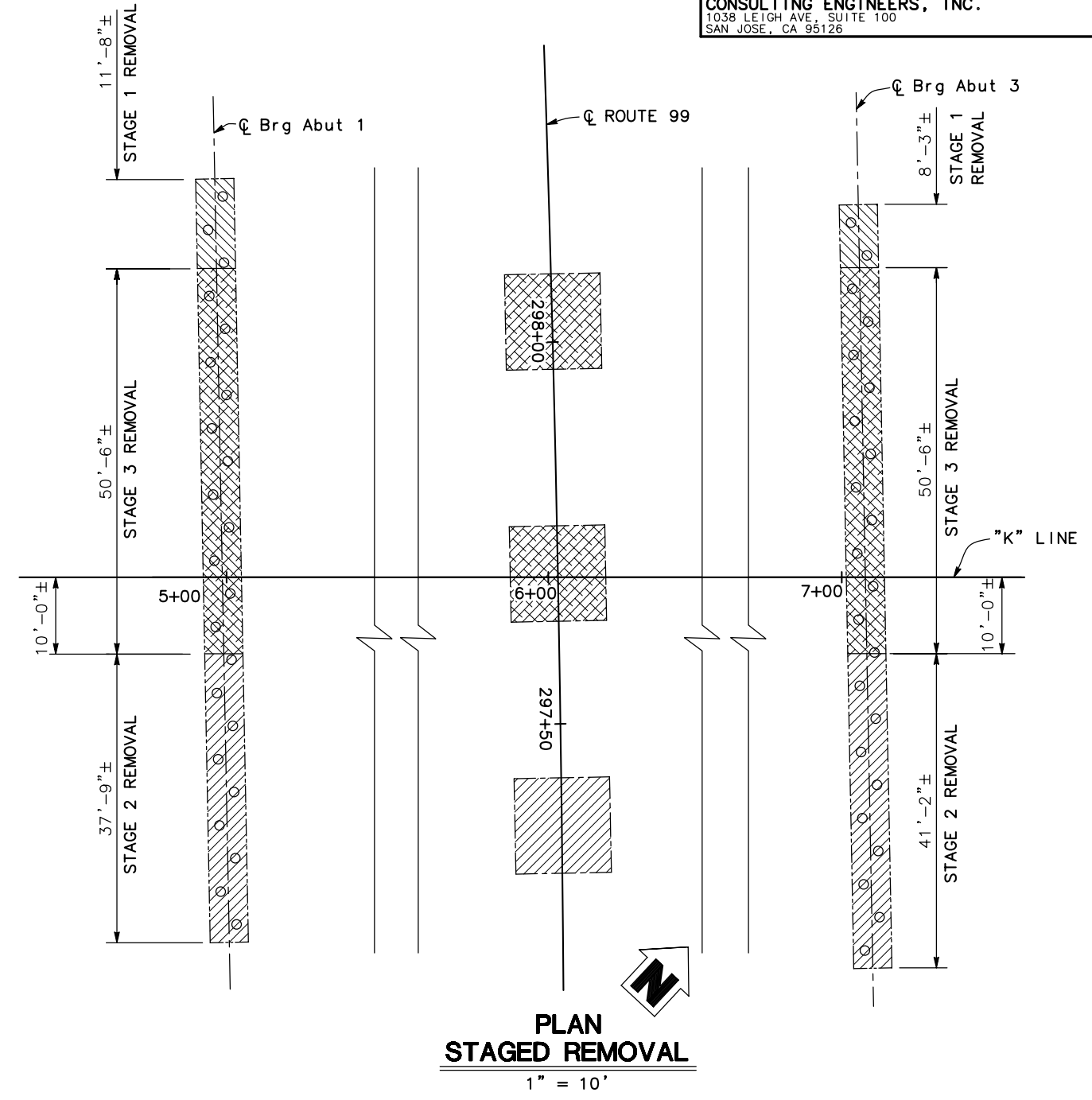
**ELEVATION
ABUTMENT 1 STAGED REMOVAL**
1/8" = 1'-0"



**ELEVATION
ABUTMENT 3 STAGED REMOVAL**
1/8" = 1'-0"

NOTE: Remove all existing abutment footing and top 1 foot of existing piles. Top of partially removed existing piles shall be 6 inches minimum below bottom of proposed footing elevation.

- LEGEND:
- Denotes Stage 1 Removal
 - Denotes Stage 2 Removal
 - Denotes Stage 3 Removal



**PLAN
STAGED REMOVAL**
1" = 10'

NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) DEMOLITION PLAN No. 2
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	
		ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	FILE => \$REQUEST	DISREGARD PRINTS BEARING EARLIER REVISION DATES	SHEET 7 OF 46

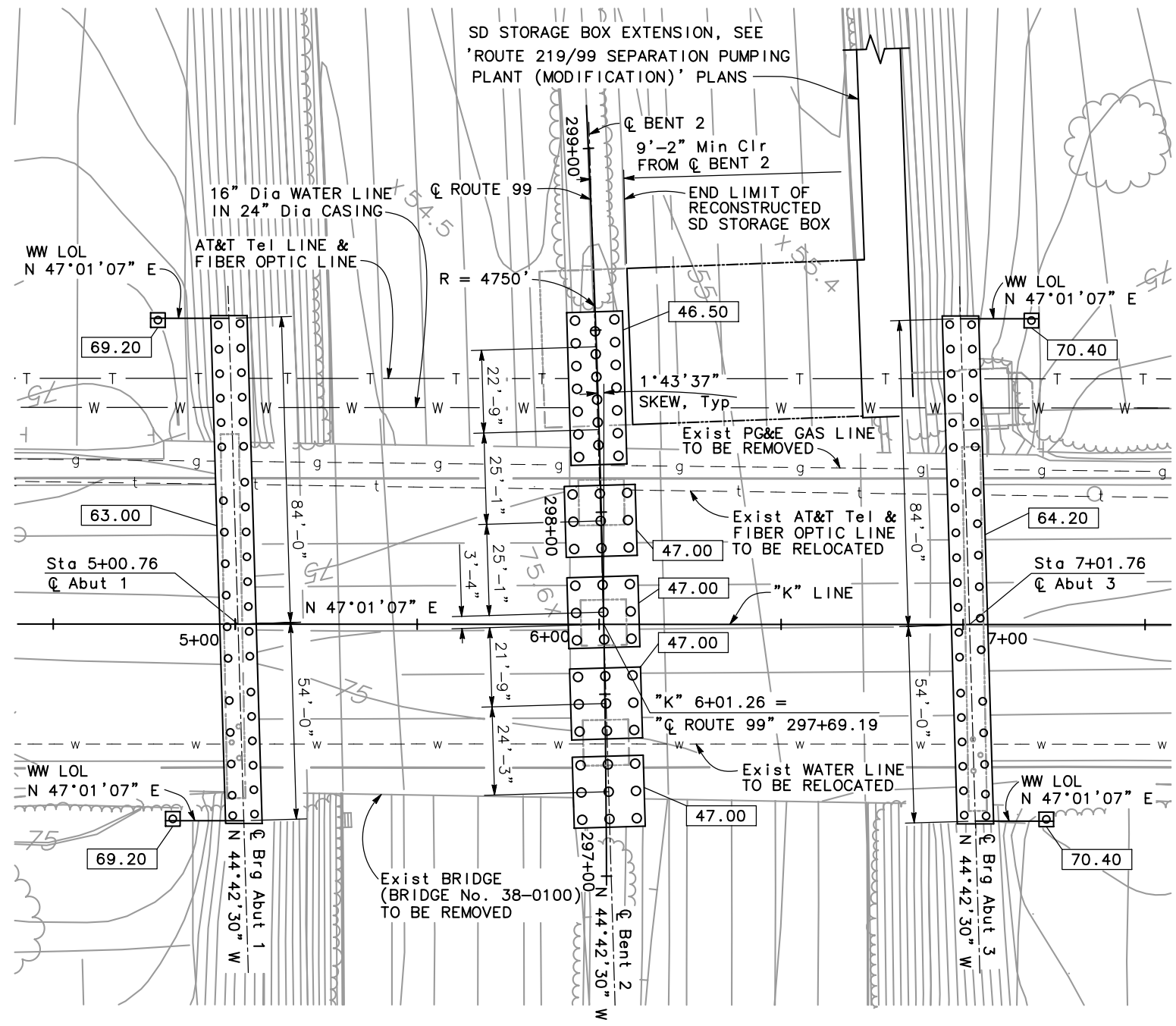
REGISTERED CIVIL ENGINEER
K. Harisaz
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12

REGISTERED PROFESSIONAL ENGINEER
KIAVOUSH HARISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 SAN JOSE, CA 95126



PLAN
 1" = 20'

"Q ROUTE 99" LINE
 R = 4750'
 Δ = 15° 59' 12"
 T = 667.01'
 L = 1325.35'

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

BENCH MARK AND DATUM

MONUMENT	COORDINATES		Elev	DESCRIPTION/LOCATION
	NORTHING	EASTING		
BASE 6	2081901.875	6395510.009	73.983	5/8" IP W/ RED PLASTIC CAP SET IN MEDIAN ISLAND ON KIERNAN ROAD OPPOSITE INTERSECTION OF NUTCRACKER
HS4474	2080776.306	6393119.753	73.42	STEEL ROD SET IN MONUMENT WELL IN NORTHERLY CROSSWALK AT INTERSECTION OF SALIDA BOULEVARD AND KIERNAN

NOTE: For design purpose, the elevations shown on the as-built plans were increased by 2.4'± to adjust to the project vertical datum.

PILE DATA TABLE

LOCATION	PILE TYPE	NOMINAL RESISTANCE (kips)		DESIGN TIP Elev (ft)	SPECIFIED TIP Elev (ft)
		COMPRESSION	TENSION		
Abut 1	24" CIDH	320	0	4.0(a); 28.0(c)	4.0
Abut 1 WW	24" CIDH	50	0	49.5(a); 39.5(c)	39.5
BENT 2 (1 st , 2 nd) ³	30" CIDH	390	110	-25.5(a); 9.5(b); -6.5(c)	-25.5
BENT 2 (3 rd , 4 th) ³	30" CIDH	380	50	-23.0(a); 22.0(b); -5.0(c)	-23.0
BENT 2 (5 th , 6 th) ³	30" CIDH	430	50	-27.5(a); 22.5(b); -5.0(c)	-27.5
Abut 3	24" CIDH	320	0	5.0(a); 26.0(c)	5.0
Abut 3 WW	24" CIDH	50	0	49.5(a); 41.5(c)	41.5

- NOTES:
- Design tip elevations are controlled by (a) Compression (b) Tension, and (c) Lateral Load.
 - The specified tip elevation shall not be raised above the design tip elevations for lateral load.
 - Numbering shown at Bent 2 indicate column numbers from the leftmost (northern) column to the rightmost (southern) column when looking from Begin Bridge to End Bridge.

- NOTES:
- Indicates existing pile
 - X 55.4 Indicates spot elevation
 - Indicates existing foundation, existing pumping plant or existing storm drain storage box
 - Indicates 24" CIDH Pile
 - Indicates 30" CIDH Pile
 - X.XX Indicates bottom of footing elevation
 - Utilities shown are for illustrative purposes only, verify with 'ROAD PLANS'
 - For locations of Temporary Railing Type K, traffic handling and detour operations during construction, see "Road Plans"

GEOTECHNICAL PROFESSIONAL APPROVAL DATE 4/30/12

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	339	423

K. Harisraz
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12
 KHANDUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

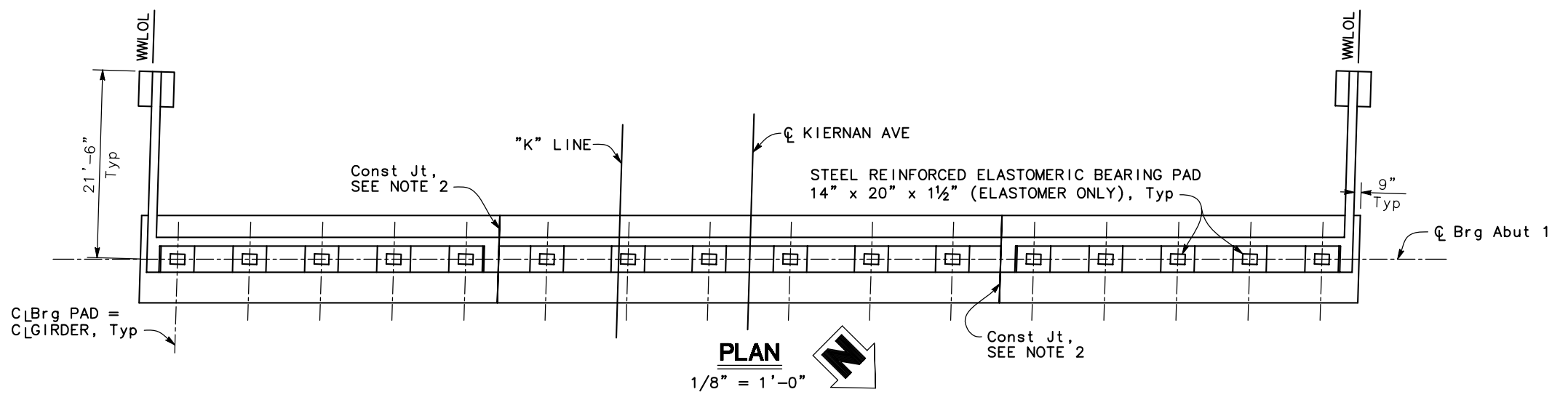
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 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

NOTES:

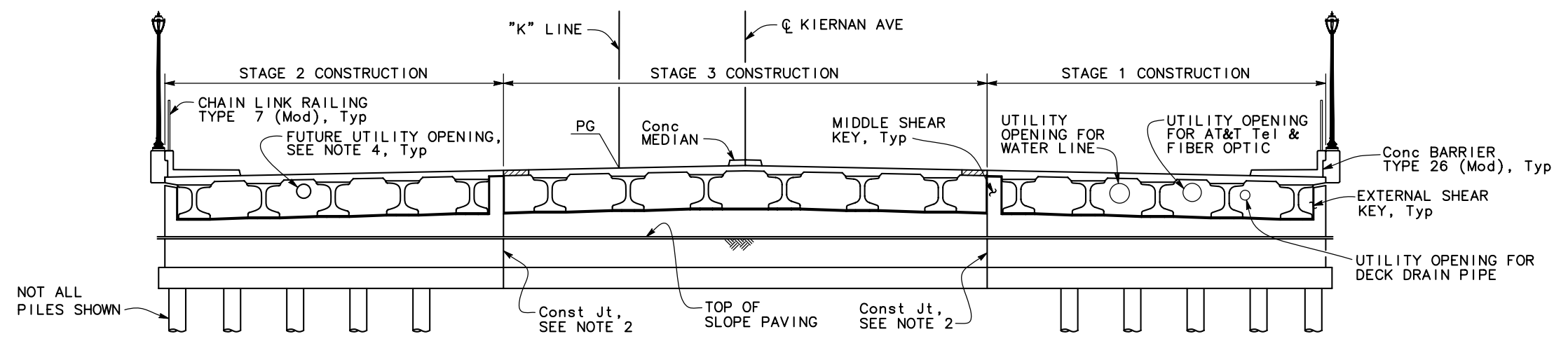
- Dimensions shown measured along centerline bearing abutment.
- Extend all footing and abutment reinforcement 30 Dia or butt splice to Stage 3 reinforcing.
- For Abutment Typical Section and Elastomeric Bearing Pad Details, see "ABUTMENT DETAILS No. 1" sheet.
- For Details of External Shear Key, Middle Shear Key and Utility Openings, see "ABUTMENT DETAILS No. 2" sheet.
- For Wingwall Details, see "ABUTMENT DETAILS No. 4" sheet.
- One foot minimum horizontal clearance between the cast-in-drilled-hole piles and the existing piles required.
- Contractor shall submit to Engineer for approval of pile spacings that differ from those shown on plans.

LEGEND:

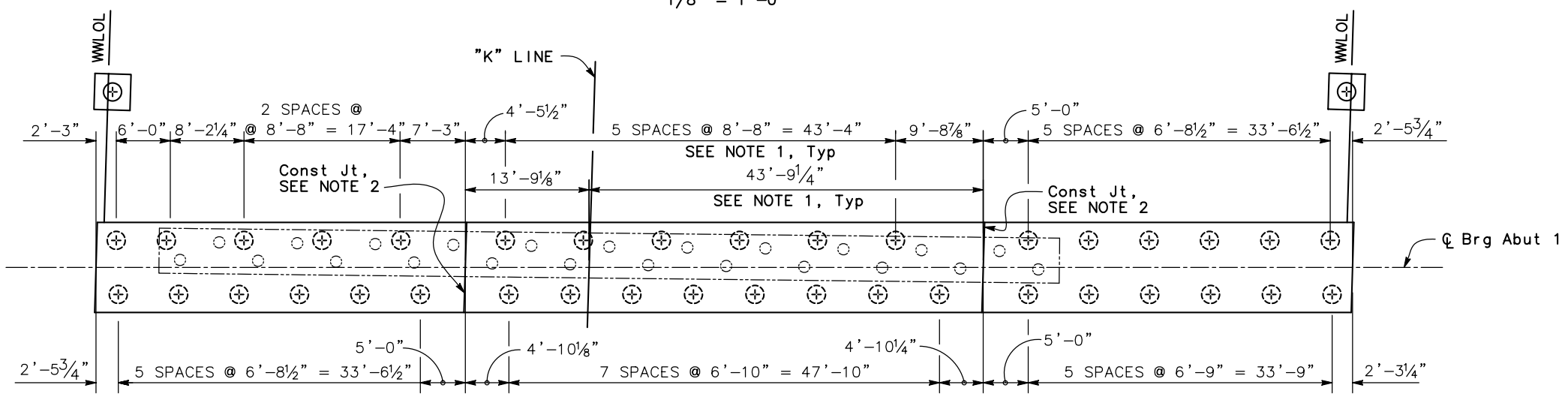
- ⊕ Indicates CIDH Pile
- Indicates Existing Pile
- ▨ Closure Pour



PLAN
1/8" = 1'-0"



ELEVATION
1/8" = 1'-0"



PILE LAYOUT
1/8" = 1'-0"

NOTE:
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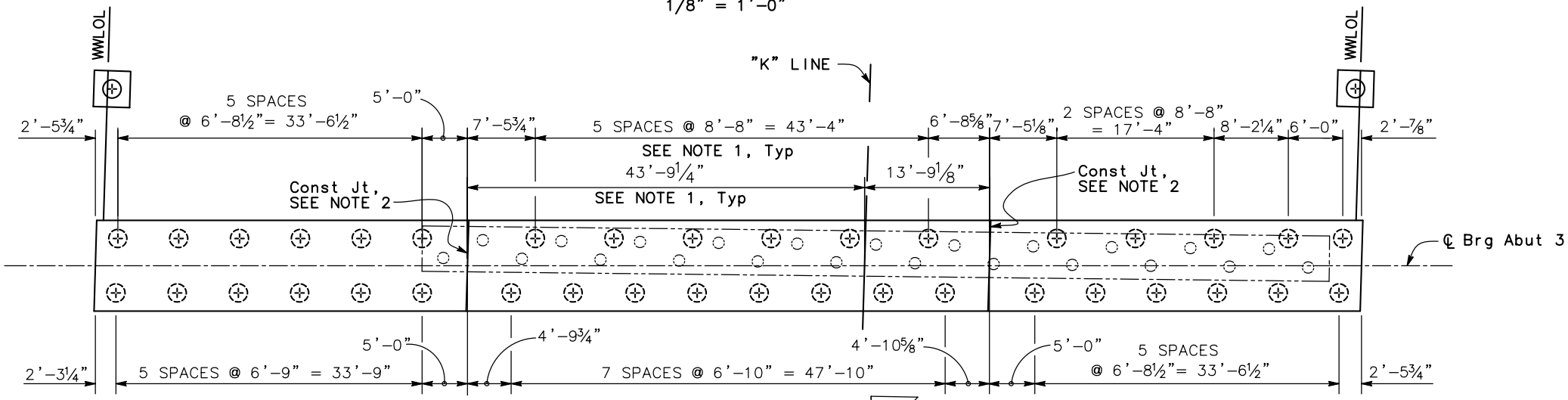
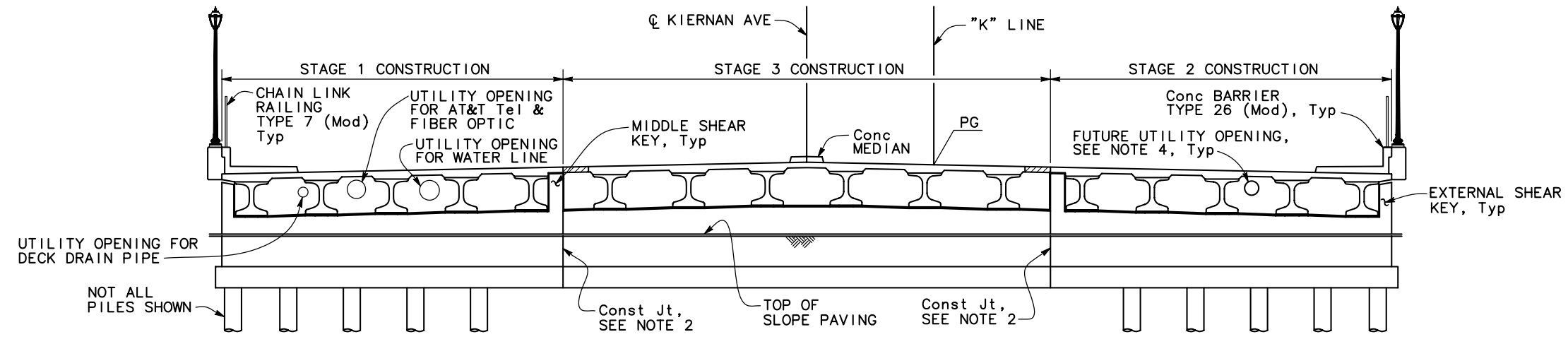
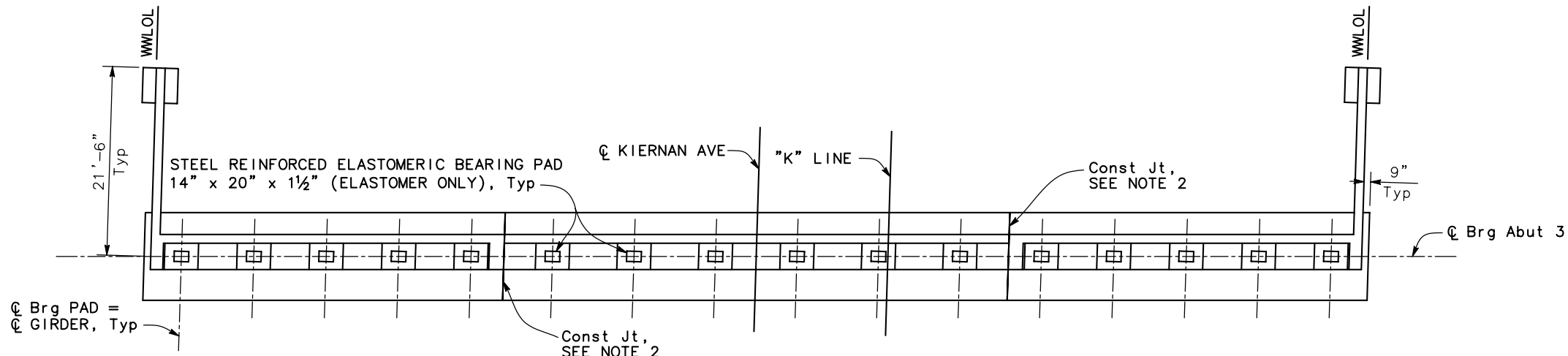
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) ABUTMENT 1 LAYOUT
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	

TIME PLOTTED => \$TIME
DATE PLOTTED => \$DATE
USERNAME => \$USER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	340	423

K. Hanj
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KHANDUSH HARRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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- NOTES:
- Dimensions shown measured along centerline bearing abutment.
 - Extend all footing and abutment reinforcement 30 Dia or butt splice to Stage 3 reinforcing.
 - For Abutment Typical Section and Elastomeric Bearing Pad Details, see "ABUTMENT DETAILS No. 1" sheet.
 - For Details of External Shear Key, Middle Shear Key and Utility Openings, see "ABUTMENT DETAILS No. 2" sheet.
 - For Wingwall Details, see "ABUTMENT DETAILS No. 4" sheet.
 - One foot minimum horizontal clearance between the cast-in-drilled-hole piles and the existing piles required.
 - Contractor shall submit to Engineer for approval of pile spacings that differ from those shown on plans.

- LEGEND:
- ⊕ Indicates CIDH Pile
 - Indicates Existing Pile
 - ▨ Closure Pour

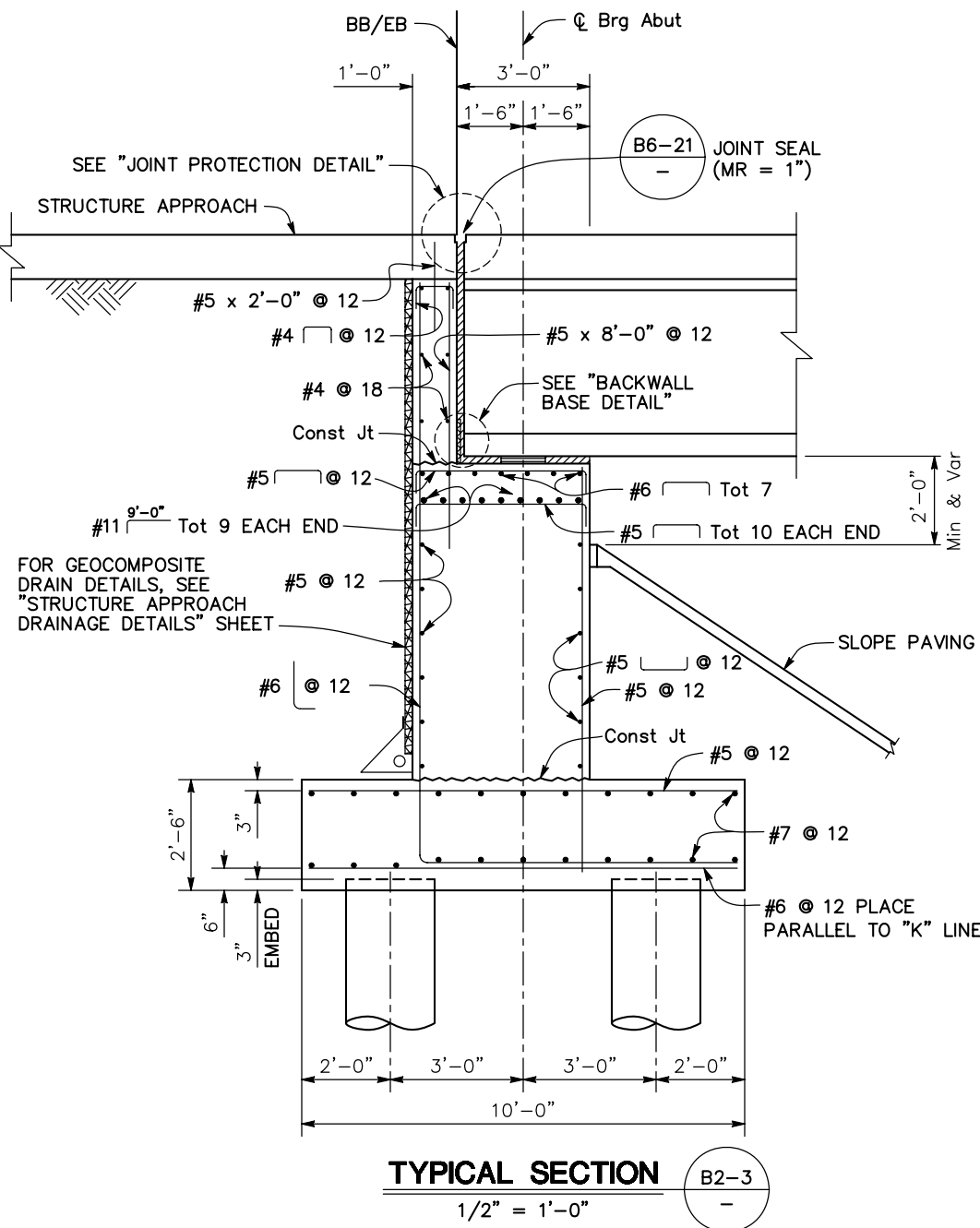
NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) ABUTMENT 3 LAYOUT
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARRISAZ			POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	

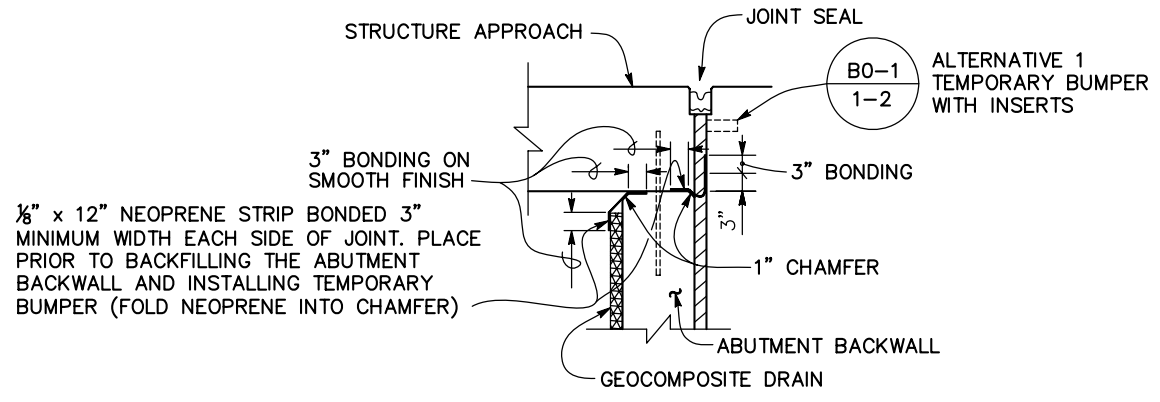
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	341	423

REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

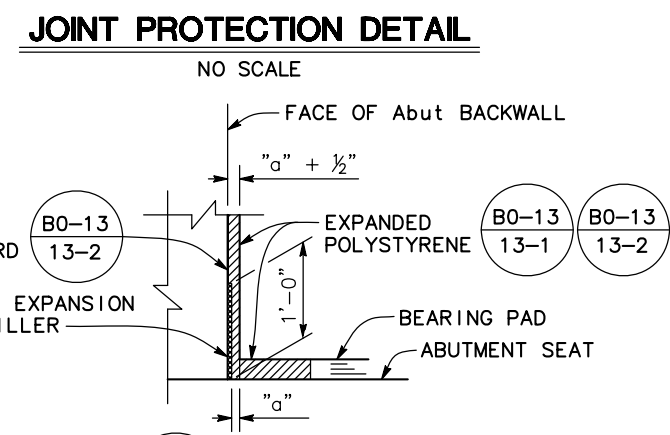
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RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



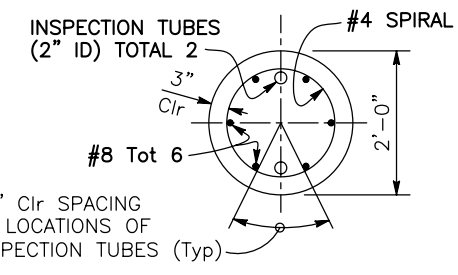
FOR GEOCOMPOSITE DRAIN DETAILS, SEE "STRUCTURE APPROACH DRAINAGE DETAILS" SHEET



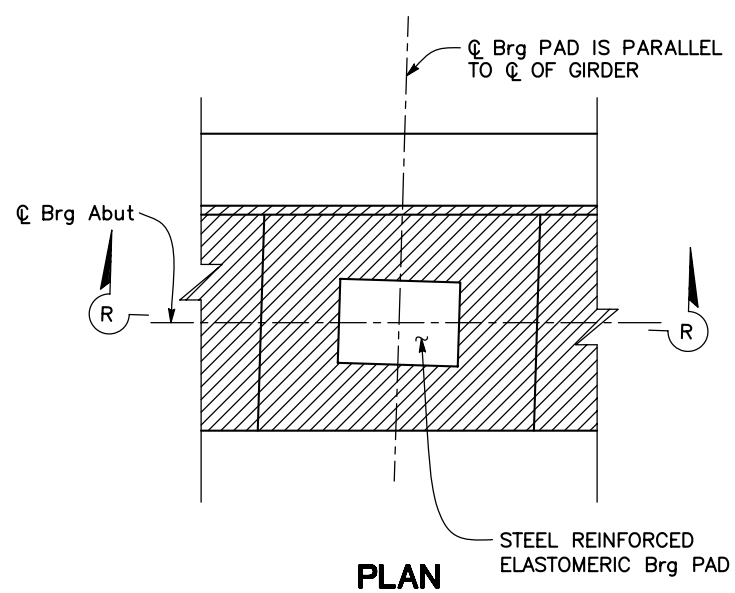
1/8" x 12" NEOPRENE STRIP BONDED 3" MINIMUM WIDTH EACH SIDE OF JOINT. PLACE PRIOR TO BACKFILLING THE ABUTMENT BACKWALL AND INSTALLING TEMPORARY BUMPER (FOLD NEOPRENE INTO CHAMFER)



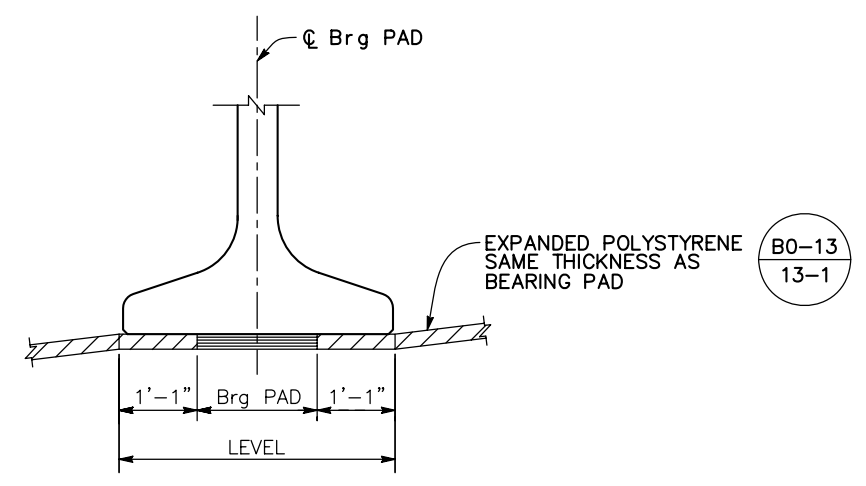
BACKWALL BASE DETAIL
 NO SCALE



- NOTES:
- Inspection pipes will not be required if the hole is dry or dewatered without the use of temporary casing to control ground water
 - For additional information and details not shown, see Std Plans B2-3



BEARING PAD DETAIL
 3/4" = 1'-0"
 (DETAILS TYPICAL AT ALL ABUTMENT BEARING PADS)



NOTE:
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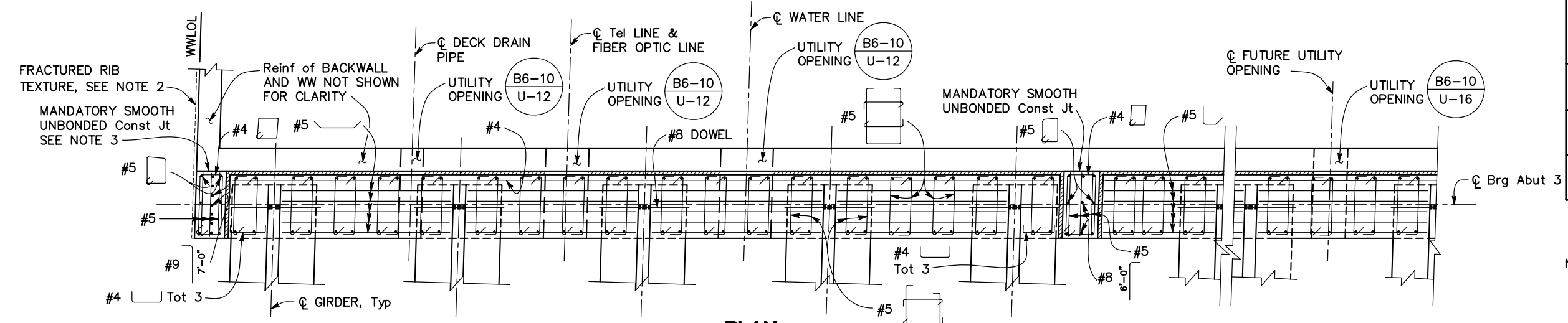
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56	ABUTMENT DETAILS No. 1	
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 11 OF 46

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	342	423

K. Haris
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
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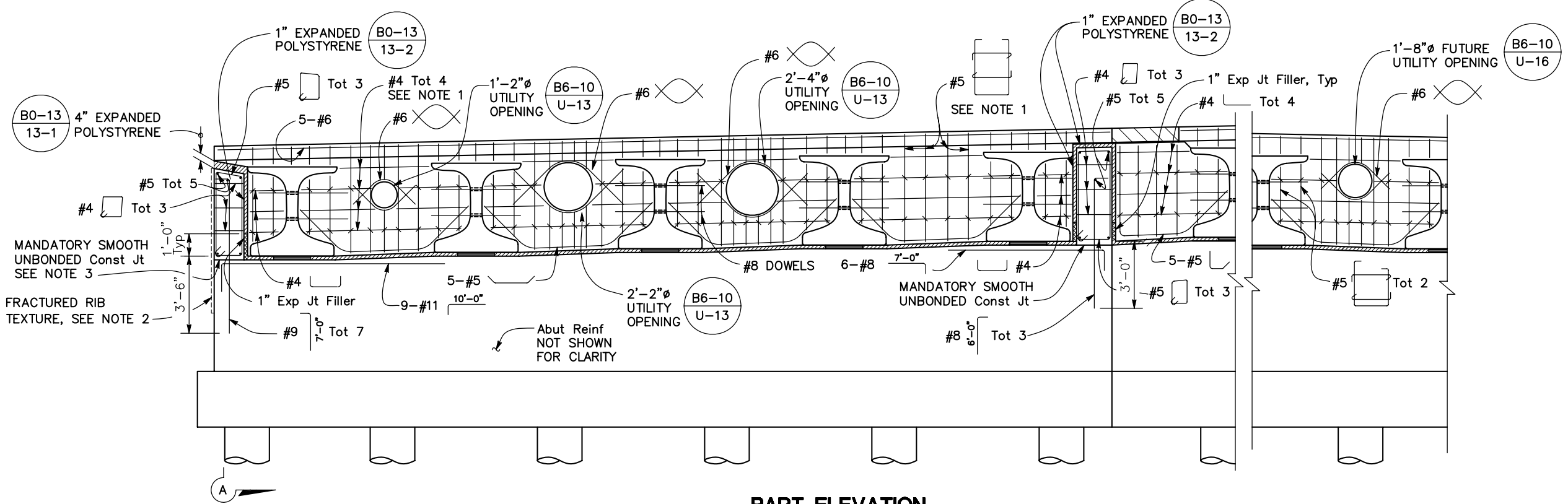


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 SAN JOSE, CA 95126



PLAN
 3/8" = 1'-0"
 (Abut 3 SHOWN, Abut 1 SIMILAR)

- NOTES:
- See "Abutment Details No. 3" sheet for additional reinforcement details.
 - For Fractured Rib Texture detail, see "Abutment Details No. 4" sheet.
 - Construction joint to be trowel-finished smooth before application of construction paper as bond breaker.



PART ELEVATION
 3/8" = 1'-0"
 (Abut 3 SHOWN, Abut 1 SIMILAR)

NOTE:
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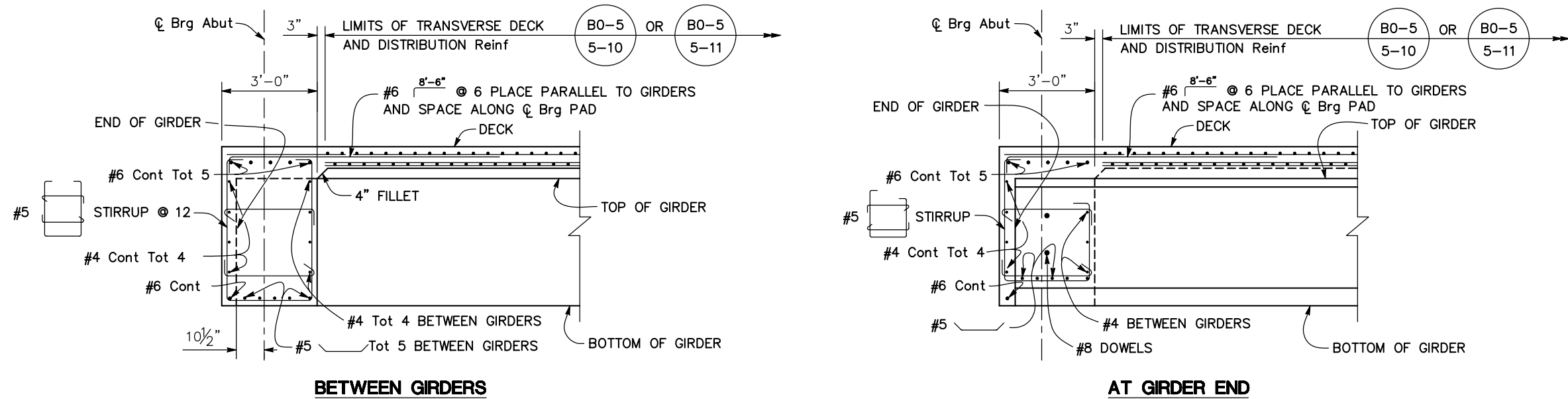
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) ABUTMENT DETAILS No. 2	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ		PROJECT ENGINEER A. WANG		POST MILES R22.56
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457		CONTRACT NO.: 10-0L3304
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS			0 1 2 3	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES: 10/21/11, 2/27/12, 4/9/12, 5/6/12 SHEET 12 OF 46	

USERNAME => \$USER DATE PLOTTED => \$DATE

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	343	423

K. Harry
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KIANOUSH HARRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



END DIAPHRAGM SECTIONS
 1/2" = 1'-0"

NOTE:
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DESIGN OVERSIGHT	DESIGN	BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO.	38-0163	ROUTE 219/99 SEPARATION (REPLACE) ABUTMENT DETAILS No. 3		
	DETAILS	BY S. DESALEGN	CHECKED K. HARRISAZ		A. WANG PROJECT ENGINEER	POST MILES		R22.56	
SIGN OFF DATE	QUANTITIES	BY S. DESALEGN	CHECKED B. SWARUP	UNIT:	1457	CONTRACT NO.:	10-0L3304		
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	10/21/11	2/24/12	4/9/12	5/6/12	SHEET 13 OF 46

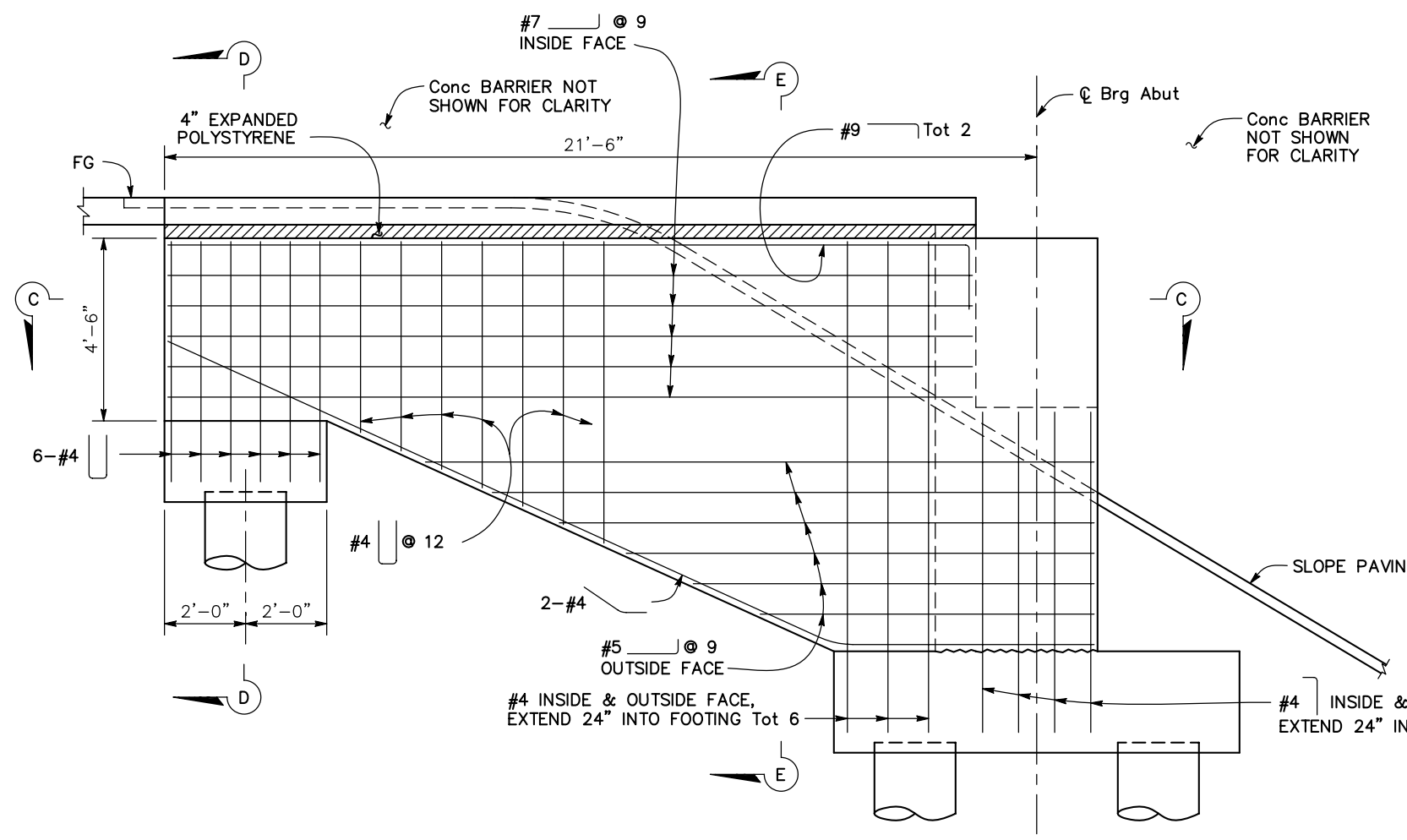
USERNAME => \$USER DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	344	423

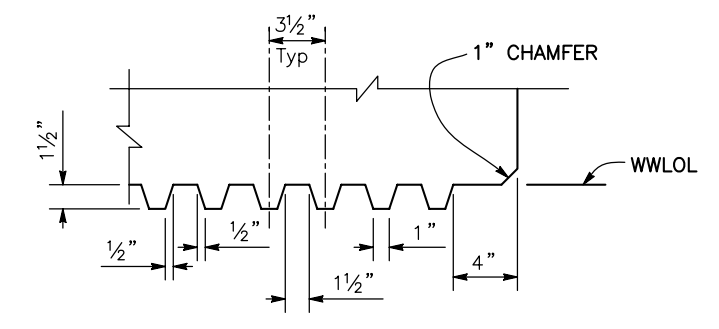
K. Harisaz
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
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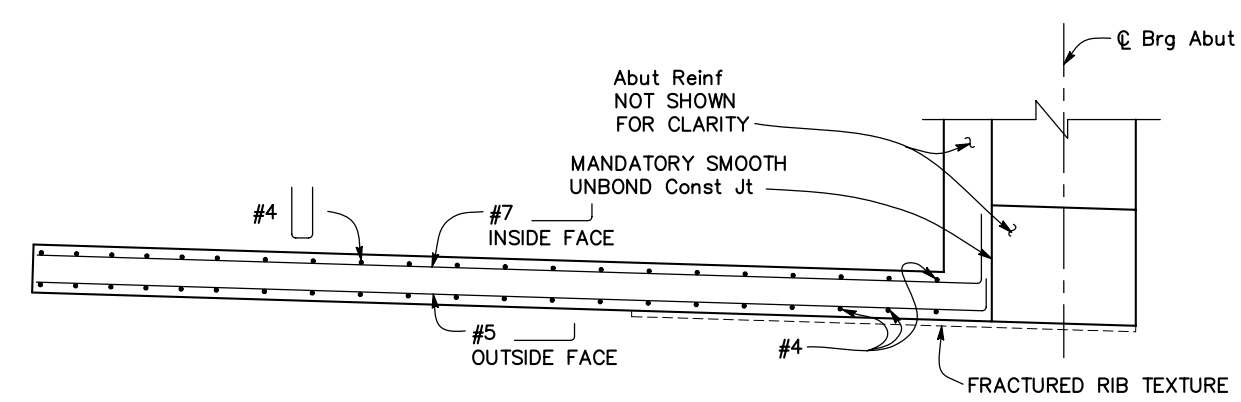
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RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



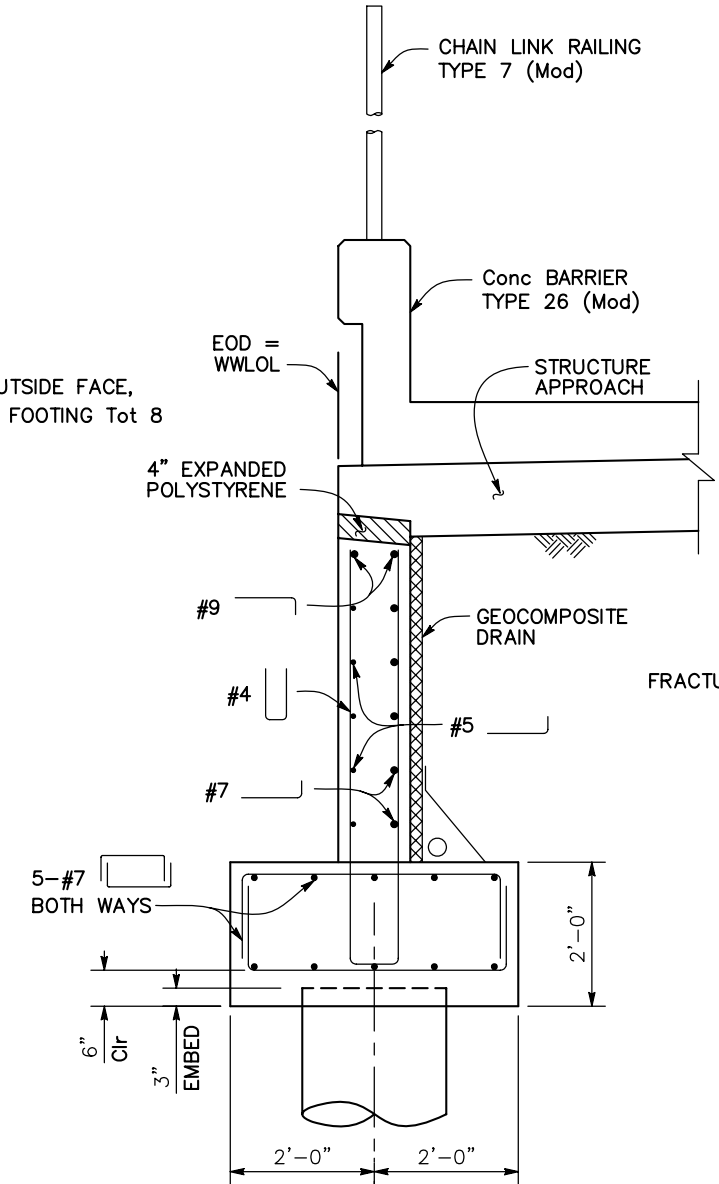
WINGWALL ELEVATION
1/2" = 1'-0"



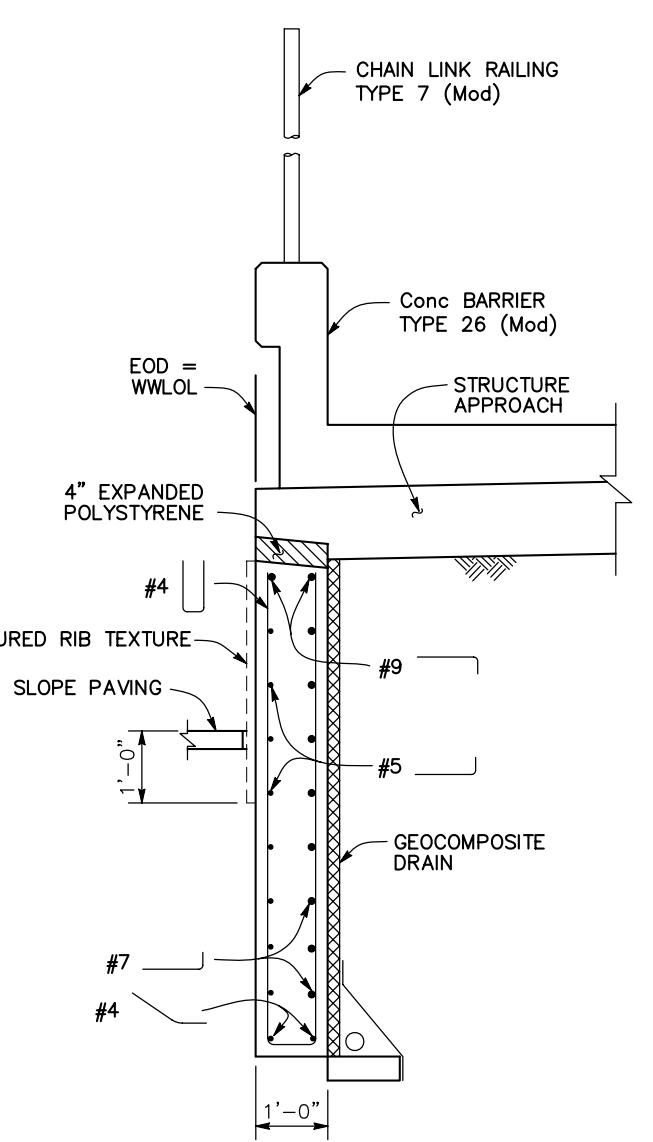
FRACTURED RIB TEXTURE DETAIL
NO SCALE



SECTION C-C
1/2" = 1'-0"



SECTION D-D
3/4" = 1'-0"



SECTION E-E
3/4" = 1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	ABUTMENT DETAILS No. 4
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	FILE => \$REQUEST	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES: 10/21/11, 2/24/12, 4/29/12, 5/6/12	SHEET 14 OF 46

TIME PLOTTED => \$DATE USERNAME => \$USER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	345	423

REGISTERED CIVIL ENGINEER
 K. Harisaz
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

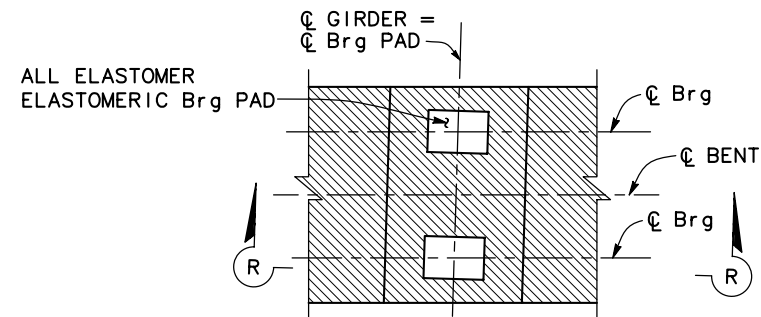
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

NOTES:

1. No Splice allowed in main cap reinforcement except at construction joint. Reinforcement across construction joint shall be mechanically spliced and meet "Ultimate Splice" requirement.
2. Main cap reinforcement extending during Stage 1 Construction and Stage 2 Construction must clear existing cap.
3. For Details of Bent Cap Reinforcement and Utility Openings, see "Bent Details No. 1" Sheet.
4. For SECTION A-A, B-B, C-C, & D-D, see "BENT DETAILS NO. 2" Sheet.

LEGEND:

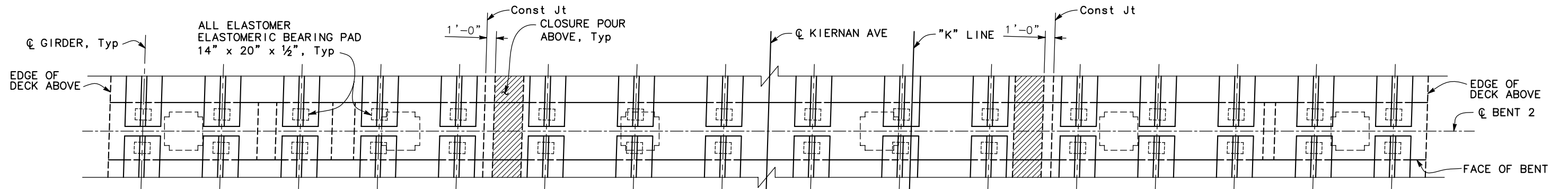
- Closure Pour
- Expanded Polystyrene



Note: FOR ADDITIONAL DETAILS AND SECTION R-R, SEE "ABUTMENT DETAILS No. 1" SHEET

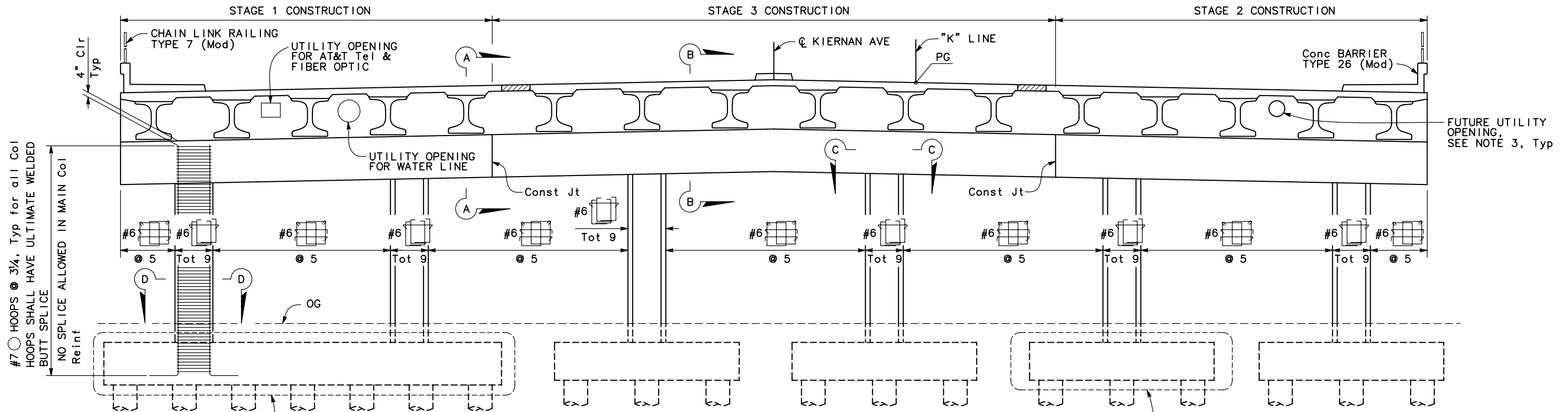
BENT CAP BEARING PAD DETAIL

3/8" = 1'-0"



PLAN

3/16" = 1'-0"



TYPICAL SECTION

3/16" = 1'-0"

SEE "SECTION G-G" AND "FOOTING PLAN No. 2" ON "BENT DETAILS No. 3" sheet

SEE "SECTION F-F" AND "FOOTING PLAN No. 1" ON "BENT DETAILS No. 3" sheet, Typ

NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56		BENT LAYOUT
	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP					

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	346	423

<i>K. Harisaz</i>	5/6/12
REGISTERED CIVIL ENGINEER	DATE
9/04/12	
PLANS APPROVAL DATE	

REGISTERED PROFESSIONAL ENGINEER
 KAMDOUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

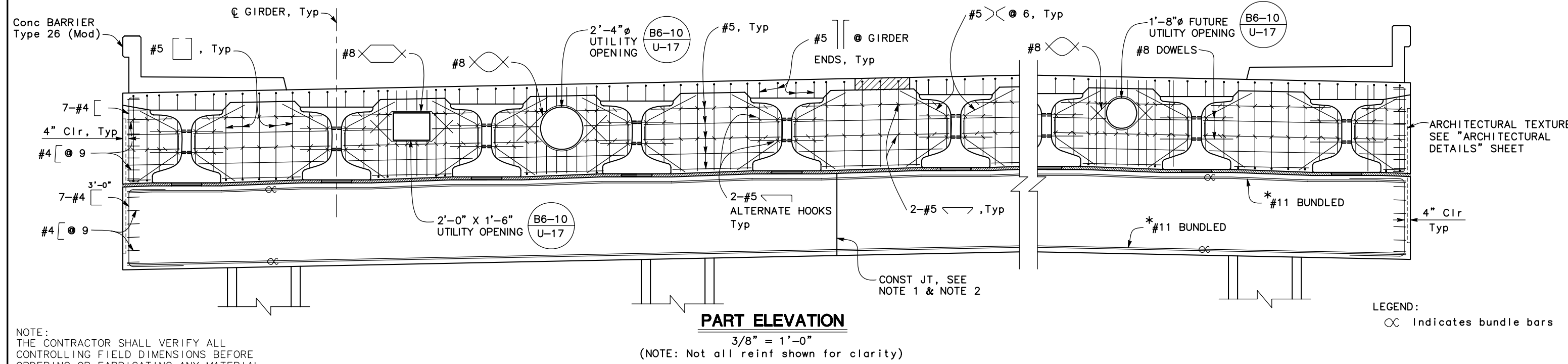
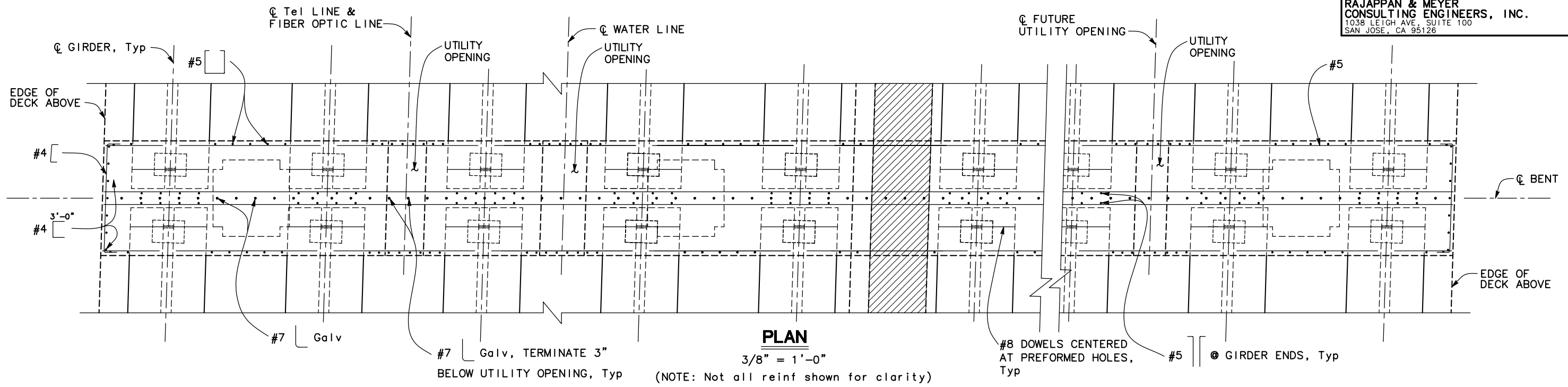
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 MODESTO, CA 95358

RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

- NOTES:
- No Splice allowed in main cap reinforcement except at construction joint. Reinforcement across construction joint shall be mechanically spliced and meet "Ultimate Splice" requirement.
 - Main cap reinforcement extending during Stage 1 Construction and Stage 2 Construction must clear existing cap.

- LEGEND:
- * Denotes main cap reinforcement
 - Closure Pour



NOTE:
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LEGEND:
∞ Indicates bundle bars

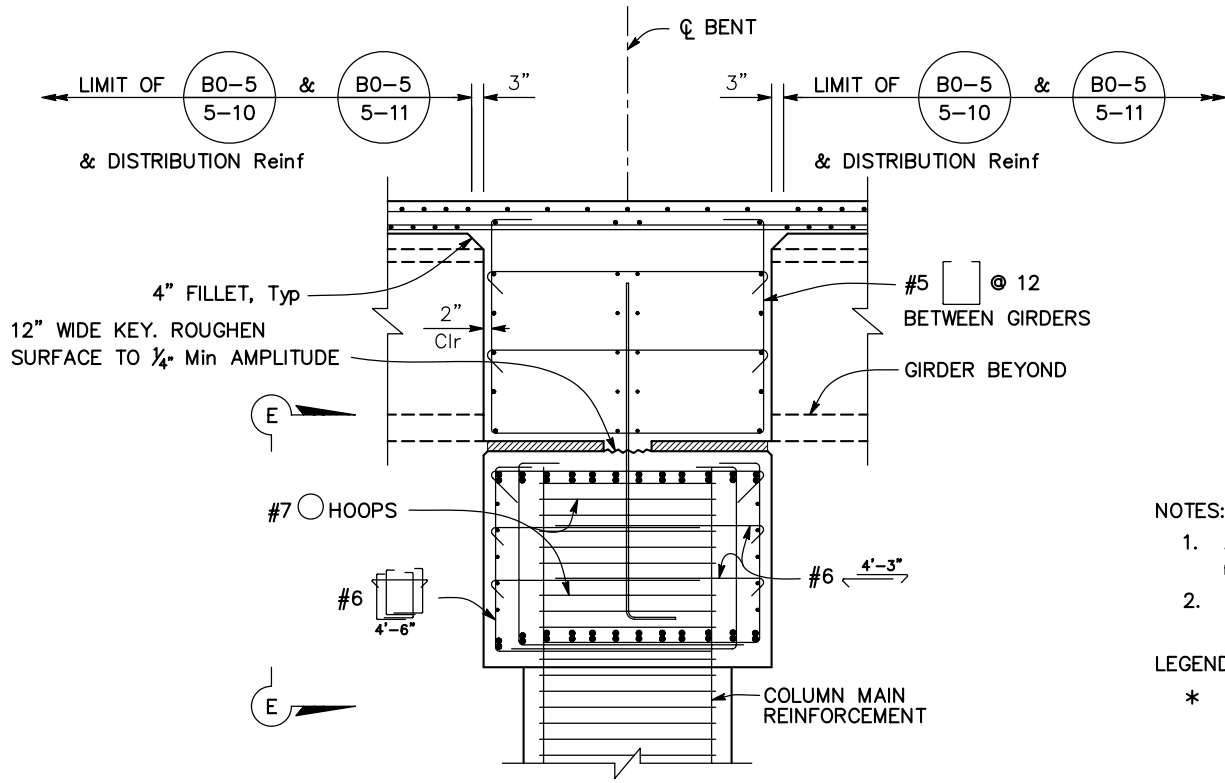
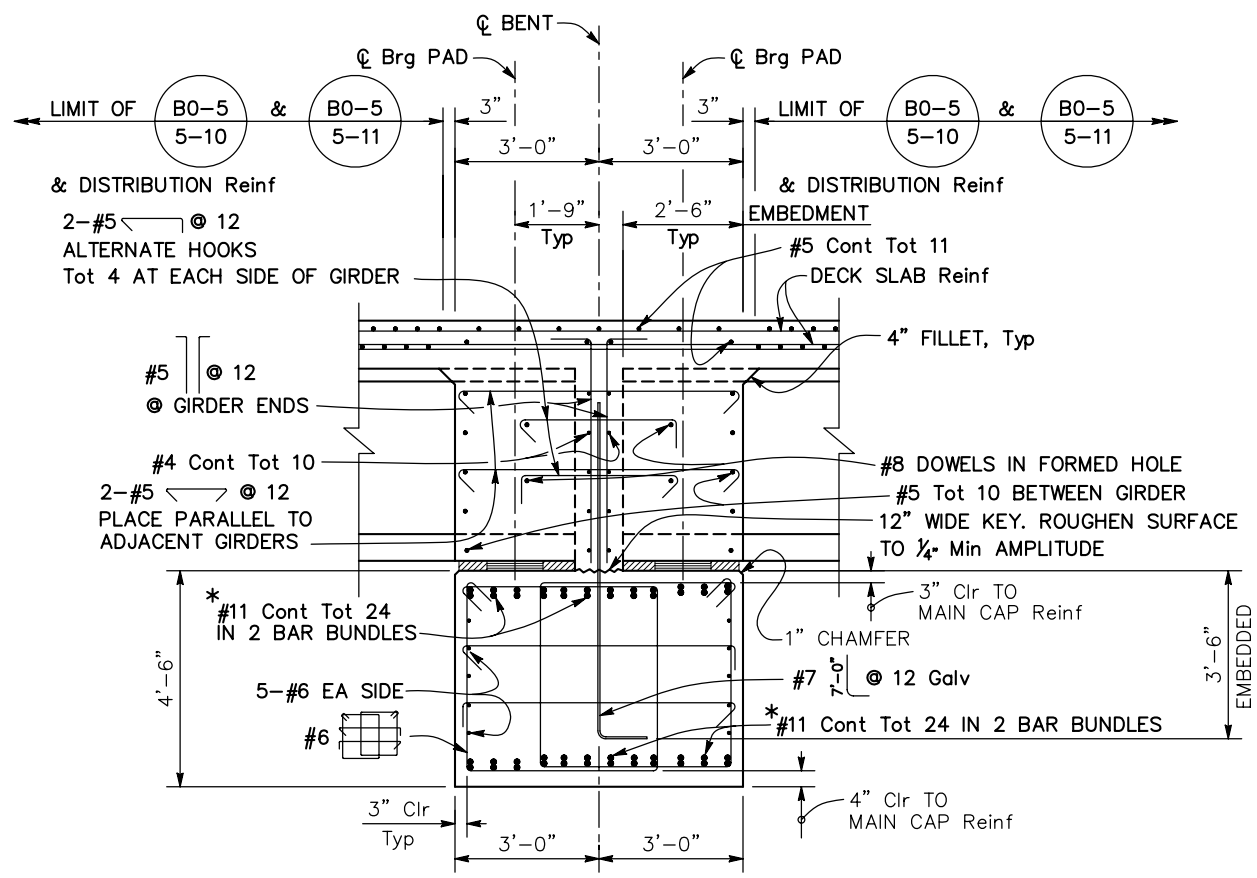
DESIGN OVERSIGHT SIGN OFF DATE	DESIGN BY: A. WANG DETAILS BY: S. DESALEGN QUANTITIES BY: S. DESALEGN	CHECKED C. SHENOY CHECKED K. HARIRSAZ CHECKED B. SWARUP	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163 POST MILES R22.56	ROUTE 219/99 SEPARATION (REPLACE) BENT DETAILS No. 1
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS 0 1 2 3	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES REVISION DATES: 10/21/11, 12/24/11, 4/9/12, 5/6/12 SHEET 16 OF 46

USERNAME => \$USER DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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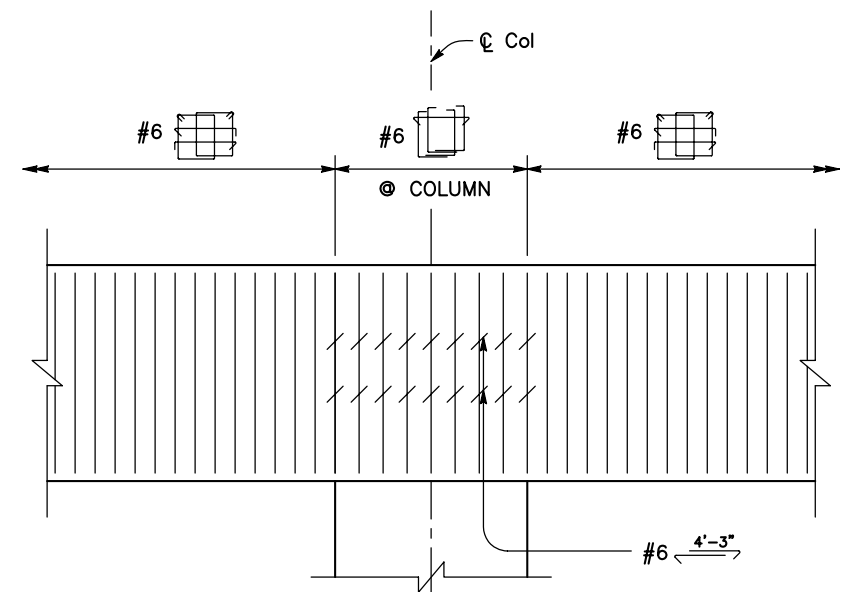
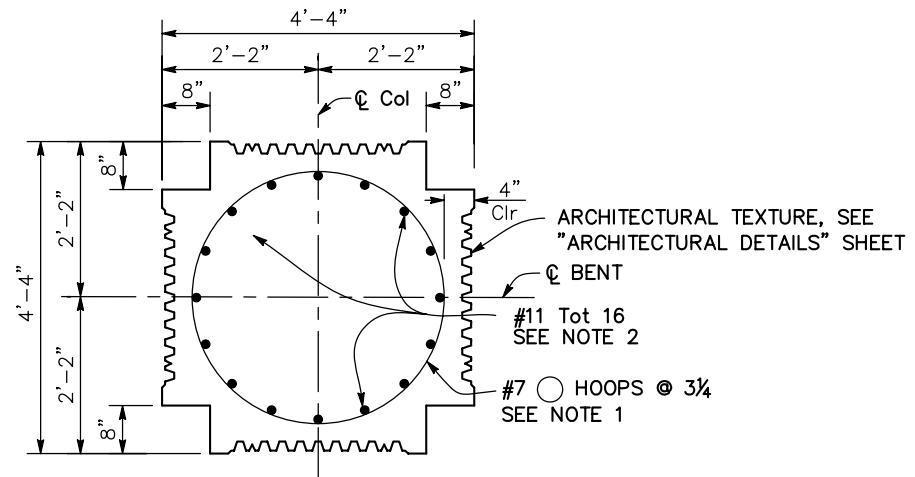
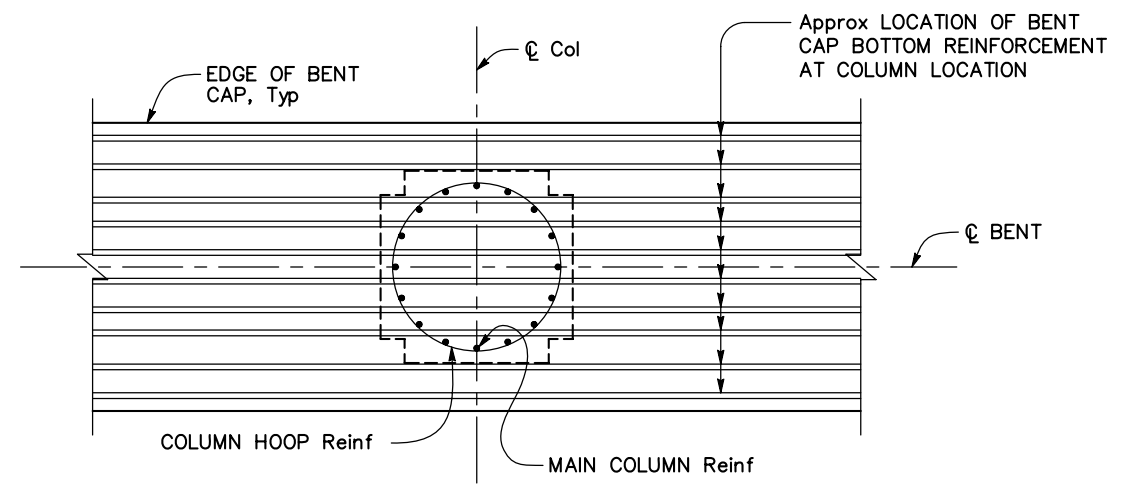
REGISTERED CIVIL ENGINEER
 K. Harisaz
 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.
 REGISTERED PROFESSIONAL ENGINEER
 KAMDOUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



- NOTES:
- All hoops shall be butt welded continuous and meet "Ultimate Splice" requirements.
 - No splice shall be permitted in main column reinforcement.

LEGEND:
 * Denotes main cap reinforcement.



NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

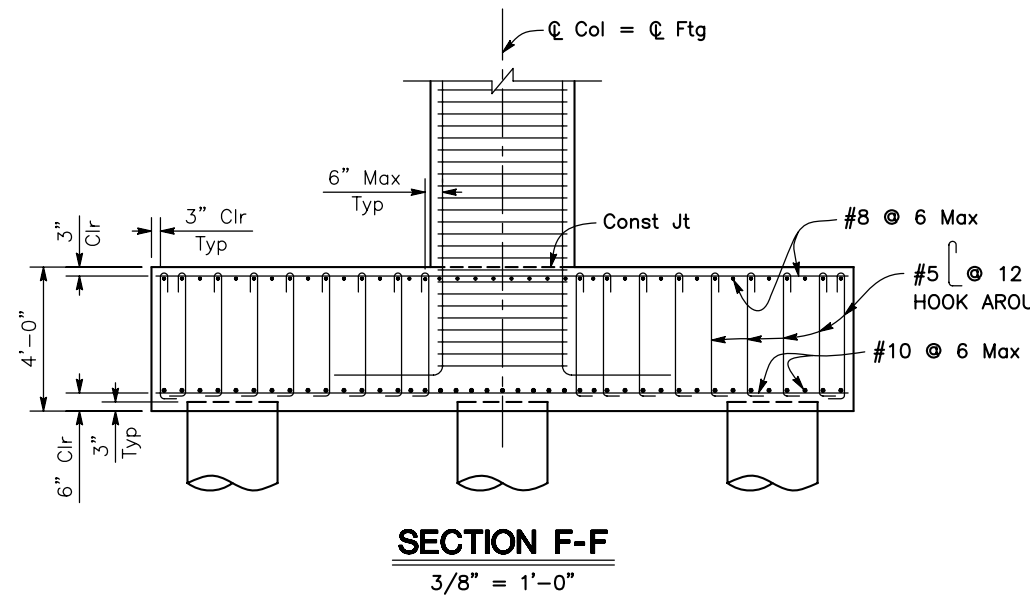
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) BENT DETAILS No. 2
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS		UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES
				FILE => \$REQUEST	REVISION DATES	SHEET 17 OF 46

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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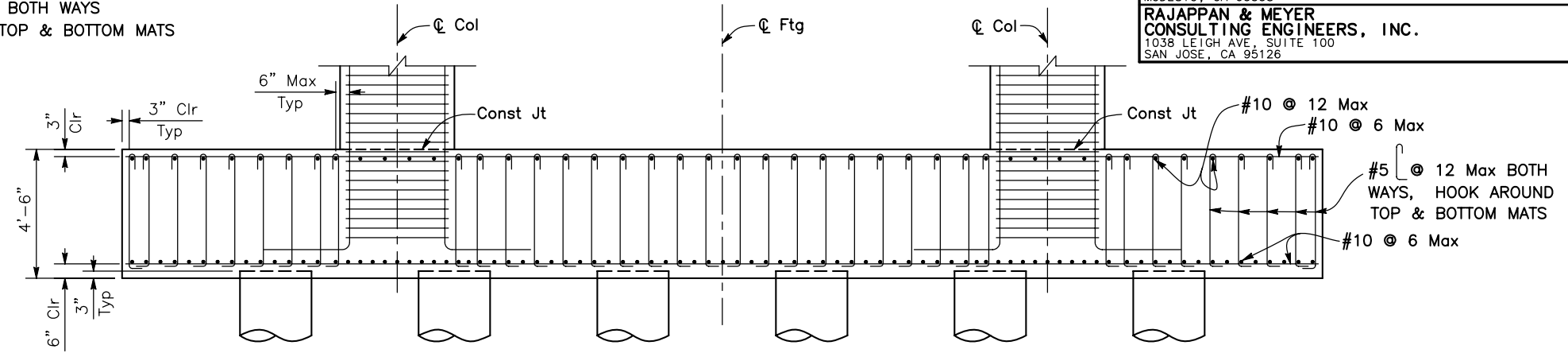
K. Harisaz
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KANADUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

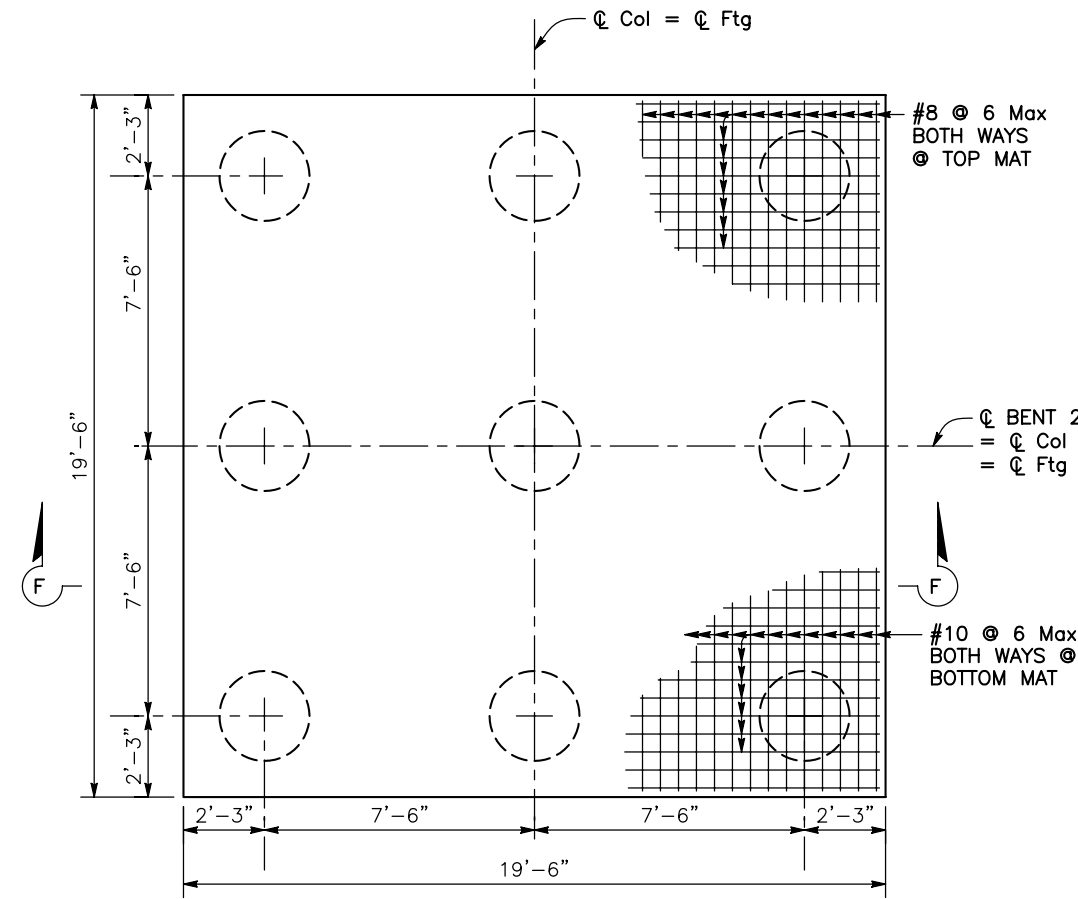
NOTE: Arrange column main reinforcement and hoops to avoid conflict with CIDH pile reinforcement. Contractor shall submit to Engineer for approval of reinforcement locations and spacings that differ from those shown on plans.



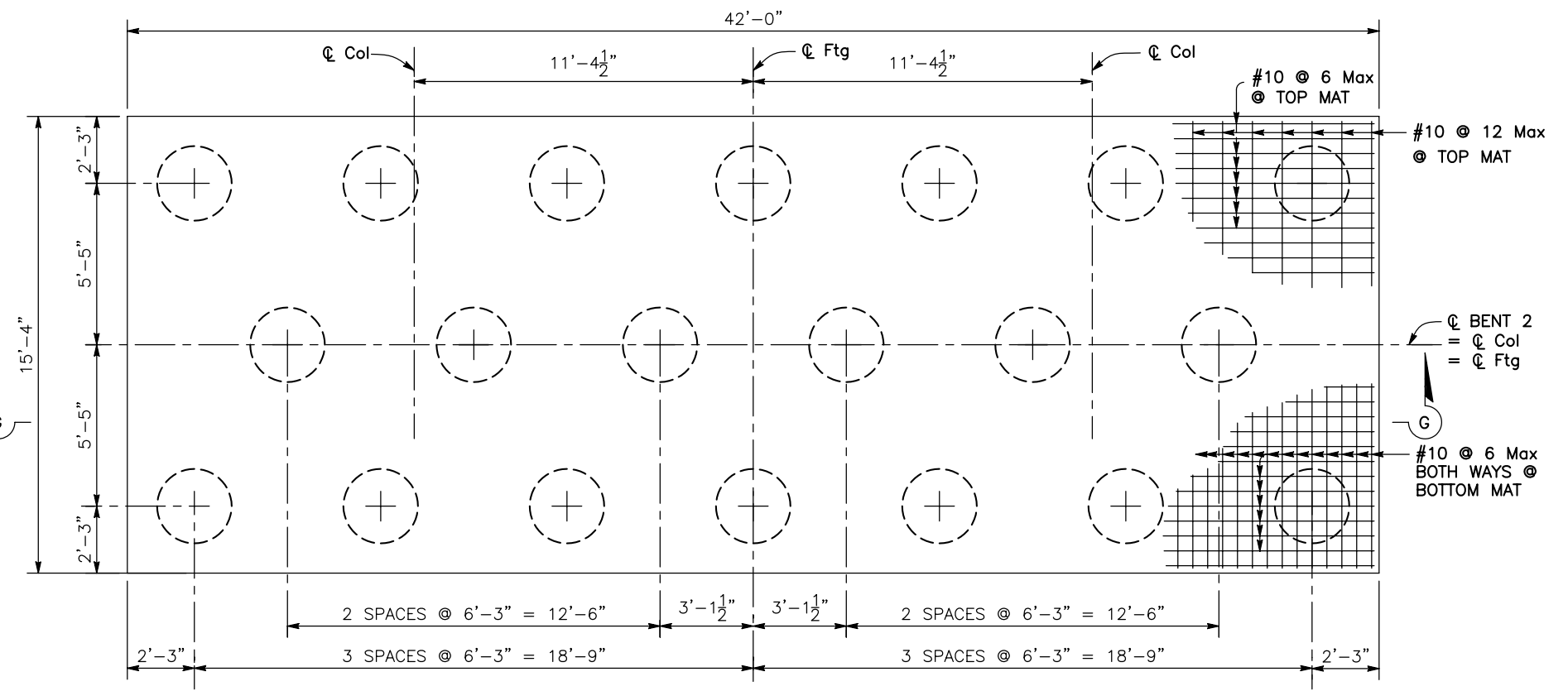
SECTION F-F
 3/8" = 1'-0"



SECTION G-G
 3/8" = 1'-0"



FOOTING PLAN No. 1
 3/8" = 1'-0"



FOOTING PLAN No. 2
 3/8" = 1'-0"

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) BENT DETAILS No. 3	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ		PROJECT ENGINEER A. WANG		POST MILES R22.56
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457		CONTRACT NO.: 10-0L3304
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS			0 1 2 3	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES 10/21/11 12/24/11 4/9/12 5/6/12 SHEET 18 OF 46	

USERNAME => \$USER DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

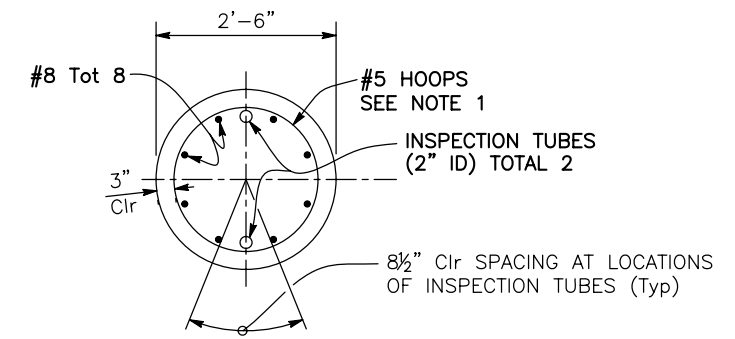
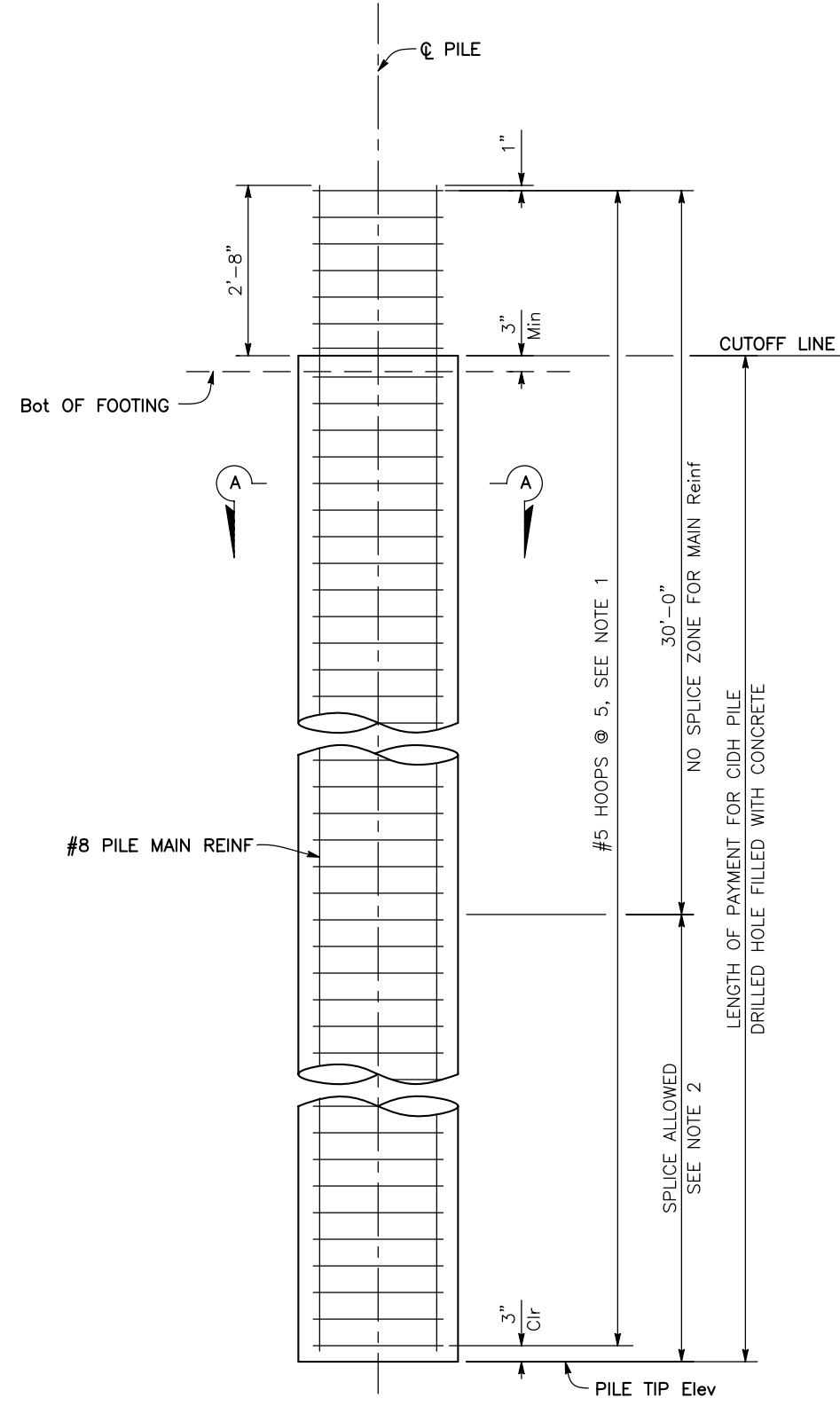
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	349	423

K. Hanj
 REGISTERED CIVIL ENGINEER DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER
 KHANDUSH HARRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 SAN JOSE, CA 95126



SECTION A-A
3/4" = 1'-0"

- NOTES:
- All CIDH hoops shall be butt welded continuous and meet "Ultimate Splice" requirements.
 - Splices in pile main reinforcement shall meet "Ultimate Splice" requirement.

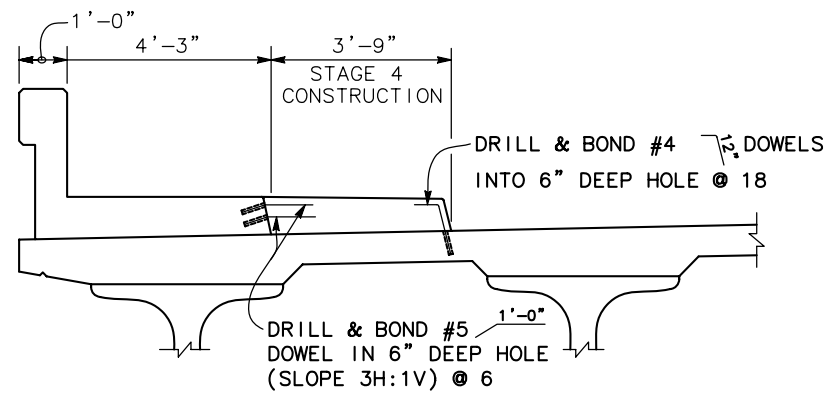
ELEVATION
30" CIDH CONCRETE PILE (AT BENT 2 ONLY)
3/4" = 1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

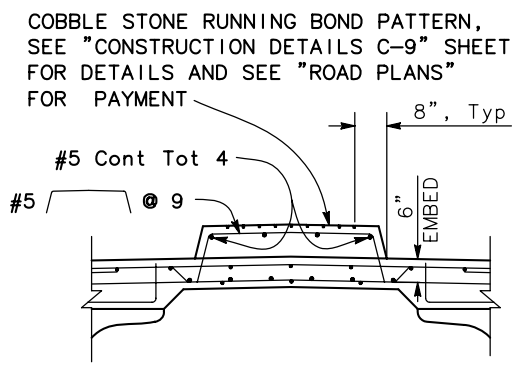
DESIGN OVERSIGHT	DESIGN	BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO.	ROUTE 219/99 SEPARATION (REPLACE)
	DETAILS	BY S. DESALEGN	CHECKED K. HARRISAZ		A. WANG PROJECT ENGINEER	
SIGN OFF DATE	QUANTITIES	BY S. DESALEGN	CHECKED B. SWARUP	PROJECT NUMBER & PHASE: 1000000100	POST MILES	BENT DETAILS No. 4
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	UNIT: 1457	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 19 OF 46

DATE PLOTTED => \$DATE USERNAME => \$USER TIME PLOTTED => \$TIME

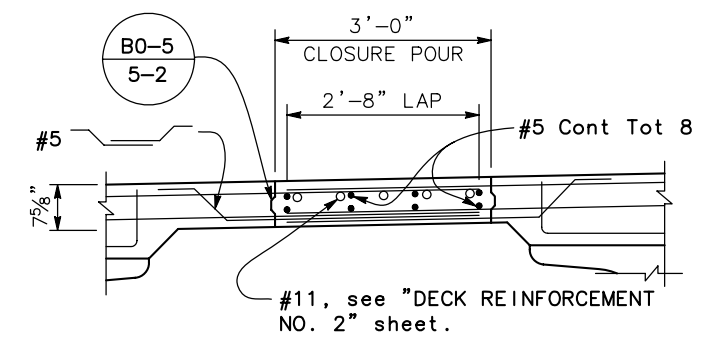
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	350	423
REGISTERED CIVIL ENGINEER			DATE	5/6/12	
K. Hany			9/04/12		
PLANS APPROVAL DATE			No. C39591		
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STANISLAUS COUNTY			RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.		
1716 MORGAN ROAD, MODESTO, CA 95358			1038 LEIGH AVE., SUITE 100, SAN JOSE, CA 95126		



CONCRETE BARRIER DETAIL
1/2" = 1'-0"

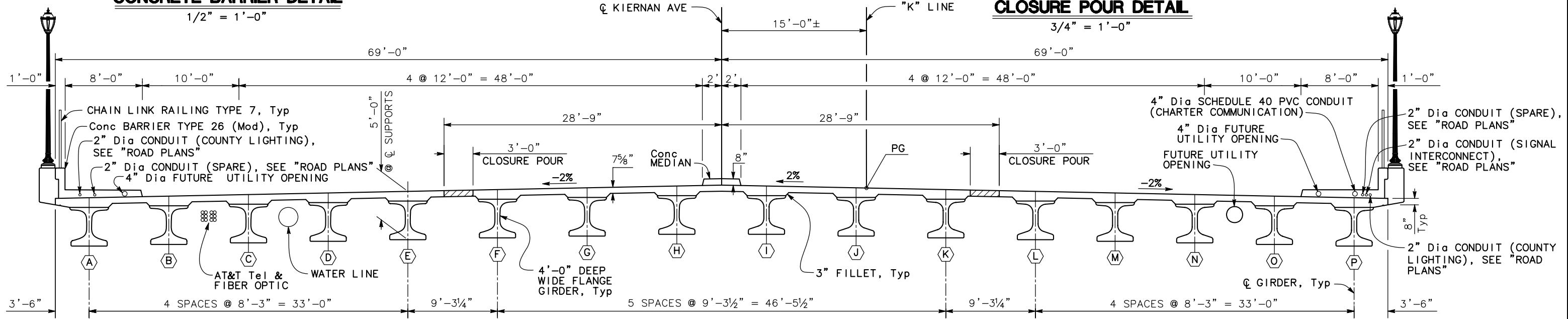


CONCRETE MEDIAN DETAIL
1/2" = 1'-0"

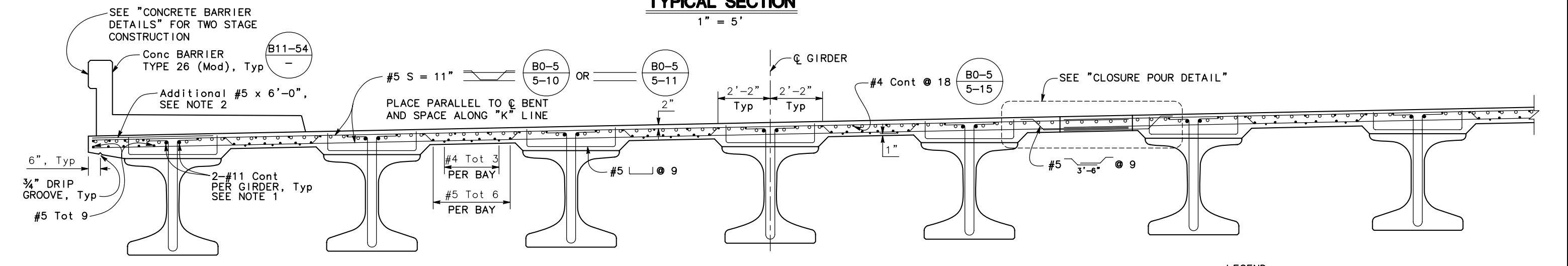


Note: Falsework shall not be released less than 28 days after the last concrete has been placed. Closure pour concrete shall not be placed sooner than 14 days after the falsework has been released.

CLOSURE POUR DETAIL
3/4" = 1'-0"



TYPICAL SECTION
1" = 5'



PARTIAL TYPICAL SECTION
1/2" = 1'-0"

- NOTES:
- Lap splices equal to two times standard splice length.
 - Additional #5 top transverse deck reinforcement shall be bundled with each alternating top transverse bar total 7 at each corner of overhang at Begin Bridge and End Bridge.

- LEGEND:
- Indicates additional deck reinforcement, see "DECK REINFORCEMENT" sheets.
 - ⊗ Indicates girder designation
 - ▨ Closure Pour

NOTE:
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DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56	TYPICAL SECTION	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 20 OF 46
		ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	FILE => \$REQUEST	DISREGARD PRINTS BEARING EARLIER REVISION DATES	10/21/11 2/24/12 4/9/12 5/6/12	

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	351	423

5/6/12
DATE

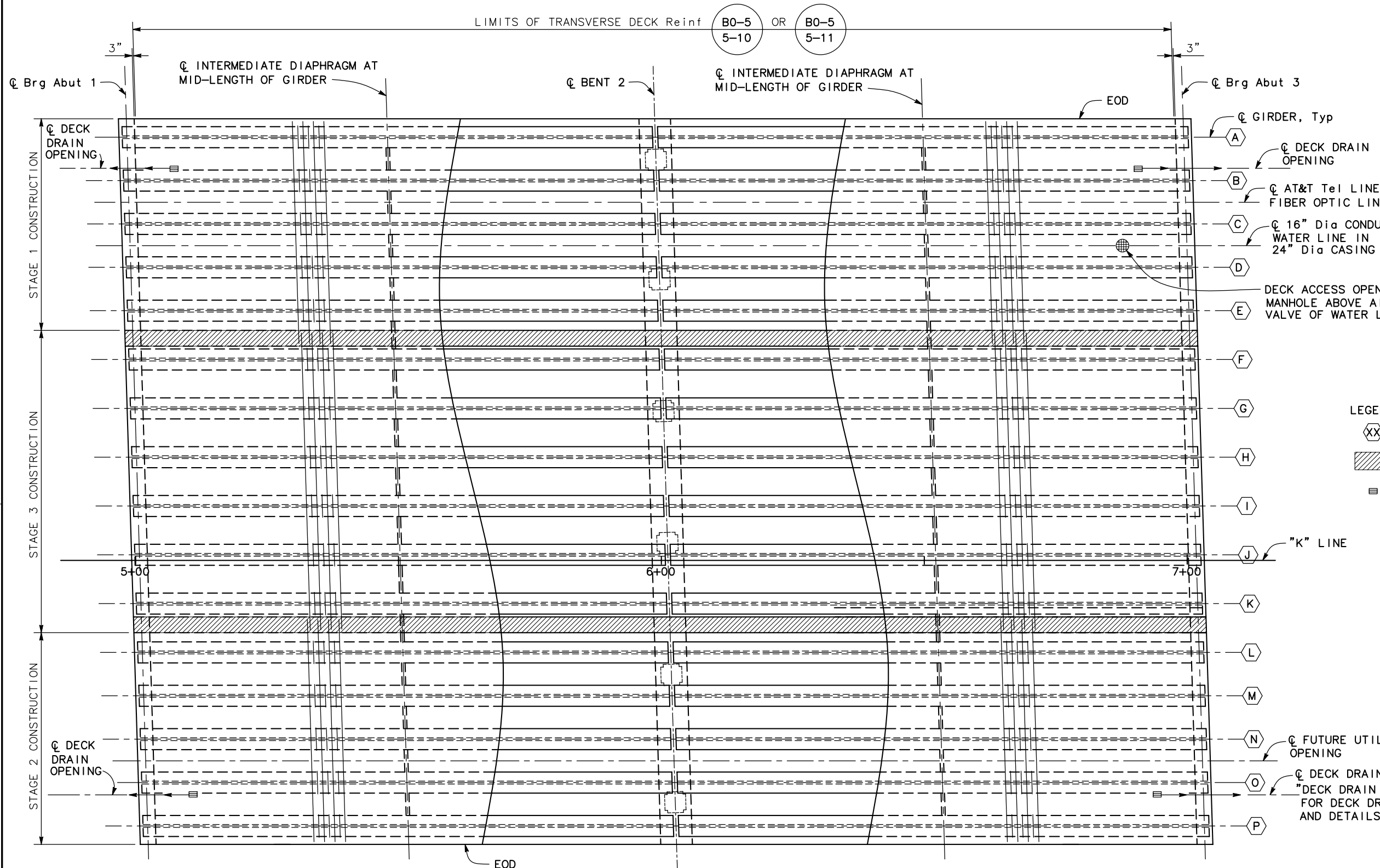
9/04/12
PLANS APPROVAL DATE

REGISTERED CIVIL ENGINEER

REGISTERED PROFESSIONAL ENGINEER
 KHANDUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
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 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



(B7-11 U43) (B7-11 U45)

- LEGEND:
- XX Indicates girder designation
 - Closure Pour
 - Indicates Deck Drain Type D-1 (Mod)

GIRDER LAYOUT
1" = 10'

NOTE:
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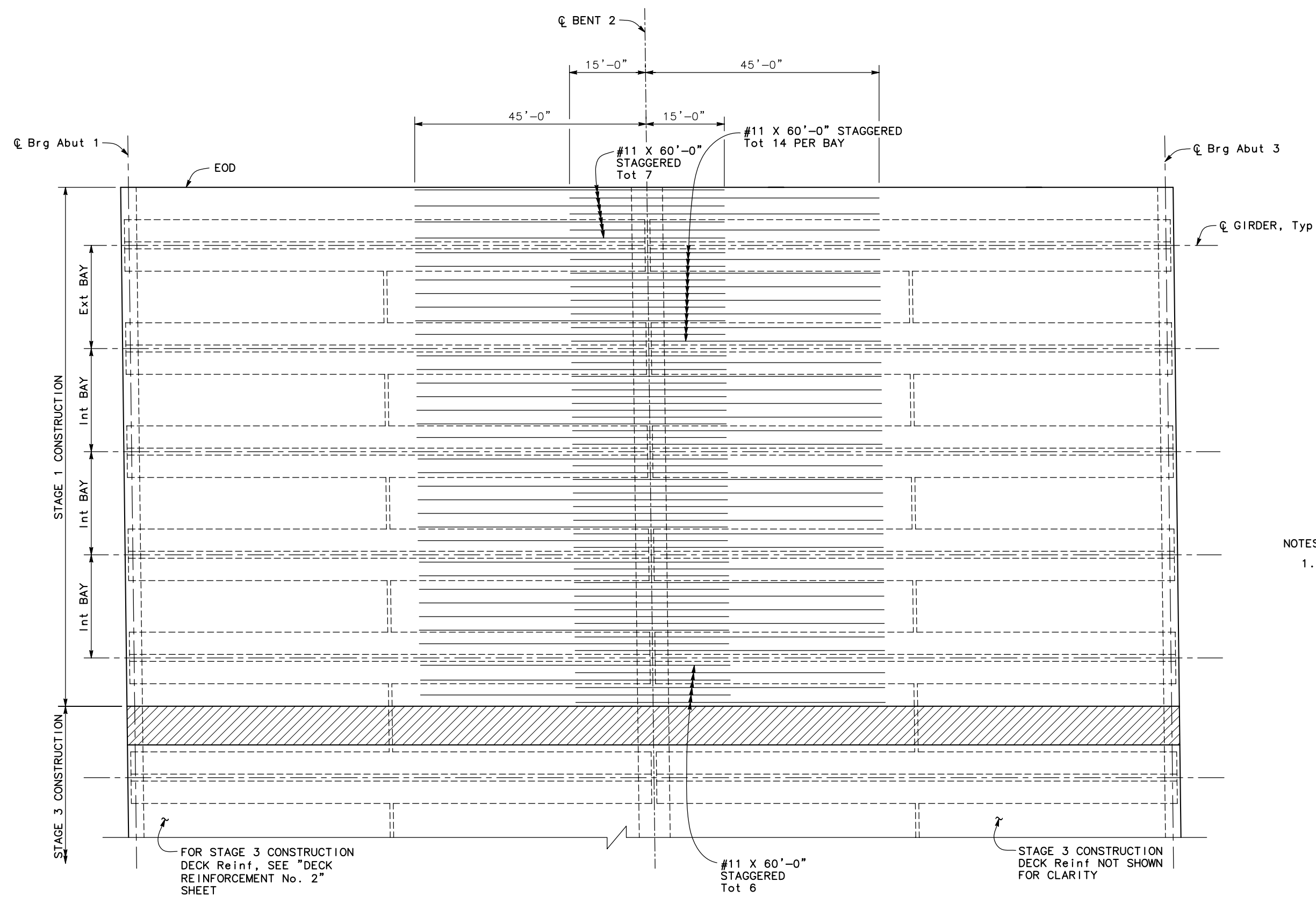
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	PROJECT ENGINEER A. WANG	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) GIRDER LAYOUT
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ		POST MILES R22.56		
	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP				
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES 10/21/11 2/24/12 4/9/12 5/6/12
				FILE => \$REQUEST	DISREGARD PRINTS BEARING EARLIER REVISION DATES	SHEET 21 OF 46

TIME PLOTTED => \$TIME
DATE PLOTTED => \$DATE
USERNAME => \$USER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	352	423

K. Haney
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
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 KANADUSH HARRISAZ
 No. C39591
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 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



LEGEND:
 Closure Pour

NOTES:
 1. Bars to be equally spaced along C Bent , stagger bars as shown.

ADDITIONAL DECK REINFORCEMENT

NO SCALE
 (STAGE 1 AND STAGE 3 CONSTRUCTION SHOWN, STAGE 2 CONSTRUCTION SIMILAR)

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

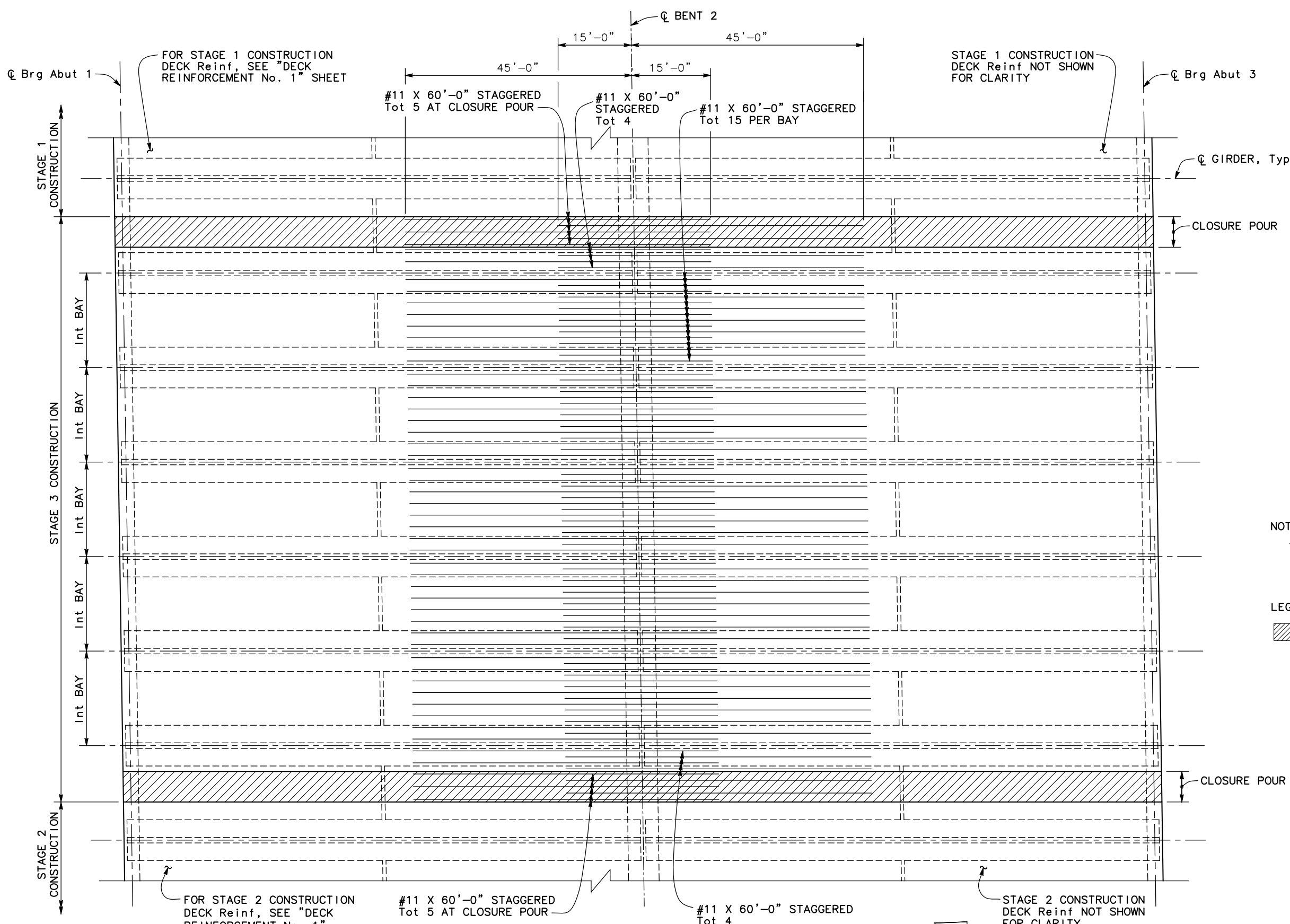
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) DECK REINFORCEMENT No. 1												
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARRISAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56													
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES												
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS				0 1 2 3	<table border="1"> <tr> <th>REVISED</th> <th>DATE</th> <th>SHEET</th> <th>OF</th> </tr> <tr> <td>10/21/11</td> <td>2/27/12</td> <td>4/9/12</td> <td>5/6/12</td> </tr> <tr> <td>22</td> <td>46</td> <td></td> <td></td> </tr> </table>	REVISED	DATE	SHEET	OF	10/21/11	2/27/12	4/9/12	5/6/12	22	46		
REVISED	DATE	SHEET	OF														
10/21/11	2/27/12	4/9/12	5/6/12														
22	46																

USERNAME => \$USER DATE PLOTTED => \$DATE TIME PLOTTED => \$TIME

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	353	423

REGISTERED CIVIL ENGINEER
 K. Hanj
 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KHANDUSH HARIKSAZ
 No. C39591
 Exp. 12/31/13
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DEPARTMENT OF PUBLIC WORKS
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 MODESTO, CA 95358
 RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



NOTES:
 1. Bars to be equally spaced along \bar{C} Bent, stagger bars as shown.

LEGEND:
 Closure Pour

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

ADDITIONAL DECK REINFORCEMENT

NO SCALE



DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) DECK REINFORCEMENT No. 2
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIKSAZ	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS		UNIT: PROJECT NUMBER & PHASE: 1457 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 23 OF 46

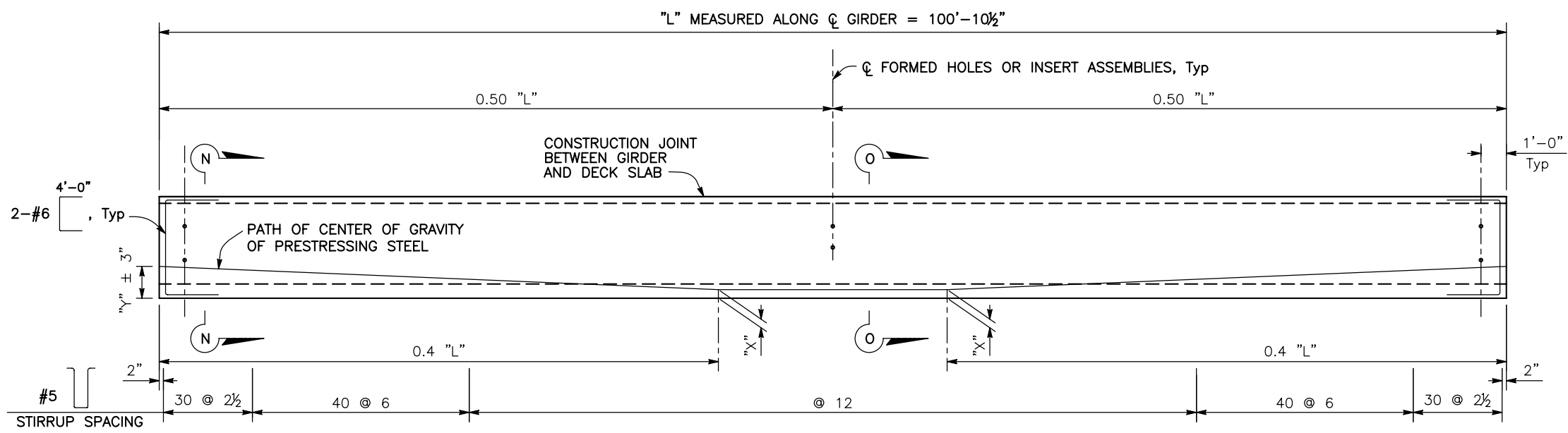
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DATE PLOTTED => \$DATE
USERNAME => \$USER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	354	423

K. Harisaz
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12

KHANDUSH HARRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

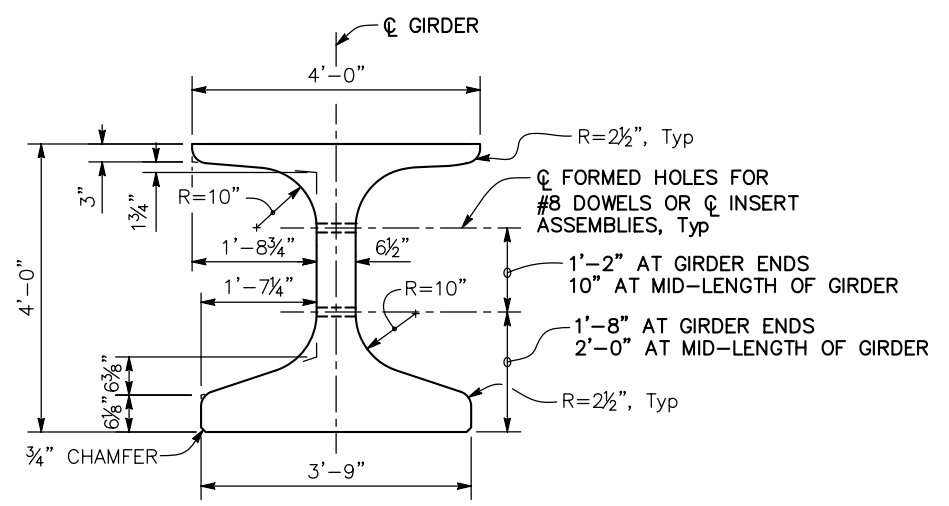
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



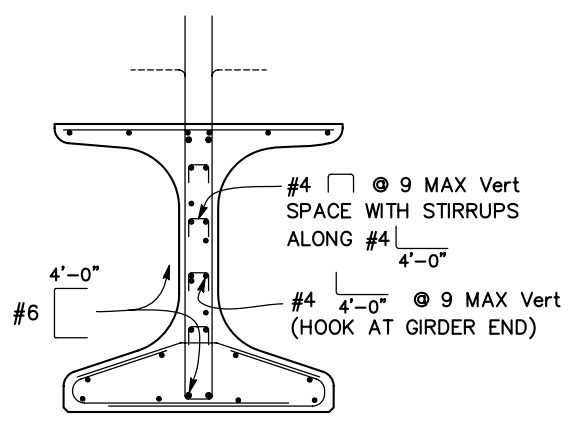
GIRDER ELEVATION
NO SCALE

NOTE: See "GIRDER LAYOUT" sheet for girder designation and location of intermediate diaphragms.

- PRESTRESSING NOTES:**
- The Jacking Force (P) is the jacking force required at the point of control along the span. The jacking force does not include any fabrication specific losses.
 - The maximum tensile stress in the prestressing steel upon release shall not exceed 75% of the specified minimum ultimate tensile strength of the prestressing steel.
 - The maximum temporary tensile stress (jacking stress) in the prestressing steel shall not exceed 80% of the specified minimum ultimate tensile strength of the prestressing steel.
 - Concrete strength:
 f'_{ci} is at time of initial stressing
 f'_c is at 28 days
 - Deflection components are informational and will be used to set screed line elevations.
 - Screed line elevations for deck concrete will be determined by the Engineer.
 - Contractor may interpolate "P" AND "X" values between the limits shown, as approved by the Engineer.
 - There shall be a minimum of two hold downs per girder for the prestressing.
 - Prestressing strand shall be 270 ksi low relaxation.

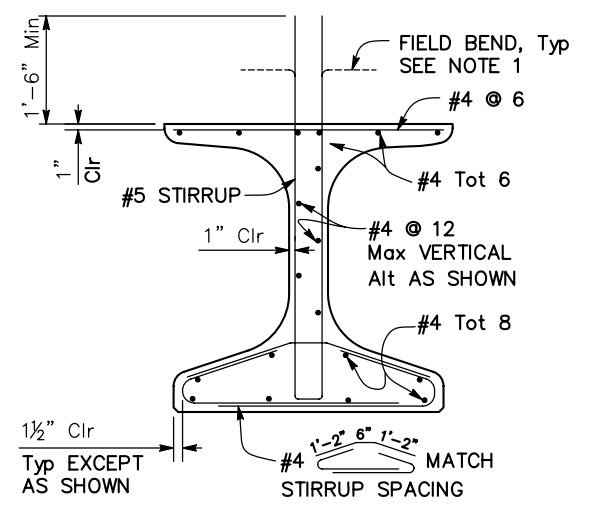


GIRDER TYPICAL SECTION
3/4" = 1'-0"



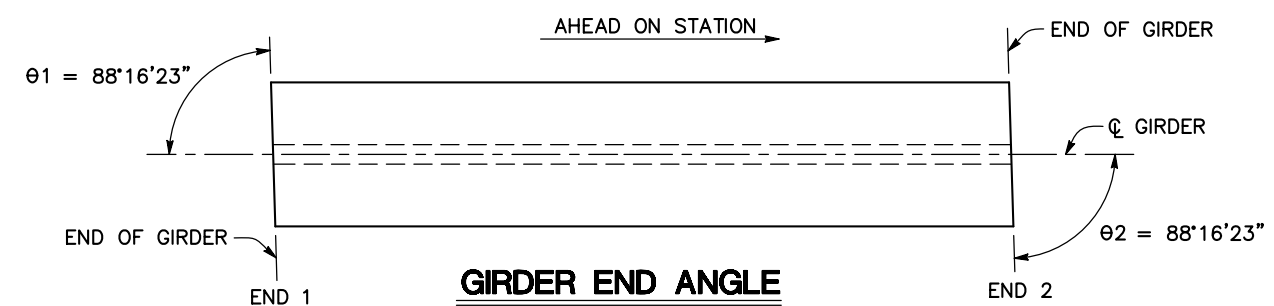
NOTE: For details not shown, see "SECTION O-O"

SECTION N-N
NO SCALE



NOTE: For "WELDED WIRE REINFORCEMENT (WWR) ALTERNATIVE", see "PC/PS CONCRETE DETAILS No. 2" sheet

SECTION O-O
NO SCALE



GIRDER END ANGLE
NO SCALE

NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

GIRDER DESIGNATION AT BOTH SPAN 1 & SPAN 2	P = JACKING FORCE (KIPS)			DEFLECTION COMPONENT AT MID-SPAN (IN)			
	"X" (IN)	"Y" (IN)	f'ci	f'c	① DECK DL	② RAIL + SIDEWALK + LIGHTING + CONC MEDIAN + UTILITY DL	
A B C N O P	4 1/2"	12 5/8"	1,580	5.0	7.2	1.55"	0.15"
	5 1/2"	12"	1,670				
D E F G H I J K L M	4 1/2"	13 1/4"	1,495	4.7	6.5	1.75"	0.05"
	5 1/2"	12 5/8"	1,580				

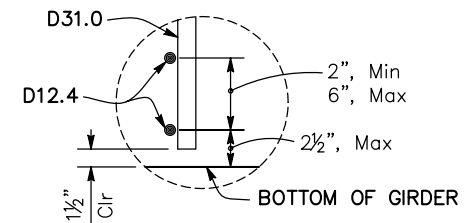
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) PC/PS CONCRETE GIRDER DETAILS No. 1
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARRISAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 24 OF 46

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS
 DISREGARD PRINTS BEARING EARLIER REVISION DATES
 FILE => \$REQUEST

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	355	423

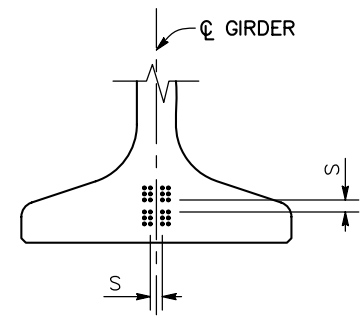
K. Harisaz
 REGISTERED CIVIL ENGINEER DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KIANOUSH HARIRSAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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 DEPARTMENT OF PUBLIC WORKS
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 MODESTO, CA 95358
RAJAPPAN & MEYER
CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



NOTE: Bottom of stirrup WWR detail shown, top similar

DETAIL A
NO SCALE

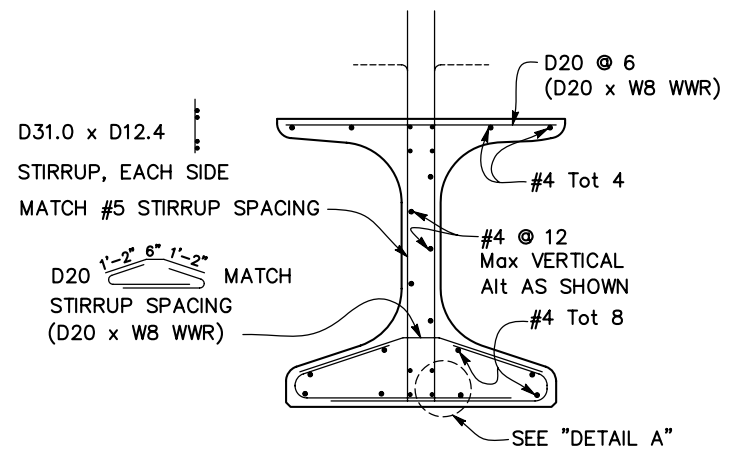


NOTES:

1. Strands may be bundled in groups consisting of 3 vertically 2 horizontally, and separated at the ends.
2. The min distance "S" between groups or individual strands is 1 1/2" for 3/8" strands, 1 3/4" for 1/2" strands and 2" for 5/8" strands.
3. "S" is measured between centers of adjacent strands.
4. Approval of Engineer is required for deviation.

CLEARANCE FOR PRETENSIONED STRANDS

$3/4" = 1'-0"$



NOTE: W8 WWR not shown

WELDED WIRE REINFORCEMENT (WWR) ALTERNATIVE

NO SCALE

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) PC/PS CONCRETE GIRDER DETAILS No. 2
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56	
	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304

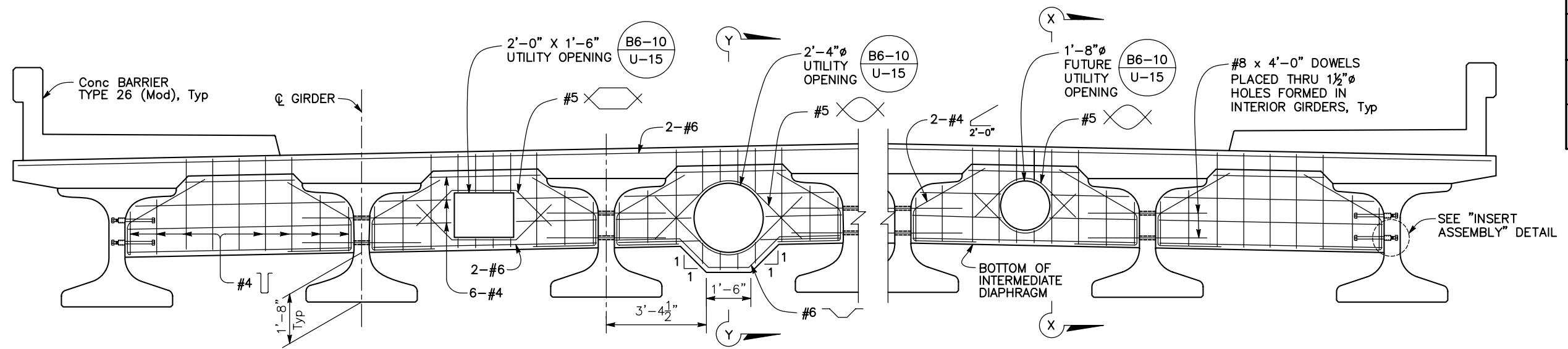
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	356	423

K. Harisaz
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.

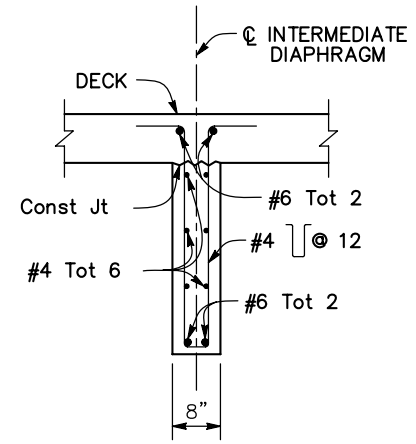


DEPARTMENT OF PUBLIC WORKS
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 MODESTO, CA 95358
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 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



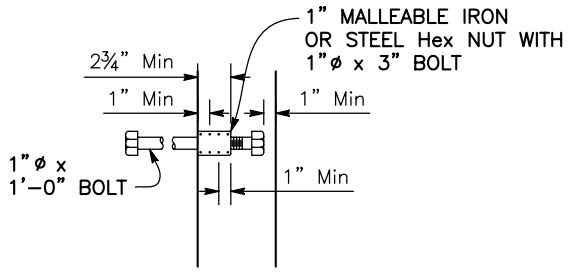
INTERMEDIATE DIAPHRAGM AT MID-LENGTH OF GIRDER

1/2" = 1'-0"



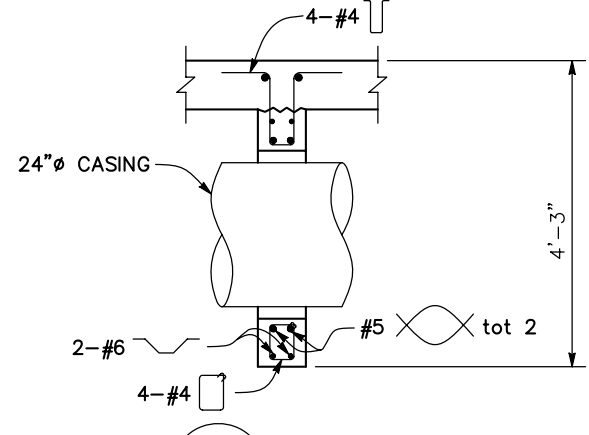
SECTION X-X

3/4" = 1'-0"



INSERT ASSEMBLY

NO SCALE



SECTION Y-Y

3/4" = 1'-0"

NOTE: SEE B6-10 U-15 FOR ADDITIONAL DETAILS.

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT
SIGN OFF DATE

DESIGN	BY A. WANG	CHECKED C. SHENOY
DETAILS	BY S. DESALEGN	CHECKED K. HARIRSAZ
QUANTITIES	BY S. DESALEGN	CHECKED B. SWARUP

PREPARED FOR THE
STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

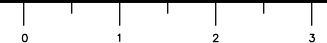
A. WANG
PROJECT ENGINEER

BRIDGE NO.	38-0163
POST MILES	R22.56

ROUTE 219/99 SEPARATION (REPLACE)
PC/PS CONCRETE GIRDER DETAILS No. 3

DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS



UNIT: 1457
PROJECT NUMBER & PHASE: 1000000100

CONTRACT NO.: 10-0L3304

DISREGARD PRINTS BEARING EARLIER REVISION DATES

REVISION DATES	SHEET	OF
10/21/11 12/24/12 4/29/12 5/6/12	26	46

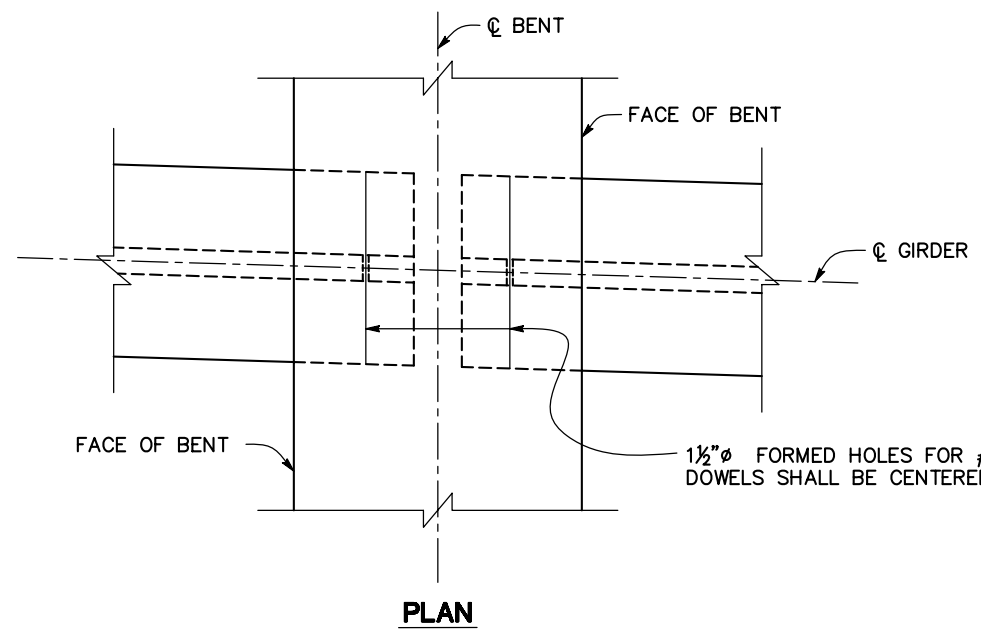
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99, 219	22.0/23.0, 0.0/0.5	357	423

K. Harry
 REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 KHANDUSH HARRISAZ
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

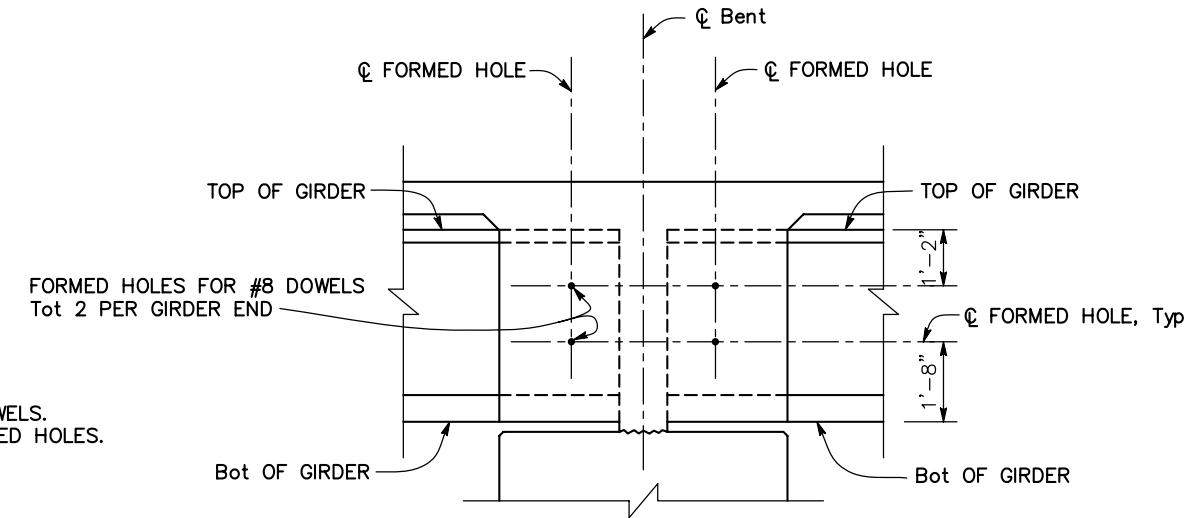
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 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



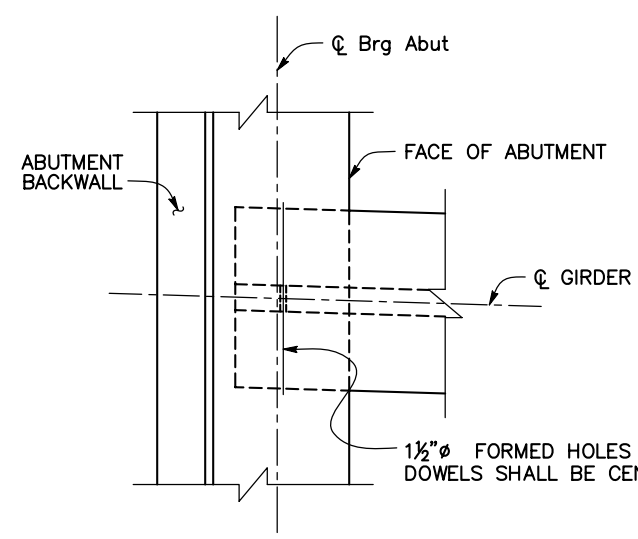
PLAN

$1\frac{1}{2}'' \phi$ FORMED HOLES FOR #8 x 4'-0" DOWELS.
 DOWELS SHALL BE CENTERED IN THE FORMED HOLES.



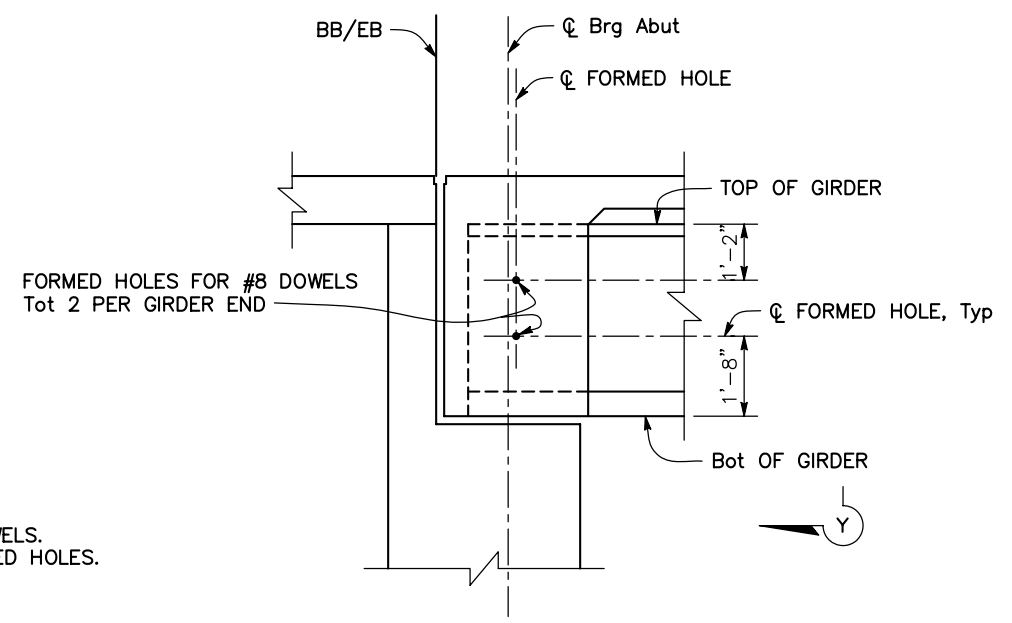
PART ELEVATION

GIRDER END DETAILS AT BENT
 $1/2'' = 1'-0''$



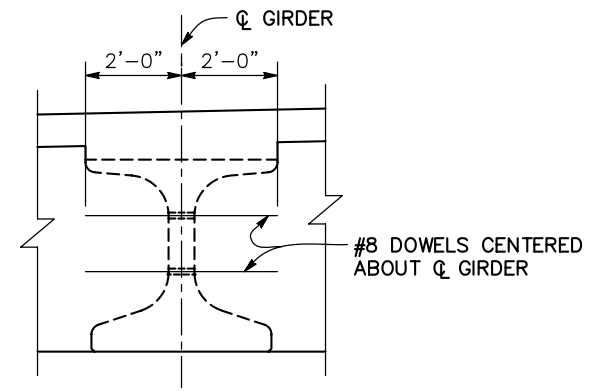
PLAN

$1\frac{1}{2}'' \phi$ FORMED HOLES FOR #8 x 4'-0" DOWELS.
 DOWELS SHALL BE CENTERED IN THE FORMED HOLES.

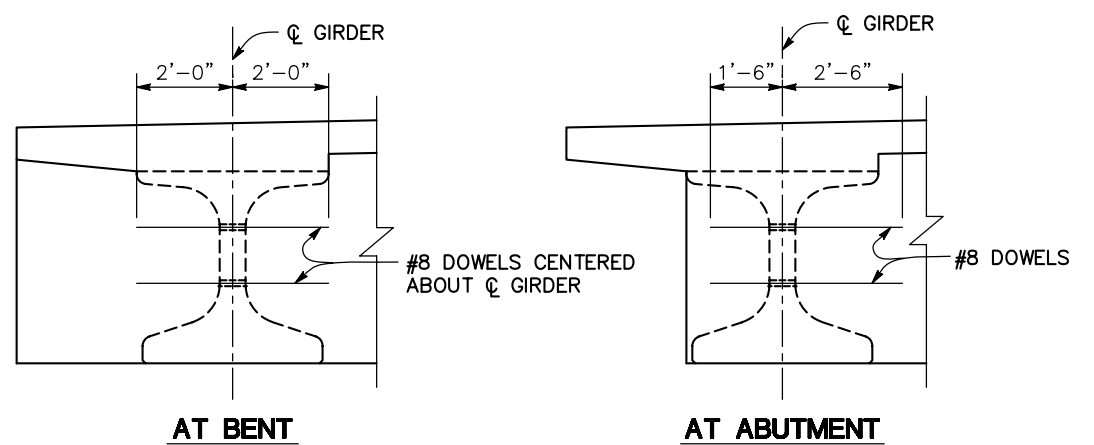


PART ELEVATION

GIRDER END DETAILS AT ABUTMENT
 $1/2'' = 1'-0''$



INTERIOR GIRDER AT ABUTMENT & AT BENT
 $1/2'' = 1'-0''$



EXTERIOR GIRDER
 $1/2'' = 1'-0''$

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) PC/PS CONCRETE GIRDER DETAILS No. 4	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARRISAZ		PROJECT NUMBER & PHASE: 1000000100	POST MILES R22.56		REVISION DATES
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457	CONTRACT NO.: 10-0L3304		10/21/11 2/24/12 4/29/12 5/6/12 SHEET 27 OF 46

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3
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 DISREGARD PRINTS BEARING EARLIER REVISION DATES
 USERNAME => \$USER
 TIME PLOTTED => \$TIME

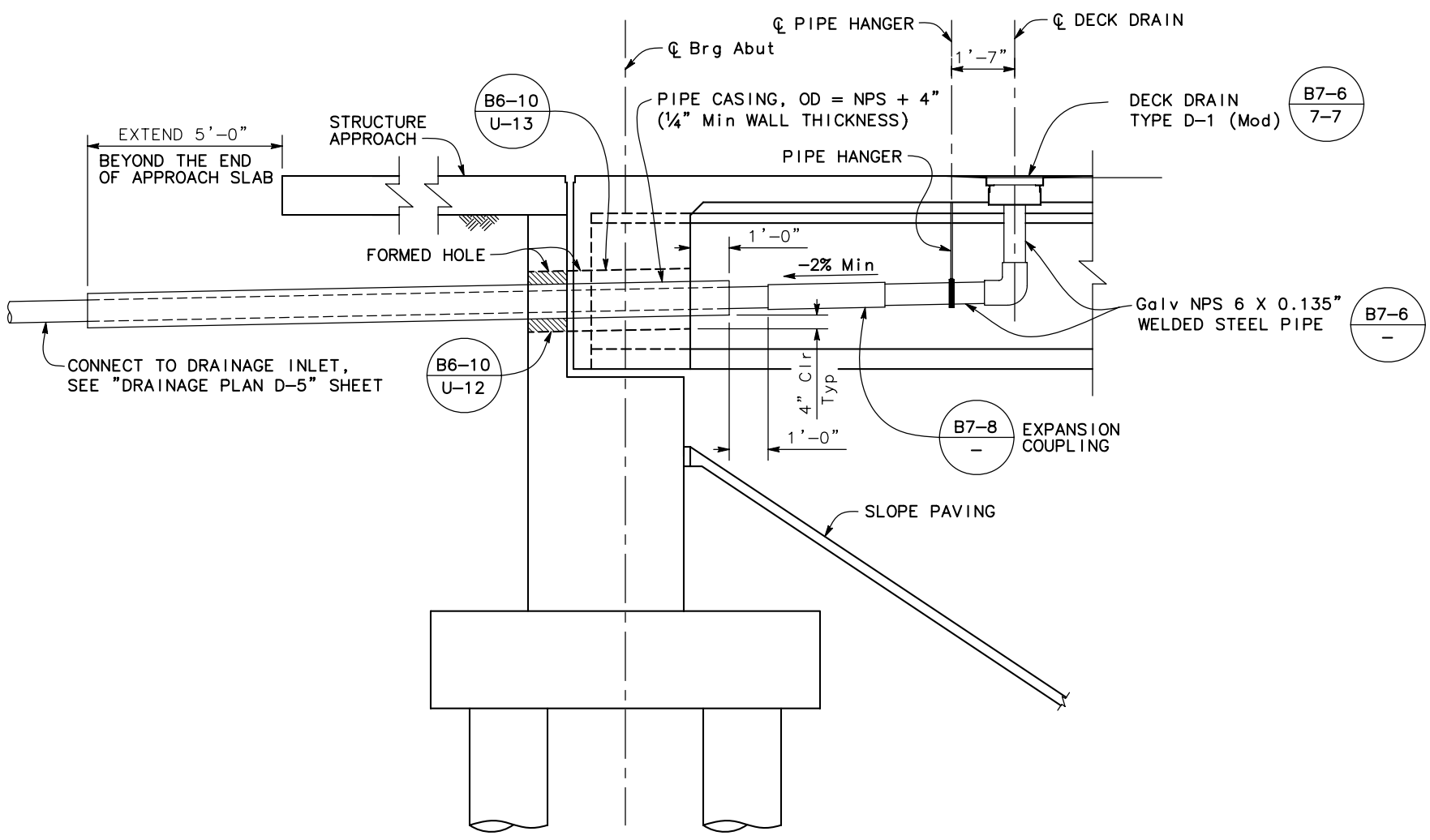
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	358	423

REGISTERED CIVIL ENGINEER
 K. Harisaz
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12
 RAJAPPAN & MEYER CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126

STATE OF CALIFORNIA
 CIVIL
 No. C39591
 Exp. 12/31/13

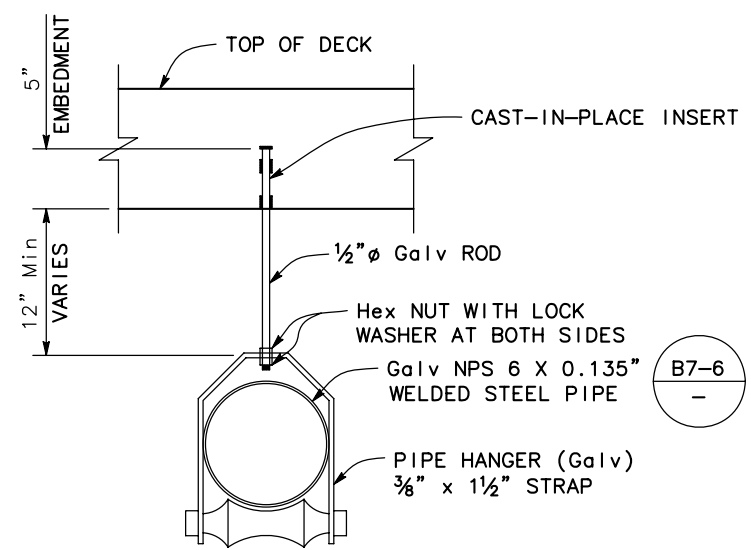
DECK DRAIN LOCATION TABLE

☐ DECK DRAIN	
EB KIERNAN Ave	WB KIERNAN Ave
"K" LINE Sta 5+11.00	"K" LINE Sta 5+07.50
"K" LINE Sta 6+94.00	"K" LINE Sta 6+90.50



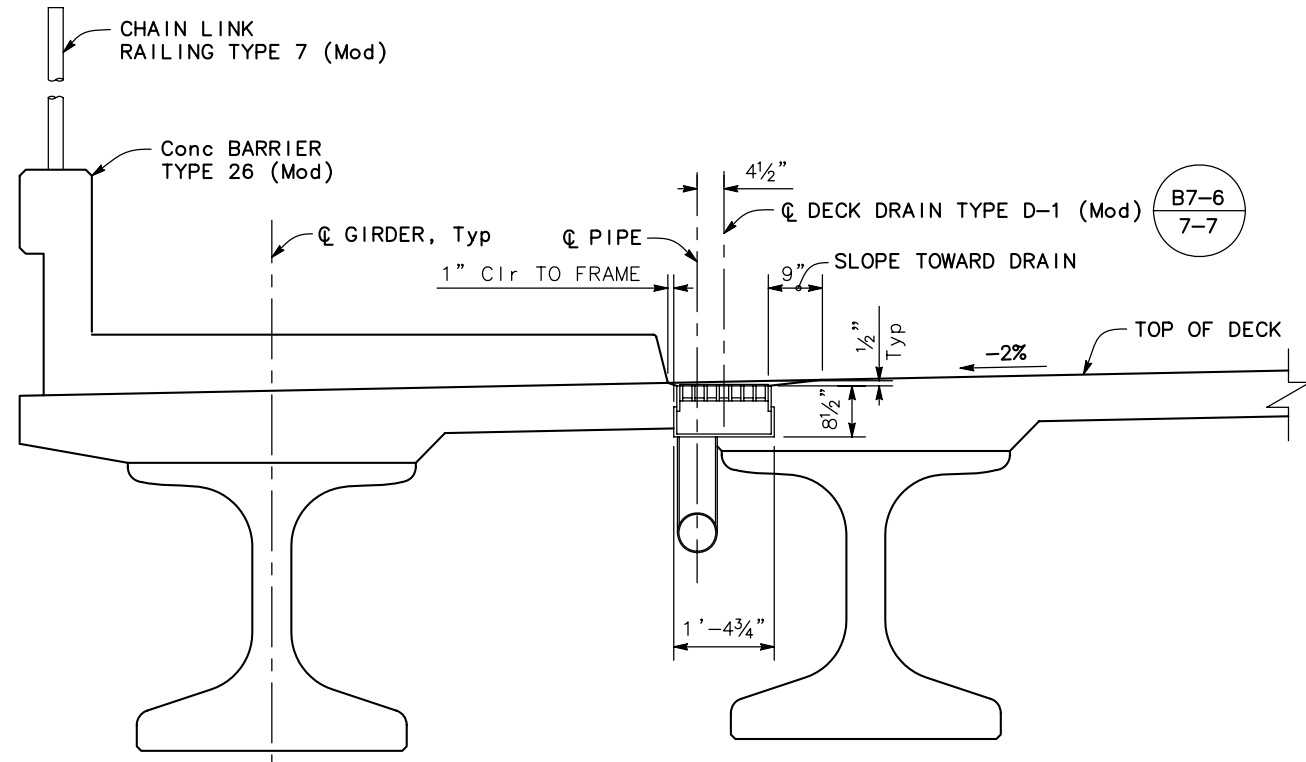
DECK DRAIN DETAILS

1/2" = 1'-0"



PIPE HANGER DETAILS

NO SCALE



ELEVATION - DECK DRAIN LOCATION

3/4" = 1'-0"

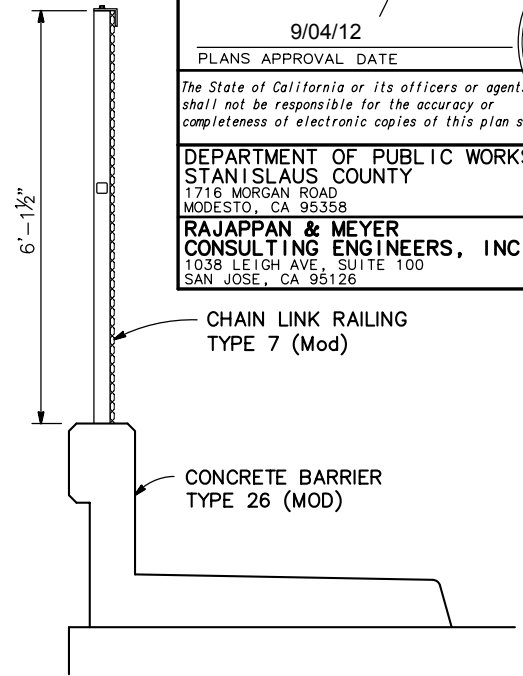
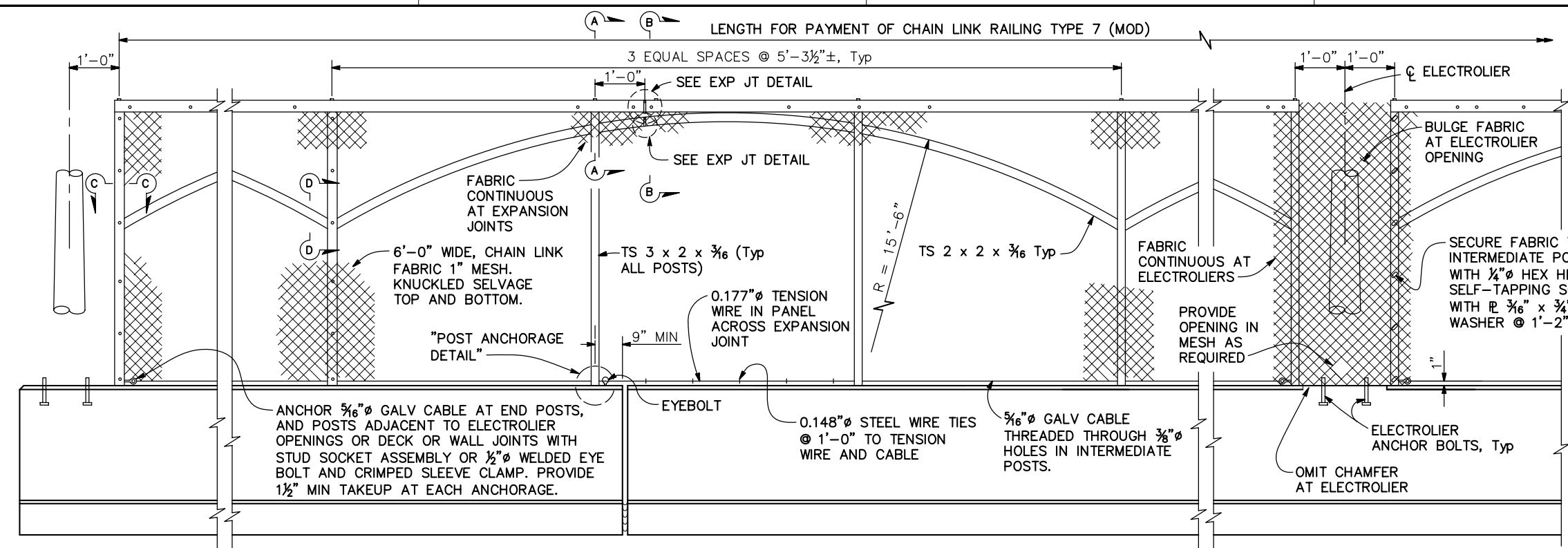
NOTES:

- All pipes in the deck drainage system shall be installed with a down slope greater than 2%.
- All drains and drainage assemblies to be galvanized.
- For all general notes, sleeve connection and other details not shown, see

NOTE:
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DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)	
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ			POST MILES R22.56	DECK DRAIN DETAILS	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 28 OF 46

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	359	423
K. Wang			5/6/12	REGISTERED CIVIL ENGINEER DATE	
9/04/12			PLANS APPROVAL DATE		
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DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126					

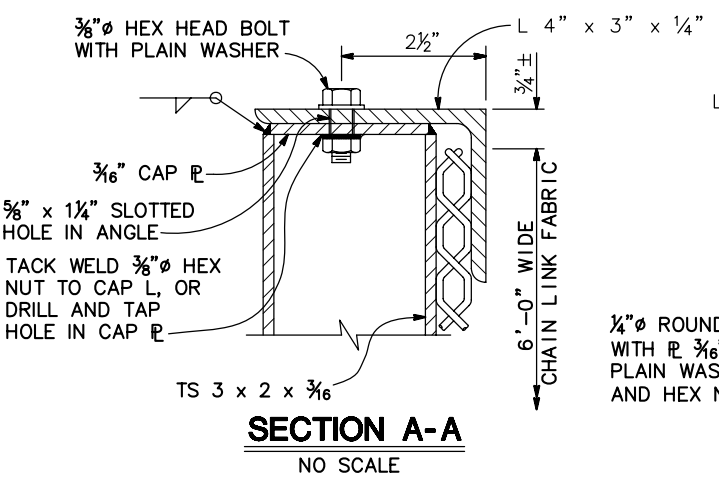


END POST

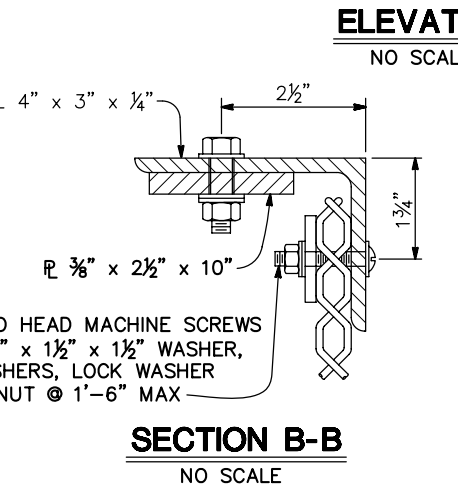
DECK OR WALL JOINT

AT ELECTROLIER

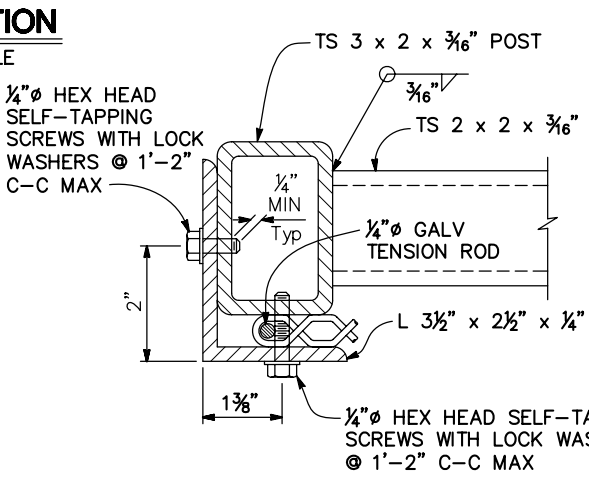
TYPICAL SECTION



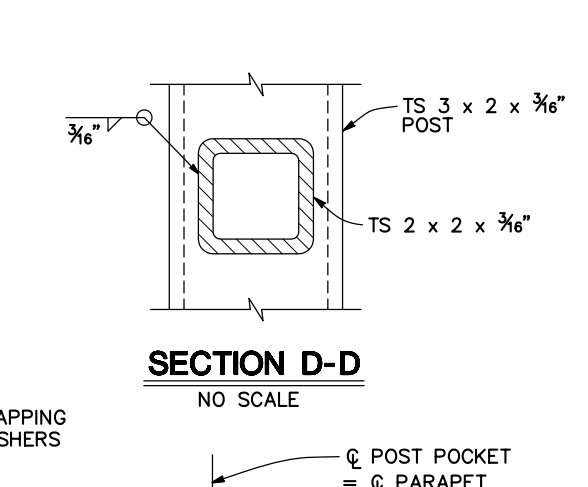
SECTION A-A
NO SCALE



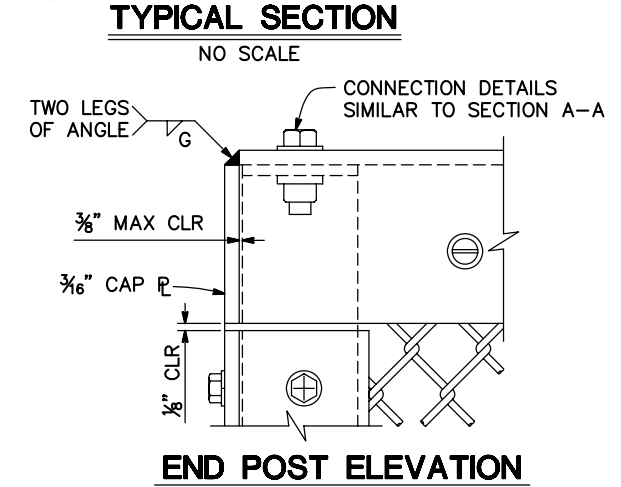
SECTION B-B
NO SCALE



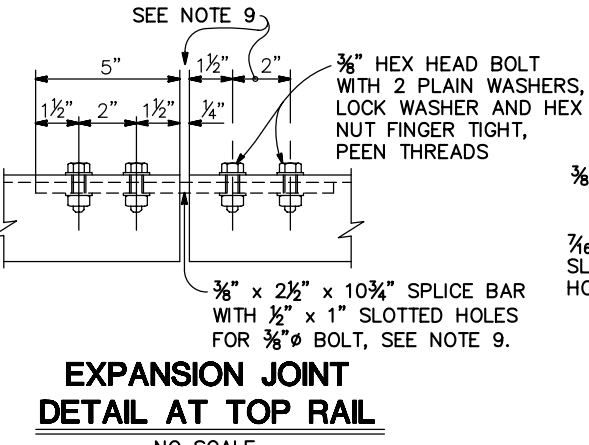
SECTION C-C
NO SCALE



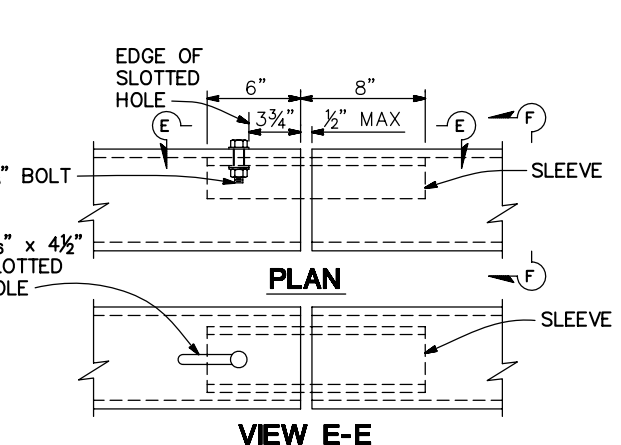
SECTION D-D
NO SCALE



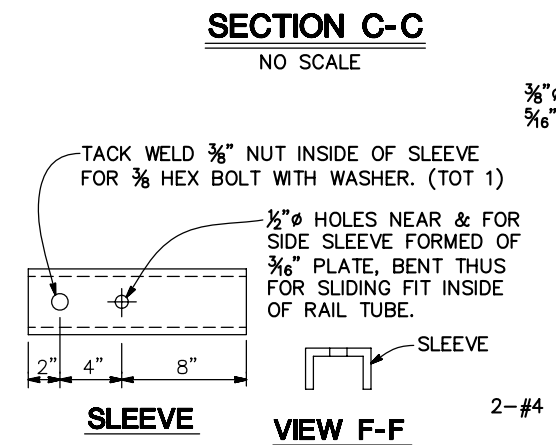
END POST ELEVATION
NO SCALE



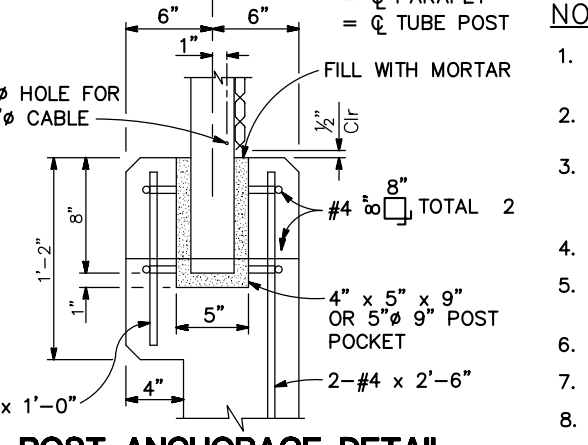
**EXPANSION JOINT
DETAIL AT TOP RAIL**
NO SCALE



**VIEW E-E
EXPANSION JOINT DETAIL AT TS 2 X 2**
NO SCALE



**SLEEVE
VIEW F-F**



POST ANCHORAGE DETAIL
NO SCALE

NOTES:

- Railing assembly except chain link fabric to be galvanized after fabrication.
- Posts shall be vertical.
- Horizontal angle shall be continuous over not less than two intermediate posts, except that a shorter length is permitted at expansion joints, electroliers and other rail discontinuities.
- When rail is on slope, place fabric parallel to slope.
- Alternative details may be submitted by Contractor for Engineer's approval.
- For details and reinforcement not shown see Standard Plan B11-54.
- See Project Plans for limits of Chain Link Railing Type 7 (Modified).
- Provide thimbles at all cable loops.
- Expansion joint same dimension as expansion joint in deck or wall. Increase slotted hole length and splice bar length correspondingly.

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

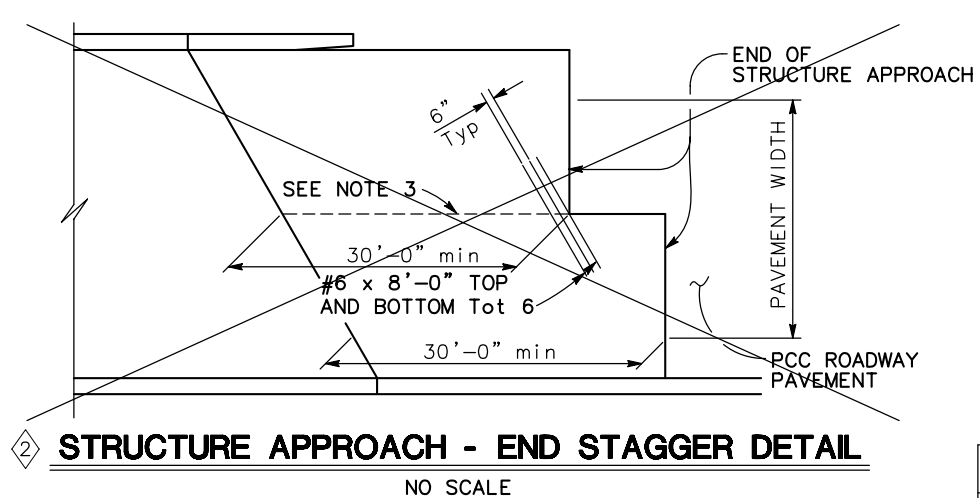
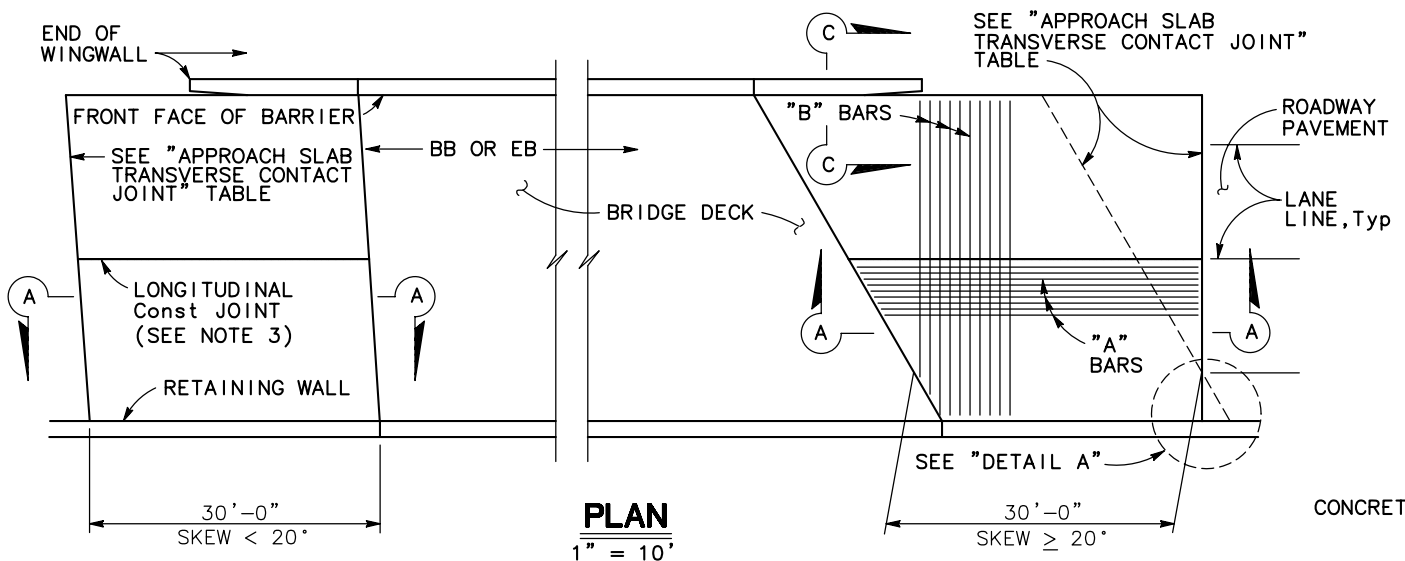
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) CHAIN LINK RAILING TYPE 7 (MOD)
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV. 7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	REVISION DATES: 10/27/11, 2/24/12, 4/9/12, 5/6/12	SHEET 29 OF 46

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	360	423

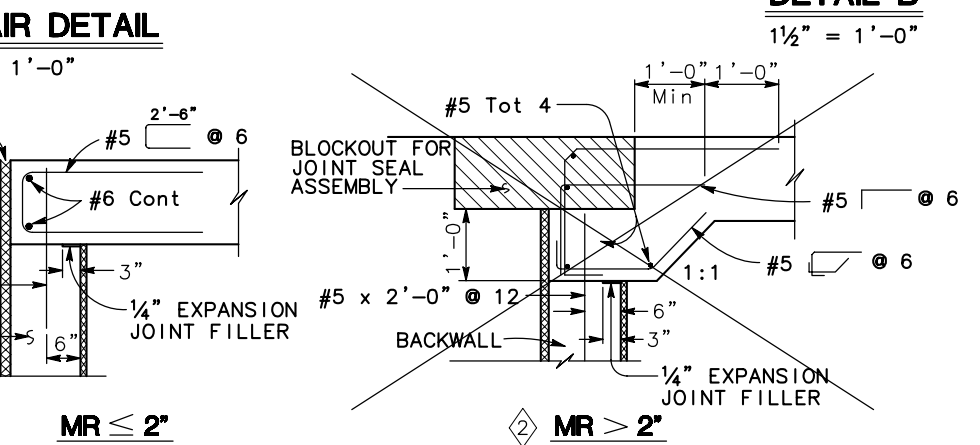
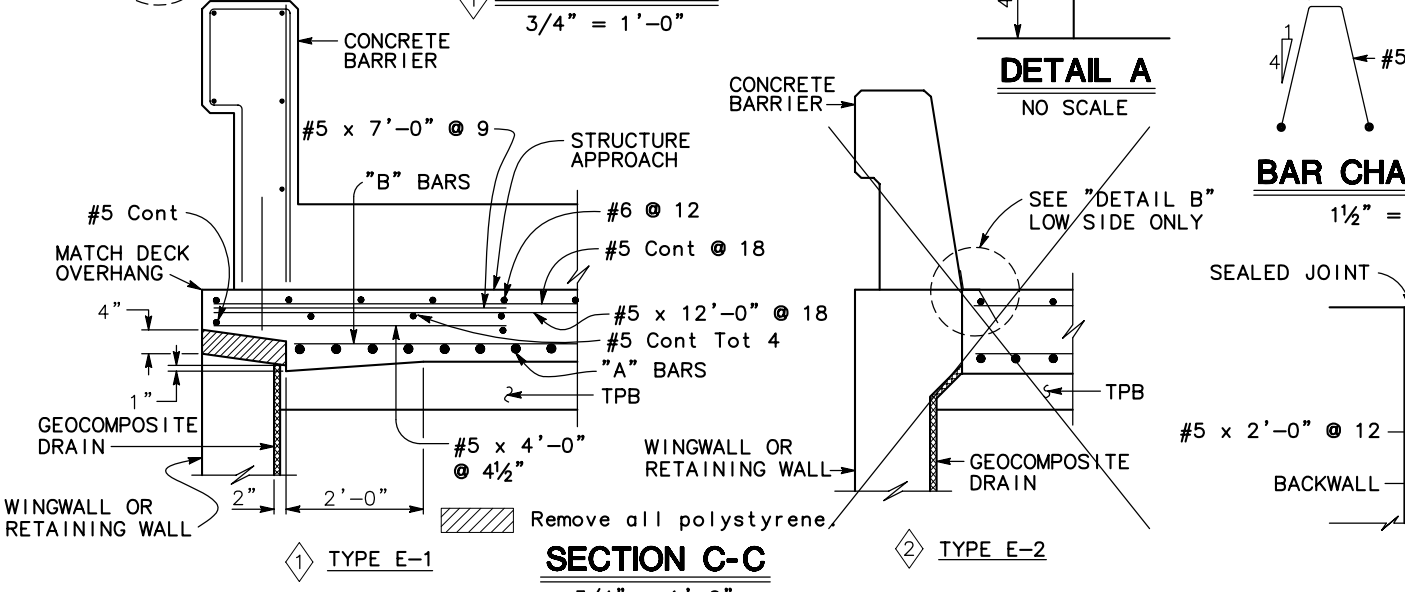
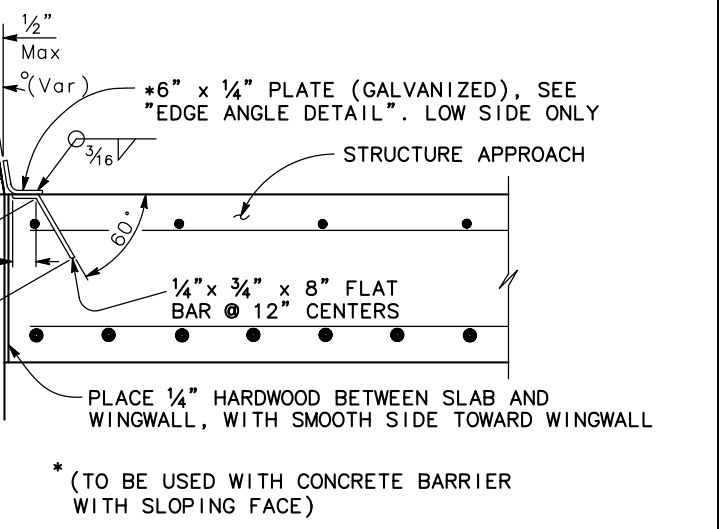
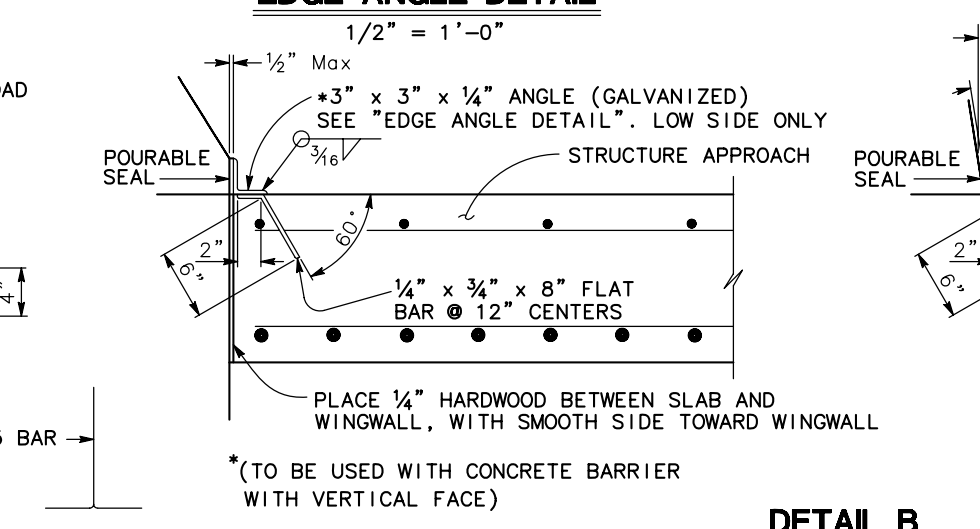
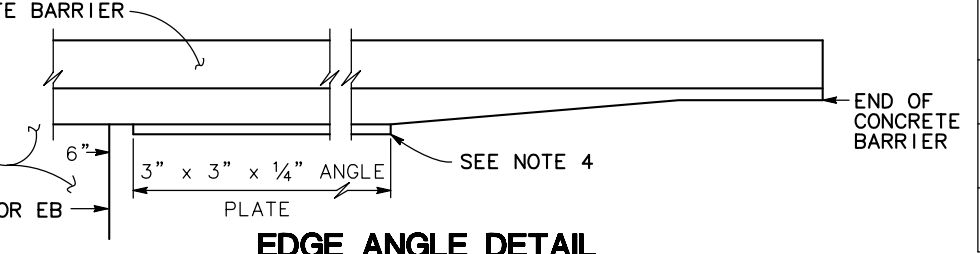
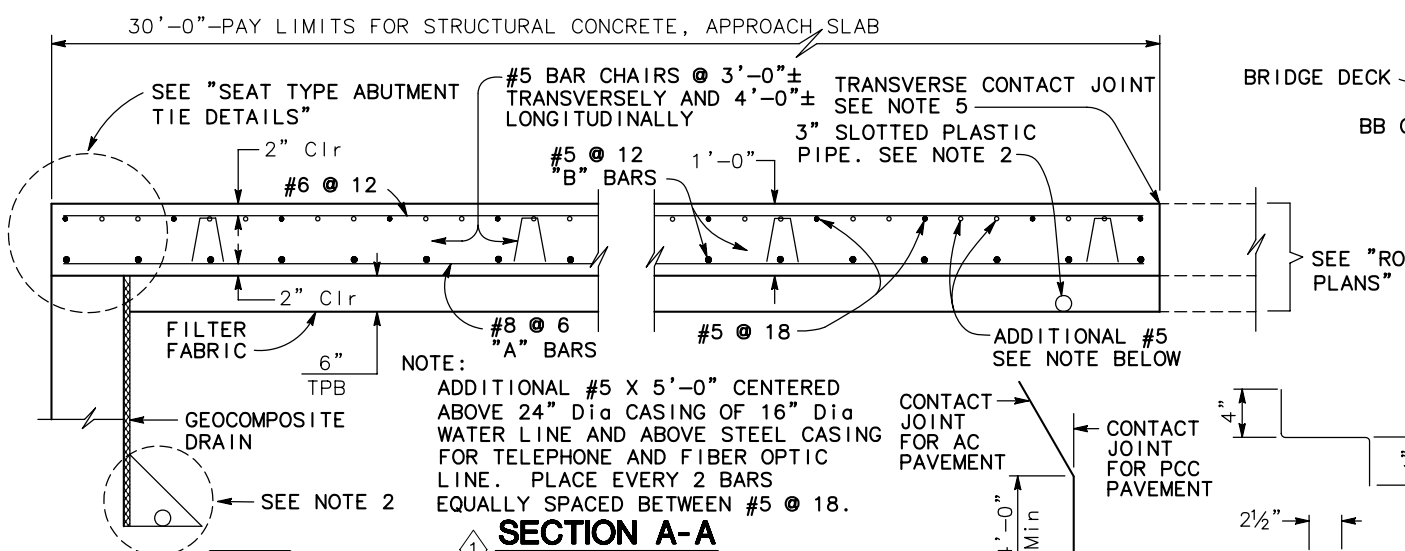
5/6/12
 REGISTERED CIVIL ENGINEER DATE
 9/04/12
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



APPROACH SLAB TRANSVERSE CONTACT JOINT		
APPROACH SKEW	WITH AC ROADWAY PAVEMENT	WITH PCC ROADWAY PAVEMENT
< 20°	PARALLEL TO FACE OF PN	PARALLEL TO FACE OF PN
20° - 45°	PARALLEL TO FACE OF PN USE "DETAIL A"	STAGGER LINES 24' TO 36' APART
> 45°	PARALLEL TO FACE OF PN USE "DETAIL A"	STAGGER AT EACH LANE LINE



- NOTES:
- For details not shown, see "STRUCTURE PLANS". For MR ≤ 2", adjust bar reinforcement to clear a sawcut for sealed joint, when required.
 - For drainage details, see "STRUCTURE APPROACH DRAINAGE DETAILS" sheet.
 - Longitudinal construction joints, when permitted by the Engineer, shall be located on lane lines.
 - End angle or plate at beginning of barrier transition, end of wingwall or end of structure approach as applicable.
 - For transverse contact joint with new PCC paving, refer to Standard Plan P10.
 - At the contractor's option, approach slab transverse reinforcement may be placed parallel to paving notch. Spacing of transverse reinforcement is measured along Q roadway.

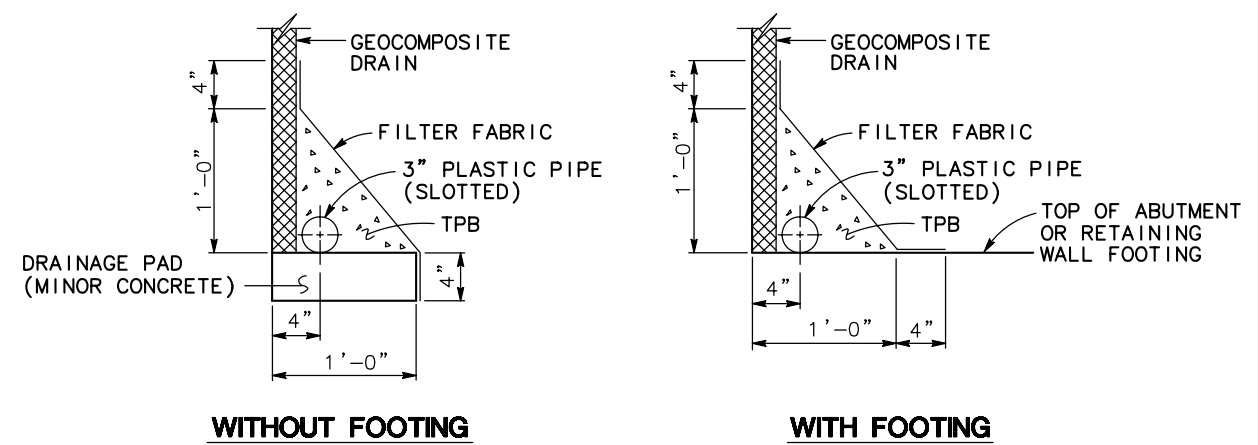
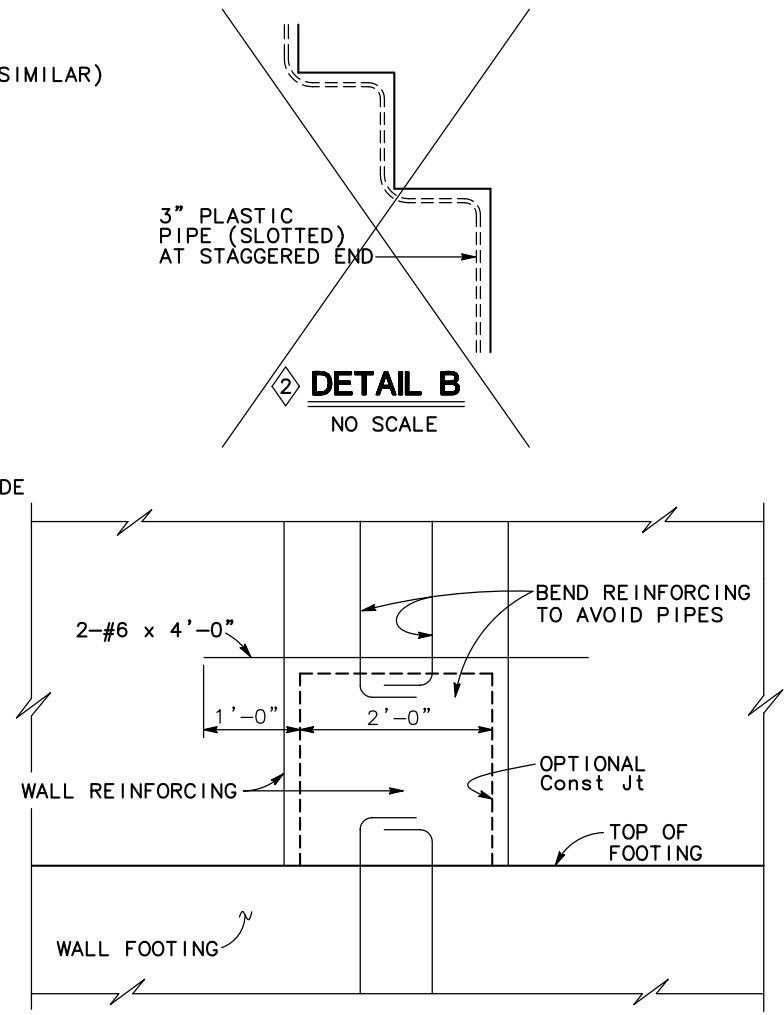
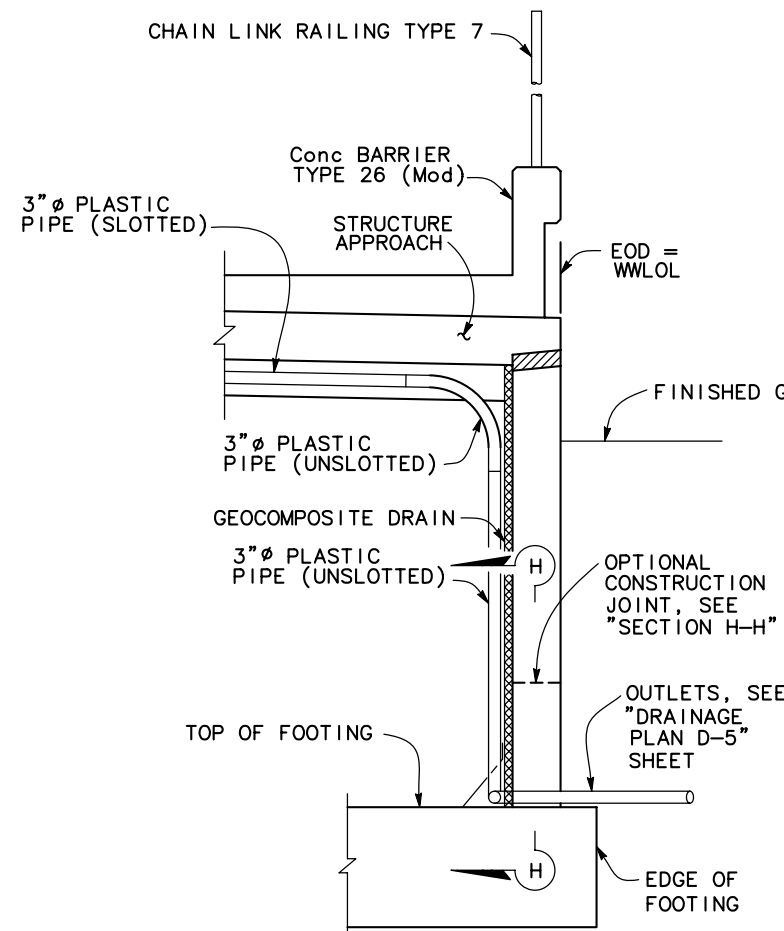
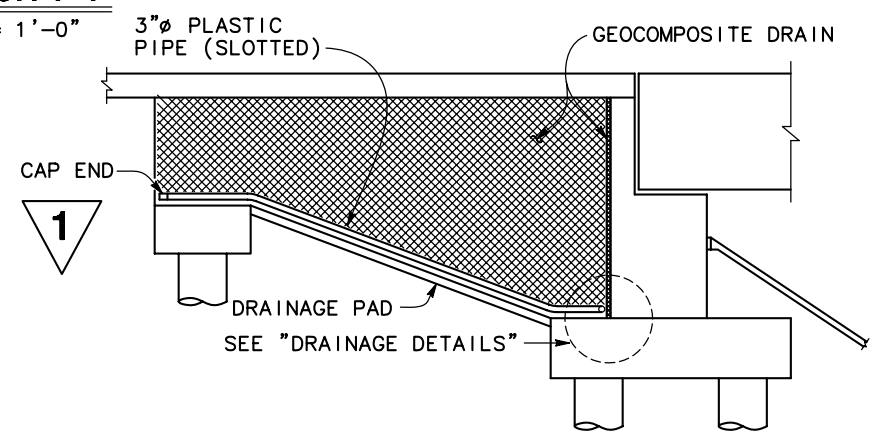
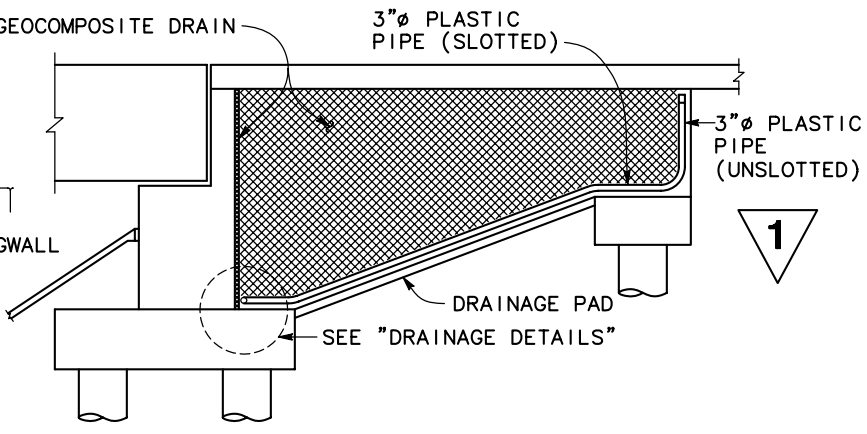
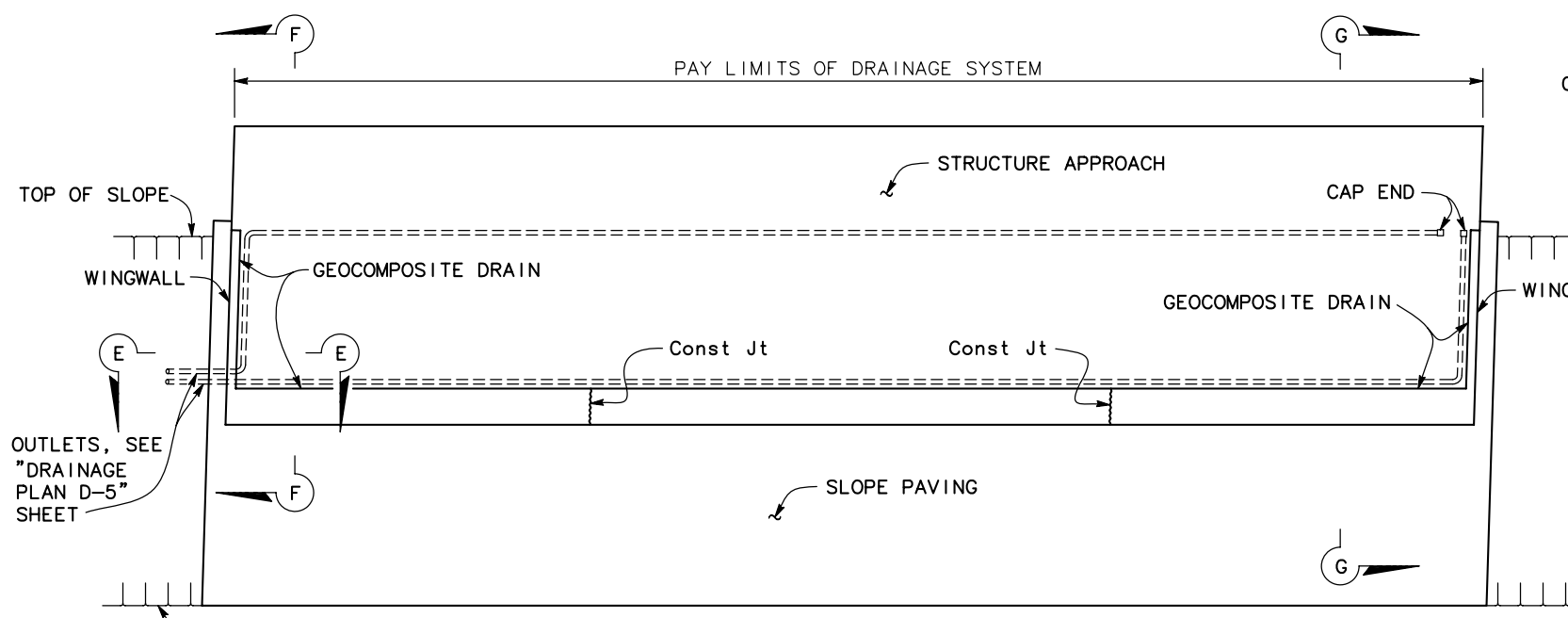
NOTE: (TYPE E-1 TO BE USED, UNLESS OTHERWISE SHOWN ON PLAN)
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

REVISED STANDARD DRAWING	① REVISED DETAIL ② DETAIL NOT USE	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	DIVISION OF ENGINEERING SERVICES	BRIDGE NO. 38-0163 POST MILE R22.56	ROUTE 219/99 SEPARATION (REPLACE) STRUCTURE APPROACH TYPE N(30S)
FILE NO. xs3-120 APPROVAL DATE Revised		ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES SHEET 30 OF 46

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	361	423

REGISTERED CIVIL ENGINEER
 K. Hamy
 5/6/12
 DATE
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
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NOTE: BENDS AND JUNCTIONS IN 3" PLASTIC PIPE ARE 30" RADIUS MINIMUM

REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

SPECIAL DETAILS

REVISED STANDARD DRAWING	1 REVISED DETAIL
FILE NO. xs3-110	2 DETAIL NOT USE
APPROVAL DATE Revised	

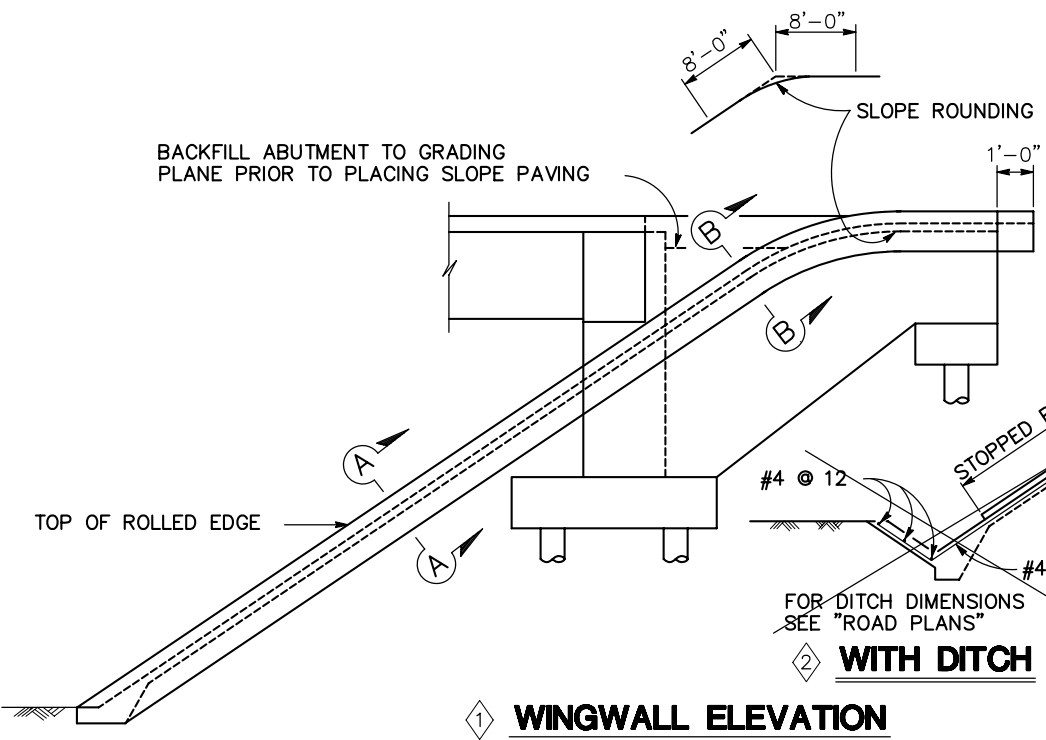
STATE OF CALIFORNIA	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)
DEPARTMENT OF TRANSPORTATION	POST MILE R22.56	STRUCTURE APPROACH DRAINAGE DETAILS

UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 31	OF 46
PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	10/21/11 12/27/12 4/9/12 5/6/12		

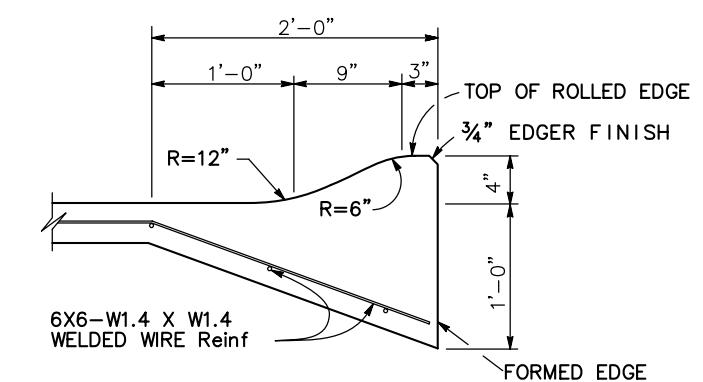
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	362	423

REGISTERED CIVIL ENGINEER
 K. Hamy
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

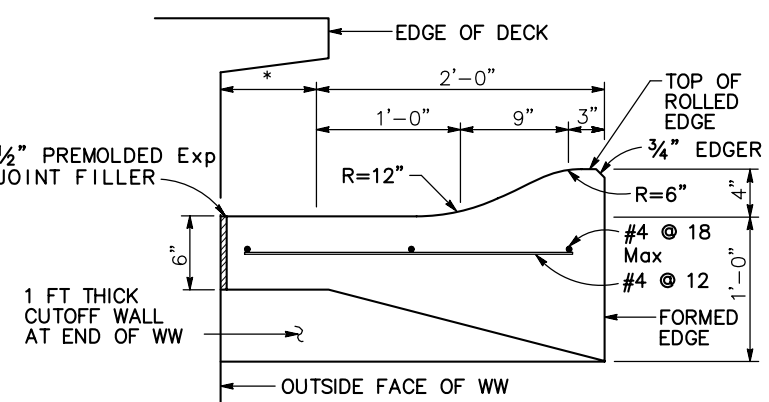
DEPARTMENT OF PUBLIC WORKS
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 CONSULTING ENGINEERS, INC.
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1 WINGWALL ELEVATION



SECTION A-A



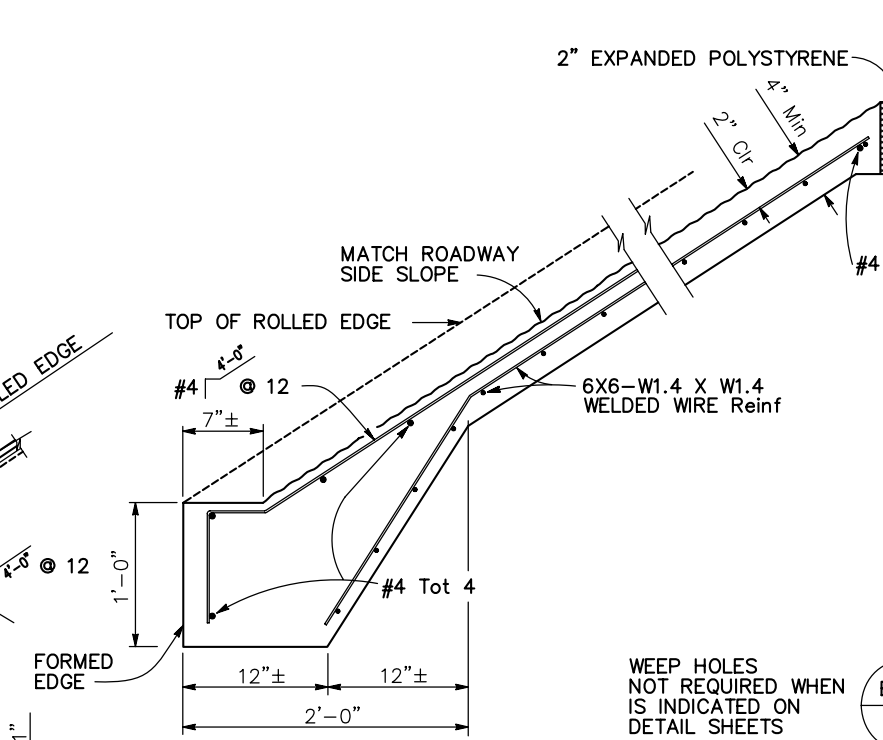
SECTION B-B

* THIS DIMENSION BECOMES ZERO WHEN EDGE OF DECK IS AT OUTSIDE FACE OF WW

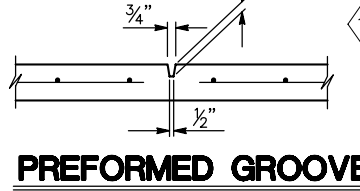
NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

REVISED STANDARD DRAWING	1 REVISED DETAIL	3 DETAIL ADDED
FILE NO. x84-210	2 DETAIL NOT USE	
APPROVAL DATE Revised		

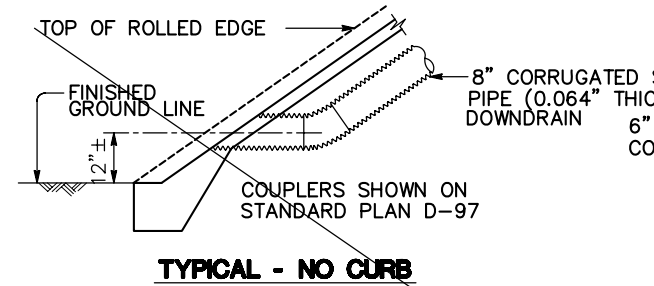
DS OSD 2147A (ENGLISH STANDARD DRAWING "XS" BORDER REV. (02-02-11))



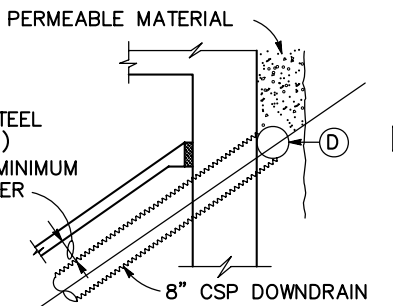
1 TYPICAL SECTION SLOPE PAVING (PATTERNED CONCRETE)



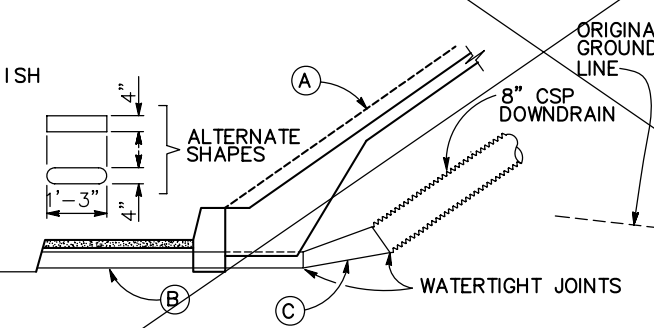
PREFORMED GROOVE



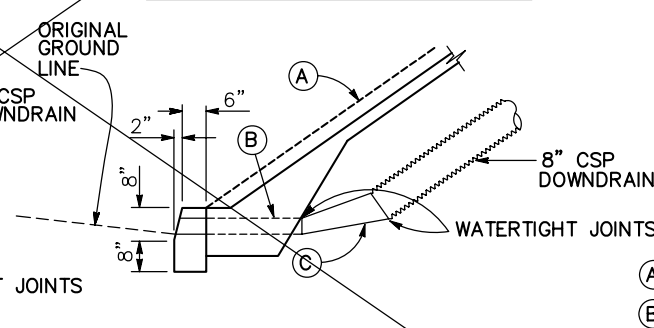
TYPICAL - NO CURB



TYPICAL - DRAIN CONNECTION



TYPICAL - WITH SIDEWALK

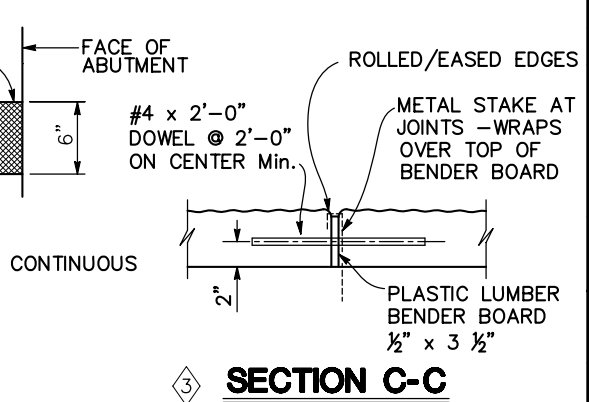


TYPICAL - WITH CURB

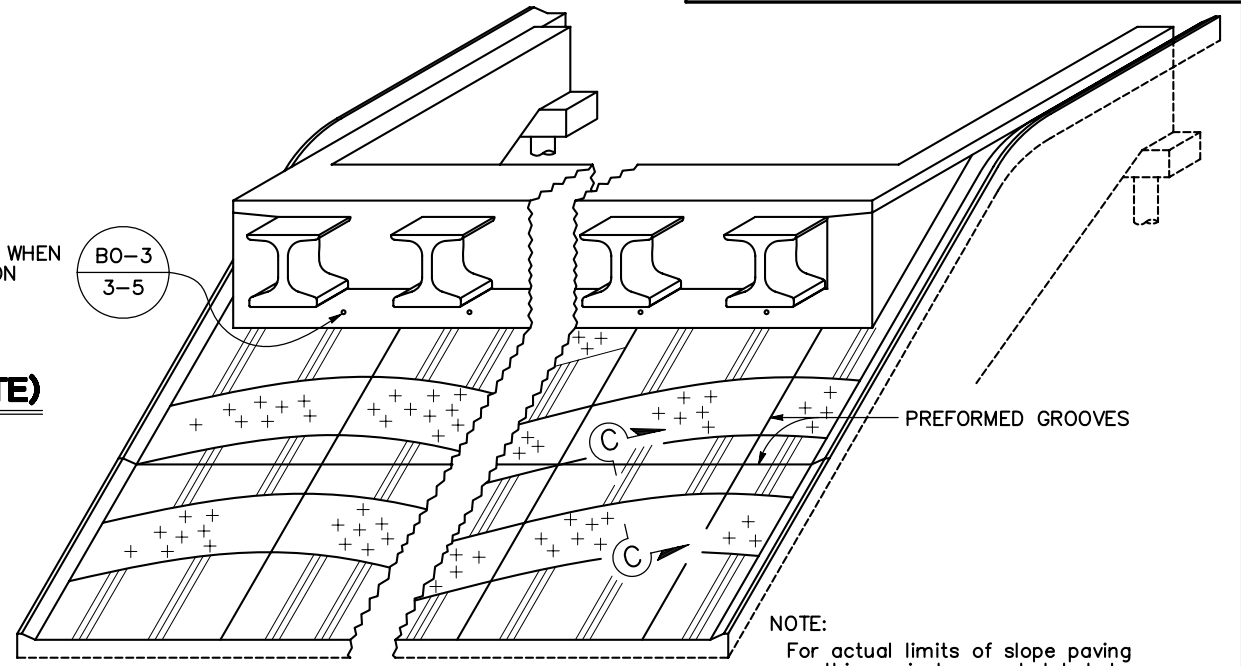
2 DRAINAGE DETAILS

NOTE: Drainage details are only applicable when is indicated on detail sheets.

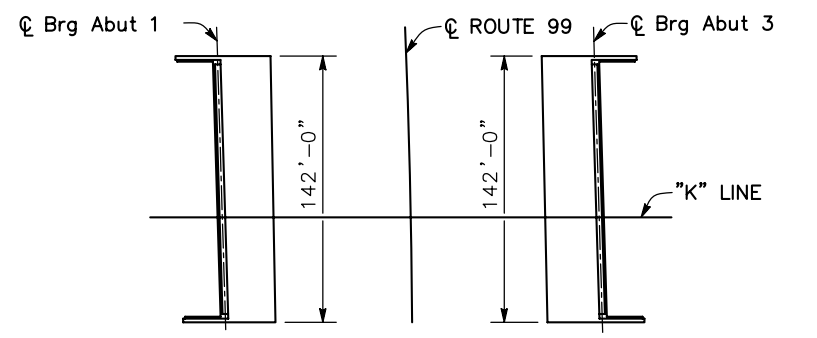
BO-3
3-5



3 SECTION C-C



1 PICTORIAL VIEW OF TYPICAL INSTALLATION



2 LIMITS OF SLOPE PAVING (PATTERNED CONCRETE)

- Note: Construct Slope Paving after all construction finished.
- (A) Top of rolled edge
 - (B) Conduit: 0.064" galv corrugated steel or 0.109" smooth galv steel
 - (C) Taper: { 0.064" galv corrugated steel or 0.109" smooth galv steel
 - (D) 8" perforated steel pipe (0.064" thick) underdrain behind abutment. Connect to downdrain as shown on limits of Slope Paving & Drainage layout.

NO SCALE
SPECIAL DETAILS

STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	DIVISION OF ENGINEERING SERVICES	BRIDGE NO. 38-0100 POST MILE R22.56	ROUTE 219/99 SEPARATION (REPLACE) SLOPE PAVING (PATTERNED CONCRETE) - FULL SLOPE
---	----------------------------------	--	---

UNIT: PROJECT NUMBER & PHASE: 1457 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES: 10/21/11, 12/27/12, 4/9/12, 5/6/12	SHEET 32 OF 46
---	-------------------------	---	--	----------------



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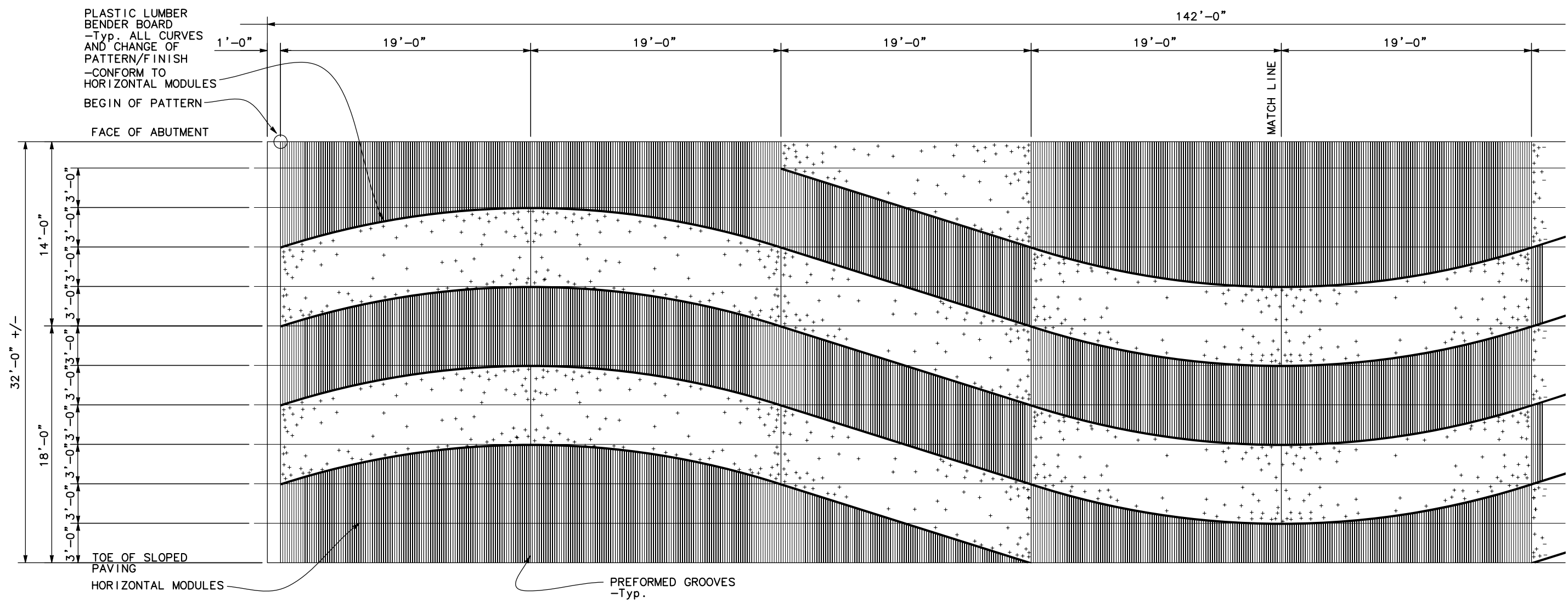
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	363	423

K. Hamy
 REGISTERED CIVIL ENGINEER
 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA
 REGISTERED PROFESSIONAL ENGINEER
 KANADUSH HARRISZ

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 SAN JOSE, CA 95126
 DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CA 95203

PATTERN LEGEND

- 
 Pattern A
 Vertical Scored Tooling
 over Rough Broom Finish
 -Scoring shall be 1" on
 center (1/2" Depth Max)
- 
 Pattern B
 Randomly Seeded Aggregate
 (40% of Surface Covered)
 with 1 1/2"-2" Aggregate



EAST ELEVATION **WEST ELEVATION - SIM**
 1/4" = 1'-0" WEST ELEVATION SHALL BE MIRRORED

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL
 CONTROLLING FIELD DIMENSIONS BEFORE
 ORDERING OR FABRICATING ANY MATERIAL



DESIGN OVERSIGHT	DESIGN BY C.D.B. FRANKEL	CHECKED A. WANG	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) SLOPE PAVING (PATTERNED CONCRETE) DETAILS No. 1
SIGN OFF DATE	DETAILS BY C.D.B. FRANKEL	CHECKED A. WANG		POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP		UNIT: 1457	
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS			PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES 10/21/11 2/24/12 4/9/12 5/6/12
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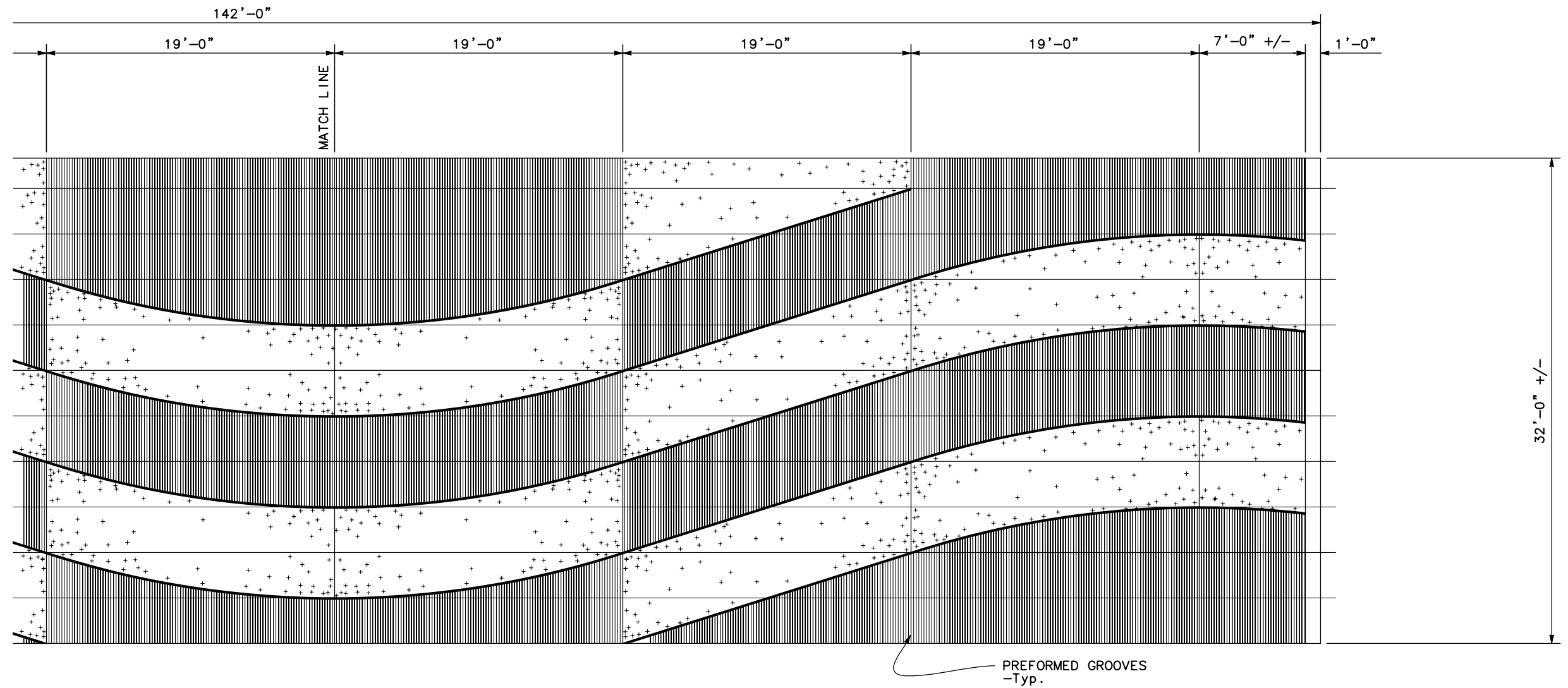
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	364	423

K. Hamy
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA
 REGISTERED PROFESSIONAL ENGINEER
 KANADUSH HARRISZ

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 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126
 DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CA 95203

PATTERN LEGEND

- 
 Pattern A
 Vertical Scored Tooling
 over Rough Broom Finish
 -Scoring shall be 1" on
 center (1/2" Depth Max)
- 
 Pattern B
 Randomly Seeded Aggregate
 (40% of Surface Covered)
 with 1 1/2"-2" Aggregate



EAST ELEVATION-CONTINUED
1/4" = 1'-0"

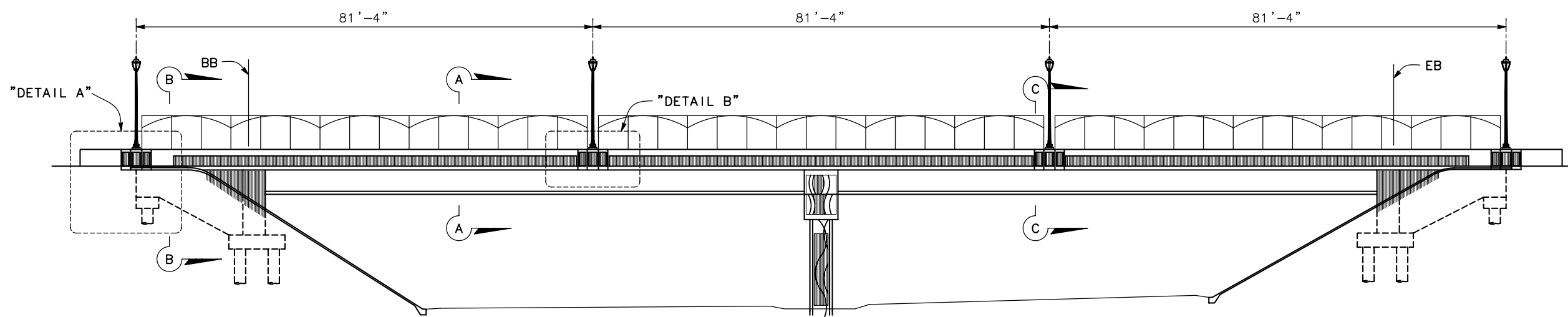
WEST ELEVATION - SIM
WEST ELEVATION SHALL BE MIRRORED

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ORDERING OR FABRICATING ANY MATERIAL

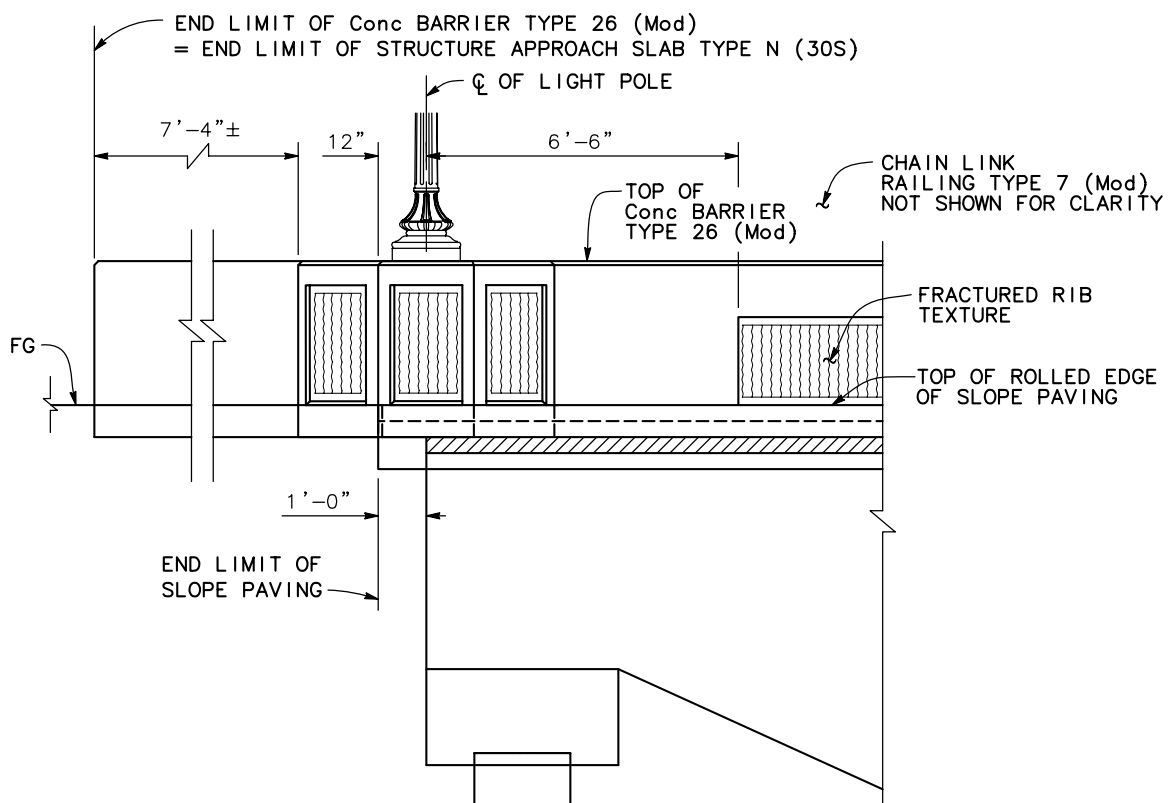
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SIGN OFF DATE	DETAILS BY C.D.B. FRANKEL	CHECKED A. WANG			POST MILES R22.56	
	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP				
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)			ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES REVISION DATES: 10/21/11, 2/24/12, 4/9/12, 5/6/12 SHEET 34 OF 46

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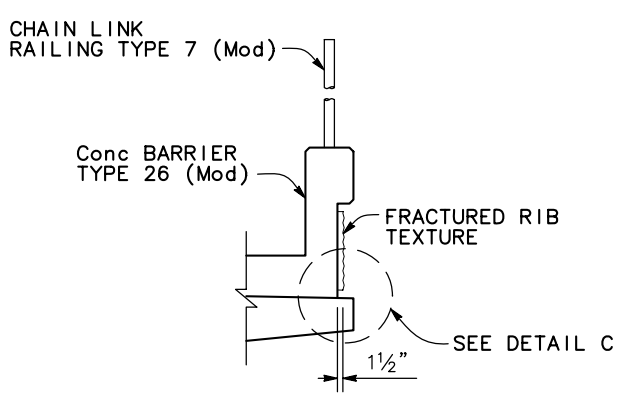
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	365	423
K. Wang			5/6/12	REGISTERED CIVIL ENGINEER DATE	
9/04/12			PLANS APPROVAL DATE		
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DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
RAJAPPAN & MEYER CONSULTING ENGINEERS, INC. 1038 LEIGH AVE., SUITE 100 SAN JOSE, CA 95126					



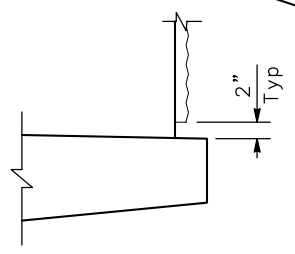
ELEVATION
NO SCALE



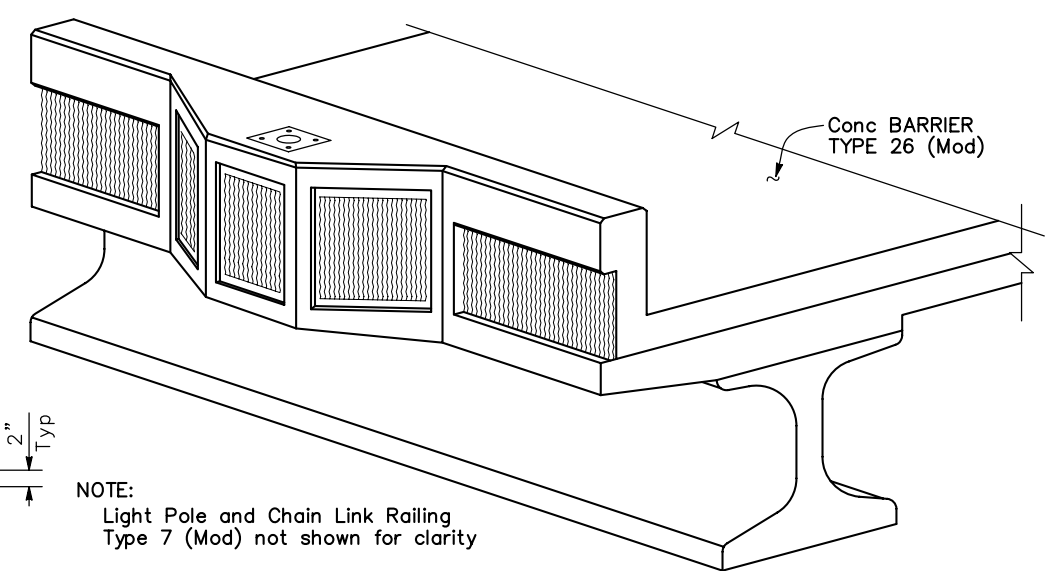
DETAIL A
1/2" = 1'-0"



SECTION A-A
1/2" = 1'-0"

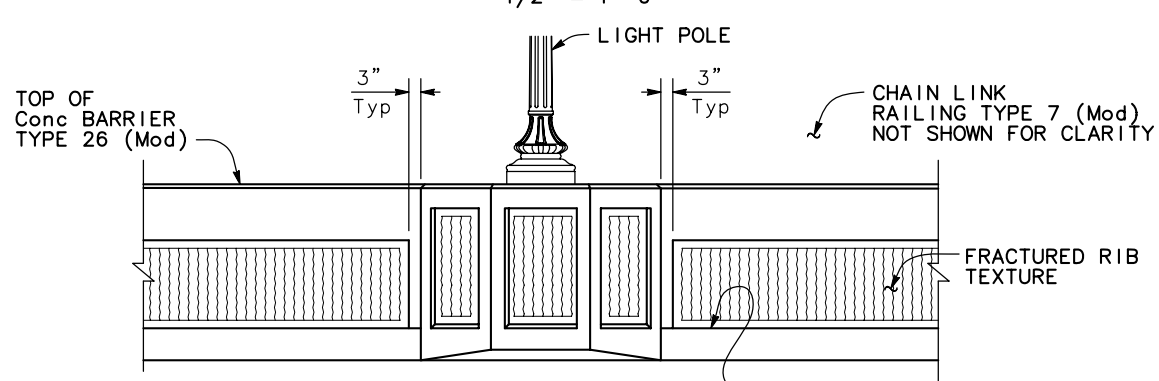


DETAIL C
1" = 1'-0"

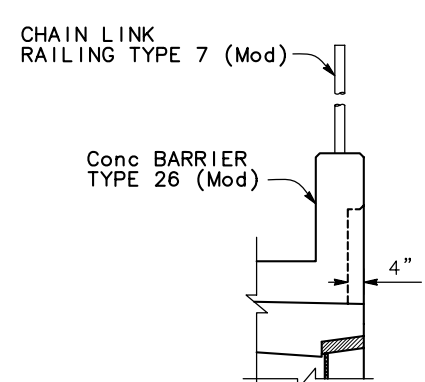


PICTORIAL VIEW
NO SCALE

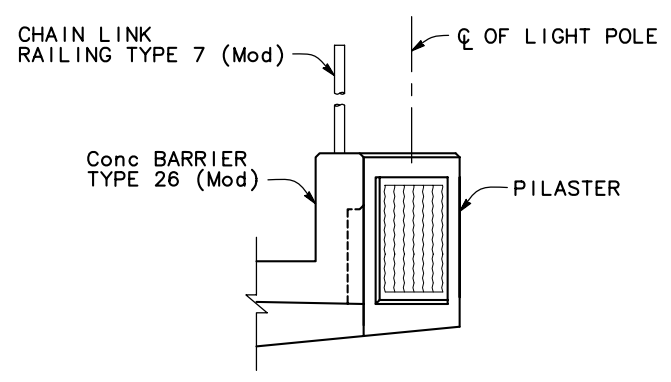
NOTE:
Light Pole and Chain Link Railing
Type 7 (Mod) not shown for clarity



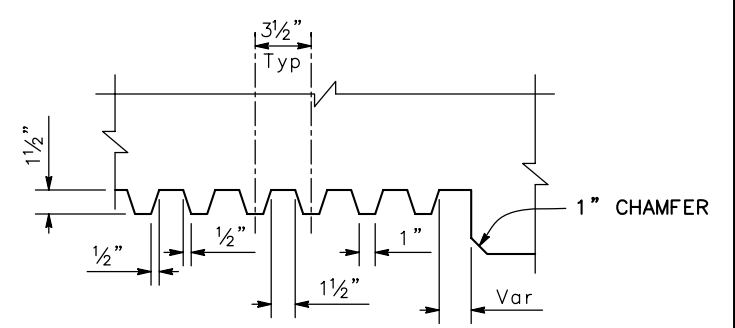
DETAIL B
1/2" = 1'-0"



SECTION B-B
1/2" = 1'-0"



SECTION C-C
1/2" = 1'-0"



FRACTURED RIB TEXTURE DETAIL
NO SCALE

NOTE:
THE CONTRACTOR SHALL VERIFY ALL
CONTROLLING FIELD DIMENSIONS BEFORE
ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT
SIGN OFF DATE

DESIGN	BY A. WANG	CHECKED C. SHENOY
DETAILS	BY S. DESALEGN	CHECKED K. HARISAZ
QUANTITIES	BY S. DESALEGN	CHECKED B. SWARUP

PREPARED FOR THE
STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

A. WANG
PROJECT ENGINEER

BRIDGE NO.	38-0163
POST MILES	R22.56

ROUTE 219/99 SEPARATION (REPLACE)
CONCRETE BARRIER TYPE 26 (MOD) DETAILS No. 1

DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)

ORIGINAL SCALE IN INCHES
FOR REDUCED PLANS

0 1 2 3

UNIT: 1457
PROJECT NUMBER & PHASE: 1000000100

CONTRACT NO.: 10-0L3304

DISREGARD PRINTS BEARING
EARLIER REVISION DATES

REVISION DATES	SHEET	OF
10/21/11 12/24/12 4/9/12 5/6/12	35	46

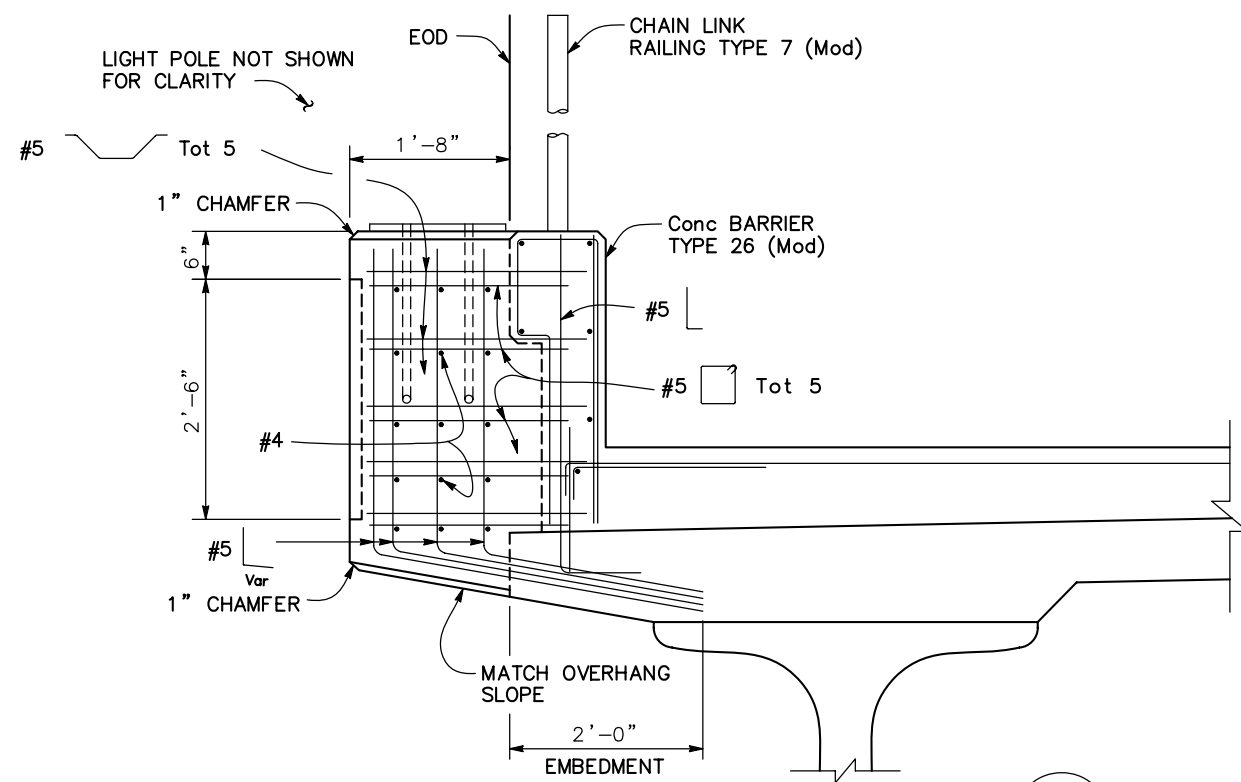
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	366	423

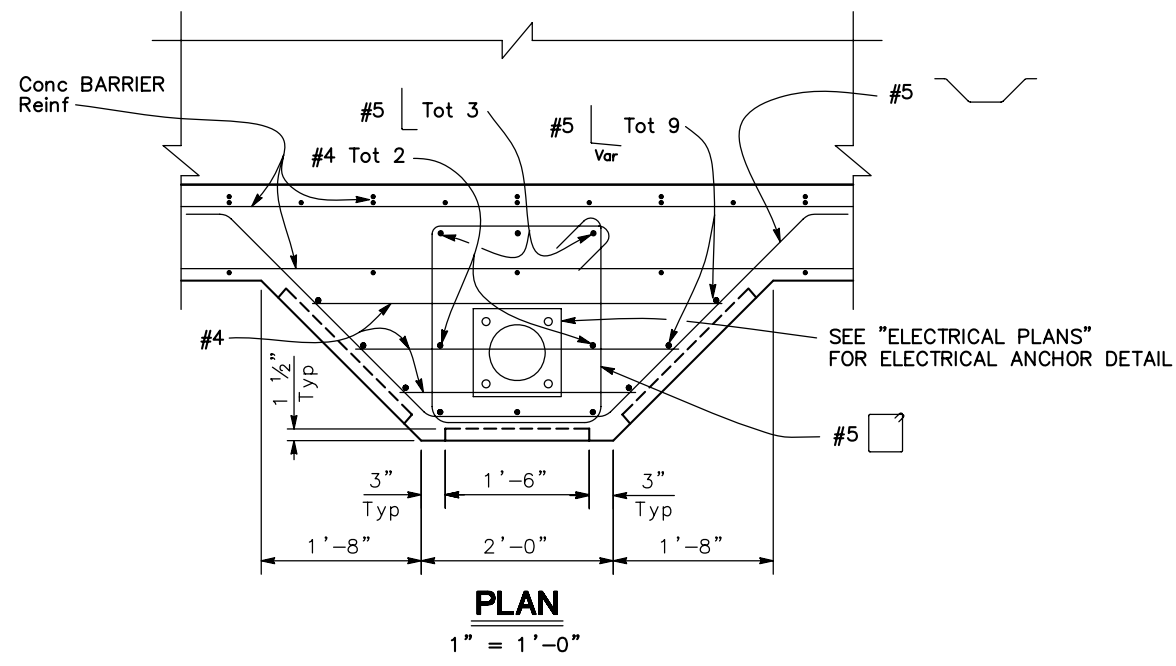
K. Hanj
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12
 PLANS APPROVAL DATE
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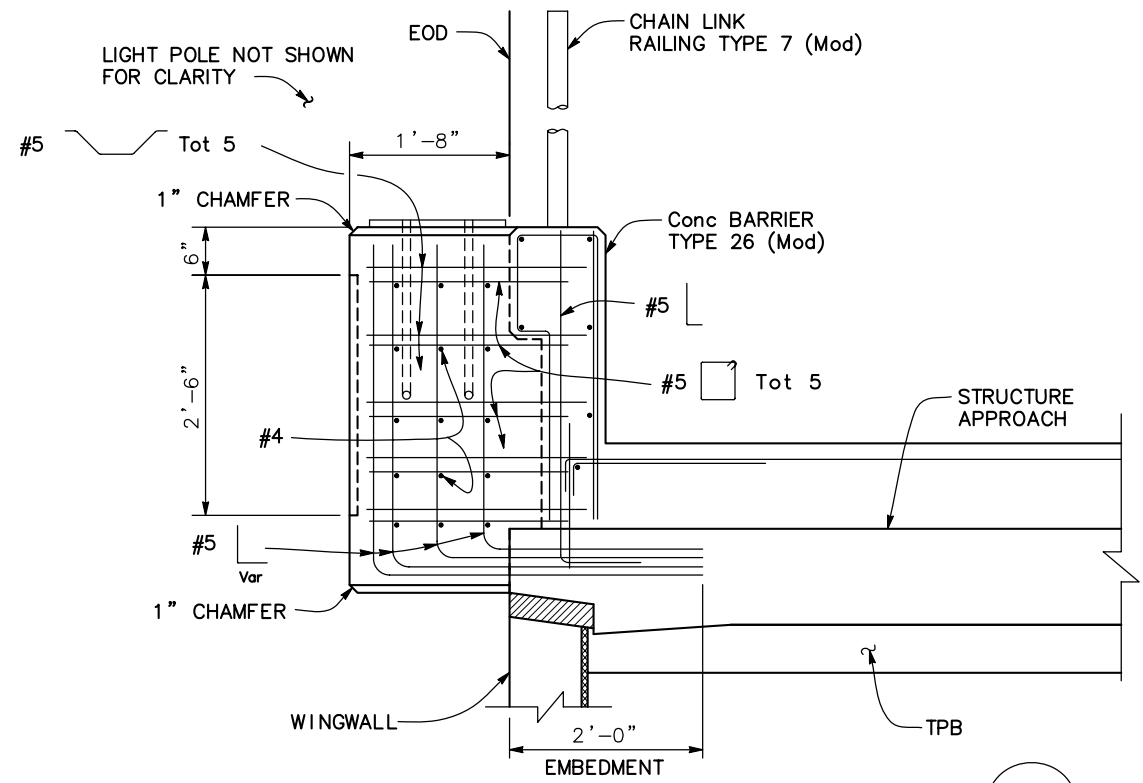
DEPARTMENT OF PUBLIC WORKS
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 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



NOTE: For information not shown, see Std Plans **B11-54**
SIDE VIEW OF PILASTER AT MID-SPAN
 1" = 1'-0"



PLAN
 1" = 1'-0"



Note: Pilaster at end of wingwall shown
 For information not shown, see Std Plans **B11-54**
SIDE VIEW OF PILASTER AT END OF WINGWALL
 1" = 1'-0"

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

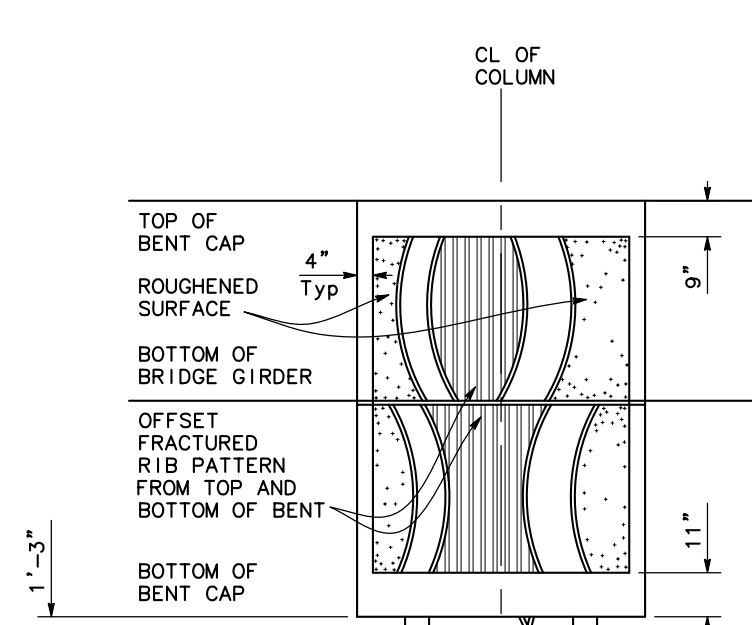
DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE)
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457	REVISION DATES	CONCRETE BARRIER TYPE 26 (MOD) DETAILS No. 2
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TIME PLOTTED => \$TIME USERNAME => \$USER

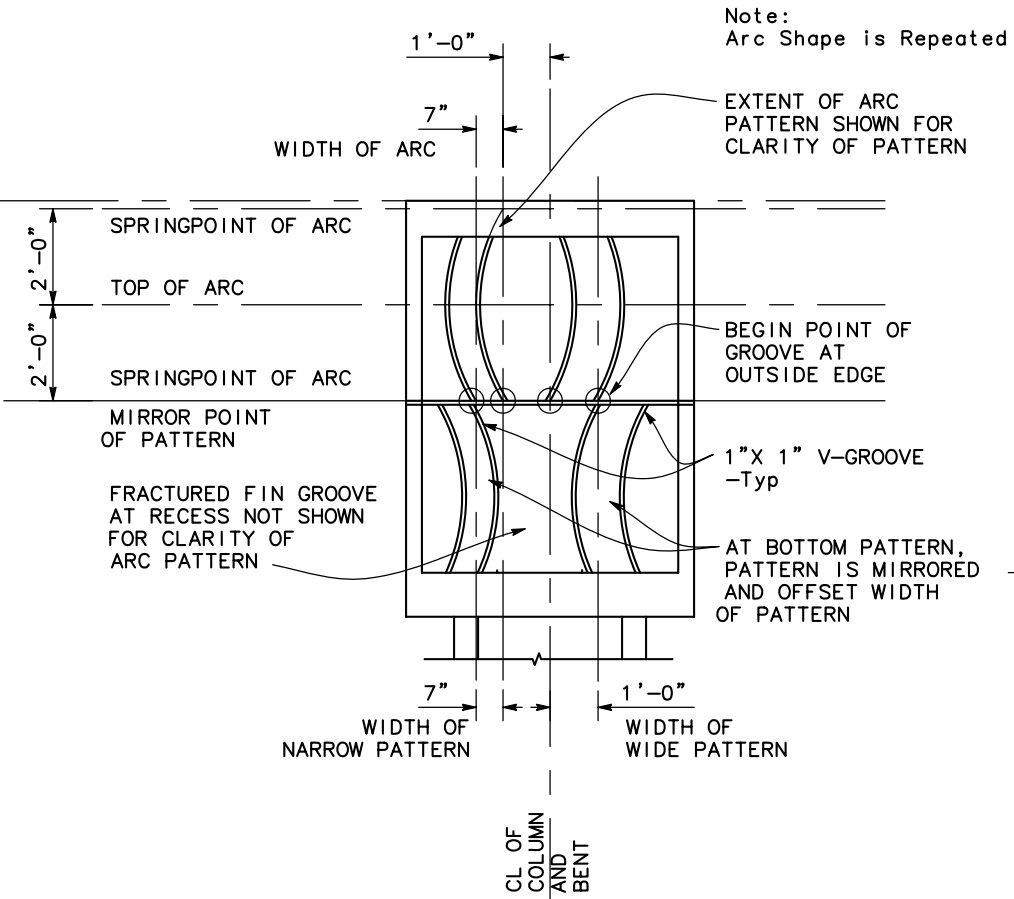
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	367	423

REGISTERED CIVIL ENGINEER
 DATE 5/6/12
 PLANS APPROVAL DATE 9/04/12
 No. C39591
 Exp. 12/31/13
 CIVIL
 STATE OF CALIFORNIA

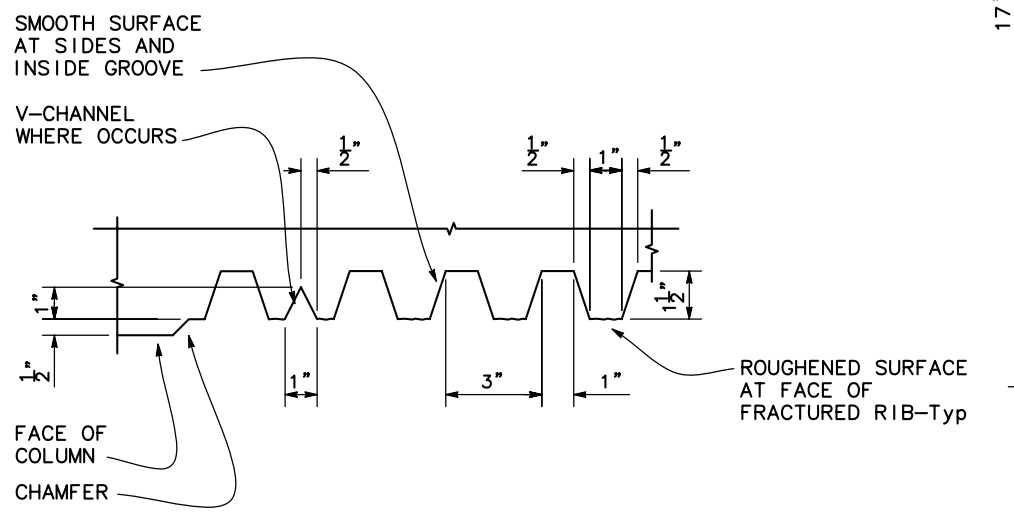
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126
DERIVI CONSTRUCTION & ARCHITECTURE, INC.
 924 N. YOSEMITE STREET
 STOCKTON, CA 95203



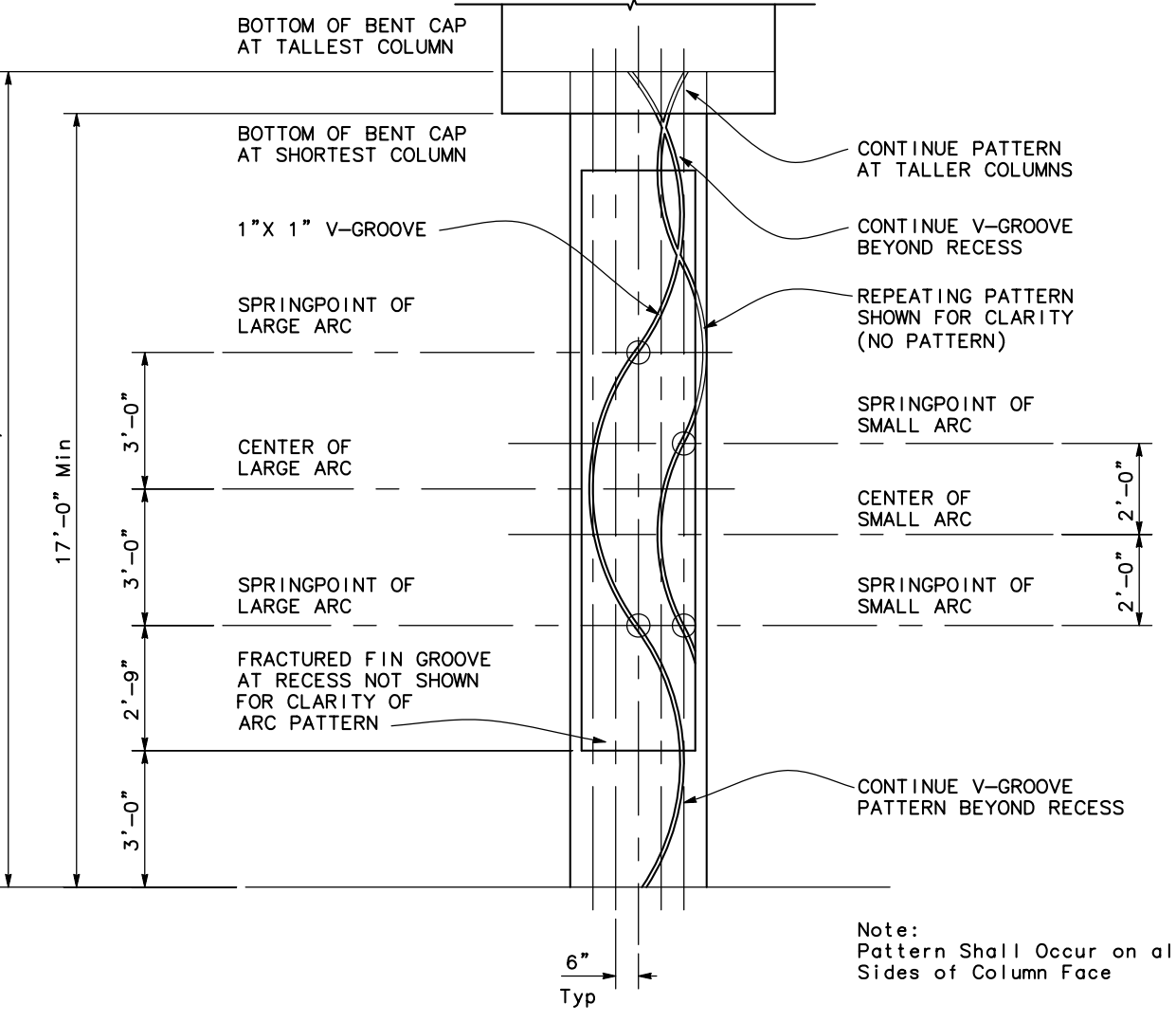
COLUMN ELEVATION
1/2" = 1'-0"



BENT PATTERN
1/2" = 1'-0"



VIEW A-A (FRACTURED RIB PATTERN)
NO SCALE



COLUMN PATTERN
1/2" = 1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	
SIGN OFF DATE	

DESIGN	BY C.D.B. FRANKEL	CHECKED A. WANG
DETAILS	BY C.D.B. FRANKEL	CHECKED A. WANG
QUANTITIES	BY S. DESALEGN	CHECKED B. SWARUP

PREPARED FOR THE
STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION

A. WANG PROJECT ENGINEER	BRIDGE NO. 38-0163
	POST MILES R22.56

ROUTE 219/99 SEPARATION (REPLACE)
ARCHITECTURAL DETAILS

DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS



UNIT: 1457
 PROJECT NUMBER & PHASE: 1000000100

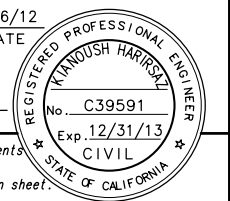
CONTRACT NO.: 10-0L3304

DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 37	OF 46
	10/21/11, 2/27/12, 4/9/12, 5/6/12		

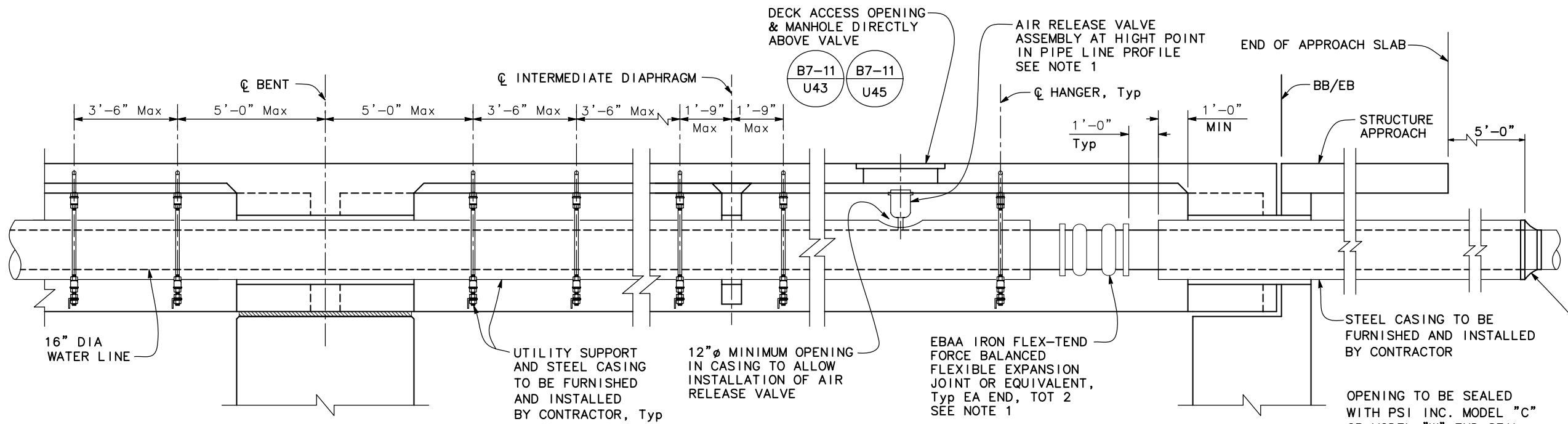
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	369	423

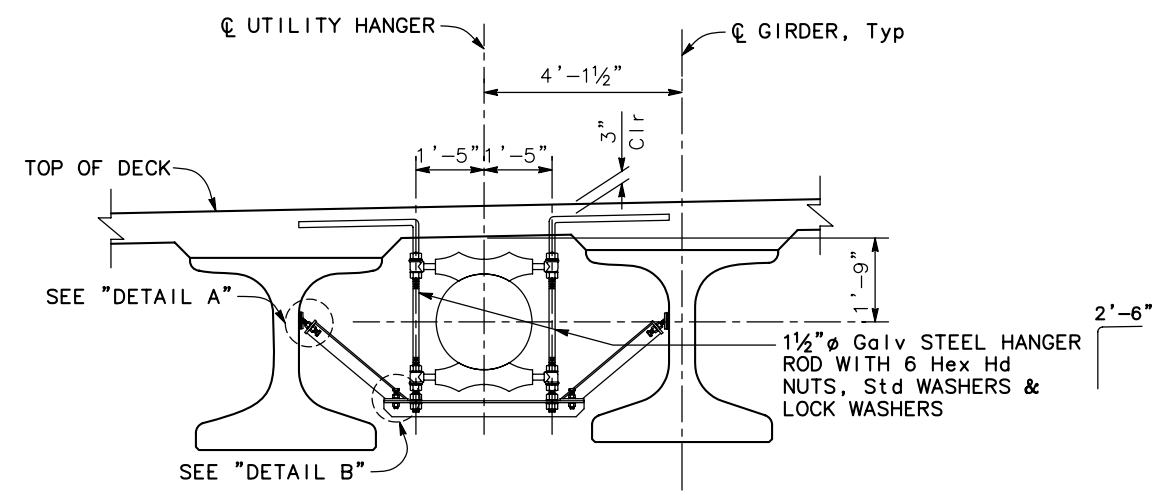
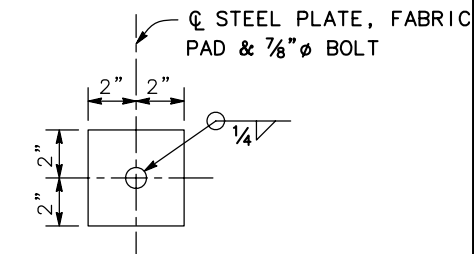
K. Haruy
 REGISTERED CIVIL ENGINEER 5/6/12 DATE
 9/04/12 PLANS APPROVAL DATE
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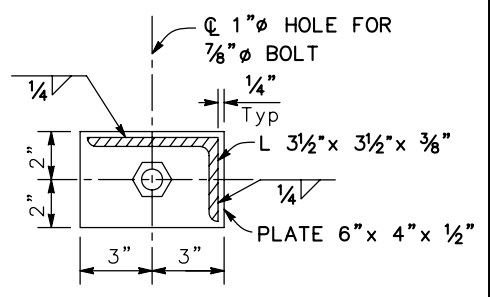
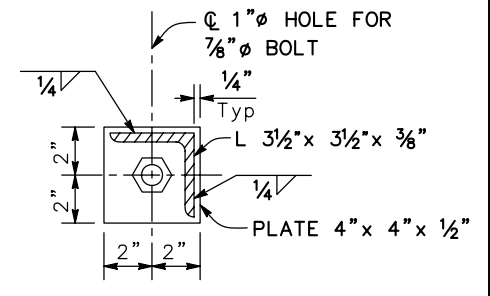
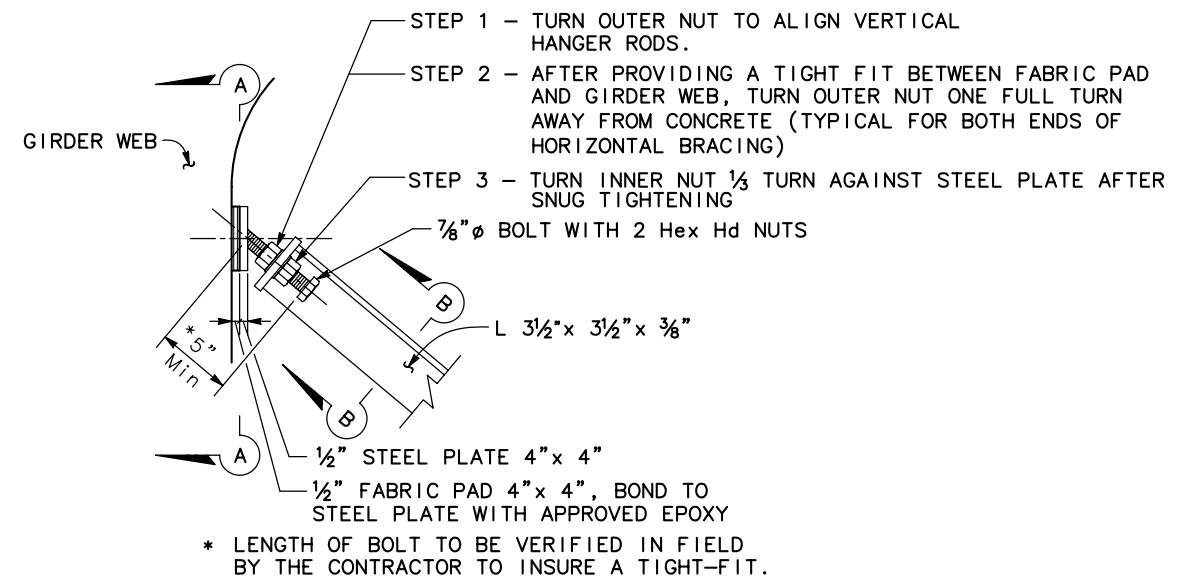
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
RAJAPPAN & MEYER
 CONSULTING ENGINEERS, INC.
 1038 LEIGH AVE., SUITE 100
 SAN JOSE, CA 95126



WATER LINE INSTALLATION DETAIL
1/2" = 1'-0"



HANGER UTILITY SUPPORT
1/2" = 1'-0"

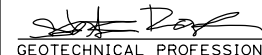


NOTE:
 1. Expansion Joint, Air Release Valve Assembly, Opening Seal and Water Line to be furnished and installed by contractor. See "ROAD PLANS" for limits of Water Line and quantity for the above items.

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL








DESIGN OVERSIGHT	DESIGN BY A. WANG	CHECKED C. SHENOY	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163	ROUTE 219/99 SEPARATION (REPLACE) UTILITY DETAILS No. 2
SIGN OFF DATE	DETAILS BY S. DESALEGN	CHECKED K. HARIRSAZ	A. WANG PROJECT ENGINEER	POST MILES R22.56	
DESIGN DETAIL SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. DESALEGN	CHECKED B. SWARUP	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 39 OF 46

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0 0.0/0.5	370	423


 GEOTECHNICAL PROFESSIONAL DATE 4/20/12
 PLANS APPROVAL DATE 9/04/12
 No. 2915
 Exp. 6/30/12
 REGISTERED PROFESSIONAL ENGINEER
 STATE OF CALIFORNIA

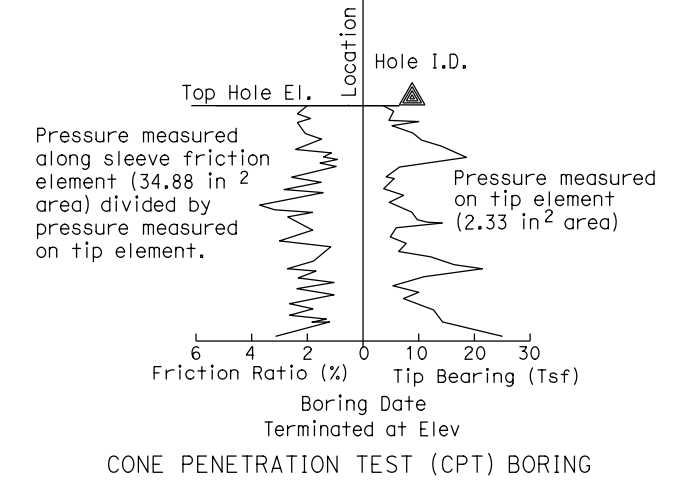
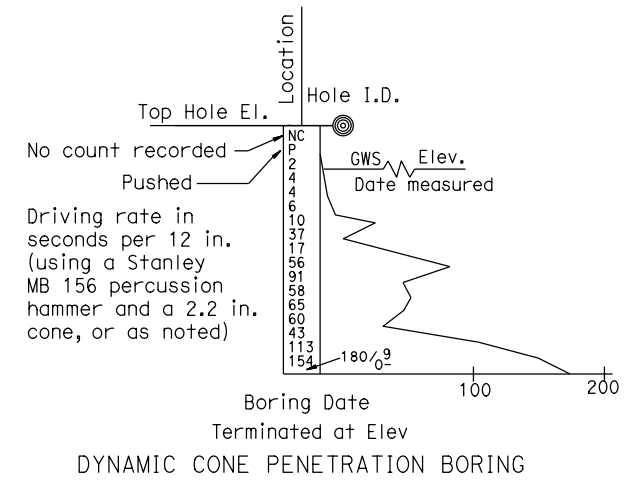
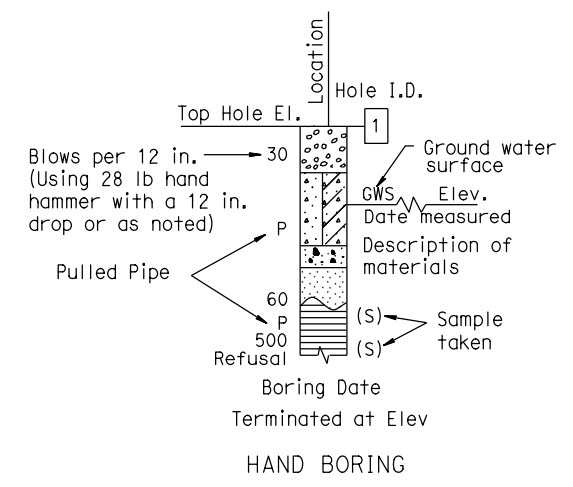
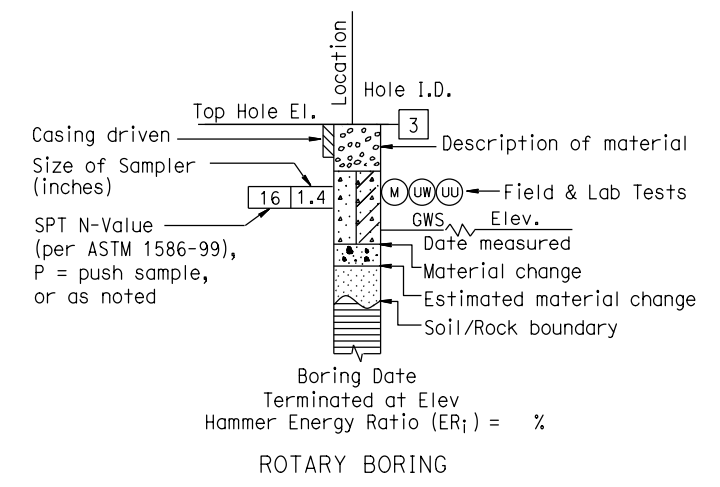
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 GEOCON CONSULTANTS, INC.
 6671 BRISA STREET
 LIVERMORE, CA 94550

CEMENTATION	
Description	Criteria
Weak	Crumbles or breaks with handling or little finger pressure.
Moderate	Crumbles or breaks with considerable finger pressure.
Strong	Will not crumble or break with finger pressure.

BOREHOLE IDENTIFICATION		
Symbol	Hole Type	Description
	A	Auger Boring (hollow or solid stem bucket)
	R	Rotary drilled boring (conventional)
	RW	Rotary drilled with self-casing wire-line
	RC	Rotary core with continuously-sampled, self-casing wire-line
	P	Rotary percussion boring (air)
	R	Rotary drilled diamond core
	HD	Hand driven (1-inch soil tube)
	HA	Hand Auger
	D	Dynamic Cone Penetration Boring
	CPT	Cone Penetration Test (ASTM D 5778)
	O	Other (note on LOTB)

Note: Size in inches.

CONSISTENCY OF COHESIVE SOILS				
Description	Shear Strength (tsf)	Pocket Penetrometer Measurement, PP, (tsf)	Torvane Measurement, TV, (tsf)	Vane Shear Measurement, VS, (tsf)
Very Soft	Less than 0.12	Less than 0.25	Less than 0.12	Less than 0.12
Soft	0.12 - 0.25	0.25 - 0.5	0.12 - 0.25	0.12 - 0.25
Medium Stiff	0.25 - 0.5	0.5 - 1	0.25 - 0.5	0.25 - 0.5
Stiff	0.5 - 1	1 - 2	0.5 - 1	0.5 - 1
Very Stiff	1 - 2	2 - 4	1 - 2	1 - 2
Hard	Greater than 2	Greater than 4	Greater than 2	Greater than 2



4/20/12
DATE

9/04/12
PLANS APPROVAL DATE

REGISTRED PROFESSIONAL ENGINEER
No. 2915
Exp. 6/30/12
STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

GEOCON CONSULTANTS, INC.
6671 BRISA STREET
LIVERMORE, CA 94550

GROUP SYMBOLS AND NAMES			
Graphic/Symbol	Group Names	Graphic/Symbol	Group Names
[Symbol]	Well-graded GRAVEL	[Symbol]	CL Lean CLAY Lean CLAY with SAND Lean CLAY with GRAVEL SANDY lean CLAY SANDY lean CLAY with GRAVEL GRAVELLY lean CLAY GRAVELLY lean CLAY with SAND
	Well-graded GRAVEL with SAND		
[Symbol]	Poorly-graded GRAVEL	[Symbol]	CL-ML SILTY CLAY SILTY CLAY with SAND SILTY CLAY with GRAVEL SANDY SILTY CLAY SANDY SILTY CLAY with GRAVEL GRAVELLY SILTY CLAY GRAVELLY SILTY CLAY with SAND
	Poorly-graded GRAVEL with SAND		
[Symbol]	Well-graded GRAVEL with SILT	[Symbol]	ML SILT SILT with SAND SILT with GRAVEL SANDY SILT SANDY SILT with GRAVEL GRAVELLY SILT GRAVELLY SILT with SAND
	Well-graded GRAVEL with SILT and SAND		
[Symbol]	Well-graded GRAVEL with CLAY (or SILTY CLAY)	[Symbol]	OL ORGANIC lean CLAY ORGANIC lean CLAY with SAND ORGANIC lean CLAY with GRAVEL SANDY ORGANIC lean CLAY SANDY ORGANIC lean CLAY with GRAVEL GRAVELLY ORGANIC lean CLAY GRAVELLY ORGANIC lean CLAY with SAND
	Well-graded GRAVEL with CLAY and SAND (or SILTY CLAY and SAND)		
[Symbol]	Poorly-graded GRAVEL with SILT	[Symbol]	OL ORGANIC SILT ORGANIC SILT with SAND ORGANIC SILT with GRAVEL SANDY ORGANIC SILT SANDY ORGANIC SILT with GRAVEL GRAVELLY ORGANIC SILT GRAVELLY ORGANIC SILT with SAND
	Poorly-graded GRAVEL with SILT and SAND		
[Symbol]	Poorly-graded GRAVEL with CLAY (or SILTY CLAY)	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	Poorly-graded GRAVEL with CLAY and SAND (or SILTY CLAY and SAND)		
[Symbol]	SILTY GRAVEL	[Symbol]	OH ORGANIC elastic SILT ORGANIC elastic SILT with SAND ORGANIC elastic SILT with GRAVEL SANDY ORGANIC elastic SILT SANDY ORGANIC elastic SILT with GRAVEL GRAVELLY ORGANIC elastic SILT GRAVELLY ORGANIC elastic SILT with SAND
	SILTY GRAVEL with SAND		
[Symbol]	CLAYEY GRAVEL	[Symbol]	OH ORGANIC elastic SILT ORGANIC elastic SILT with SAND ORGANIC elastic SILT with GRAVEL SANDY ORGANIC elastic SILT SANDY ORGANIC elastic SILT with GRAVEL GRAVELLY ORGANIC elastic SILT GRAVELLY ORGANIC elastic SILT with SAND
	CLAYEY GRAVEL with SAND		
[Symbol]	SILTY, CLAYEY GRAVEL	[Symbol]	OH ORGANIC elastic SILT ORGANIC elastic SILT with SAND ORGANIC elastic SILT with GRAVEL SANDY ORGANIC elastic SILT SANDY ORGANIC elastic SILT with GRAVEL GRAVELLY ORGANIC elastic SILT GRAVELLY ORGANIC elastic SILT with SAND
	SILTY, CLAYEY GRAVEL with SAND		
[Symbol]	Well-graded SAND	[Symbol]	CH Fat CLAY Fat CLAY with SAND Fat CLAY with GRAVEL SANDY fat CLAY SANDY fat CLAY with GRAVEL GRAVELLY fat CLAY GRAVELLY fat CLAY with SAND
	Well-graded SAND with GRAVEL		
[Symbol]	Poorly-graded SAND	[Symbol]	MH Elastic SILT Elastic SILT with SAND Elastic SILT with GRAVEL SANDY elastic SILT SANDY elastic SILT with GRAVEL GRAVELLY elastic SILT GRAVELLY elastic SILT with SAND
	Poorly-graded SAND with GRAVEL		
[Symbol]	Well-graded SAND with SILT	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	Well-graded SAND with SILT and GRAVEL		
[Symbol]	Well-graded SAND with CLAY (or SILTY CLAY)	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	Well-graded SAND with CLAY and GRAVEL (or SILTY CLAY and GRAVEL)		
[Symbol]	Poorly-graded SAND with SILT	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	Poorly-graded SAND with SILT and GRAVEL		
[Symbol]	Poorly-graded SAND with CLAY (or SILTY CLAY)	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	Poorly-graded SAND with CLAY and GRAVEL (or SILTY CLAY and GRAVEL)		
[Symbol]	SILTY SAND	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	SILTY SAND with GRAVEL		
[Symbol]	CLAYEY SAND	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	CLAYEY SAND with GRAVEL		
[Symbol]	SILTY, CLAYEY SAND	[Symbol]	OH ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	SILTY, CLAYEY SAND with GRAVEL		
[Symbol]	PEAT	[Symbol]	OL/OH ORGANIC SOIL ORGANIC SOIL with SAND ORGANIC SOIL with GRAVEL SANDY ORGANIC SOIL SANDY ORGANIC SOIL with GRAVEL GRAVELLY ORGANIC SOIL GRAVELLY ORGANIC SOIL with SAND
	COBBLES COBBLES and BOULDERS BOULDERS		



FIELD AND LABORATORY TESTING	
(C)	Consolidation (ASTM D 2435)
(CL)	Collapse Potential (ASTM D 5333)
(CP)	Compaction Curve (CTM 216)
(CR)	Corrosivity Testing (CTM 643, CTM 422, CTM 417)
(CU)	Consolidated Undrained Triaxial (ASTM D 4767)
(DS)	Direct Shear (ASTM D 3080)
(EI)	Expansion Index (ASTM D 4829)
(M)	Moisture Content (ASTM D 2216)
(OC)	Organic Content-% (ASTM D 2974)
(P)	Permeability (CTM 220)
(PA)	Particle Size Analysis (ASTM D 422)
(PI)	Plasticity Index (AASHTO T 90) Liquid Limit (AASHTO T 89)
(PL)	Point Load Index (ASTM D 5731)
(PM)	Pressure Meter
(R)	R-Value (CTM 301)
(SE)	Sand Equivalent (CTM 217)
(SG)	Specific Gravity (AASHTO T 100)
(SL)	Shrinkage Limit (ASTM D 427)
(SW)	Swell Potential (ASTM D 4546)
(UC)	Unconfined Compression-Soil (ASTM D 2166) Unconfined Compression-Rock (ASTM D 2938)
(UU)	Unconsolidated Undrained Triaxial (ASTM D 2850)
(UW)	Unit Weight (ASTM D 4767)

APPARENT DENSITY OF COHESIONLESS SOILS	
Description	SPT N ₆₀ (Blows / 12 in.)
Very Loose	0 - 5
Loose	5 - 10
Medium Dense	10 - 30
Dense	30 - 50
Very Dense	Greater than 50

MOISTURE	
Description	Criteria
Dry	No discernable moisture
Moist	Moisture present, but no free water
Wet	Visible free water

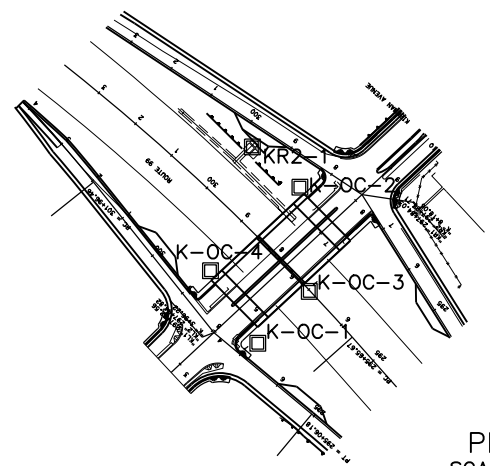
PERCENT OR PROPORTION OF SOILS	
Description	Criteria
Trace	Particles are present but estimated to be less than 5%
Few	5% - 10%
Little	15% - 25%
Some	30% - 45%
Mostly	50% - 100%

PARTICLE SIZE		
Description	Size (in.)	
Boulder	Greater than 12	
Cobble	3 - 12	
Gravel	Coarse	3/4 - 3
	Fine	1/5 - 3/4
Sand	Coarse	1/16 - 1/5
	Medium	1/64 - 1/16
	Fine	1/300 - 1/64
Silt and Clay	Less than 1/300	

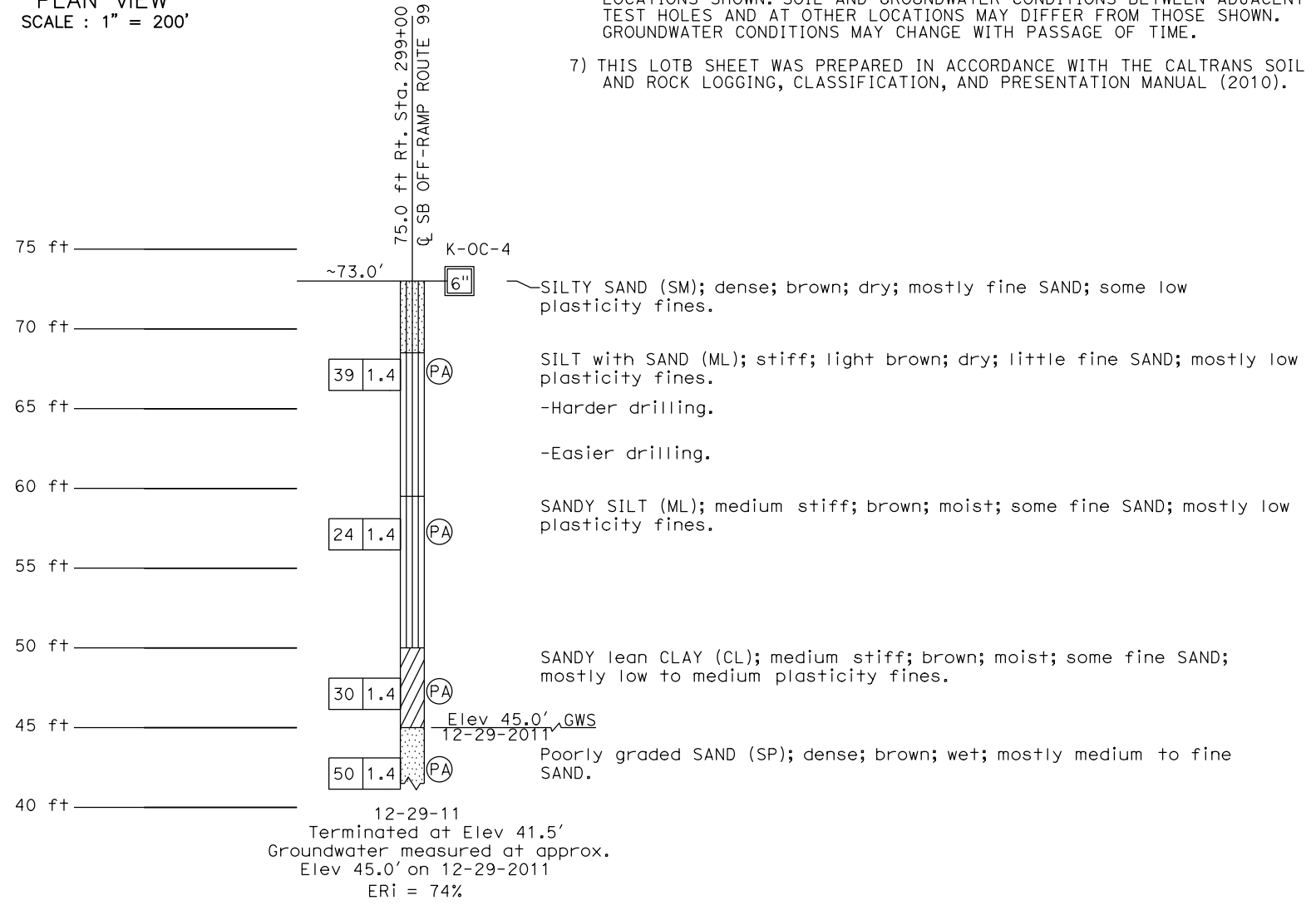
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0 0.0/0.5	372	423
 GEOTECHNICAL PROFESSIONAL			4/20/12	DATE	
9/04/12 PLANS APPROVAL DATE					
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DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
GEOCON CONSULTANTS, INC. 6671 BRISA STREET LIVERMORE, CA 94550					

NOTES:

- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
- 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
- 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
- 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
- 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
- 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



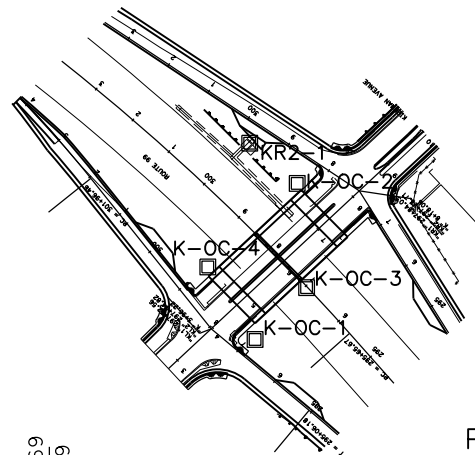
PLAN VIEW
SCALE : 1" = 200'



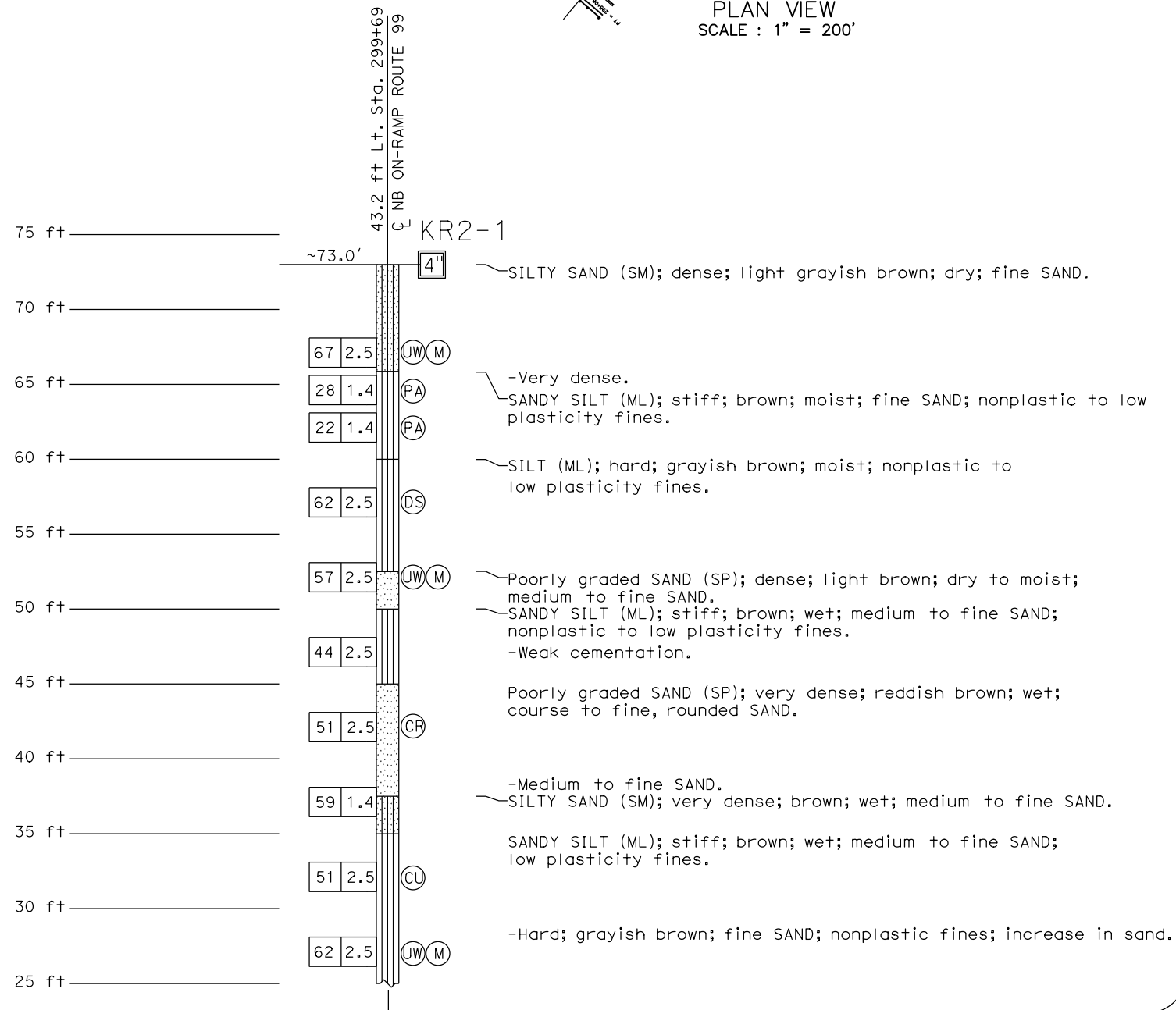
PROFILE VIEW
VERT. SCALE : 1" = 5'

X DESIGN OVERSIGHT X SIGN OFF DATE	DRAWN BY	R. AGUILAR	B. von DESSONNECK	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE No.	38-0163	ROUTE 219/99 SEPARATION (REPLACE) LOG OF TEST BORINGS 3 OF 7	
	CHECKED BY	S. RODACKER	DATE: 8/22/2011-8/31/2011, 12/29/2011		PROJECT ENGINEER	POST MILES		R22.56
GS LOTB SOIL LEGEND SHEET 1 (ENGLISH) (REV. 07/16/10)				ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES 8/28/11 2/13/12	SHEET OF 42 46

DATE PLOTTED => 01-MAY-2012 USERNAME => a116r



PLAN VIEW
SCALE : 1" = 200'

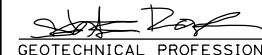


PROFILE VIEW
VERT. SCALE : 1" = 5'

NOTES:

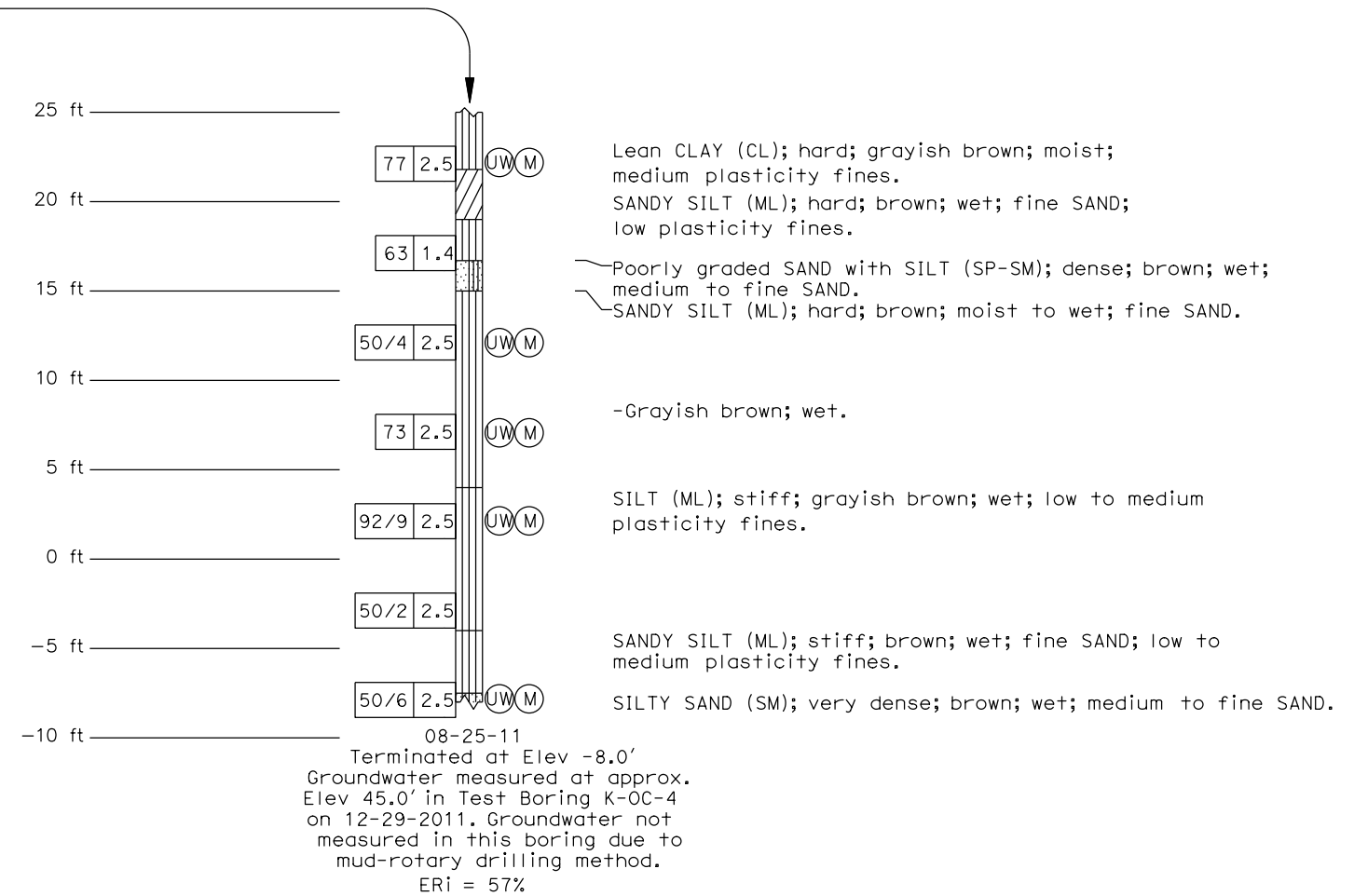
- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
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- 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
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- 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
- 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).

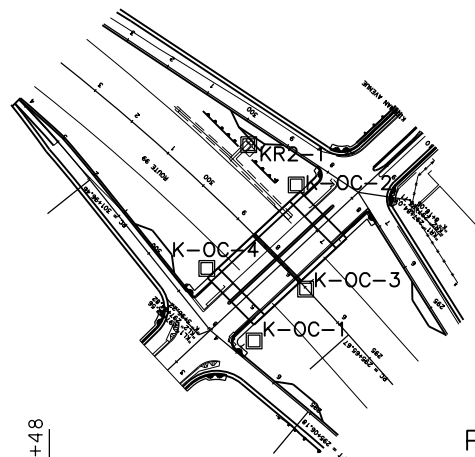
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0 0.0/0.5	373	423


 GEOTECHNICAL PROFESSIONAL DATE 4/20/12
 PLANS APPROVAL DATE 9/04/12
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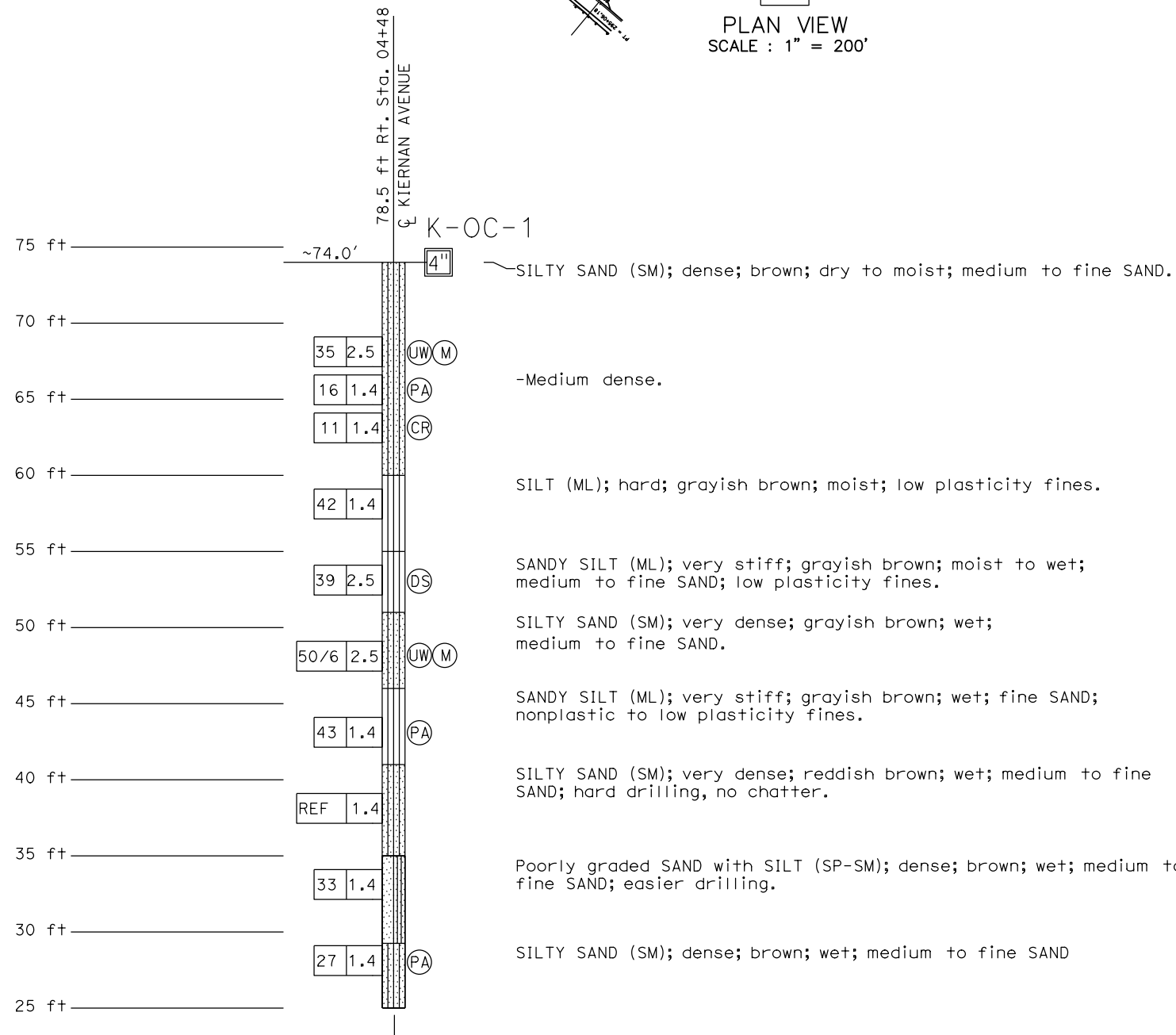
REGISTERED PROFESSIONAL ENGINEER
 STATE OF CALIFORNIA
 No. 2915
 Exp. 6/30/12
 GEOTECHNICAL

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 GEOCON CONSULTANTS, INC.
 6671 BRISA STREET
 LIVERMORE, CA 94550



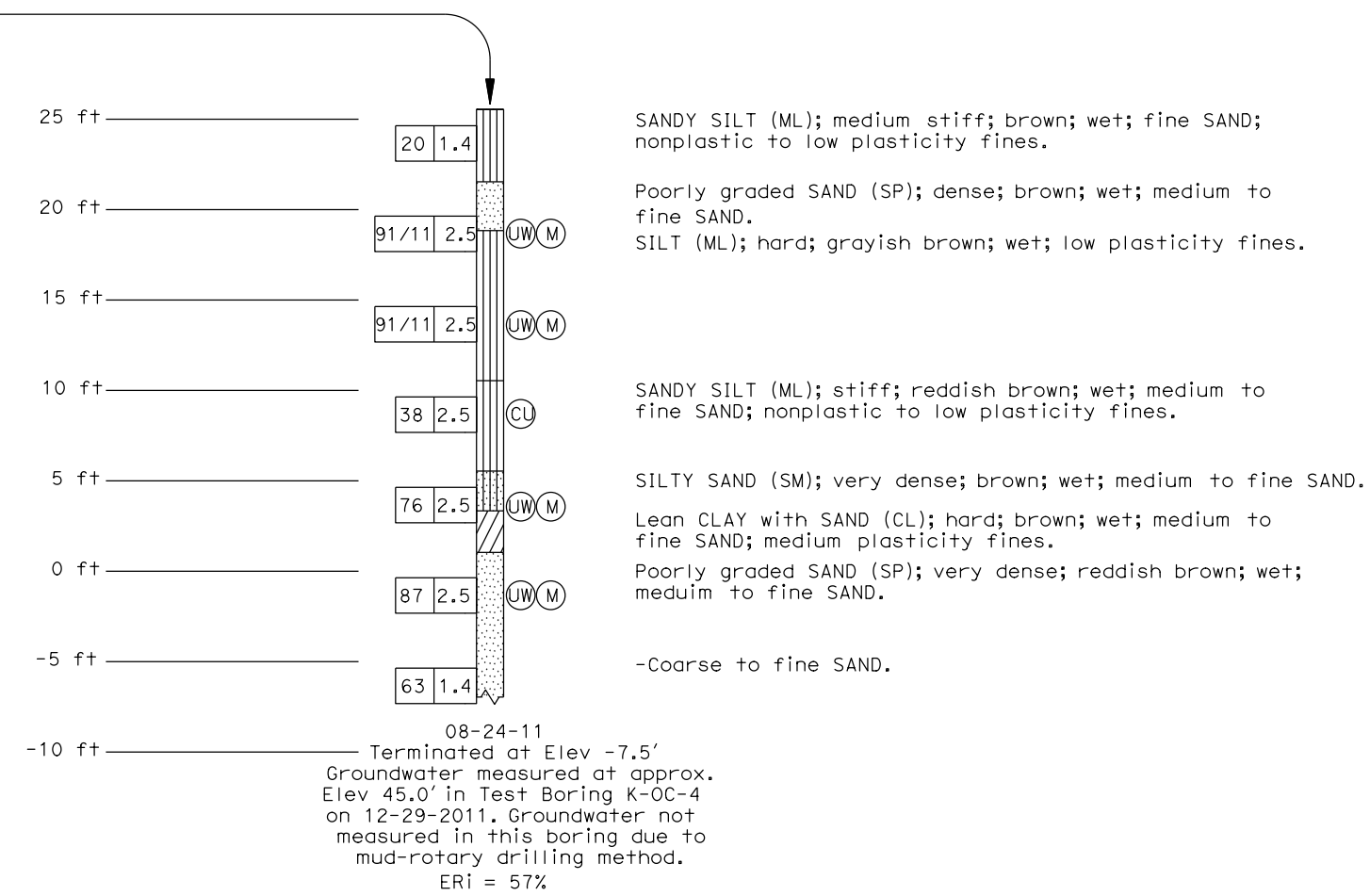


PLAN VIEW
SCALE : 1" = 200'



PROFILE VIEW
VERT. SCALE : 1" = 5'

- NOTES:
- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
 - 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
 - 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
 - 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
 - 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
 - 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
 - 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).

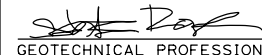


DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0 0.0/0.5	374	423

Signature: *S. Rodacker*
 GEOTECHNICAL PROFESSIONAL DATE: 4/20/12
 9/04/12
 PLANS APPROVAL DATE
 No. 2915
 Exp. 6/30/12
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
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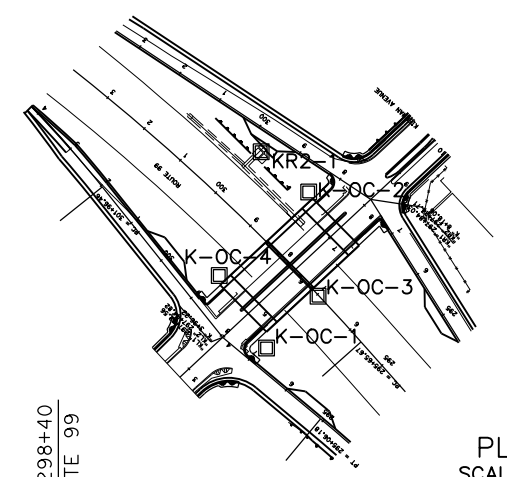
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0 0.0/0.5	375	423


 GEOTECHNICAL PROFESSIONAL DATE 4/20/12
 PLANS APPROVAL DATE 9/04/12
 No. 2915
 Exp. 6/30/12
 STATE OF CALIFORNIA

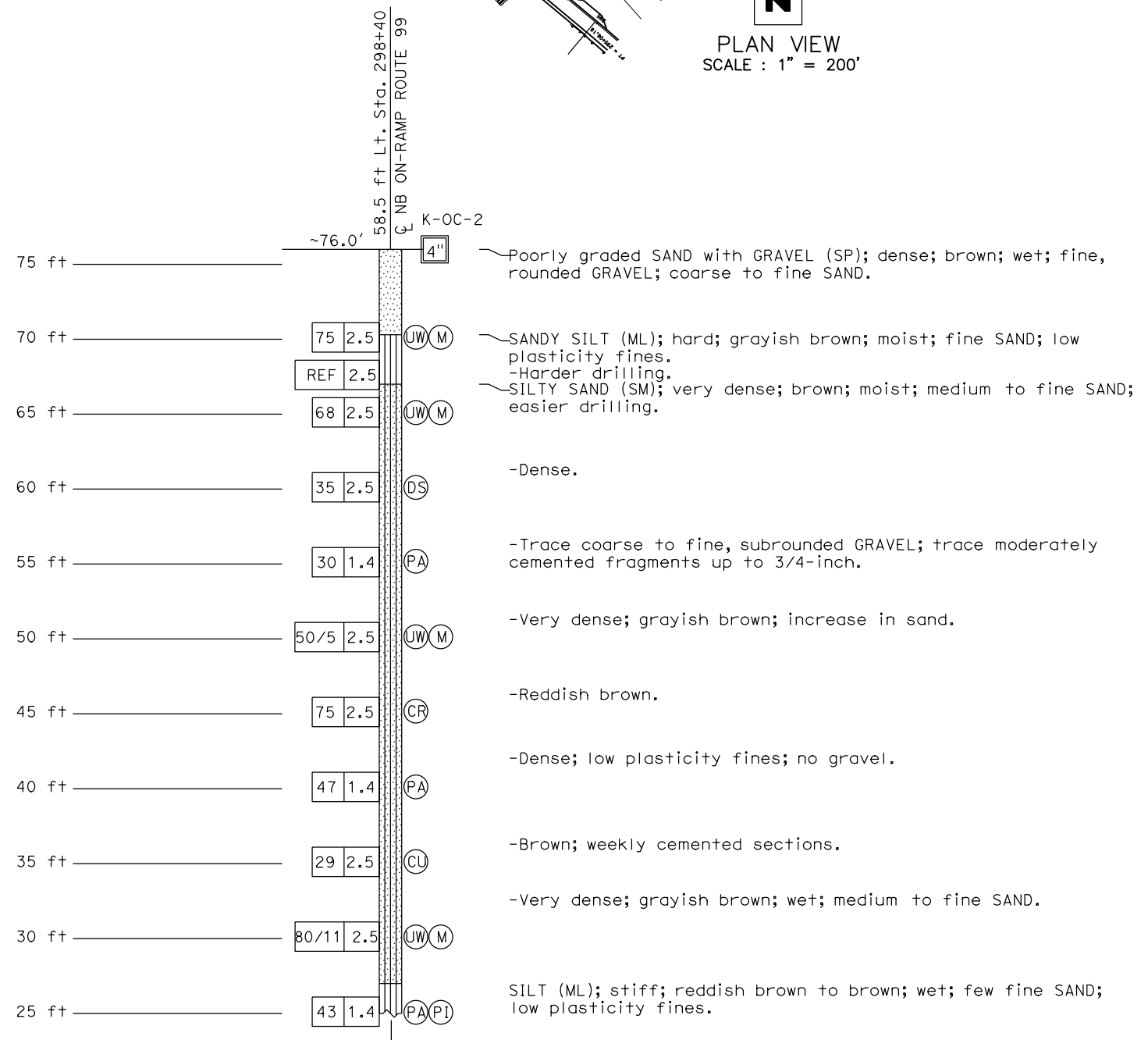
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 GEOCON CONSULTANTS, INC.
 6671 BRISA STREET
 LIVERMORE, CA 94550

NOTES:

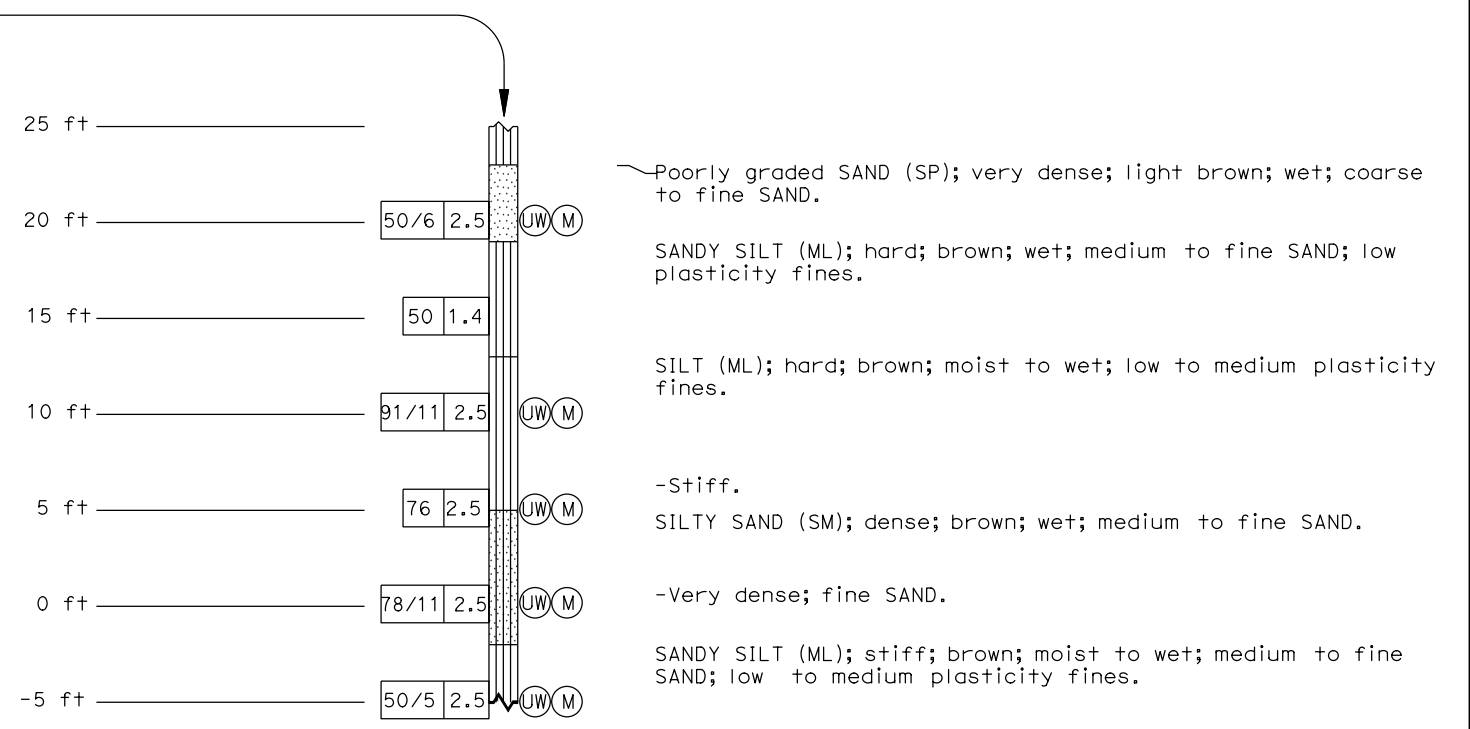
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PLAN VIEW
SCALE : 1" = 200'



PROFILE VIEW
VERT. SCALE : 1" = 5'



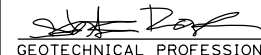
08-26-11
 Terminated at Elev -5.0'
 Groundwater measured at approx. Elev 45.0' in Test Boring K-OC-4 on 12-29-2011. Groundwater not measured in this boring due to mud-rotary drilling method.
 ERi = 57%

DESIGN OVERSIGHT	DRAWN BY R. AGUILAR	B. von DESSONNECK	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	S. RODACKER PROJECT ENGINEER	BRIDGE NO. 38-0163 POST MILES R22.56	ROUTE 219/99 SEPARATION (REPLACE) LOG OF TEST BORINGS 6 OF 7
SIGN OFF DATE	CHECKED BY S. RODACKER	FIELD INVESTIGATION BY: DATE: 8/22/2011-8/31/2011, 12/29/2011	UNIT: PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES 8/26/11	SHEET OF 45 46

GS LOTB SOIL LEGEND SHEET 1 (ENGLISH) (REV. 07/16/10) ORIGINAL SCALE IN INCHES FOR REDUCED PLANS 0 1 2 3 FILE => 38-0163-z-1tb06.dgn

DATE PLOTTED => 01-MAY-2012 USERNAME => qlt16n TIME PLOTTED => 10:16

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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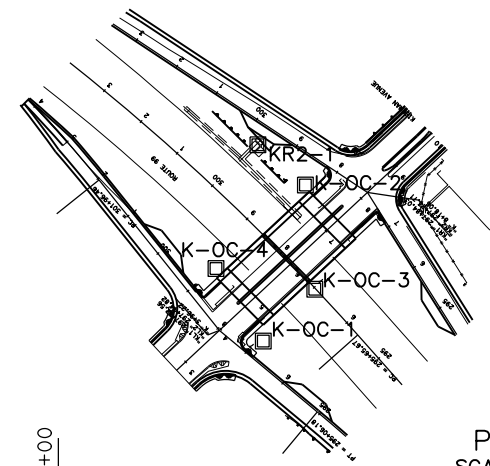

 GEOTECHNICAL PROFESSIONAL DATE 4/20/12
 PLANS APPROVAL DATE 9/04/12
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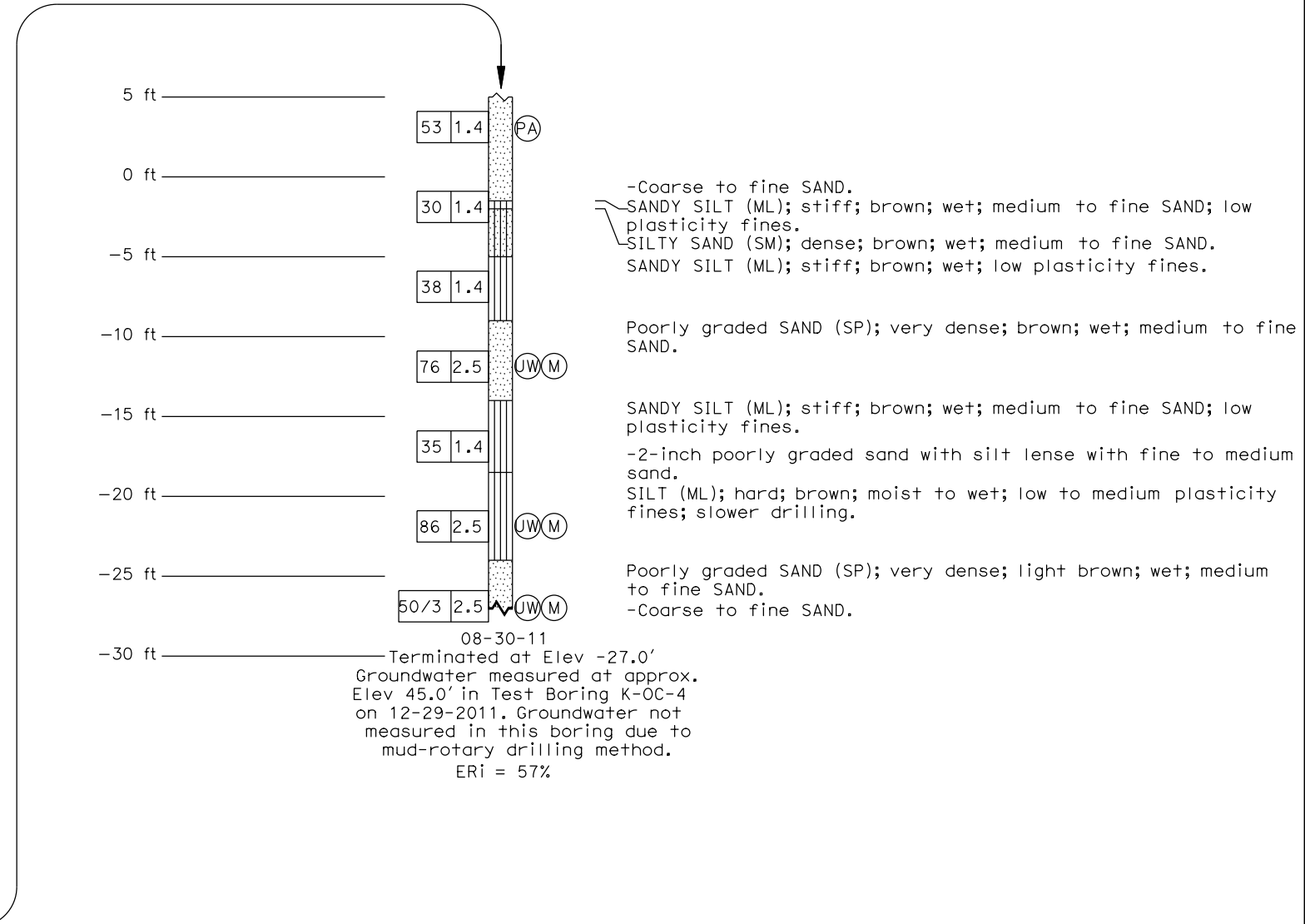
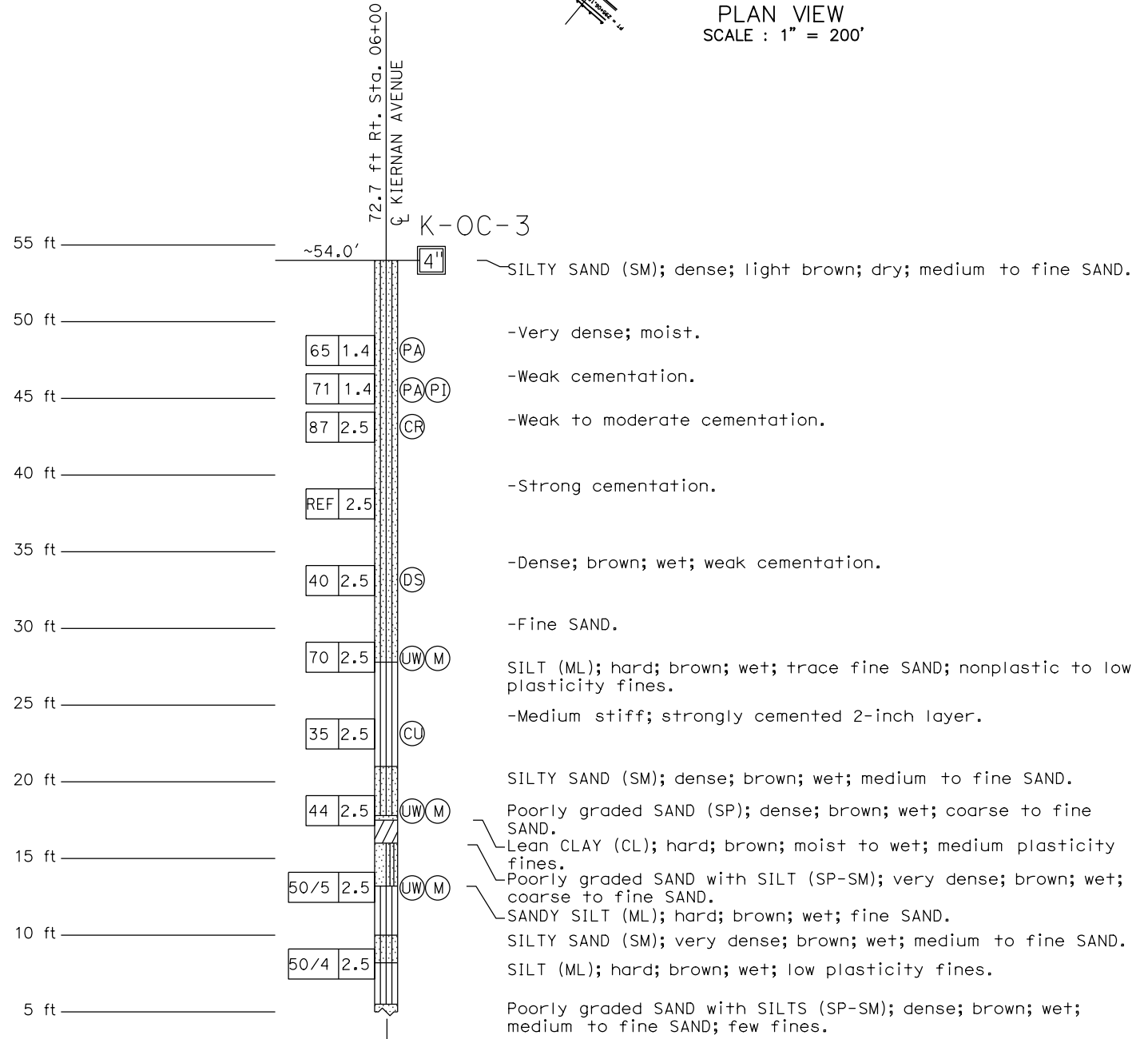
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 GEOCON CONSULTANTS, INC.
 6671 BRISA STREET
 LIVERMORE, CA 94550

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 PLAN VIEW
 SCALE : 1" = 200'



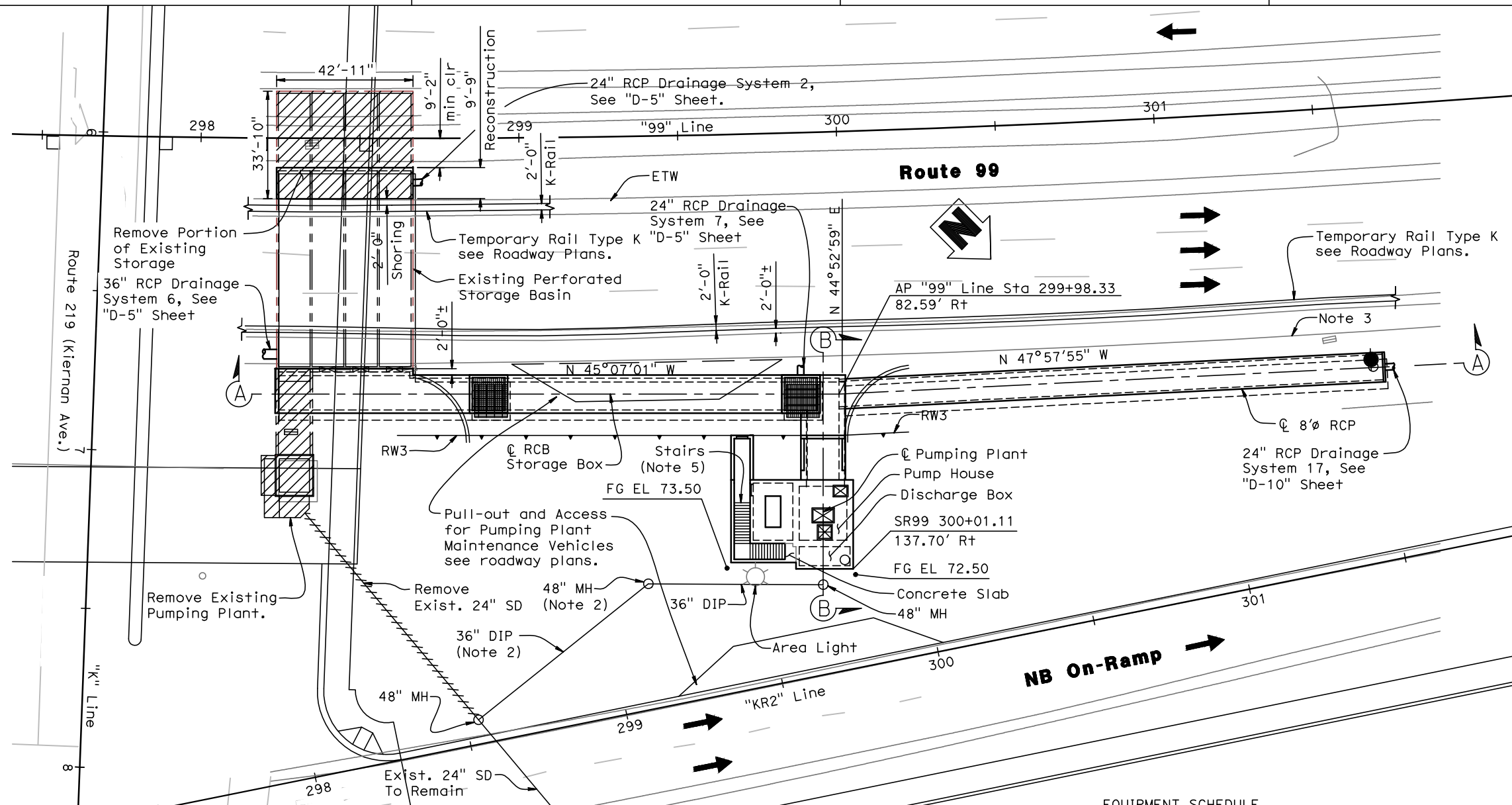
PROFILE VIEW
 VERT. SCALE : 1" = 5'

DESIGN OVERSIGHT	DRAWN BY	R. AGUILAR	B. von DESSONNECK	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO.	38-0163	ROUTE 219/99 SEPARATION (REPLACE) LOG OF TEST BORINGS 7 OF 7
	CHECKED BY	S. RODACKER	FIELD INVESTIGATION BY:		PROJECT ENGINEER	POST MILES	
SIGN OFF DATE			DATE: 8/22/2011-8/31/2011, 12/29/2011	UNIT:	1457	CONTRACT NO.:	10-0L3304
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS				PROJECT NUMBER & PHASE: 1000000100		DISREGARD PRINTS BEARING EARLIER REVISION DATES	
65 LOTB SOIL LEGEND SHEET 1 (ENGLISH) (REV. 07/16/10)				FILE => 38-0163-z-1tb07.dgn		REVISION DATES 8/26/11	
						SHEET	46
						OF	46

DATE PLOTTED => 01-MAY-2012
 USERNAME => g116n
 TIME PLOTTED => 10:16

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	377	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER MICHAEL A. PUGH No. S3967 Exp. 03-31-13
 PLANS APPROVAL DATE 9/04/12
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 DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358
 NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833



INDEX TO PLANS

Sheet No.	Title
GP-1	GENERAL PLAN No. 1
GP-2	GENERAL PLAN No. 2
ST-1	PUMPING PLANT PLAN
ST-2	PUMPING PLANT SECTIONS AND DETAILS
ST-3	PUMPING PLANT LAYOUT DETAILS
ST-4	RCB STORAGE BOX SECTION
ST-5	WET PIT DEBRIS SUMP SECTIONS
ST-6	RCB STORAGE BOX SECTIONS
ST-7	WET PIT & INLET SECTIONS
ST-8	DISCHARGE BOX SECTIONS
ST-9	ROOF DETAILS
ST-10	CONTROL ROOM DETAILS AND SECTIONS
ST-11	STAIR LAYOUT AND SECTIONS
ST-12	LANDING DETAILS No. 1
ST-13	LANDING DETAILS No. 2
ST-14	LANDING DETAILS No. 3
ST-15	EQUIPMENT ACCESS COVER DETAILS
ST-16	ACCESS OPENING DETAILS No. 1
ST-17	ACCESS OPENING DETAILS No. 2
ST-18	PIPE SUPPORT AND HANGER DETAILS
ST-19	CONTROL ROOM DOOR DETAILS
ST-20	STORAGE BOX DEMO & IMPROVEMENTS
ST-21	STORAGE BOX DETAILS
ST-22	LOG OF TEST BORINGS 1 OF 7
ST-23	LOG OF TEST BORINGS 2 OF 7
ST-24	LOG OF TEST BORINGS 3 OF 7
ST-25	LOG OF TEST BORINGS 4 OF 7
ST-26	LOG OF TEST BORINGS 5 OF 7
ST-27	LOG OF TEST BORINGS 6 OF 7
ST-28	LOG OF TEST BORINGS 7 OF 7
M-1	WET PIT PLAN AND PROFILE
M-2	SUMP PLAN AND ELEVATION
M-3	DETAILS
M-4	DISCHARGE PIPELINE PROFILE
EE-1	SYMBOLS & ABBREVIATIONS
EE-2	ELECTRICAL SITE PLAN
EE-3	PUMP STATION PLAN & PROFILE
EE-4	PUMP STATION LIGHTING PLAN
EE-5	ELECTRICAL THREE-LINE
EE-6	ELECTRICAL ELEVATIONS
EE-7	LEVEL CONTROL SCHEMATIC
EE-8	PUMP SCHEMATICS
EE-9	ELECTRICAL DETAILS 1
EE-10	ELECTRICAL DETAILS 2
PSC-1	PHASING PLAN No. 1
PSC-2	PHASING PLAN No. 2
PSC-3	PHASING PLAN No. 3

Notes:

- For section "A-A" and "B-B" see "GP-2" sheet.
- For MH see "M-3" Sheet.
- Protect and/or Re-establish existing 8"ø perforated pipe and underdrain system during/after pumping plant construction. Drainage to remain in service except when re-establishing system.
- For pumping plant layout control see "ST-1" Sheet.
- Concrete cover and staircase cover not shown, see "ST-9" sheet.
- Groundwater as high as Elev 51.5 ft is anticipated during construction. See SSP Section 99-02220.

Legend:

- Indicates existing structure
- ▨ Indicates concrete removal (portion)
- ➔ Indicates Direction of Traffic.

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

PLAN

1"=20'

PDS (in/hr)		
Duration	2 yr	50 yr
5 min	1.63	3.74
10 min	1.17	2.68
15 min	0.944	2.16
30 min	0.674	1.84
60 min	0.424	0.971

$I_{50} = 0.97D^{-0.55}$
 Based on NOAA Atlas 14, Volume 6 IDF Curves

EQUIPMENT SCHEDULE

Pumps	Flow, gpm	Head, ft	Speed, rpm	HP	Volts	Phase
1 & 2	2,500	39	1,200	50	460	3
	3,500	34				
	4,500	28				
3	650	37	900	15	460	3
	700	36				
	800	34				

Drainage Area: 20.98 AC Paved
 9.39 AC Unpaved
 30.37 AC Total

Storage: 19786 cf Existing Storage
 1993 cf Existing Piping
 8561 cf Additional Storage Pipe
 15360 cf Extension Storage
 45700 cf Available Storage

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	GP-1
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	M. PUGH PROJECT ENGINEER	POST MILES R22.56		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 1 OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3

FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163W-a-gp01.dwg

TIME PLOTTED => 16:43 USERNAME => Carrick, Steven

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	378	423

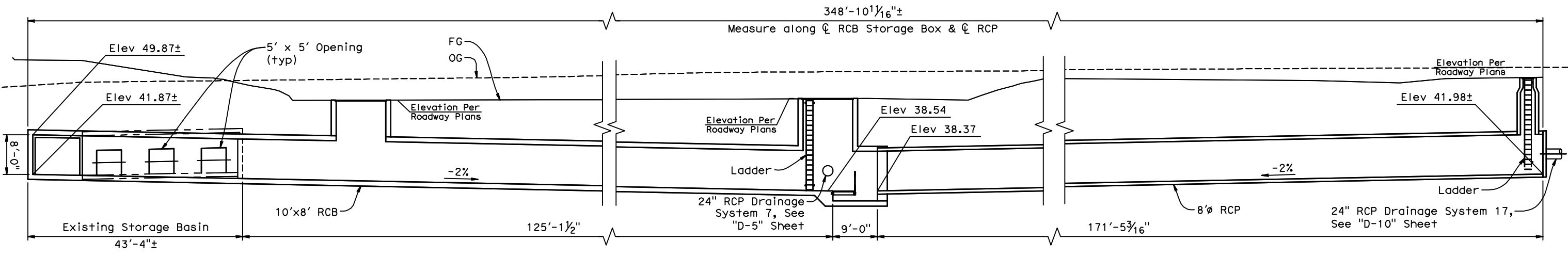
REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE 9/04/12

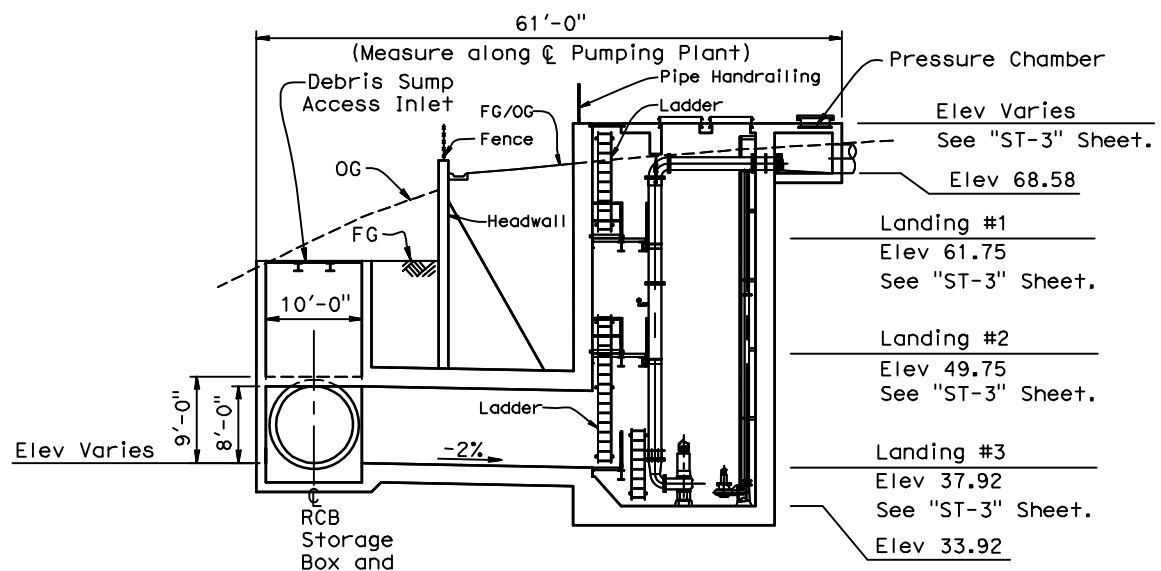
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



DEVELOPED SECTION A-A
 1"=10'



SECTION B-B
 1"=10'

Notes:
 1. Pumping Plant is to be constructed on undisturbed alluvial soil, Per Geotech Report.

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		GP-2
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION		POST MILES R22.56	GENERAL PLAN No. 2		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 2 OF 47

FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163W-a-gp02.dwg

TIME PLOTTED => 9-Sep-12 USERNAME => Carrick, Steven

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	379	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 No. S3967 Exp. 03-31-13
 STRUCTURAL STATE OF CALIFORNIA
 PLANS APPROVAL DATE
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 SACRAMENTO, CA 95833

- NOTES:
- For Sections 'B-B' and 'C-C' See "ST-2" Sheet.
 - For Section 'D-D' See "ST-5" Sheet.
 - For Section 'S-S' See "ST-10" Sheet.
 - For Section 'W-W' See "ST-8" Sheet.

Indicates Concrete Removal (portion)

GENERAL NOTES

PUMPING PLANT DESIGN NOTES

DESIGN: AASHTO LRFD Bridge Design Specifications, 4th edition and 2010 California Amendments except details taken from 2010 Standard Plans. ACI 318-08, AISC 360-05.

- a. Loads:
- Vertical Box Lateral Loads:
 - Active Earth Loads (Equivalent Fluid Unit Weight): 35 lbs/cf above GWT; 80 lbs/cf below GWT
 - At-Rest Earth Loads (Equivalent Fluid Unit Weight): 55 lbs/cf above GWT; 100 lbs/cf Below GWT
 - Seismic Soil loads (Inverted Triangular Load): 15 lbs/cf combined with active earth loads
 - Vertical Box Roof Loads:
 - Live Load: HL93 Truck
 - Pump Station Landing Load: 100 psf
 - Reinforced Concrete Pipe:
 - Caltrans Standard Plan D-79 (20 ft Max Cover)
 - Culvert with less than 20 ft of soil cover:
 - Caltrans Standard Plans D-80
 - Culvert with over 20 ft of soil cover:
 - Earth Loads (Equivalent Fluid Unit Weight):
 - Load Case 1) For vertical earth pressure = 140 lbs/cf For lateral earth pressure = 42 lbs/cf
 - Load Case 2) For vertical earth pressure = 140 lbs/cf for lateral earth pressure = 140 lbs/cf
 - Culvert Cut-Walls, Culvert Intersection Slab, and Walls Below Debris Sump:
 - Earth Loads (Equivalent Fluid Unit Weight):
 - Load Case 1) For vertical earth pressure = 140 lbs/cf FOR lateral earth pressure = 42 lbs/cf
 - Load Case 2) For vertical earth pressure = 140 lbs/cf For lateral earth pressure = 140 lbs/cf
 - Live Loads: Surcharges = 10 lbs/sf
 - Landings, Grate Walkways & Platforms:
 - Live Load: 100 lbs/sf
 - Buoyancy: Safety Factor = 1.35, no load factor on gravity or buoyancy loads.
 - Structural Steel: Fy = 50 ksi for wide flange sections.
 - Unit Stresses: F'c = 4.0 ksi; Fy = 60 ksi, ASTM A706.
 - Miscellaneous Metal (Working Stress Design): Fy = 36 ksi unless otherwise noted.
- b. Soil Report: Foundation Report Kiernan Ave (State Route 219) At State Rout 99 (Bridge No. 38-0163W) April, 2012. Bearing Soil is classified as indicated on Log of Test Boring (sheet "ST-22") Net allowable bearing pressure at; Group Dead and Live Loading: 6000 lbs/sf (FS = 3)

DETAILS NOTES

- Metal Work Notes:
 - All metal work shall be hot dipped galvanized after fabrication.
 - Secure metal to metal connections shown as + with a 5/8" dia hex head machine bolt, lock washer and hex nut, unless otherwise noted. Bolts shall be A325 With Standard Round Holes. Install by Snug-Tight Method.
 - Mechanical expansion anchors shall be 5/8" dia and have a 4" minimum embedment, unless otherwise shown.
 - Mechanical expansion anchors used for securing ladders inside of Pumping Plant shall be of stainless steel.
 - All lock washers shall be helical spring lock washers.
 - All railing and ladders shall have smooth edges.
 - Welded Steel Grates: Unless otherwise noted
 - Bearing Bars 1 1/4" x 1/4" @ 1 1/4" C-C
 - Cross Bars @ 4" C-C
 - Trim Bars 1 1/4" x 3/16" fillet welded all around to ends of Bearing Bars
 - Direction of Bearing Bars is indicated in Plans.
 - Welded Steel Grates (Traffic Rated)
 - Bearing Bars 3 1/2" x 3/8" @ 1 7/8" C-C
 - Cross Bars 3/8" dia @ 5 1/2" C-C
 - End Bars 3 1/2" x 3/8" Fillet Welded All Around To Ends Of Bearing Bars.
 - All Wide Flange Shapes shall conform to ASTM A992. All other Shapes and Plates shall conform to ASTM A36.
 - Welding Electrodes shall be E70XX Low Hydrogen (unless noted otherwise).
- For discharge Pipe location and elevations, see "MECHANICAL PLANS".
- For ground rod details not shown, see "ELECTRICAL PLANS".
- Provide 3/8" chamfer at concrete corners.

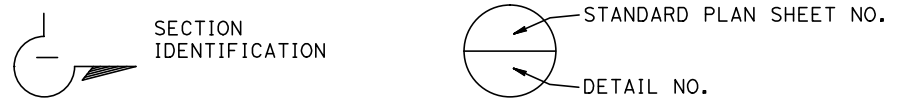
QUANTITIES

REMOVE EXISTING PUMPING PLANT (PORTION)	Lump Sum
STRUCTURE EXCAVATION (PUMPING PLANT)	4,124 cy
STRUCTURE BACKFILL (PUMPING PLANT)	1,775 cy
STRUCTURAL CONCRETE (PUMPING PLANT)	854 cy
BAR REINFORCING STEEL (PUMPING PLANT)	226,099 lb
STRUCTURAL STEEL (PUMPING PLANT)	8,479 lb
96" REINFORCED CONCRETE PIPE	171 lf
PUMPING PLANT EQUIPMENT	Lump Sum
PUMPING PLANT ELECTRICAL EQUIPMENT	Lump Sum
BUILDING MISCELLANEOUS METAL	13,745 lb
CHAIN LINK RAILING (TYPE CL-4)	21 lf

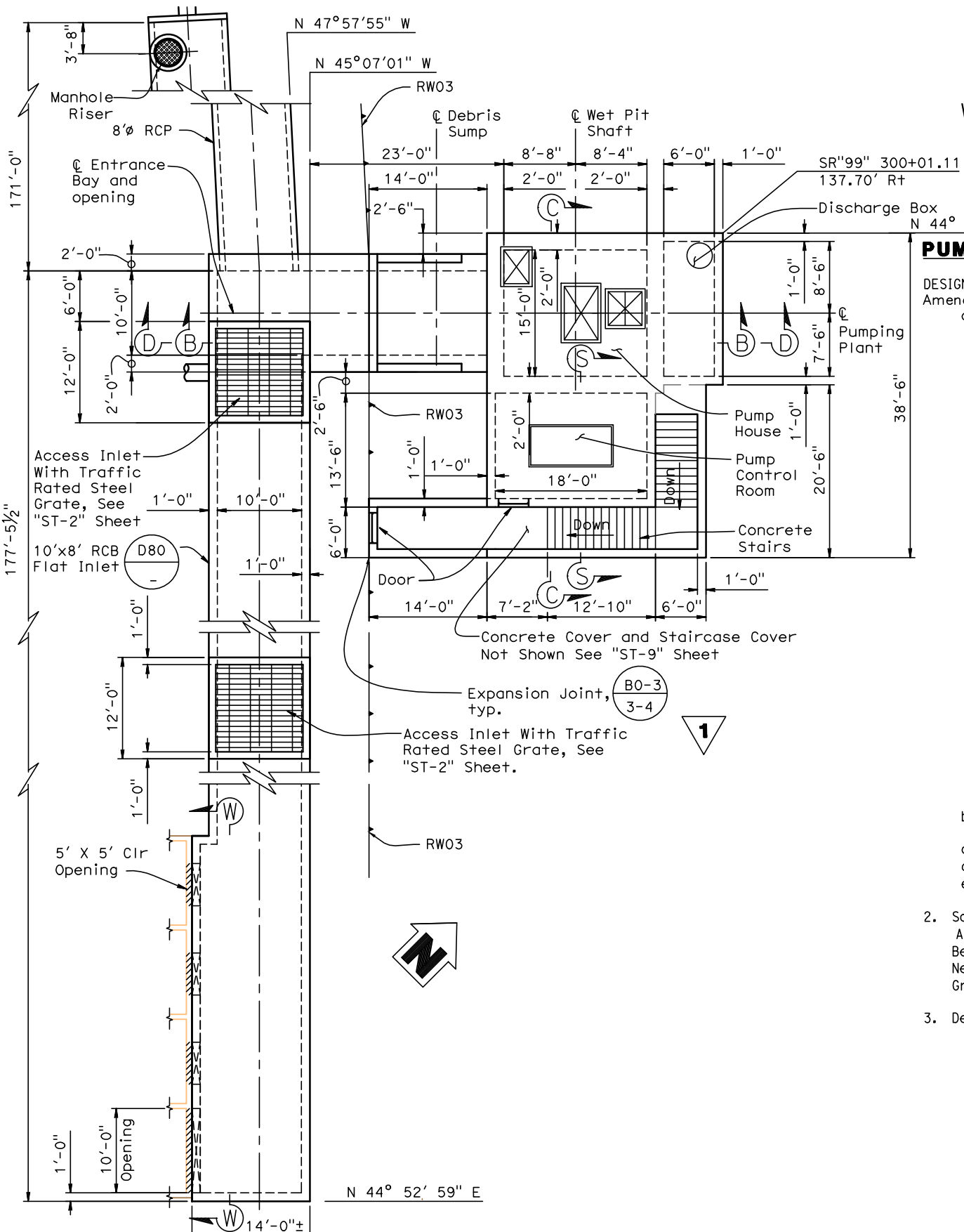
STANDARD PLANS DATED 2010

- A10A ACRONYMS AND ABBREVIATIONS (SHEET 1 OF 2)
- A10B ACRONYMS AND ABBREVIATIONS (SHEET 2 OF 2)
- B7-11 UTILITY DETAILS
- D79A PRECAST REINFORCED CONCRETE PIPE DIRECT DESIGN METHOD
- D80 CAST-IN-PLACE REINFORCED CONCRETE SINGLE BOX CULVERT
- D82 CAST-IN-PLACE REINFORCED CONCRETE BOX CULVERT MISC DETAILS
- D93A PIPE RISER CONNECTIONS

PLAN SYMBOLS



1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012



PUMP PLANT PLAN

1/8" = 1'-0"

NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-1	
DETAILS	BY S. CARRICK	CHECKED D. GAGNE	LAYOUT	CHECKED	M. PUGH PROJECT ENGINEER	R22.56	PUMPING PLANT PLAN		
SIGN OFF DATE	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS	BY	PLANS AND SPECS COMPARED				
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)					ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 3 OF 47

DATE PLOTTED => 9-Sep-12 USERNAME => Gagne, D

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	380	423

REGISTERED CIVIL ENGINEER	DATE	9/4/12
PLANS APPROVAL DATE		
9/04/12		

No. S3967
 Exp. 03-31-13
 M. MICHAEL A. PUGH
 REGISTERED PROFESSIONAL ENGINEER
 STATE OF CALIFORNIA

The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

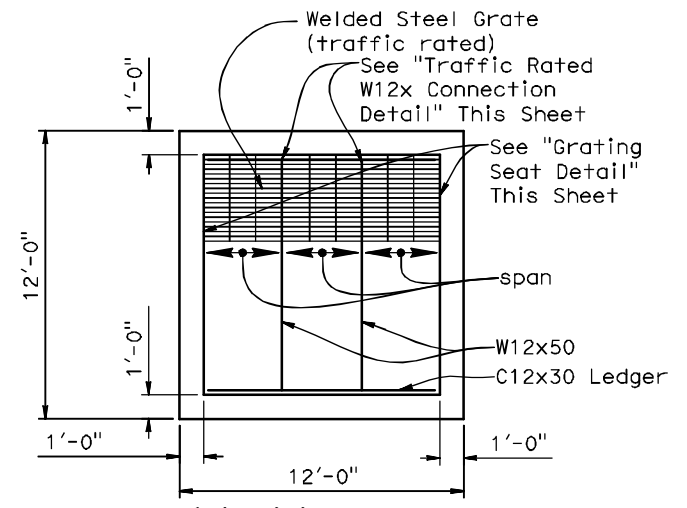
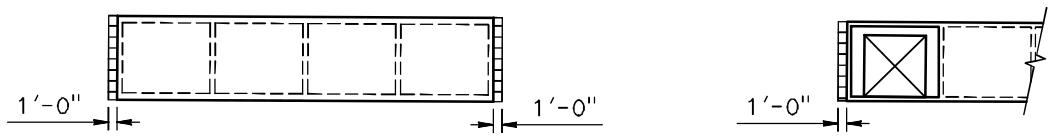
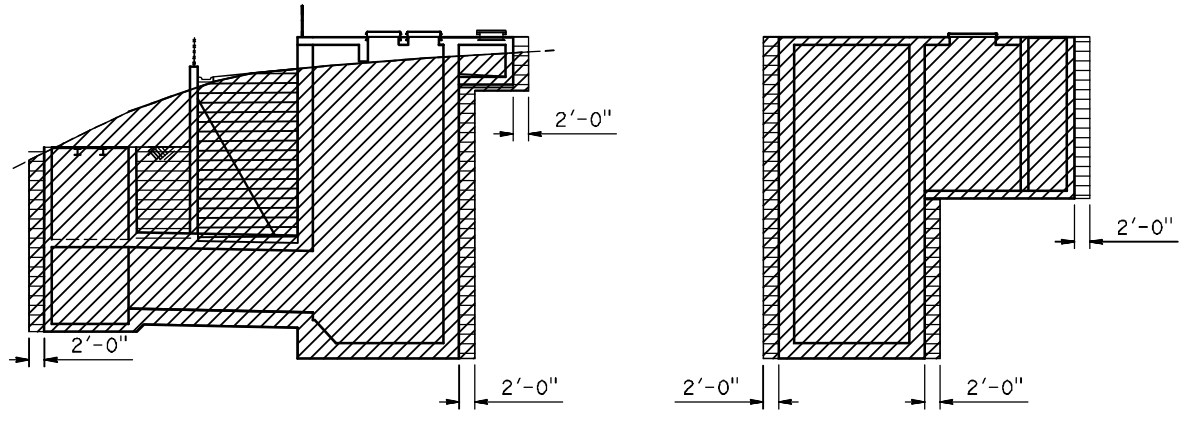
NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

LEGEND:

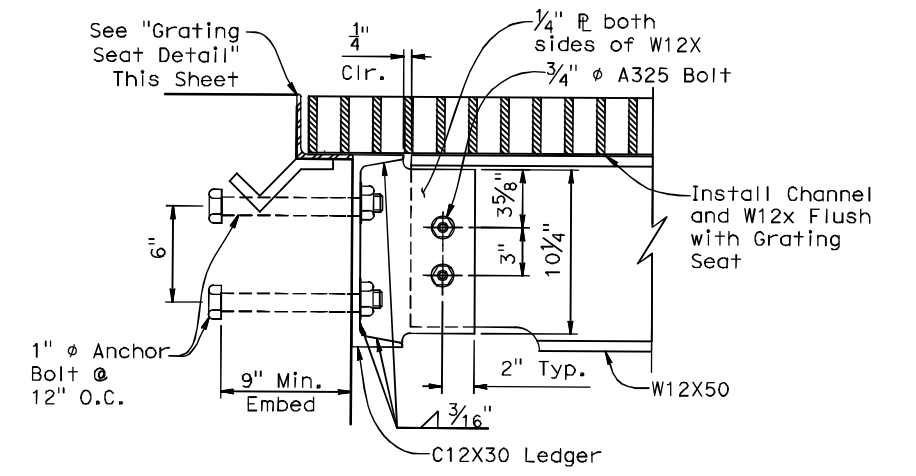
- Structural Excavation
- Structural Backfill

NOTES:

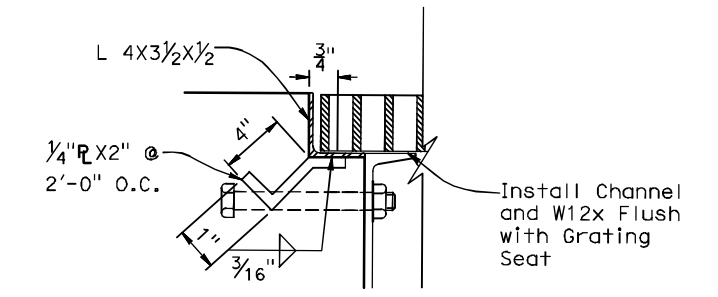
1. For Pipe Support Details, See "ST-17" Sheet.
2. For location of sections "B-B" and "C-C" See "ST-1" Sheet.
3. For section "J-J", "K-K", And "L-L" See "ST-7" Sheet.



ACCESS INLET
NO SCALE

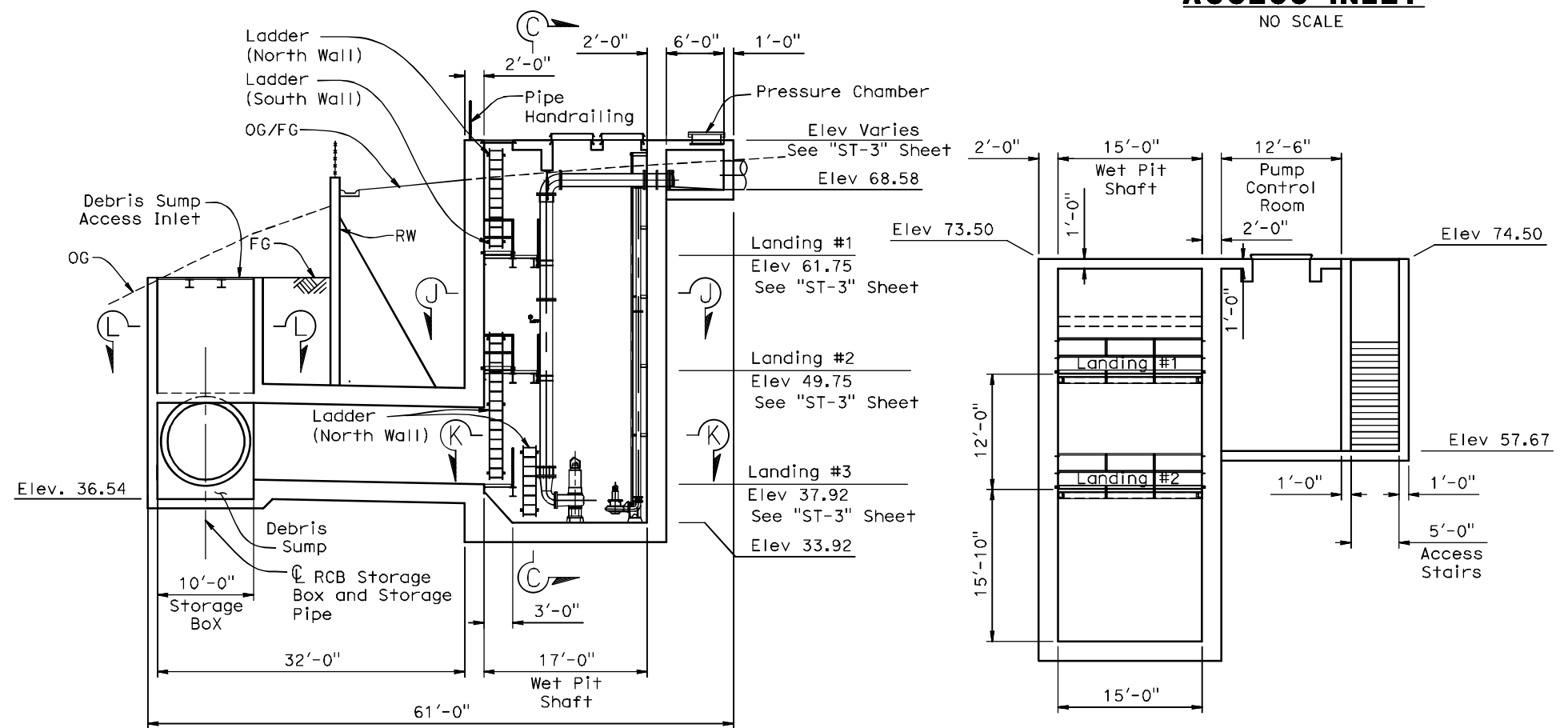


TRAFFIC RATED W12x CONNECTION DETAIL
NO SCALE



GRATING SEAT DETAIL
NO SCALE

PAYLIMITS OF STRUCTURE EXCAVATION AND BACKFILL
NO SCALE



SECTION B-B
1/8"=1'-0"

SECTION C-C
1/8"=1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

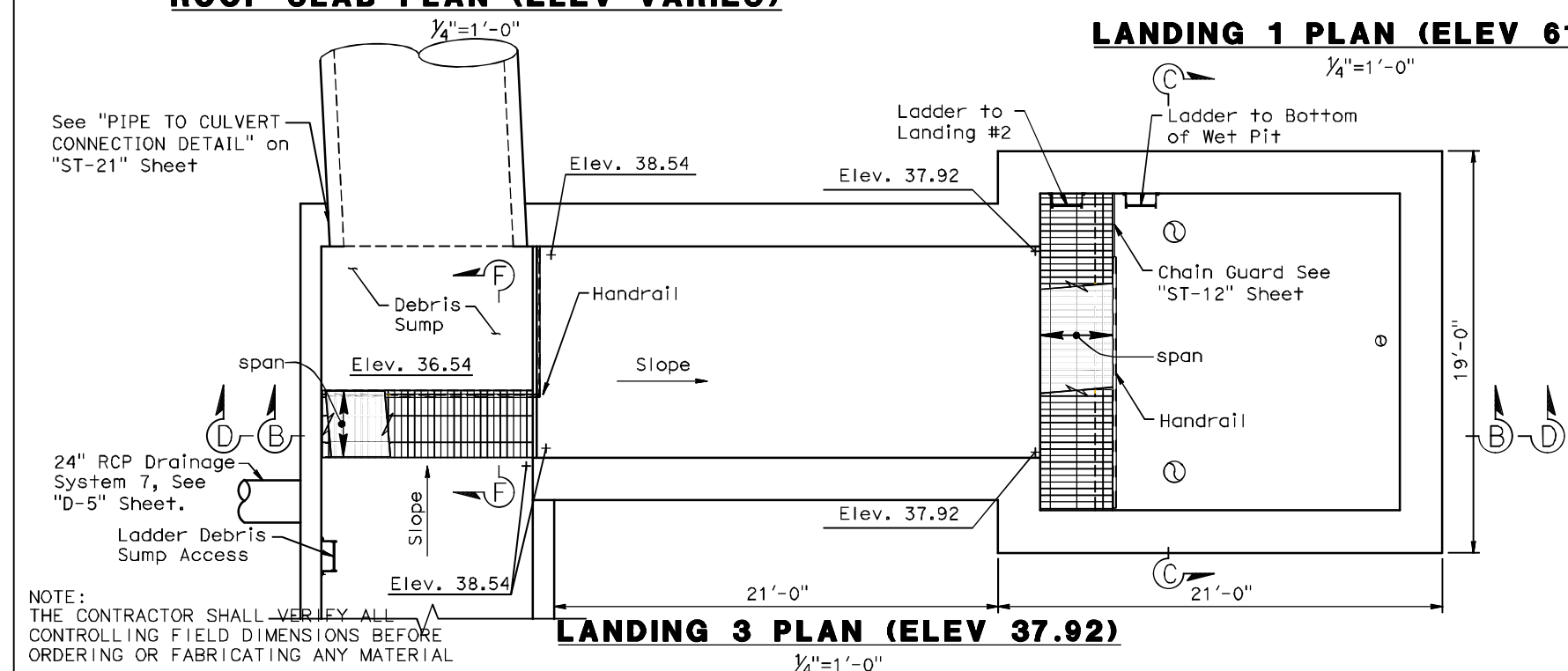
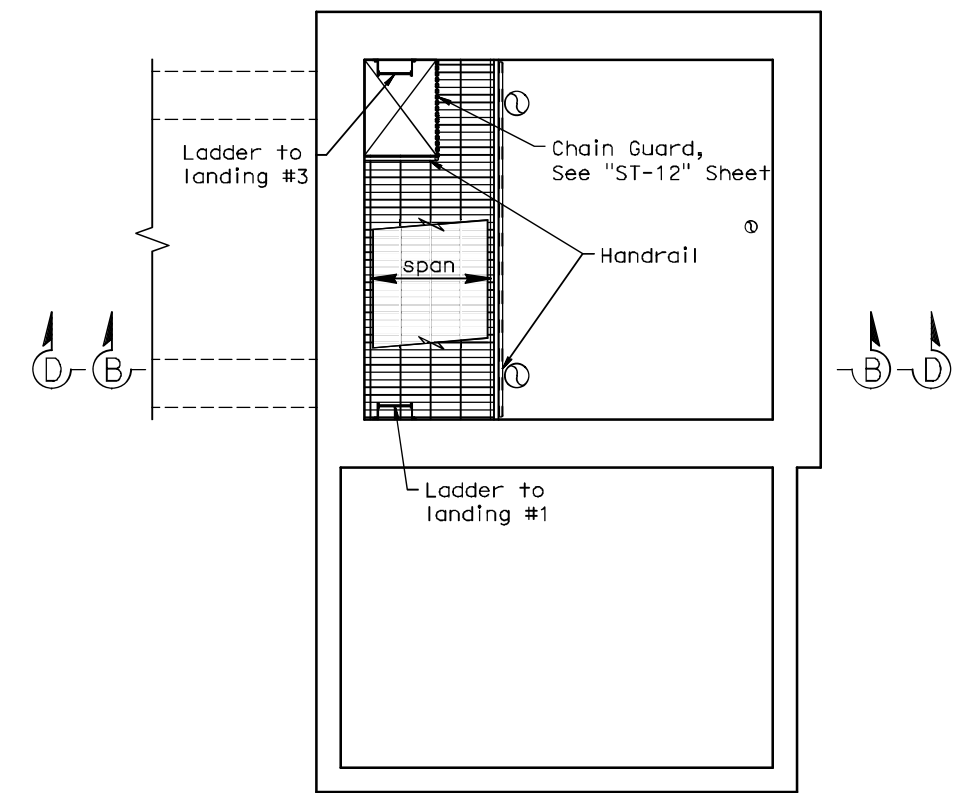
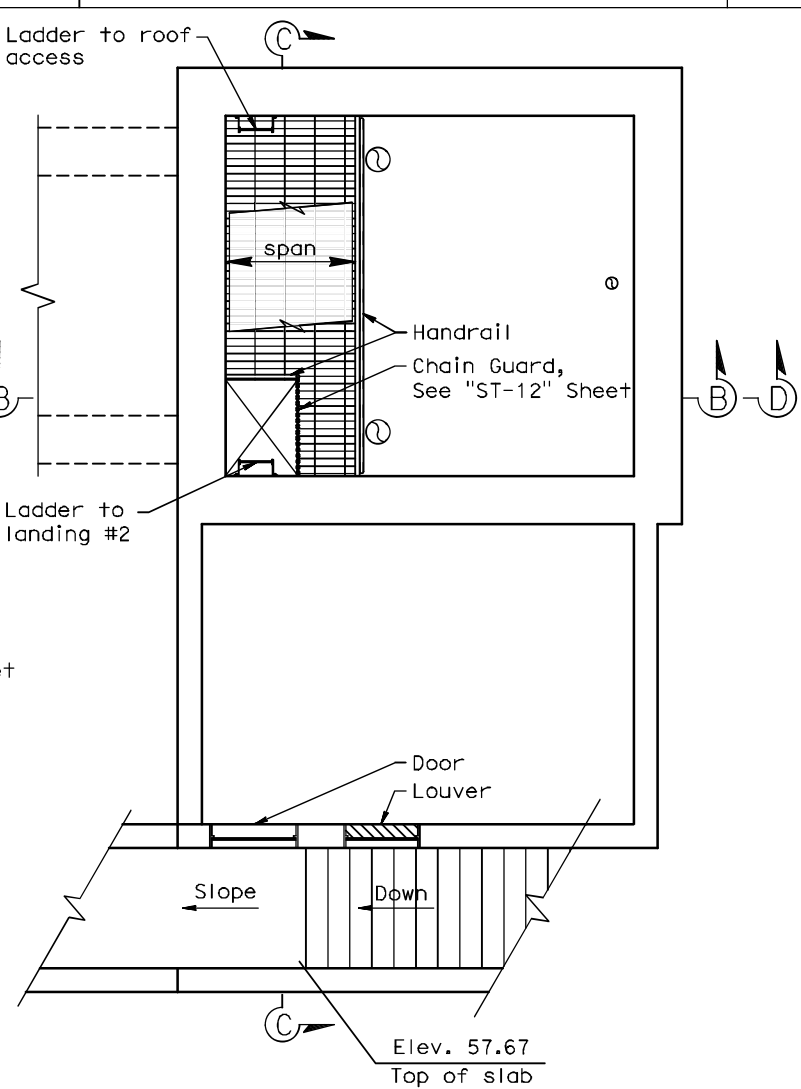
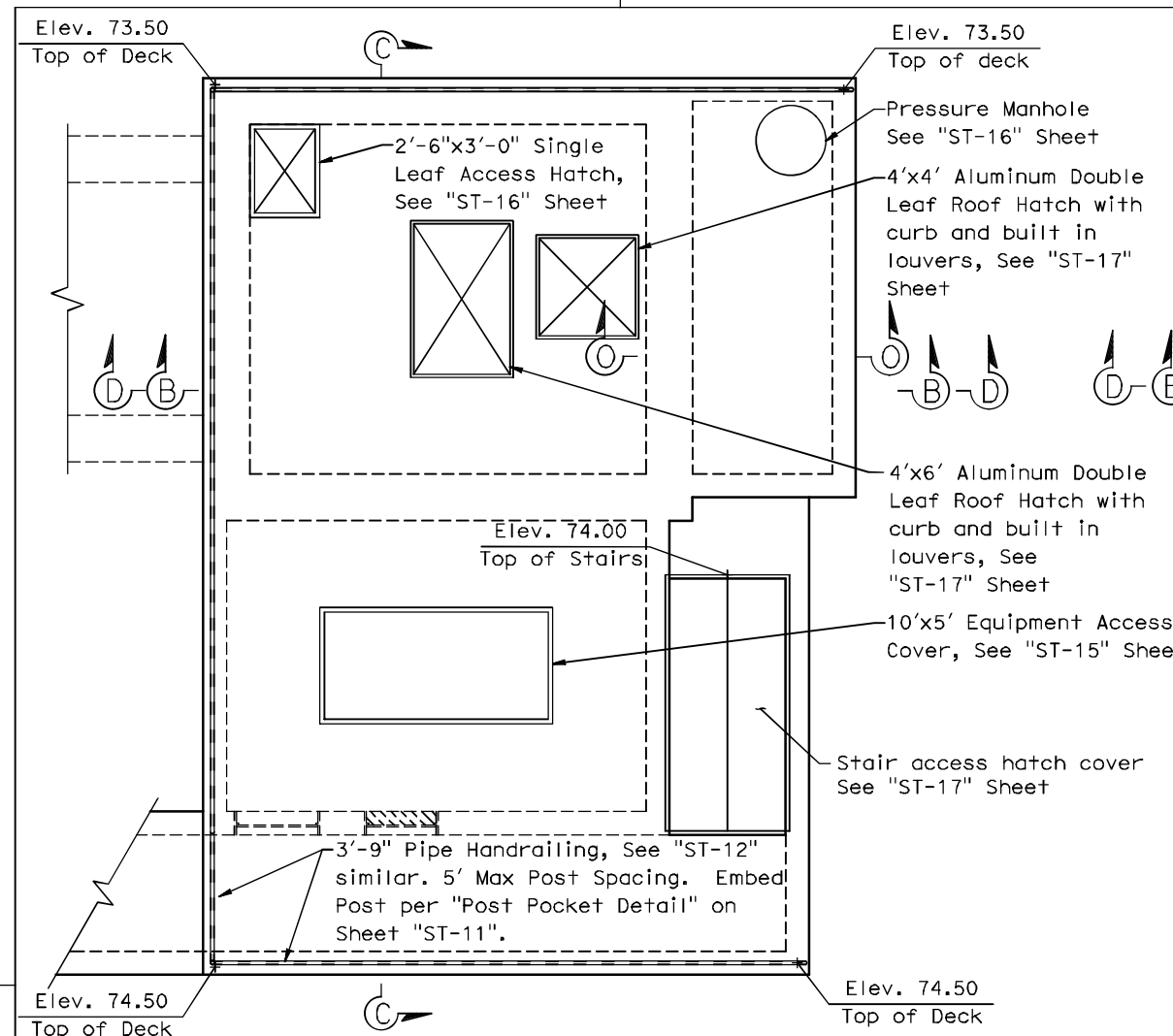
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-2
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	M. PUGH PROJECT ENGINEER	POST MILES R22.56	PUMPING PLANT SECTIONS AND DETAILS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 4	OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3
 DISREGARD PRINTS BEARING EARLIER REVISION DATES
 REVISION DATES: 2/24/12, 3/28/12, 5/1/12, 9/7/12
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USERNAME => Carrick, DATE PLOTTED => 9-Sep-12, TIME PLOTTED => 19:16

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	381	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. S3967 Exp. 03-31-13
 STRUCTURAL STATE OF CALIFORNIA
 DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



- NOTES:**
1. For Landing And HandRail Details See "ST-12", "ST-13", And "ST-14" Sheets.
 2. For Sections 'B-B' and 'C-C' See "ST-2" Sheet.
 3. For Section 'D-D' See "ST-5" Sheet.
 4. For Section 'O-O' See "ST-8" Sheet.
 5. For Section 'F-F' See "ST-6" Sheet.

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-3
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION		R22.56	PUMPING PLANT LAYOUT DETAILS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 5 OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS

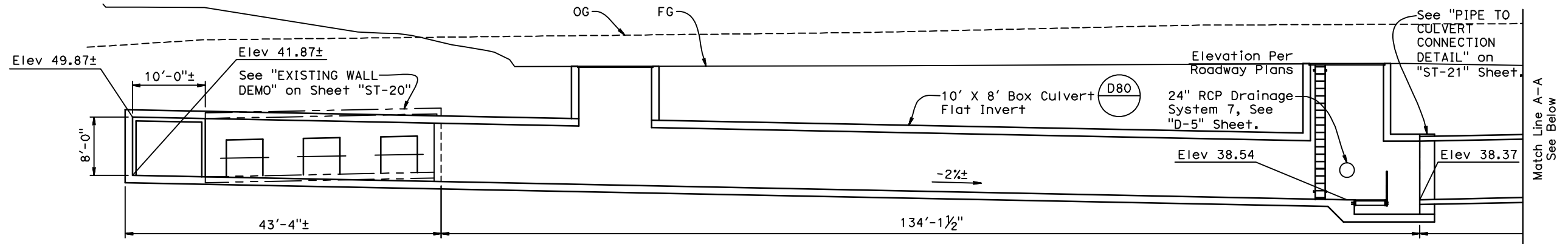
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USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:16

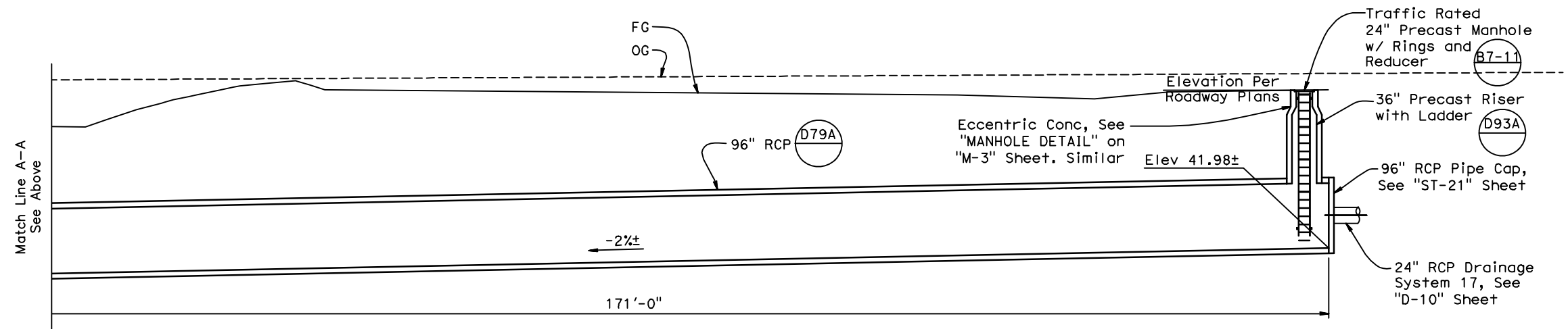
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	382	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE 9/04/12
 DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



SECTION A-A
 $\frac{1}{8}'' = 1' - 0''$



SECTION A-A
 $\frac{1}{8}'' = 1' - 0''$

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL
 CONTROLLING FIELD DIMENSIONS BEFORE
 ORDERING OR FABRICATING ANY MATERIAL

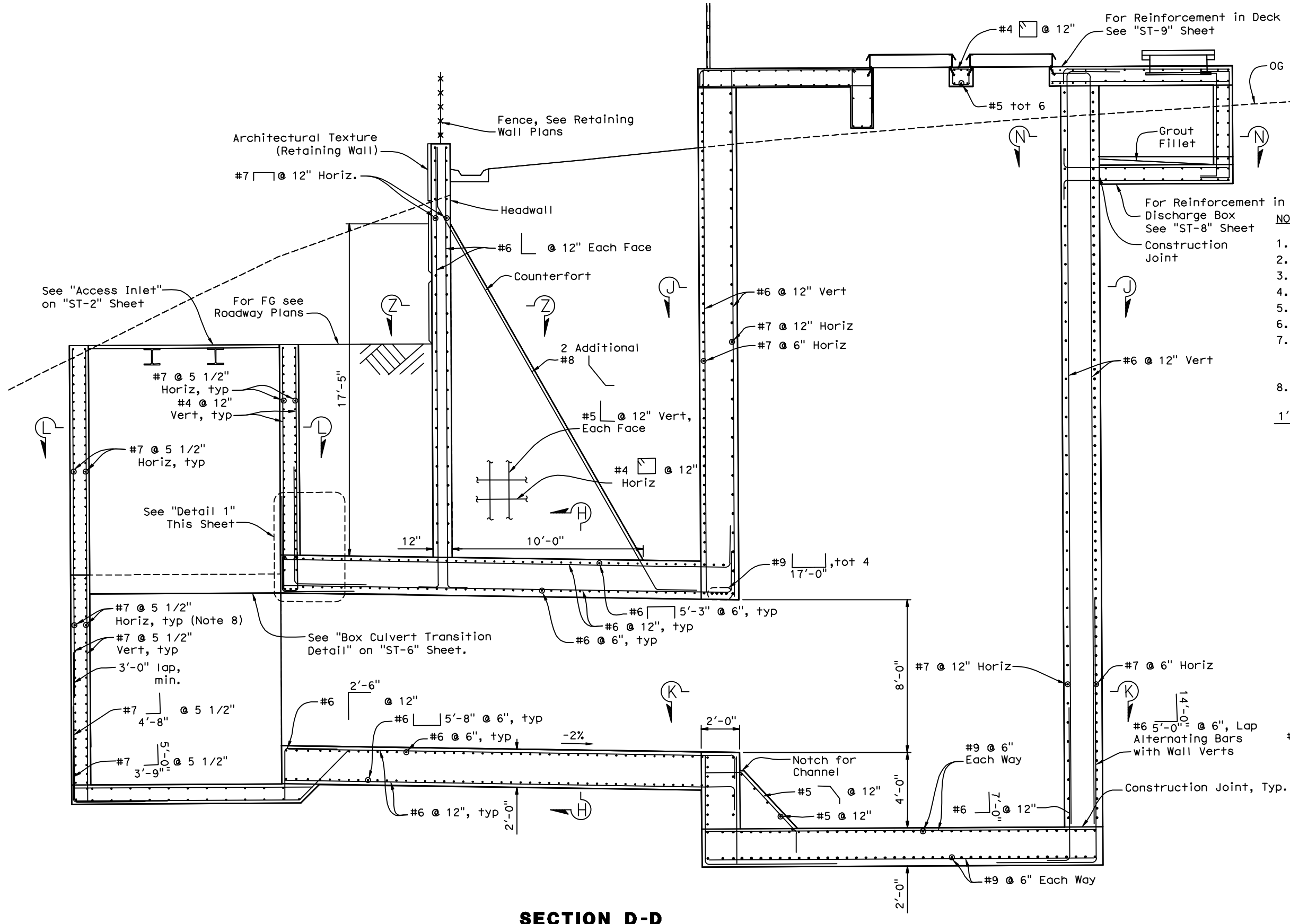
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-4
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	RCB STORAGE BOX SECTION		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 6	OF 47	

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:17

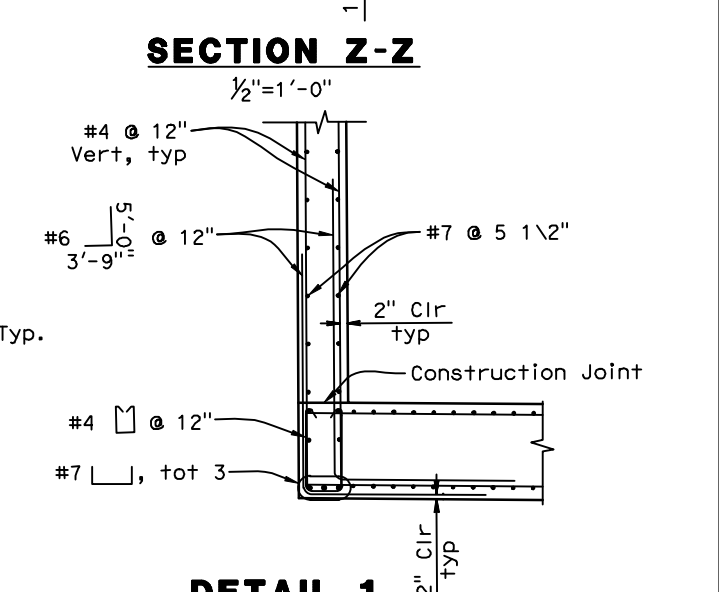
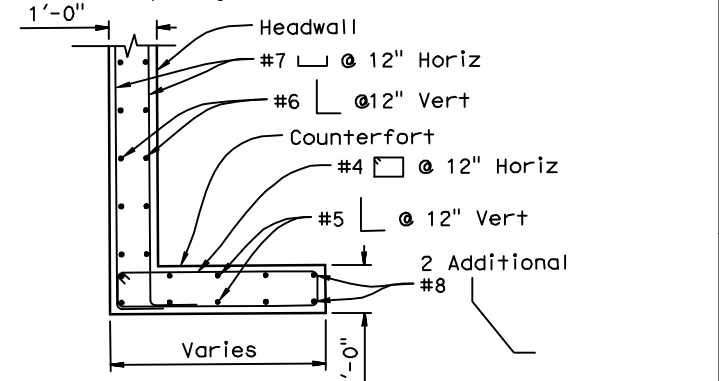
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	383	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE 9/04/12
 The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.
 DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



- NOTES:**
1. For location of Section 'D-D', see "ST-1" sheet.
 2. For Section 'N-N', see "ST-8" sheet.
 3. For Section 'H-H', see "ST-6" sheet.
 4. For Section 'L-L', see "ST-7" sheet.
 5. For Section 'J-J', see "ST-7" sheet.
 6. For Section 'K-K', see "ST-7" sheet.
 7. For typical construction joint detail see "ST-9" sheet. All construction joints shall have bentonite waterstops centered in joint.
 8. Extend #7 horizontal bars 3'-6" beyond edge of inlet opening above.



SECTION D-D
 $\frac{3}{8}'' = 1'-0''$

SECTION Z-Z
 $\frac{1}{2}'' = 1'-0''$

DETAIL 1
 $\frac{1}{2}'' = 1'-0''$

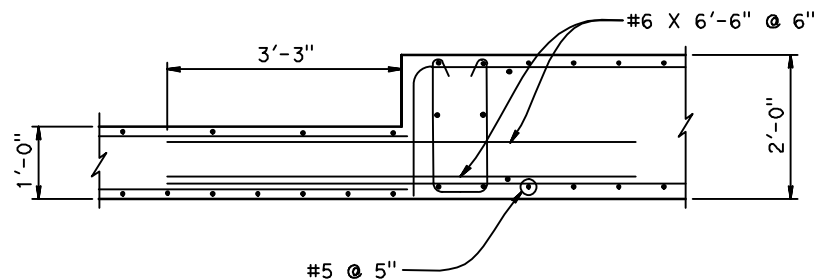
NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-5
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION	PROJECT ENGINEER	POST MILES R22.56	WET PIT DEBRIS SUMP SECTION		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 7 OF 47

TIME PLOTTED => 9-Sep-12 USERNAME => Carrick, Steven

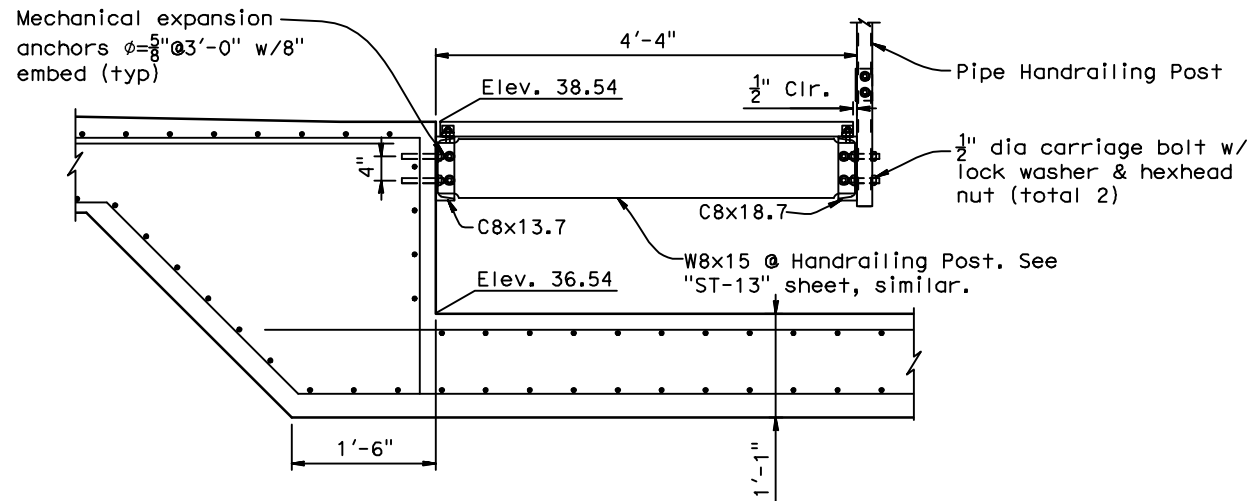
NOTES:

1. For Location Of Section 'F-F', See "ST-3" Sheet.
2. For Location Of Section 'H-H', See "ST-5" Sheet.
3. For typical construction joint detail see "ST-9" sheet. All construction joints shall have bentonite waterstops centered in joints.



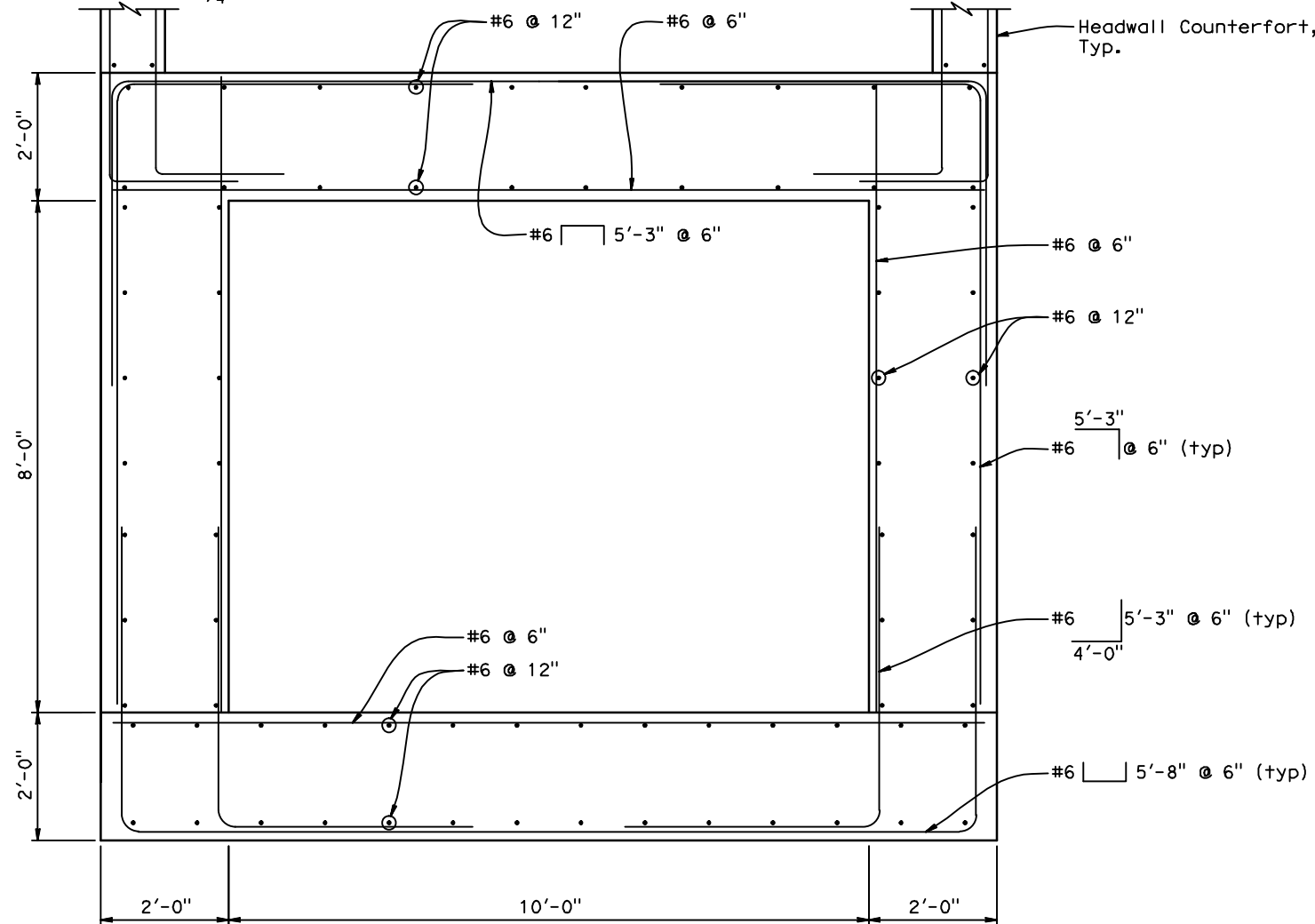
BOX CULVERT TRANSITION DETAIL

$\frac{3}{4}'' = 1'-0''$



SECTION F-F

$\frac{3}{4}'' = 1'-0''$



SECTION H-H

$\frac{3}{4}'' = 1'-0''$

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	384	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. S3967 Exp. 03-31-13
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

NOTE:
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DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-6
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	R22.56	RCB STORAGE BOX SECTIONS	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 8 OF 47	

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:18

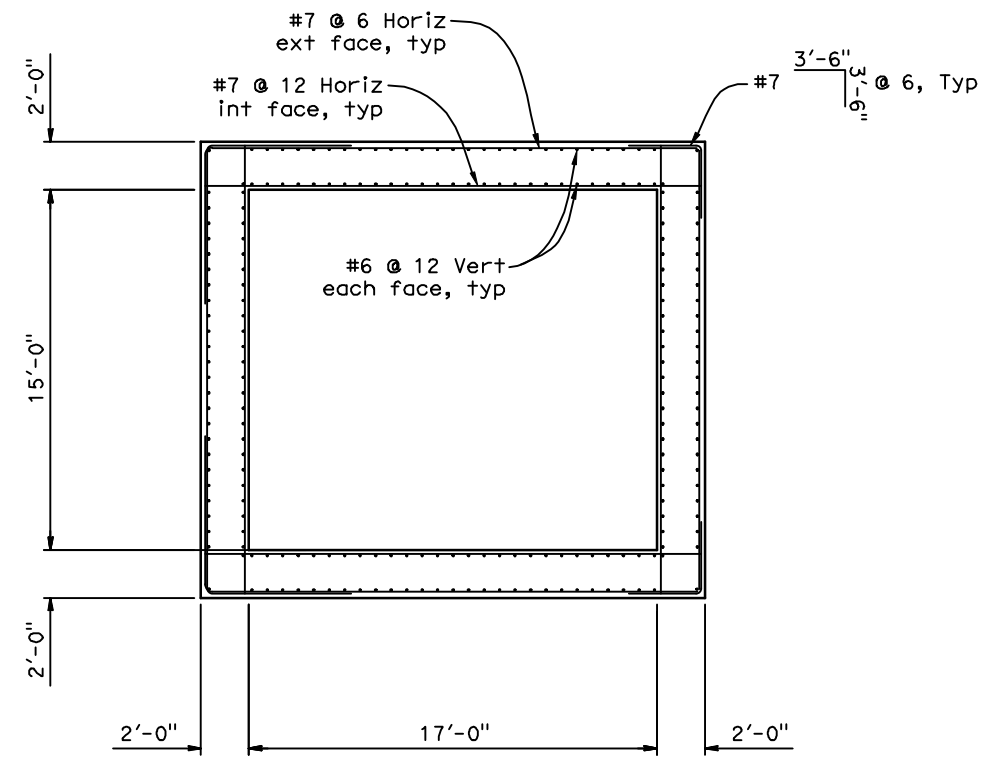
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	385	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. S3967 Exp. 03-31-13
 STATE OF CALIFORNIA
 STRUCTURAL ENGINEER
 MICHAEL A. PUGH

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

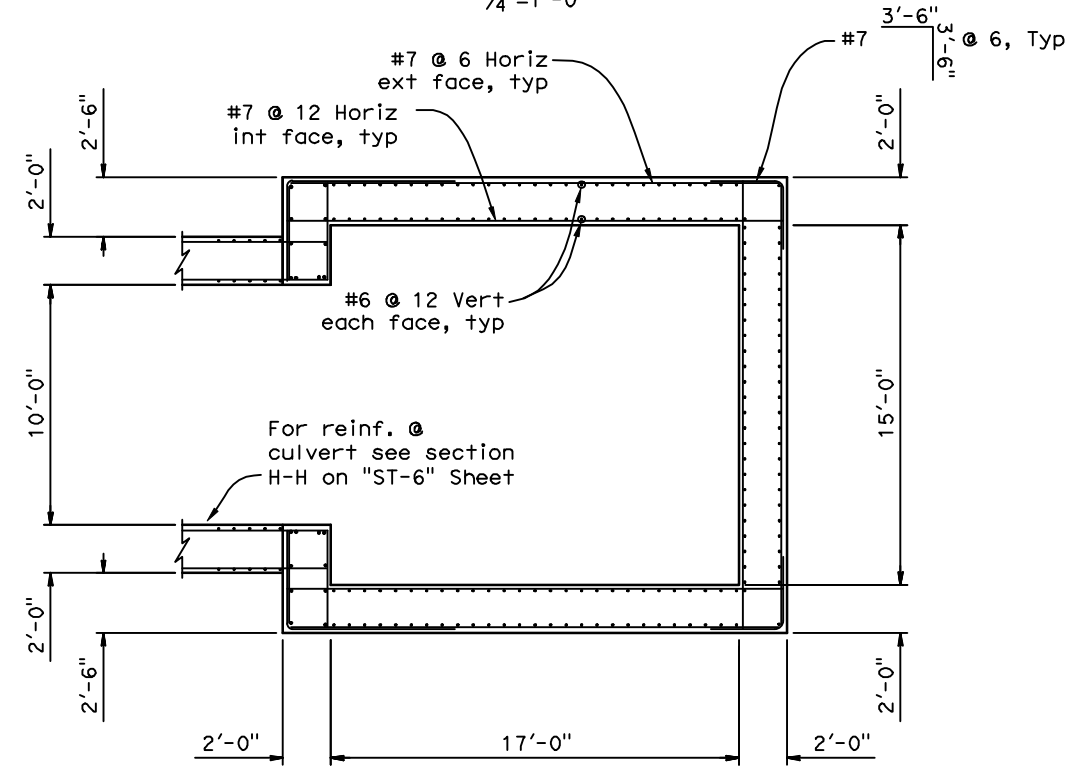
NOTES:

1. For Locations of Sections 'J-J', 'K-K', and 'L-L' see "ST-2 And "ST-5" Sheets.



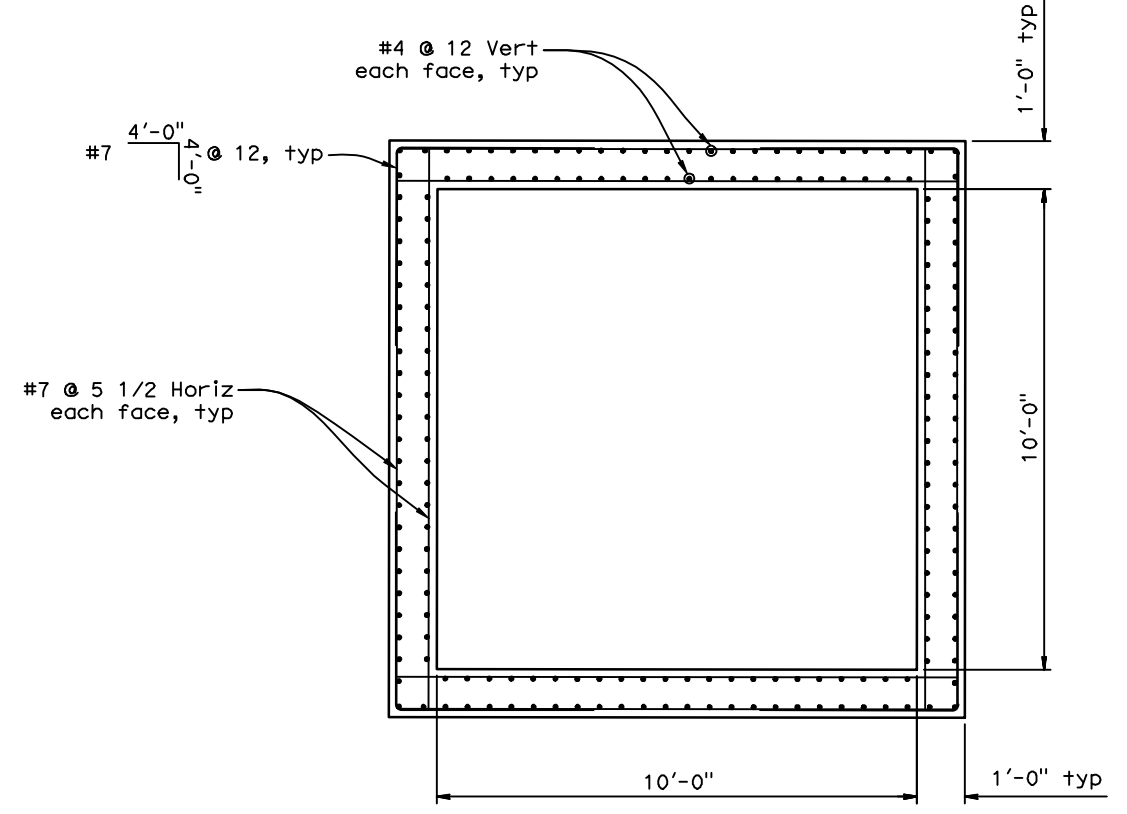
SECTION J-J

1/4"=1'-0"



SECTION K-K

1/4"=1'-0"



SECTION L-L

1/4"=1'-0"

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

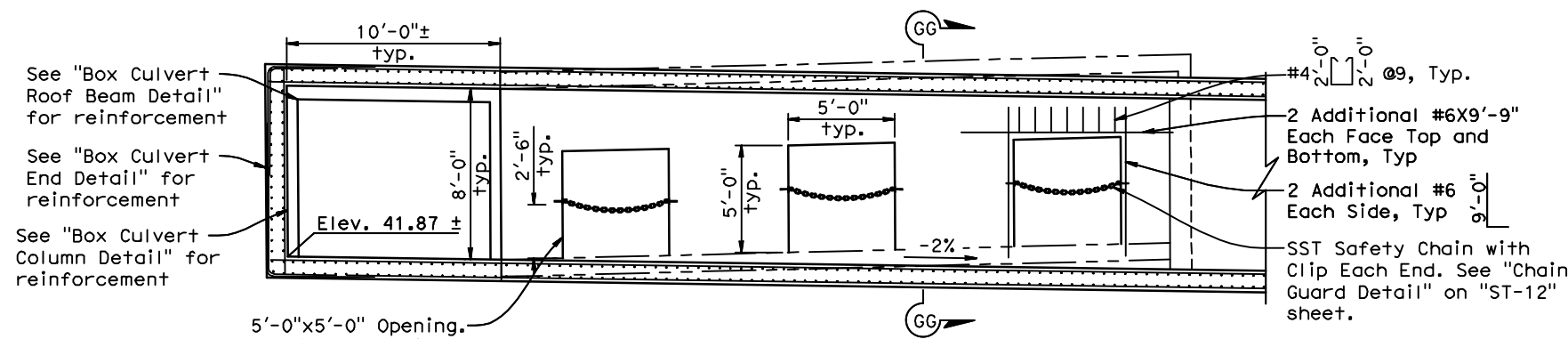
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-7
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	M. PUGH PROJECT ENGINEER	POST MILES R22.56	WET PIT & INLET SECTIONS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 9 OF 47

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:18

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	386	423

REGISTERED CIVIL ENGINEER DATE 5/1/12
 9/04/12
 PLANS APPROVAL DATE
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



SECTION W-W
 $\frac{1}{4}''=1'-0''$

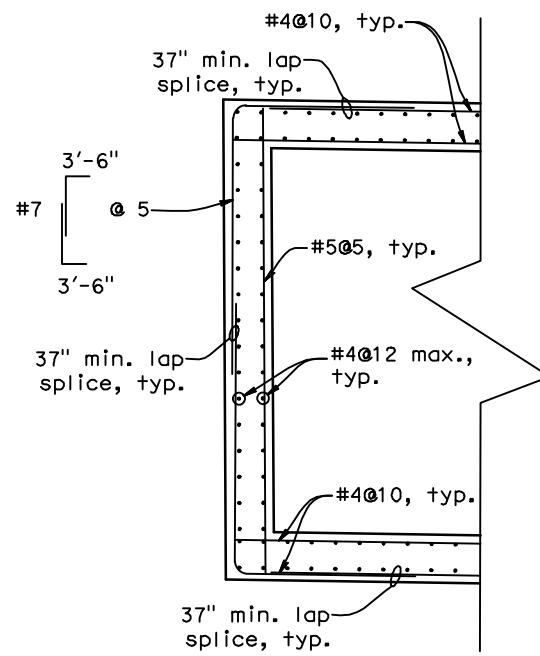
See "Box Culvert Roof Beam Detail" for reinforcement
 See "Box Culvert End Detail" for reinforcement
 See "Box Culvert Column Detail" for reinforcement

5'-0"x5'-0" Opening. Provide Bentonite Waterstop Around Perimeter of Opening in Existing Wall. See "Opening Detail" on "ST-21" Sheet

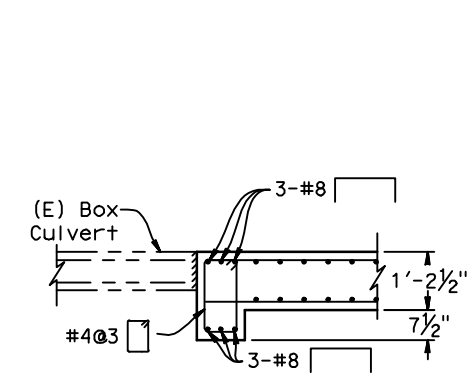
#4 @ 9", Typ.
 2 Additional #6X9'-9" Each Face Top and Bottom, Typ
 2 Additional #6 Each Side, Typ
 SST Safety Chain with Clip Each End. See "Chain Guard Detail" on "ST-12" sheet.

NOTES:

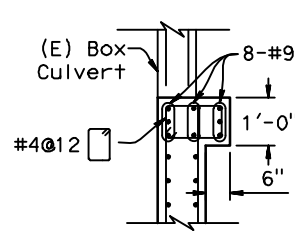
1. For Location of Section '0-0', See "ST-3" Sheet.
2. For Location of Section 'N-N', See "ST-5" Sheet.
3. For location of Section 'W-W', See "ST-20" Sheet.
4. For Section 'GG-GG', See "ST-21" Sheet.
5. For typical construction joint detail see "ST-9" sheet. All construction joints shall have bentonite waterstops centered in joints.



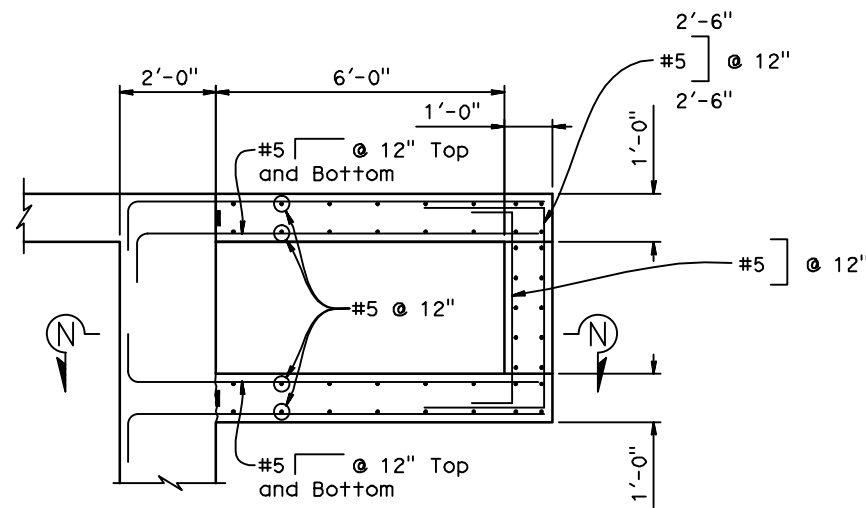
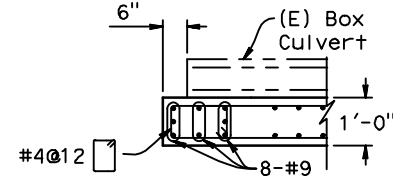
BOX CULVERT END DETAIL
 $\frac{1}{2}''=1'-0''$



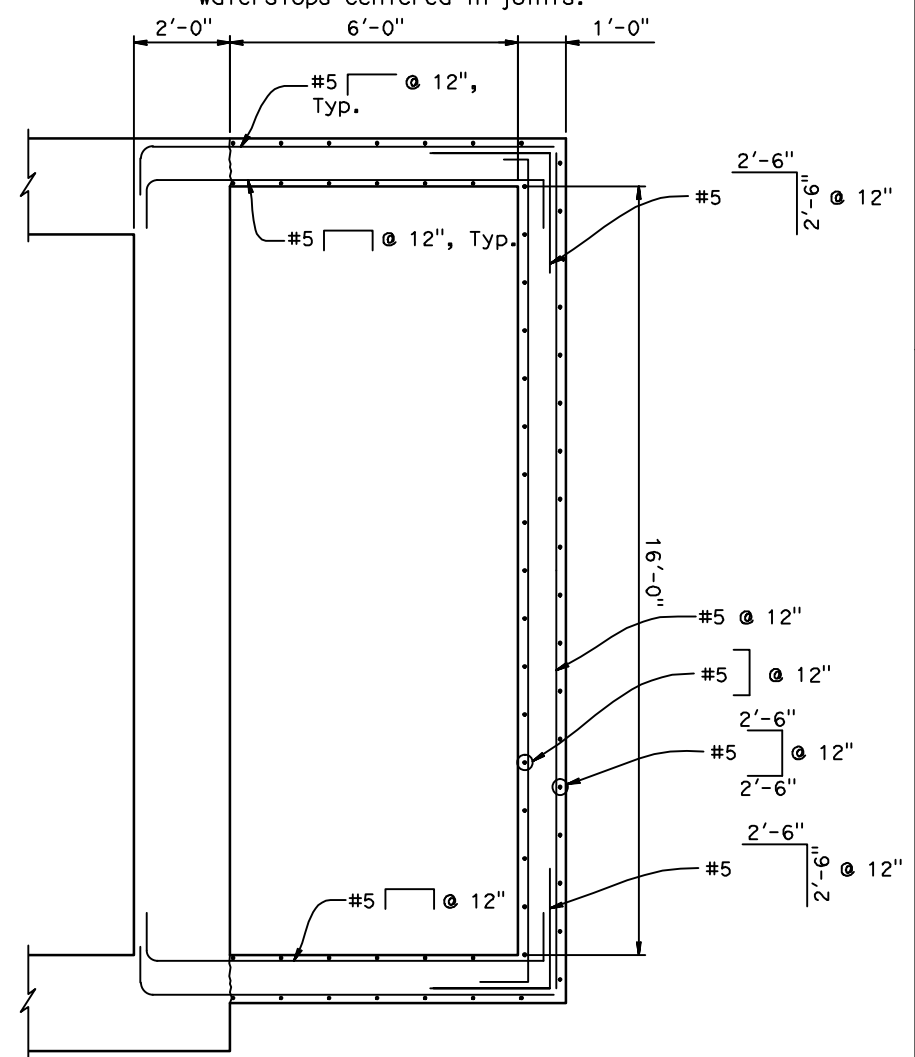
BOX CULVERT ROOF BEAM DETAIL
 $\frac{1}{2}''=1'-0''$



BOX CULVERT END DETAIL
 $\frac{1}{2}''=1'-0''$



SECTION 0-0
 $\frac{1}{2}''=1'-0''$



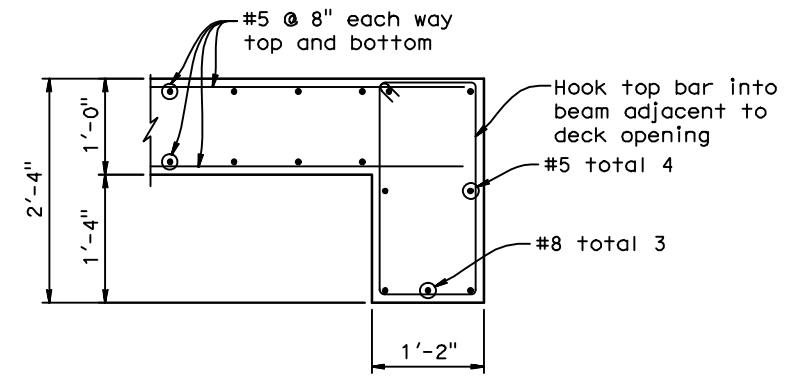
SECTION N-N
 $\frac{1}{2}''=1'-0''$

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

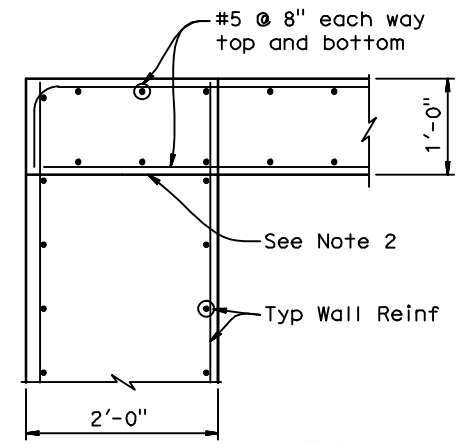
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT DISCHARGE BOX SECTIONS	ST-8
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 10 OF 47

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:19

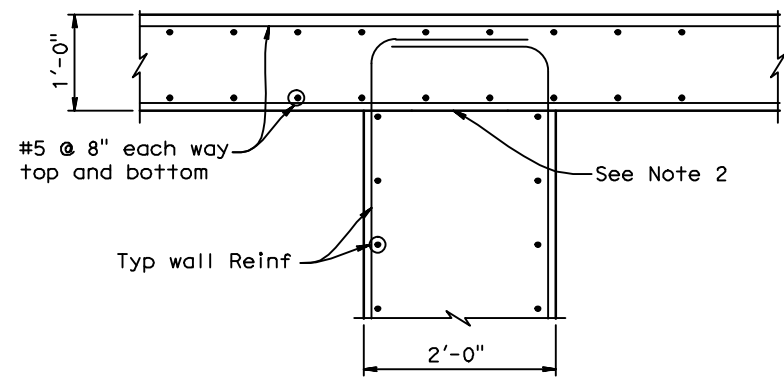
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	387	423
REGISTERED CIVIL ENGINEER			DATE	9/4/12	
PLANS APPROVAL DATE			9/04/12		
The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					



SECTION P-P
1"=1'-0'



SECTION Q-Q

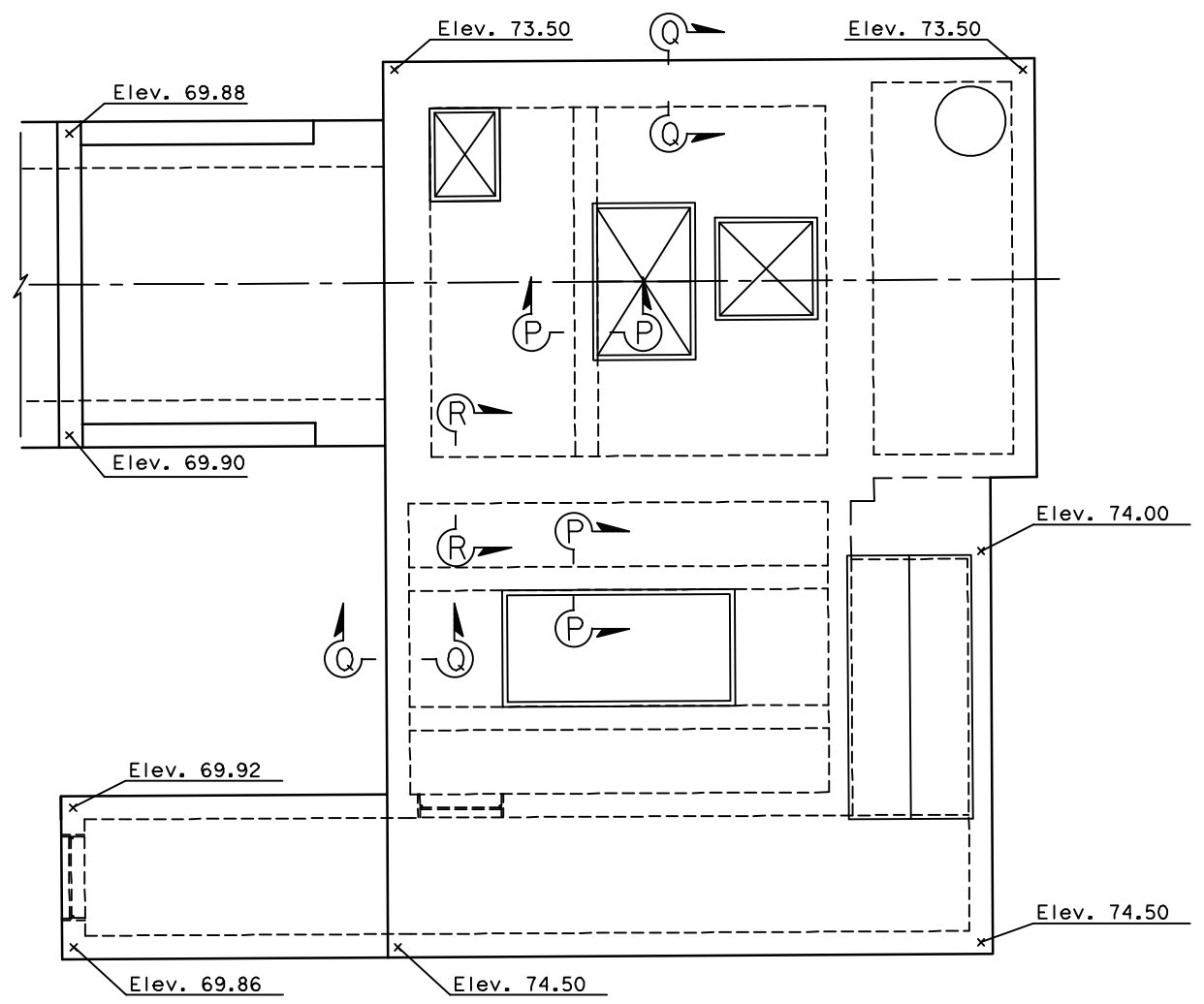


SECTION R-R
1"=1'-0'

Note:
2'-0" wall shown. 1'-0" Wall Similar.

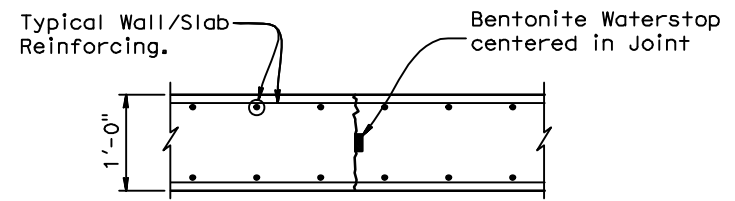
NOTES:

- All roof slab reinforcement are #5 @ 8 eachway unless otherwise noted.
- All Joints shall have bentonite waterstops centered in wall. See construction joint detail on "ST-9" Sheet.



ROOF DETAILS
1/4"=1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL



NOTES:

- Continue all reinforcing through joint.
- Provide bentonite waterstop at all joints. Waterstop shall be Volclay Waterstop - RX, or equal
- A rough surface of exposed concrete aggregates shall be produced using surface retardant at construction joints within 24 hours after placing, retarded surface mortar shall be removed by water jetting or stiff brushing to expose aggregate.

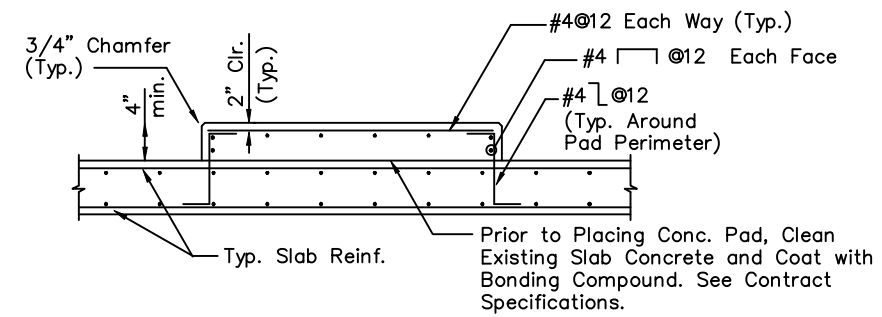
CONSTRUCTION JOINT DETAIL
N.T.S.

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-9		
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION	R22.56	ROOF DETAILS				
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 11 OF 47		
					ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163W-ST-9.dwg		2/24/12	3/28/12	5/1/12	9/7/12

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:19

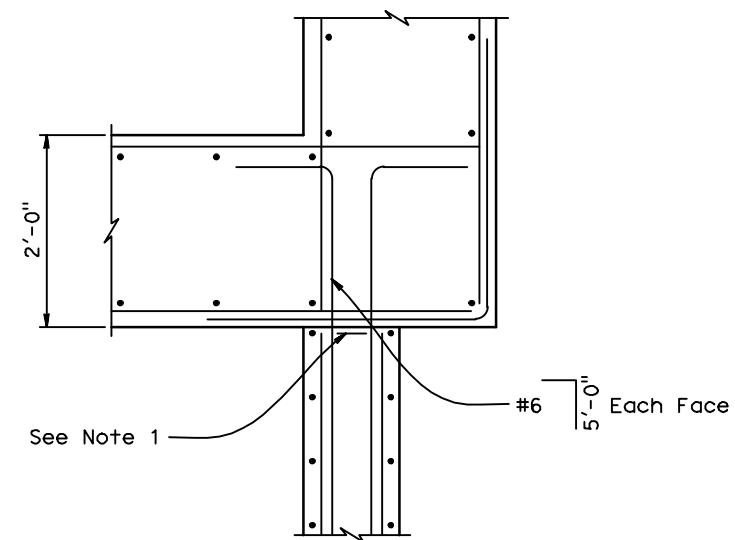
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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REGISTERED CIVIL ENGINEER			DATE	9/4/12	
PLANS APPROVAL DATE			9/04/12		
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DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					

Note: The Contractor Shall Verify All Pad Dimensions with Equipment Manufacturer Prior to Placing Pad



EQUIPMENT PAD DETAIL

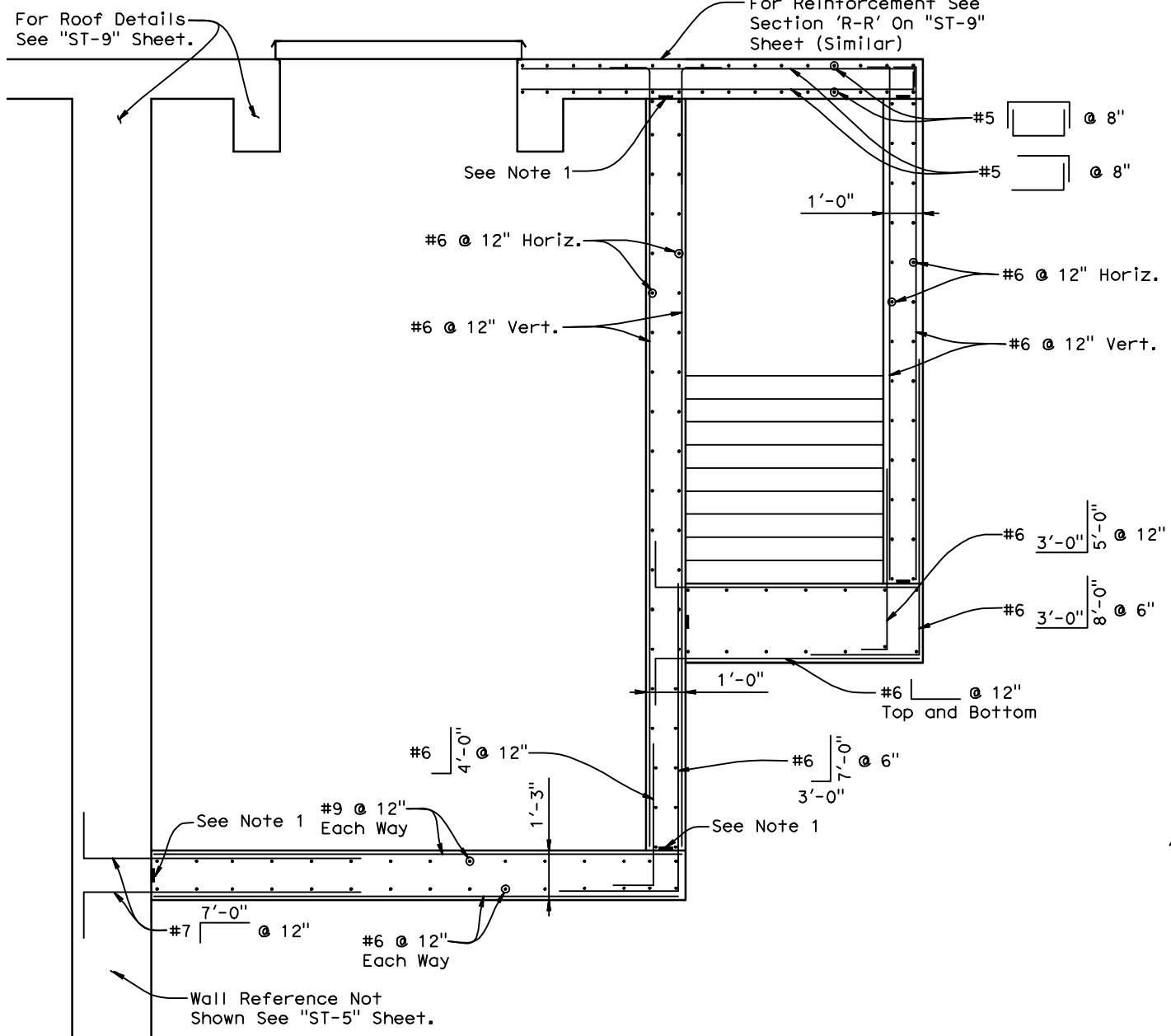
Not to Scale



WALL INTERSECTION

DETAIL 1

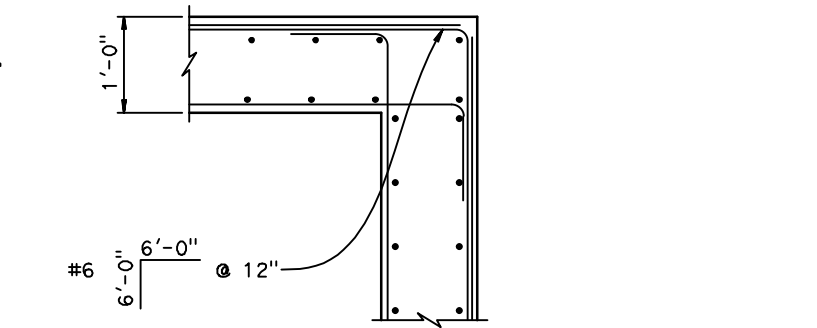
1"=1'-0'



SECTION S-S

1/2"=1'-0"

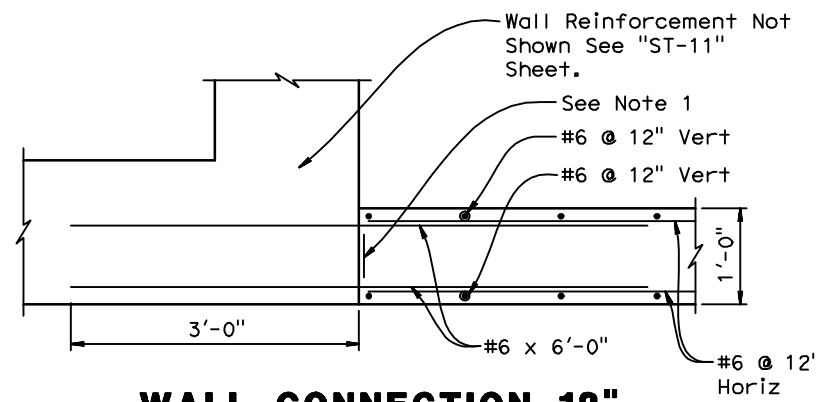
NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL



WALL CORNER

DETAIL 2

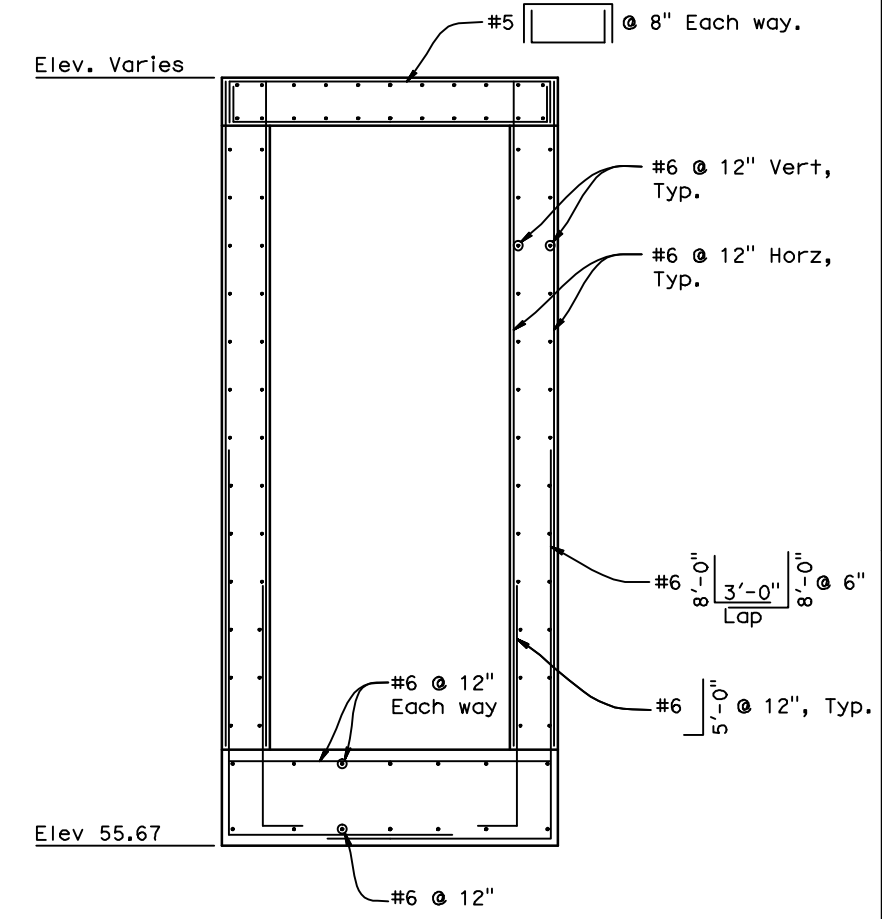
1"=1'-0'



WALL CONNECTION 12" WALL TO 24" WALL

DETAIL 3

1"=1'-0'



SECTION V-V

- Note:
1. All Joints shall have bentonite Waterstops centered in wall. See construction joint detail on "ST-9" Sheet.
 2. Equipment pads not shown. Contractor to equipment manufacturer prior to placing reinforcement. See "Equipment Pad Detail" this sheet.
 3. For location of "S-S", see "ST-1" Sheet.
 4. For location of "V-V", see "ST-11" Sheet.

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-10
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PLANS AND SPECS COMPARED	POST MILES R22.56	CONTROL ROOM DETAILS AND SECTIONS			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 12 OF 47

TIME PLOTTED => 19:20 USERNAME => Carrick, DATE PLOTTED => 9-Sep-12

- Notes:**
1. For Pipe Handrailing Location see "ST-3" Sheet.
 2. For typical construction joint detail see "ST-9" sheet. All construction joints shall have bentonite waterstops centered in joints.
 3. For Section "V-V", See "ST-10" Sheet.

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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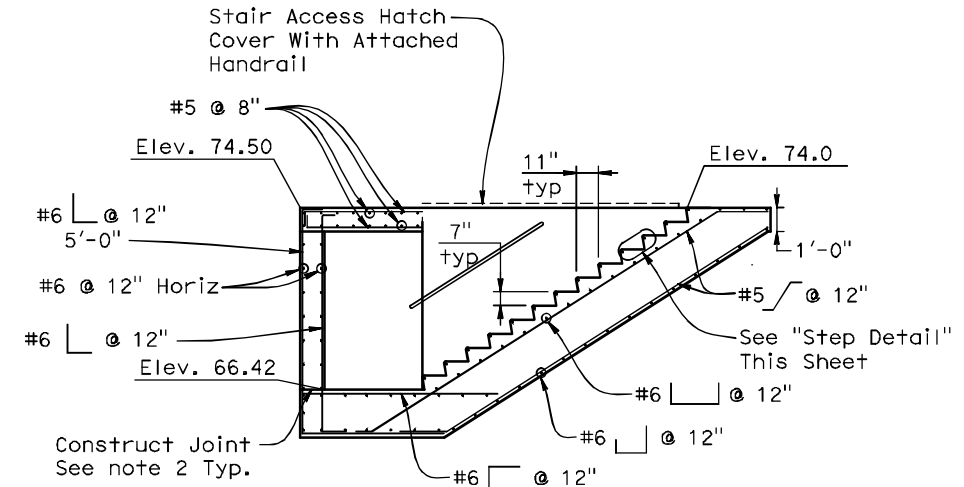
REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

9/04/12
 PLANS APPROVAL DATE

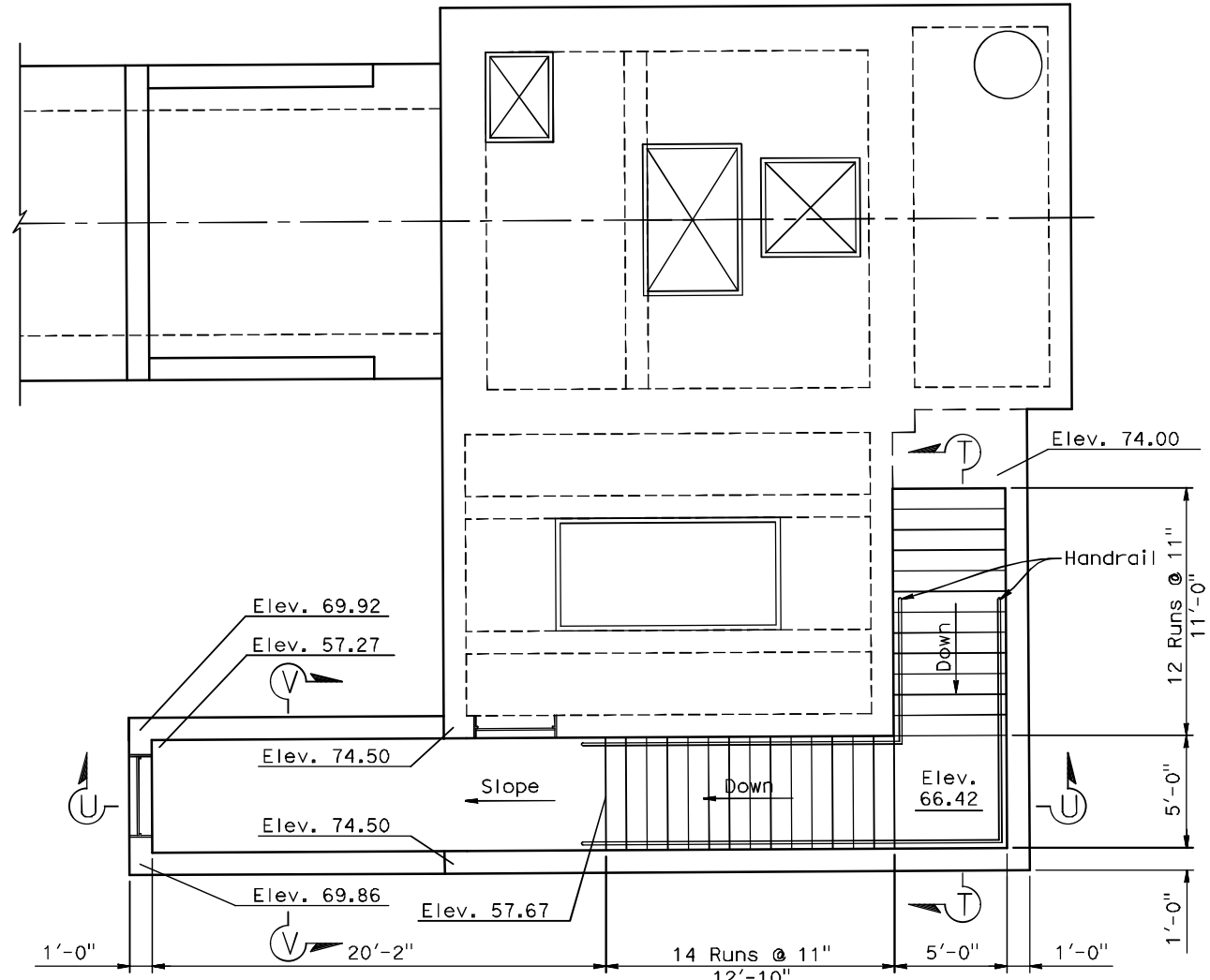
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

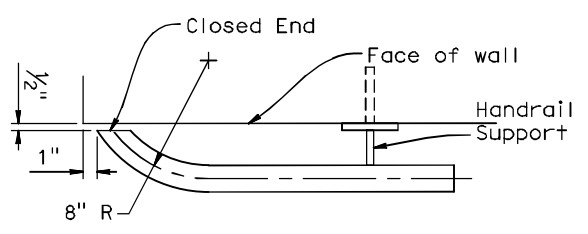


SECTION T-T
 1/4"=1'-0"



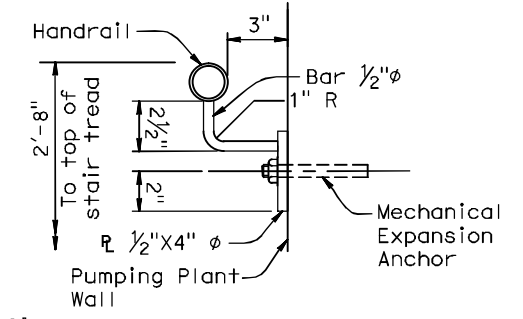
STAIR LAYOUT
 1/4"=1'-0"

Note:
 Concrete deck over stair & walk not shown.



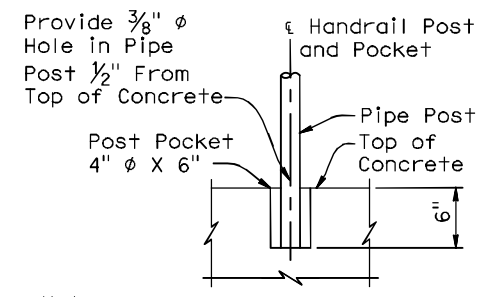
HANDRAIL RETURN WALL

NOTE:
 No Scale
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL



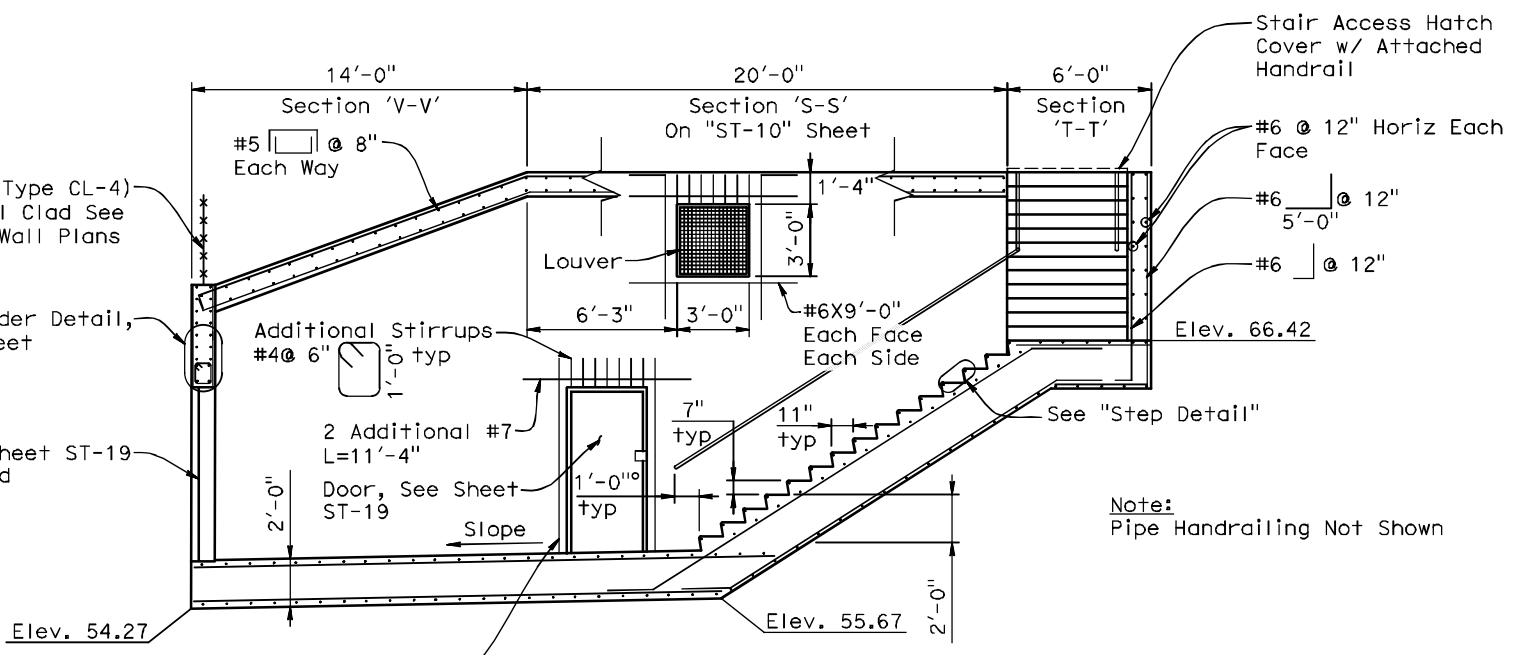
HANDRAIL SUPPORT
 No Scale

Note:
 Provide supports at top and bottom of handrail and at 4'-0" on center maximum

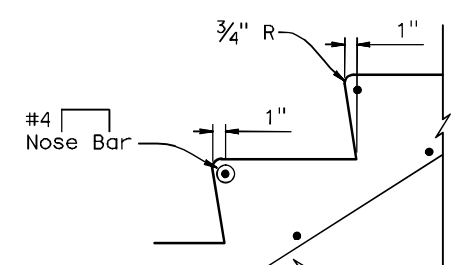


POST POCKET DETAIL
 No Scale

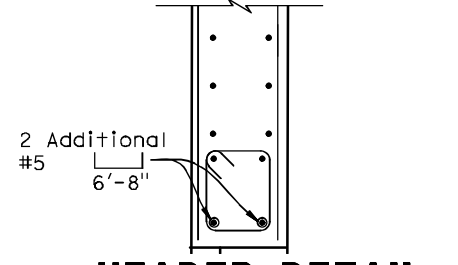
Note:
 Pack Post Pocket with Epoxy Grout, Typ



SECTION U-U
 1/4"=1'-0"



STEP DETAIL
 N.T.S.



HEADER DETAIL
 N.T.S.

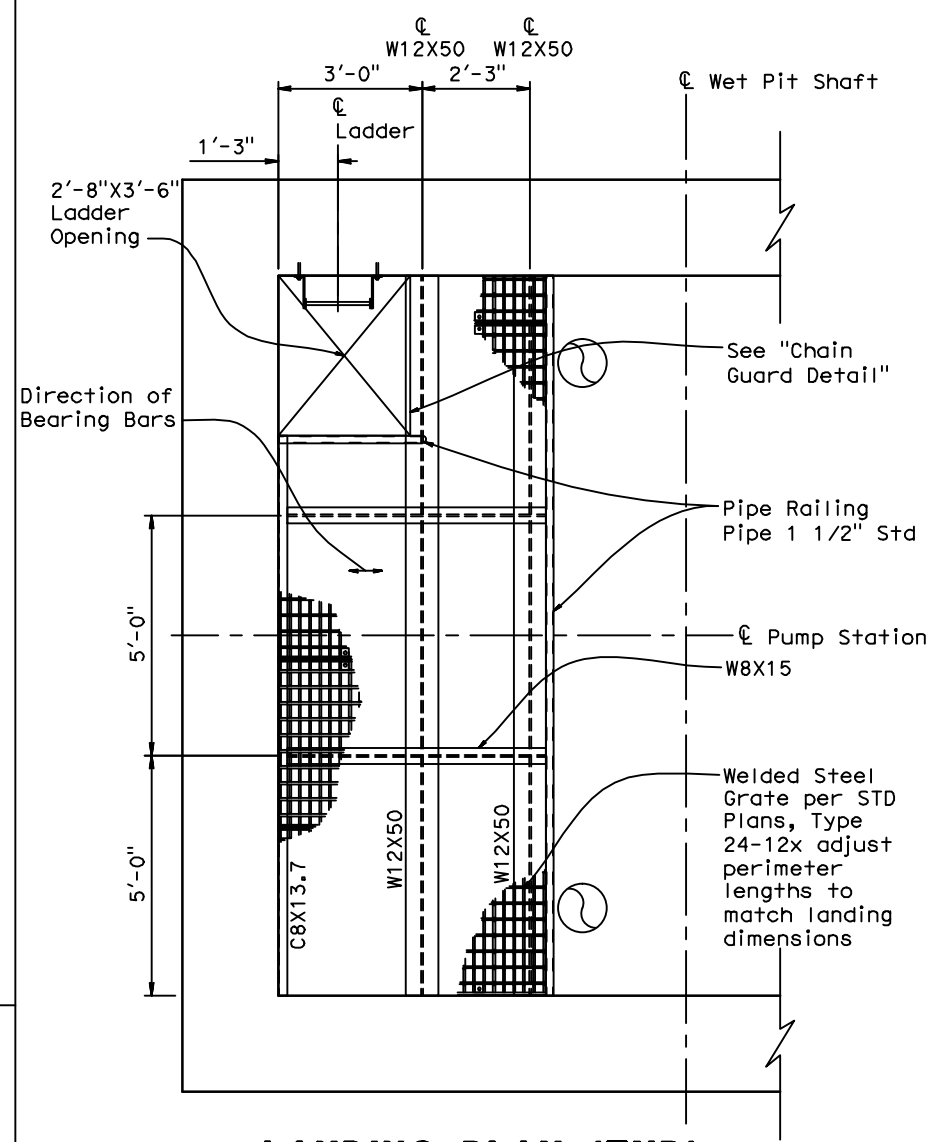
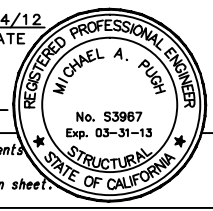
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-11
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	R22.56	STAIR LAYOUT AND SECTIONS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 13	OF 47	

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3

FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163WD1-ST-11.dwg

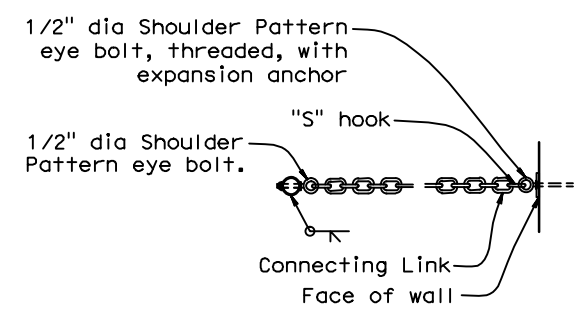
USERNAME => Carrick, DATE PLOTTED => 9-Sep-12

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	390	423
REGISTERED CIVIL ENGINEER			DATE	9/4/12	
PLANS APPROVAL DATE			9/04/12		
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DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					



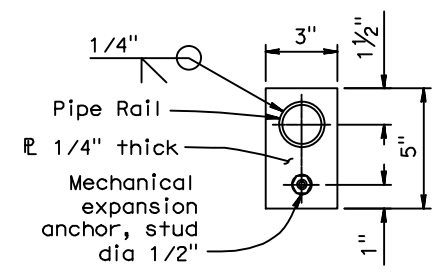
LANDING PLAN (TYP)

1/2" = 1'-0"



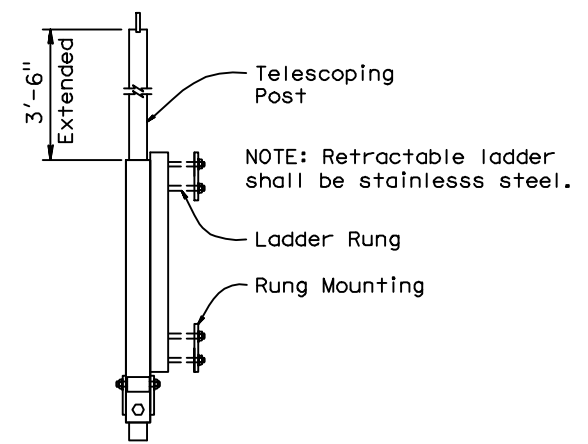
CHAIN GUARD DETAIL

NTS



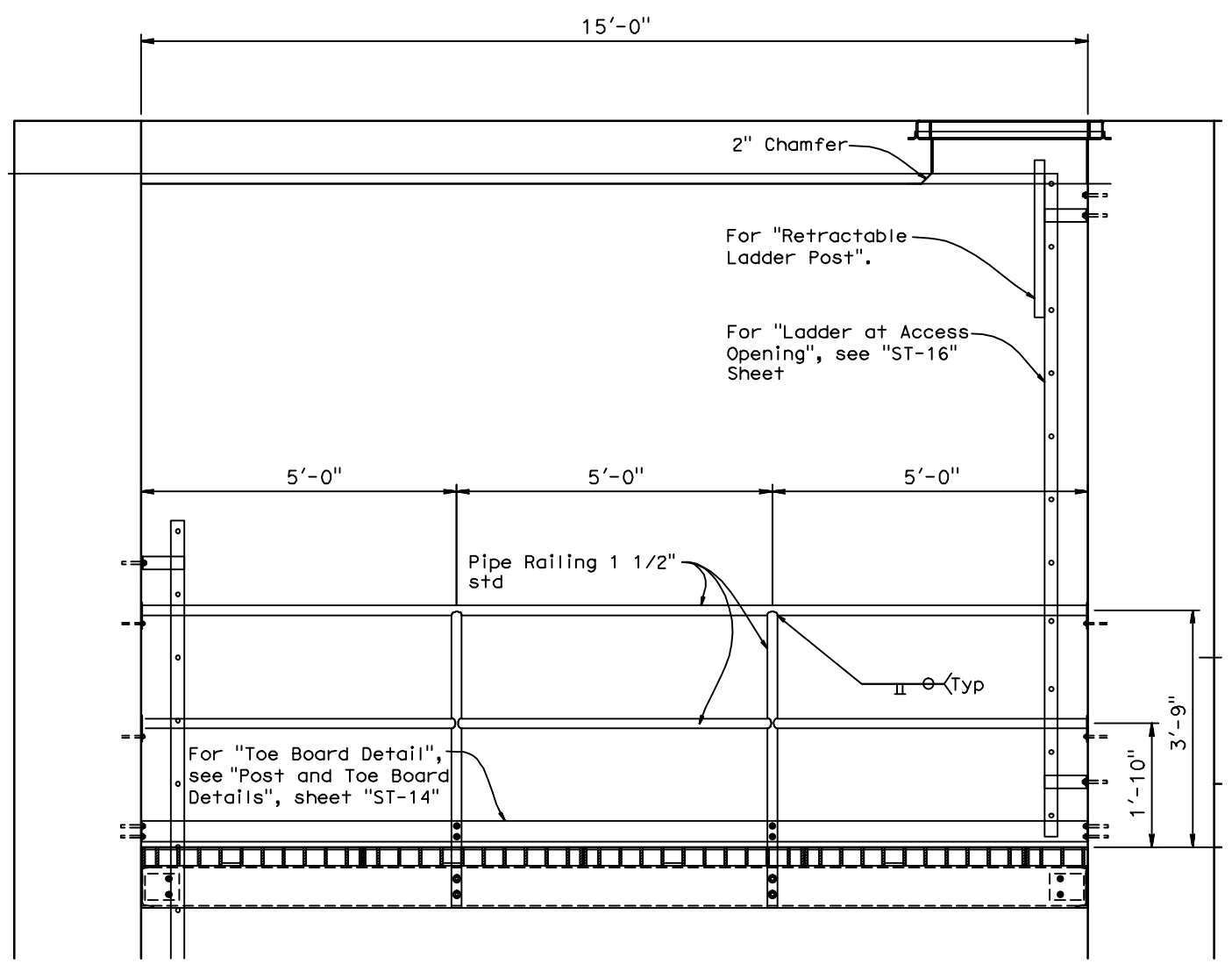
RAIL TO CONCRETE ANCHOR

1/4" = 0'-1"



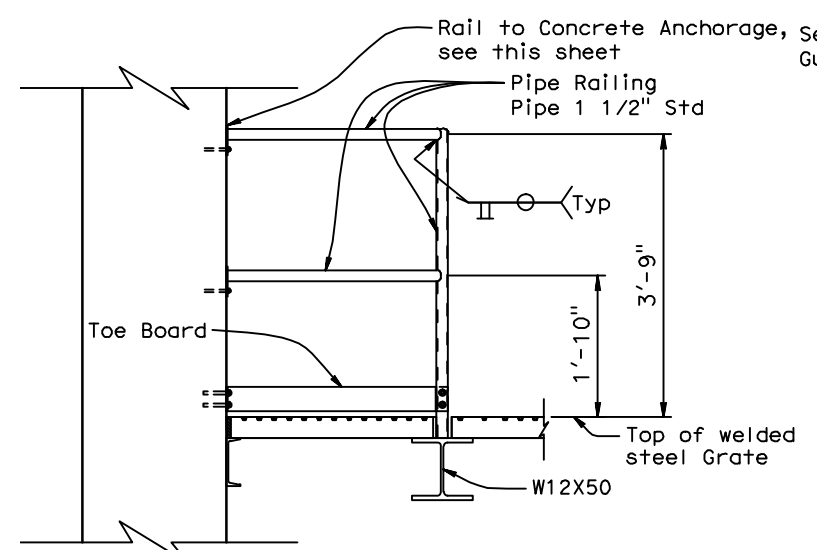
RETRACTABLE LADDER POST

NTS



LANDING ELEVATION (TYP)

3/4" = 1'-0"



RAILING LADDER (TYP)

3/4" = 1'-0"

NOTE:
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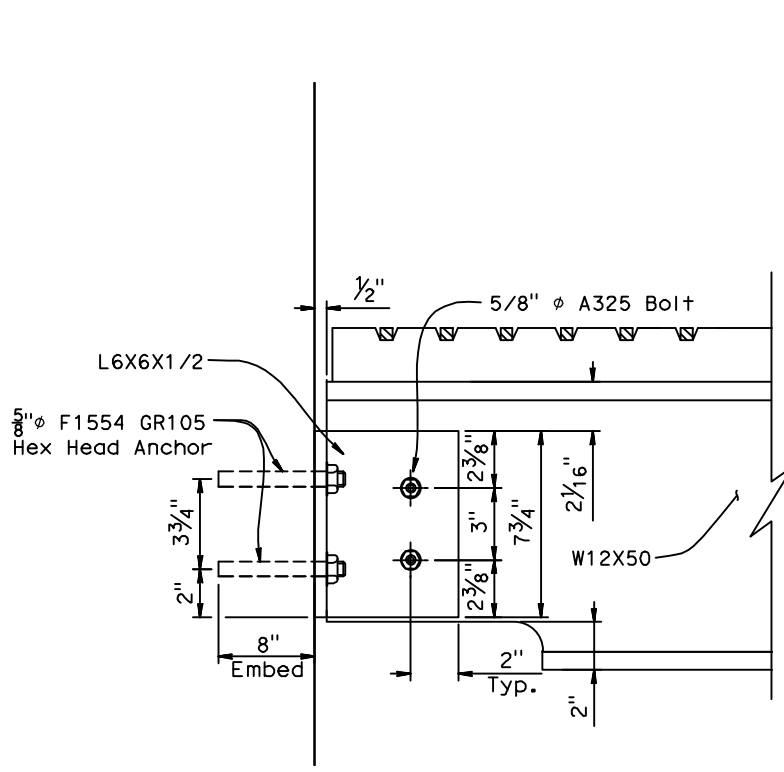
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-12
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION		R22.56	LANDING DETAILS NO. 1		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 14 OF 47

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12, TIME PLOTTED => 19:22

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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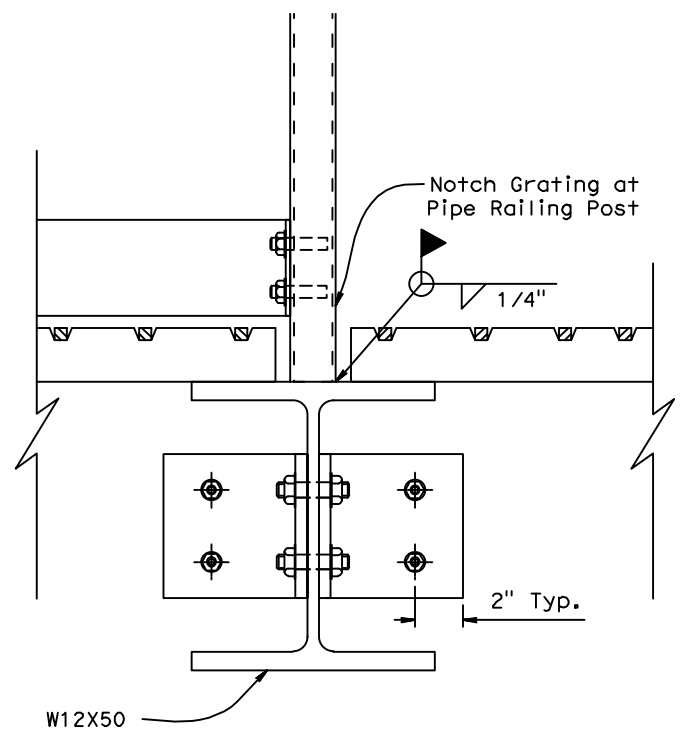
REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. S3967 Exp. 03-31-13
 STATE OF CALIFORNIA
 The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

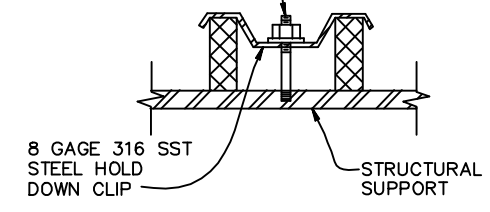


GRATE SUPPORT AT PIPE RAILING POST

3"=1'-0"

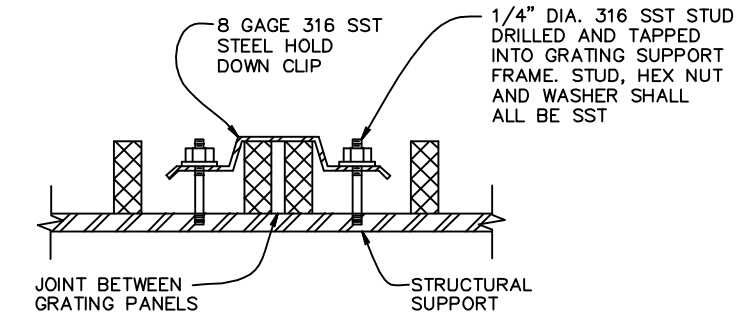


1/4" DIA. 316 SST STUD DRILLED AND TAPPED INTO GRATING SUPPORT FRAME. STUD, HEX NUT AND WASHER SHALL ALL BE SST



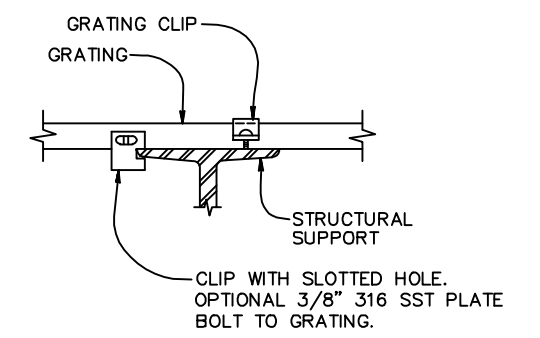
SADDLE CUP

NO SCALE



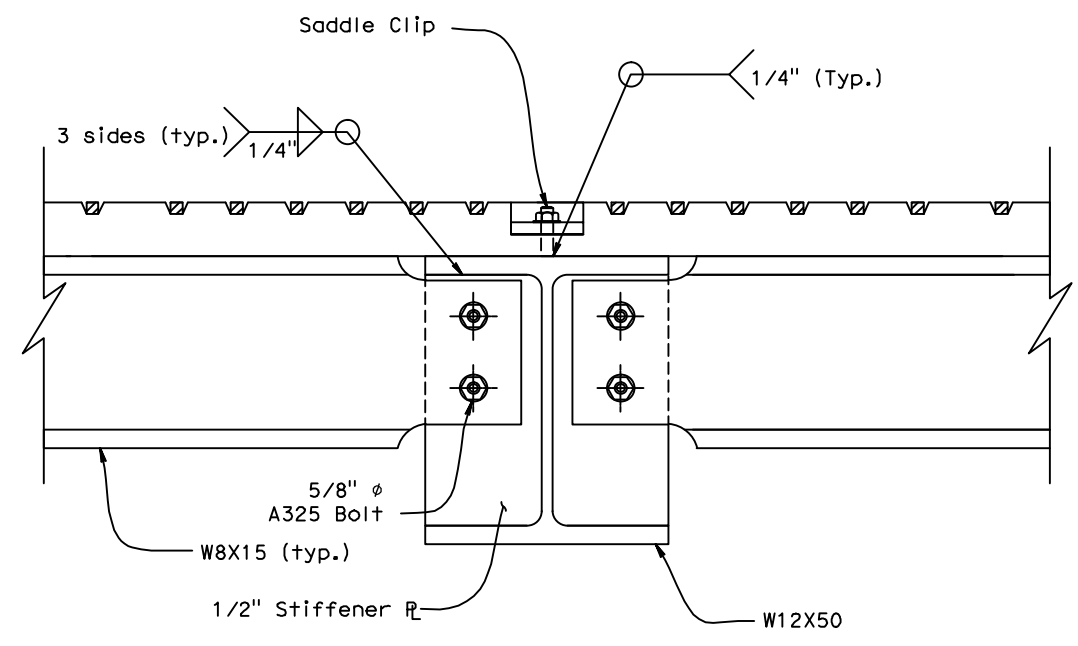
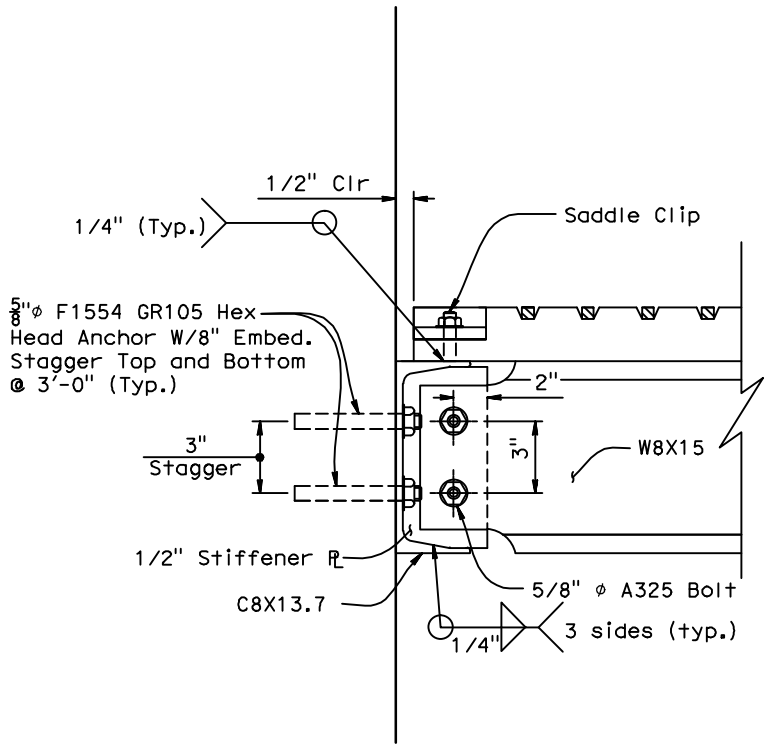
SADDLE CUP AT GRATE JOINT

NO SCALE



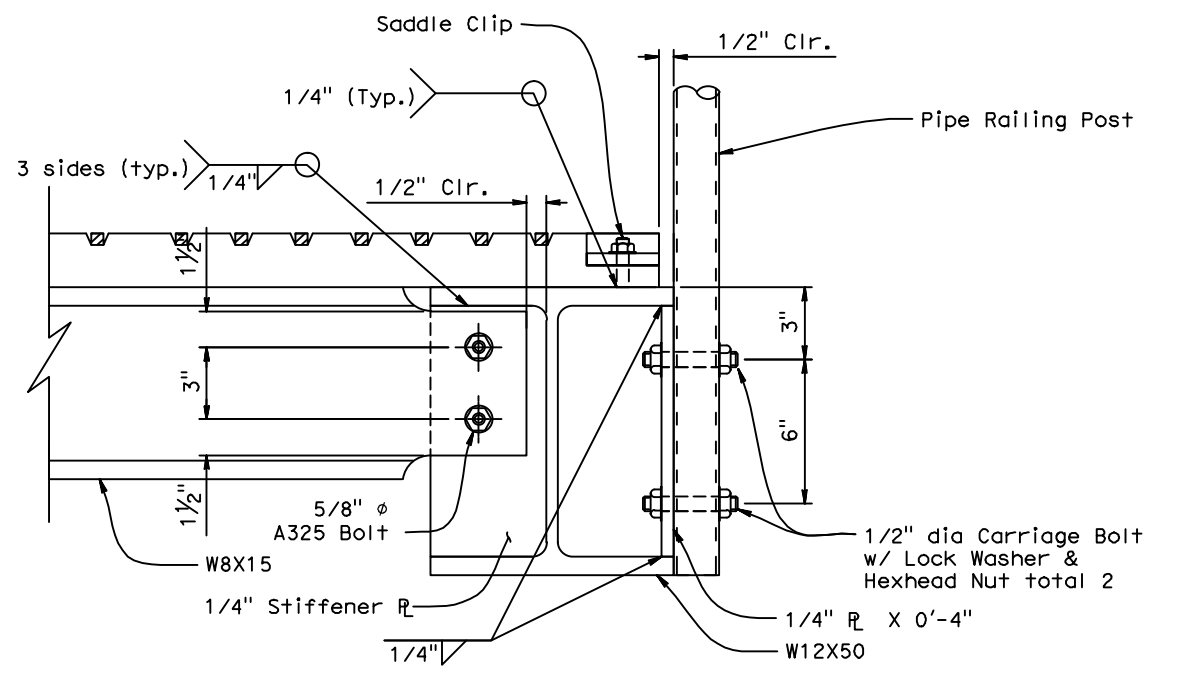
GRATING ATTACHED

NO SCALE



GRATE SUPPORT

3"=1'-0"



NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

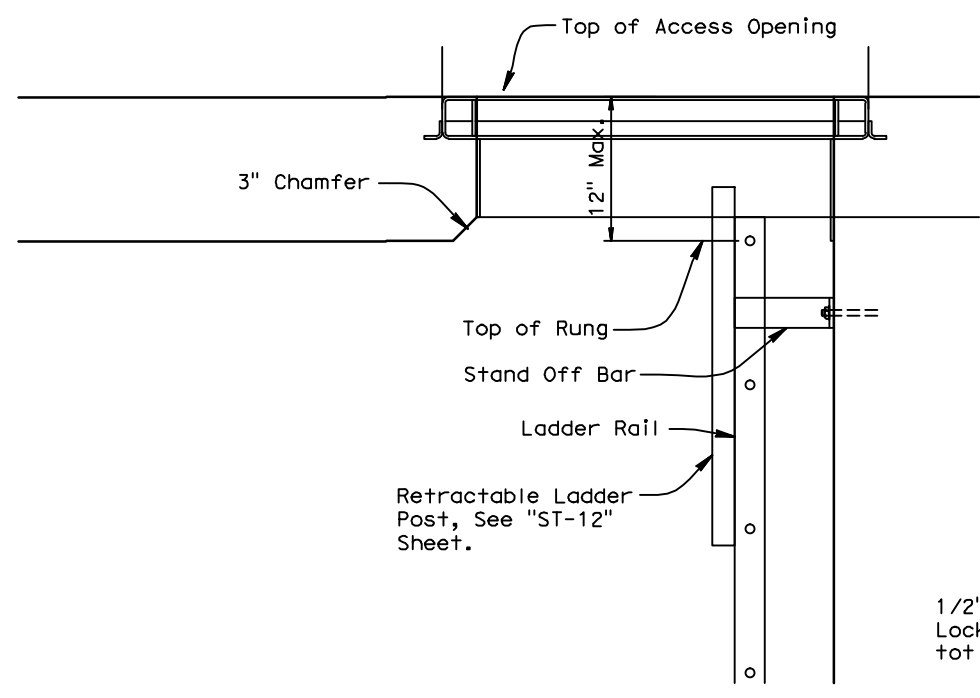
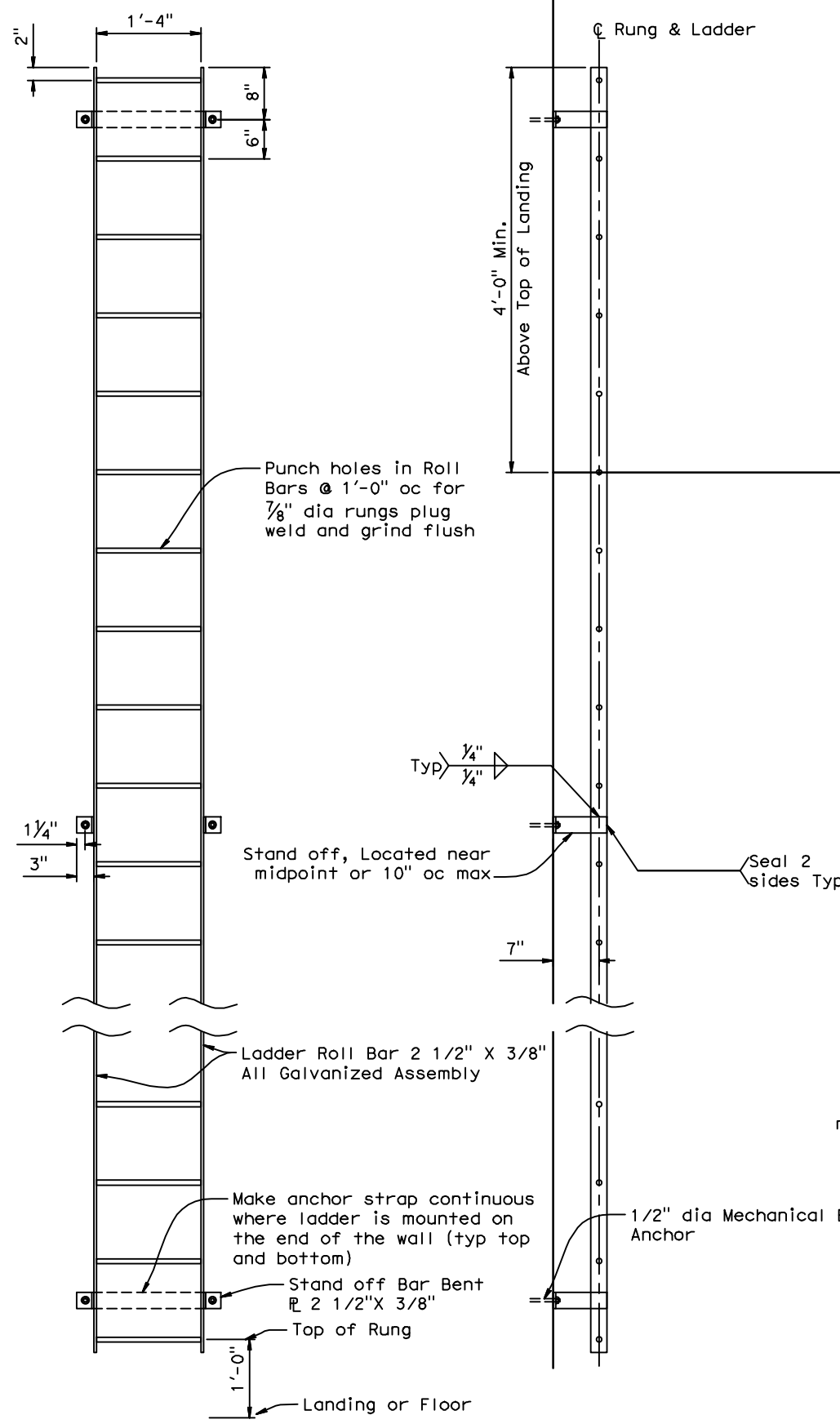
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT LANDING DETAILS NO. 2	ST-13
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 15	OF 47

FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163WD1-ST-13.dwg
 USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:24

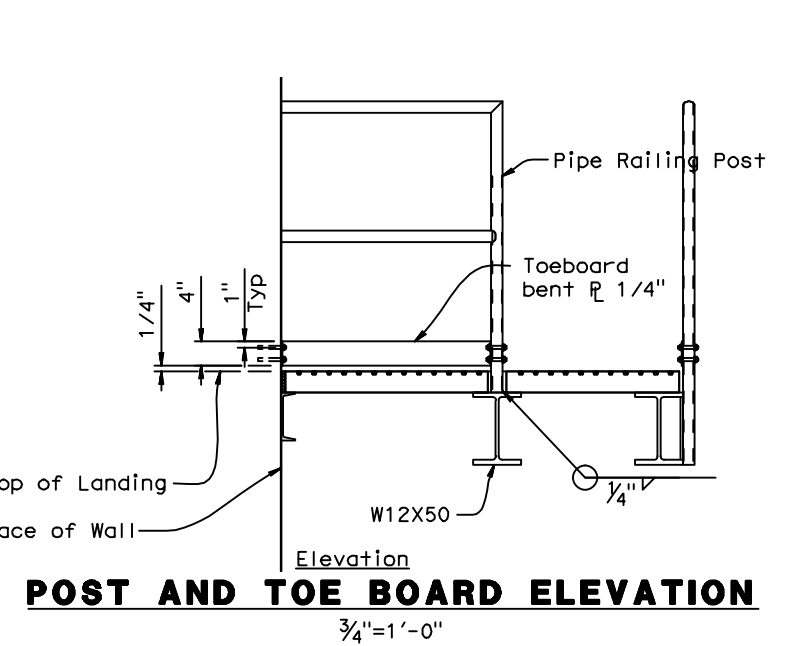
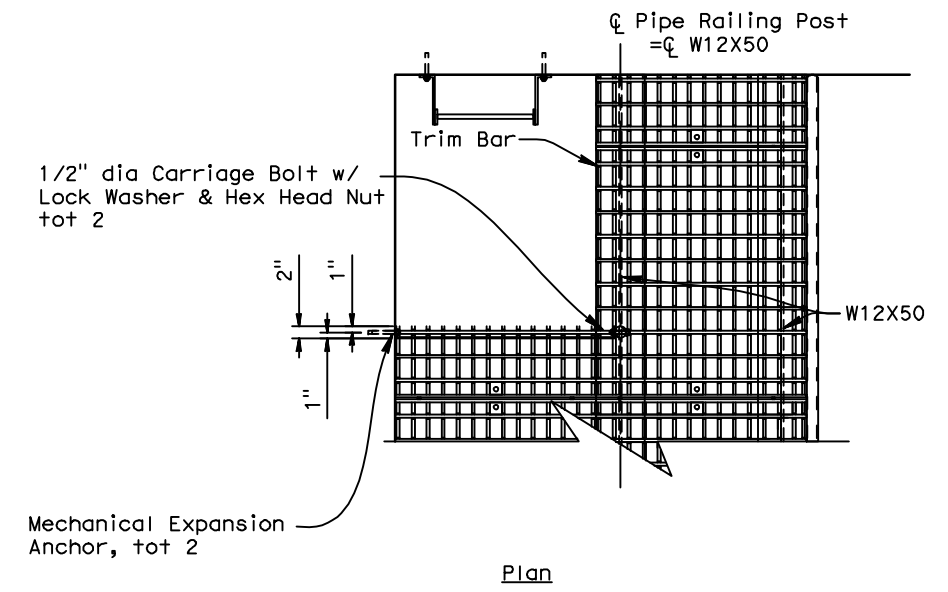
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	392	423

REGISTERED CIVIL ENGINEER
 DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. S3967
 Exp. 03-31-13
 STATE OF CALIFORNIA
 STRUCTURAL ENGINEER

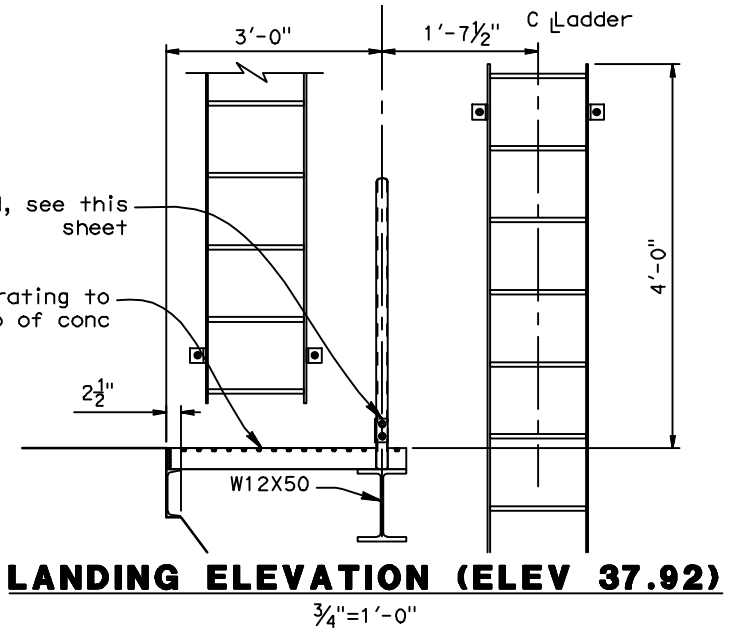
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



LADDER AT ACCESS OPENING
 1/2"=1'-0"



POST AND TOE BOARD ELEVATION
 3/4"=1'-0"



LANDING ELEVATION (ELEV 37.92)
 3/4"=1'-0"

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

STEEL LADDER
 1"=1'-0"

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-14
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION		POST MILES R22.56	LANDING DETAILS NO. 3		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 16 OF 47

USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:25

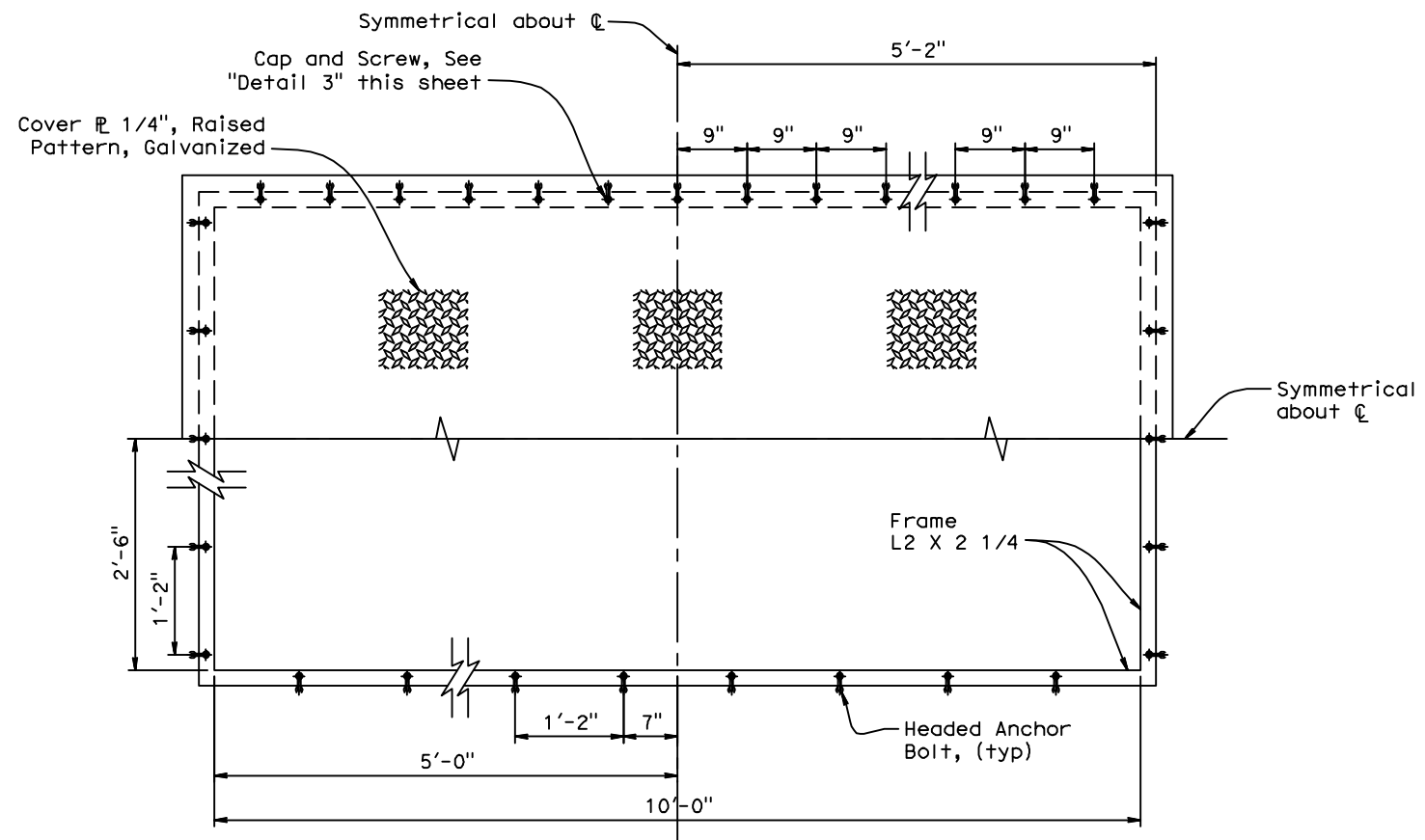
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	393	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. 53967 Exp. 03-31-13
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 STRUCTURAL
 STATE OF CALIFORNIA

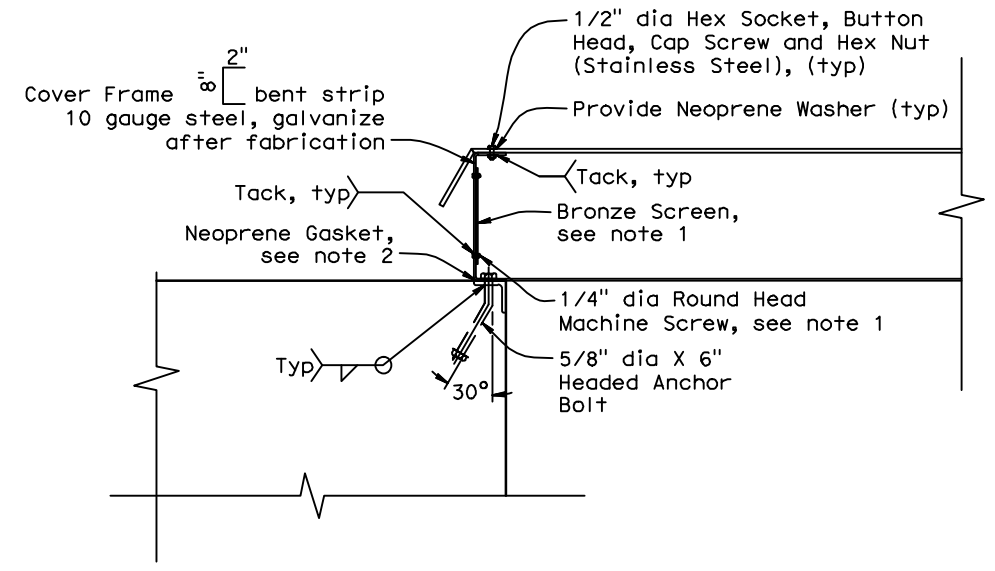
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

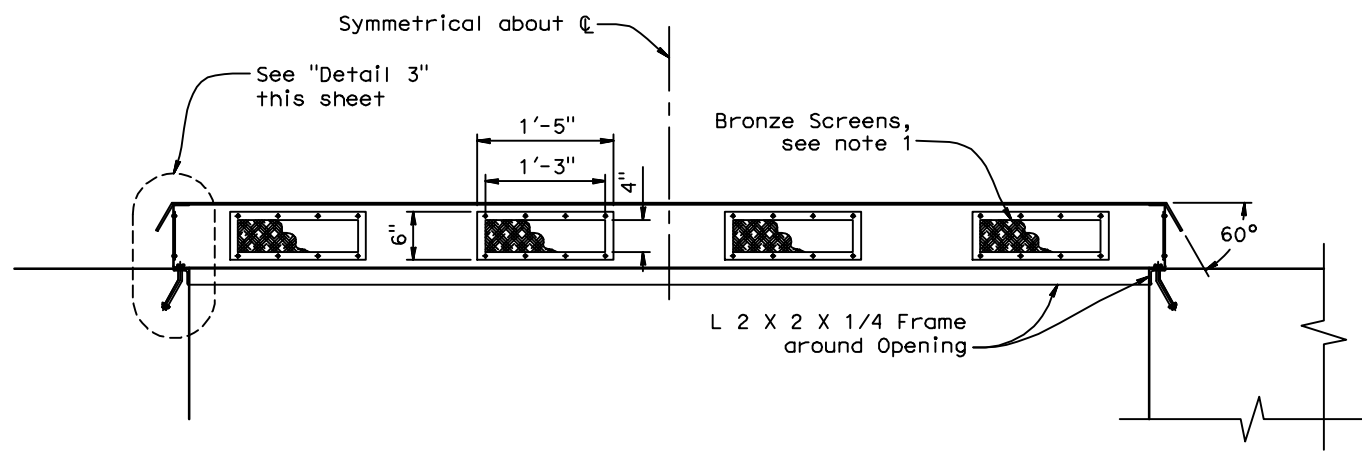


EQUIPMENT ACCESS OPENING AND COVER PLAN
1"=1'-0"



DETAIL '3'
3"=1'-0"

- Notes:**
- Opening shall have Bronze Mesh Screen with 16 mesh/inch with a 0.018" wire diameter. Attach to 1/8" Galvanized Steel Frame. Fasten with 6.4 Brass or Stainless Steel Round Headed Machine Screws and Hex Nuts. Tack welded to outside surface, total 8 per Screen Frame.
 - Provide 2" X 1/8" Neoprene Gasket all around. Seal joints in gasket with Neoprene Caulking Compound.



EQUIPMENT ACCESS OPENING AND COVER ELEVATION
1"=1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-15
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED			R22.56	EQUIPMENT ACCESS COVER DETAILS	
	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED					

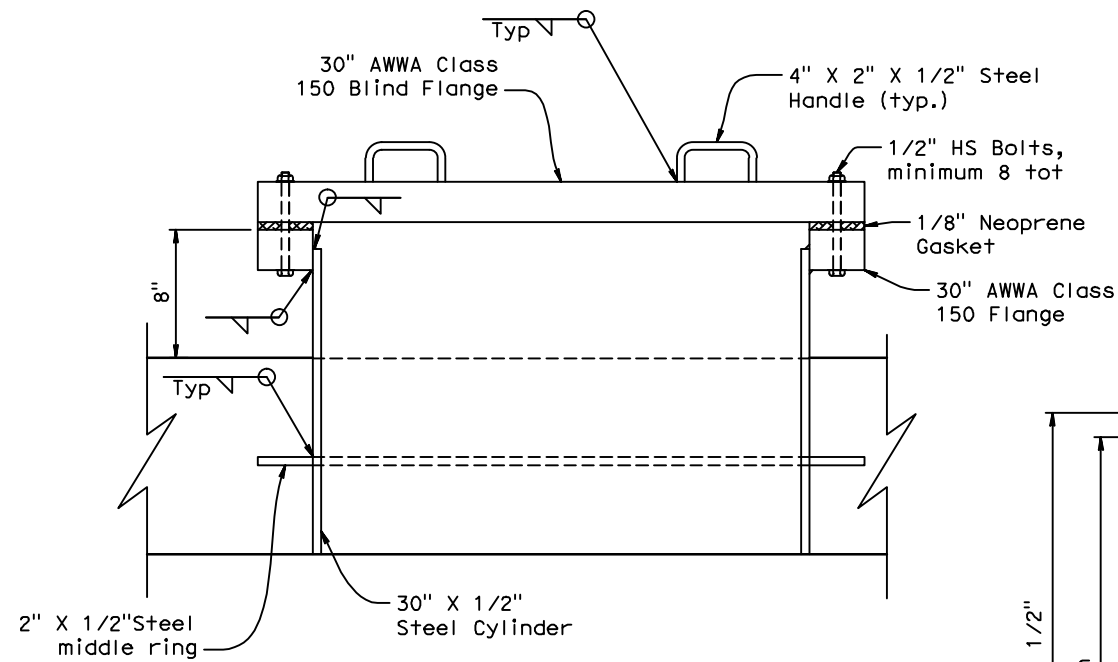
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10) ORIGINAL SCALE IN INCHES FOR REDUCED PLANS 0 1 2 3
 UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304 DISREGARD PRINTS BEARING EARLIER REVISION DATES
 REVISION DATES: 2/24/12, 3/28/12, 5/1/12, 9/7/12 SHEET 17 OF 47
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USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:26

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	394	423

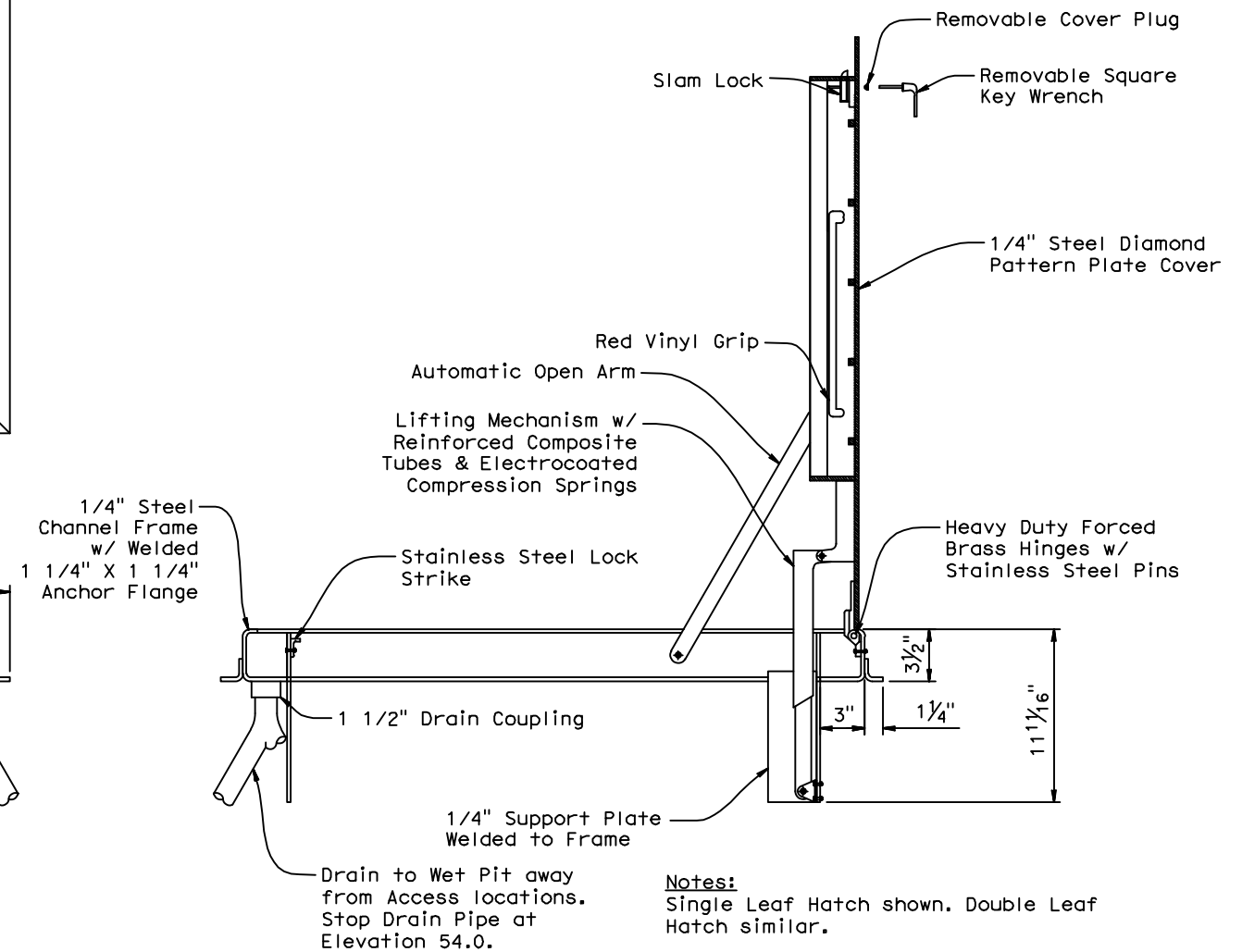
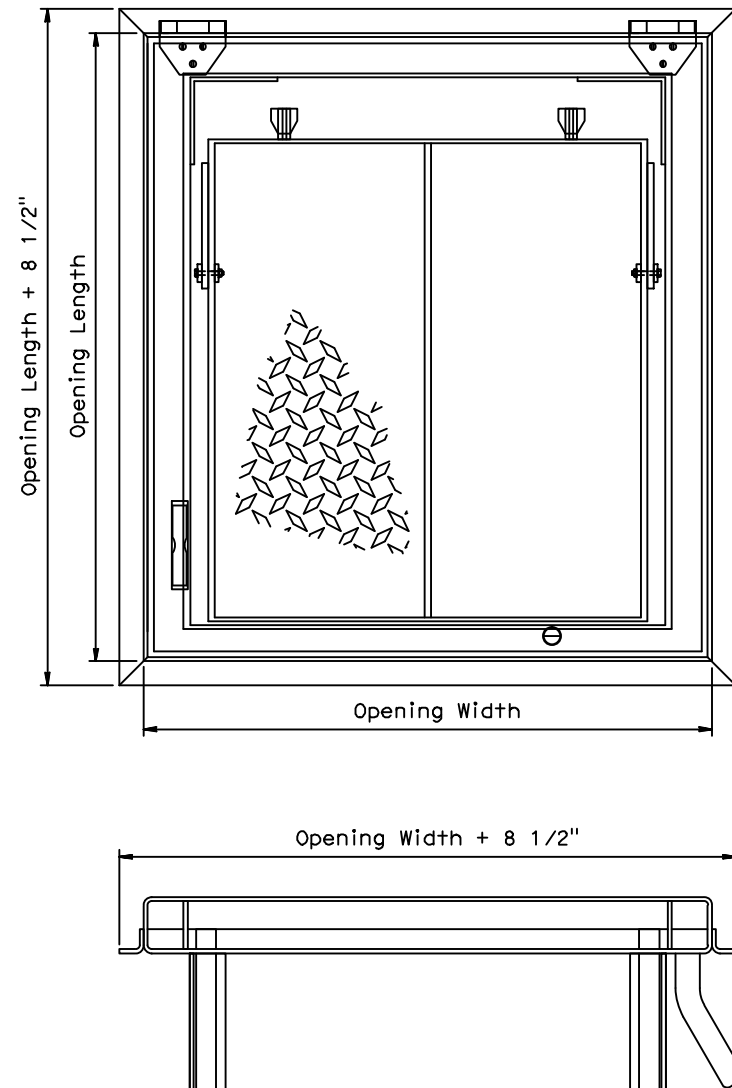
REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. S3967 Exp. 03-31-13
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 STRUCTURAL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



- Notes:
 1. Hot dip galvanized after fabrication.
 2. Provide galvanized bolts, nuts and washers.

PRESSURE MANHOLE DETAIL
 No Scale



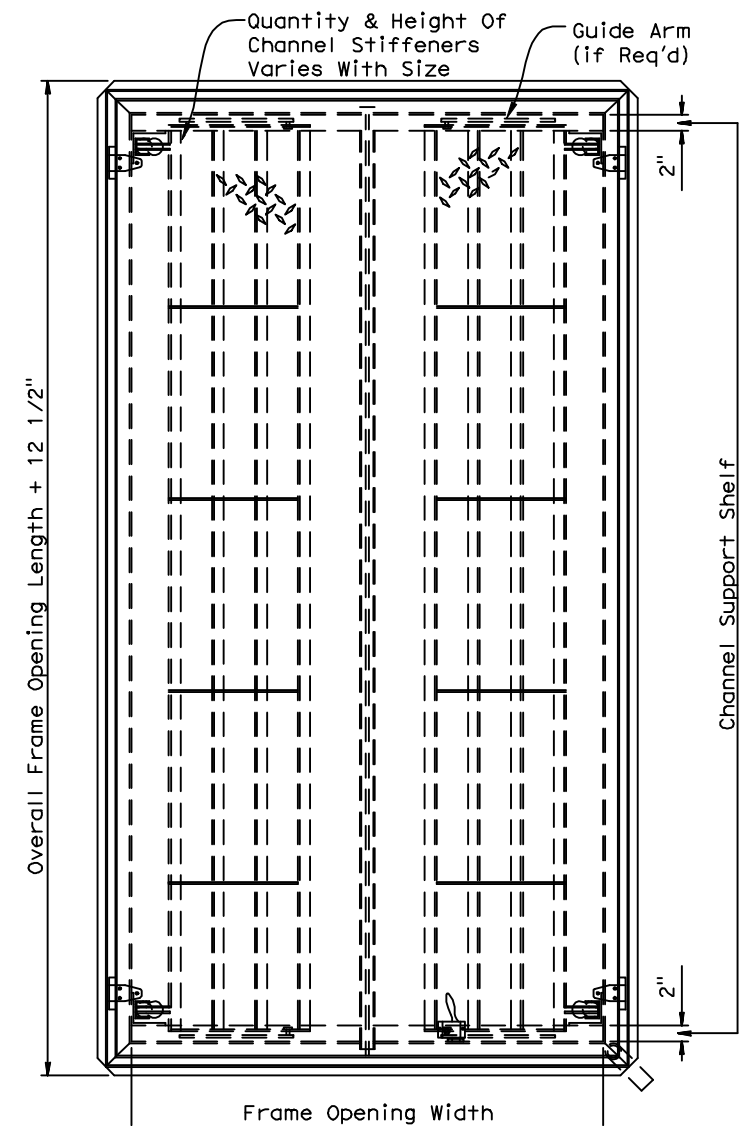
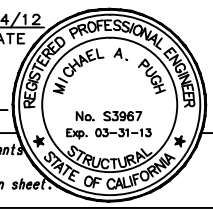
ACCESS HATCH DETAILS
 No Scale

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

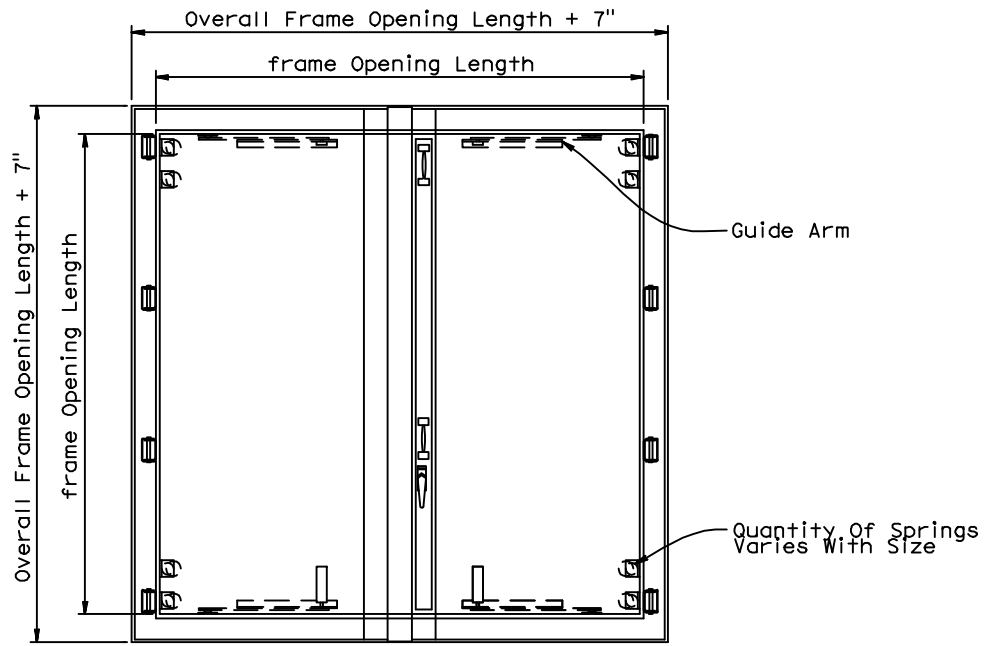
DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-16
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PLANS AND SPECS COMPARED	PROJECT NUMBER & PHASE: 1000000100	POST MILES R22.56	ACCESS OPENING DETAILS No. 1	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY			UNIT: 1457	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 18 OF 47

TIME PLOTTED => 19:27
 USERNAME => Carrick, DATE PLOTTED => 9-Sep-12

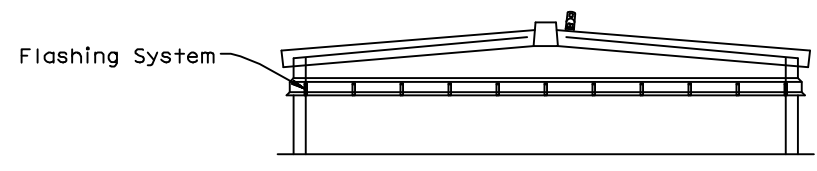
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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REGISTERED CIVIL ENGINEER			DATE	9/4/12	
PLANS APPROVAL DATE			9/04/12		
The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					



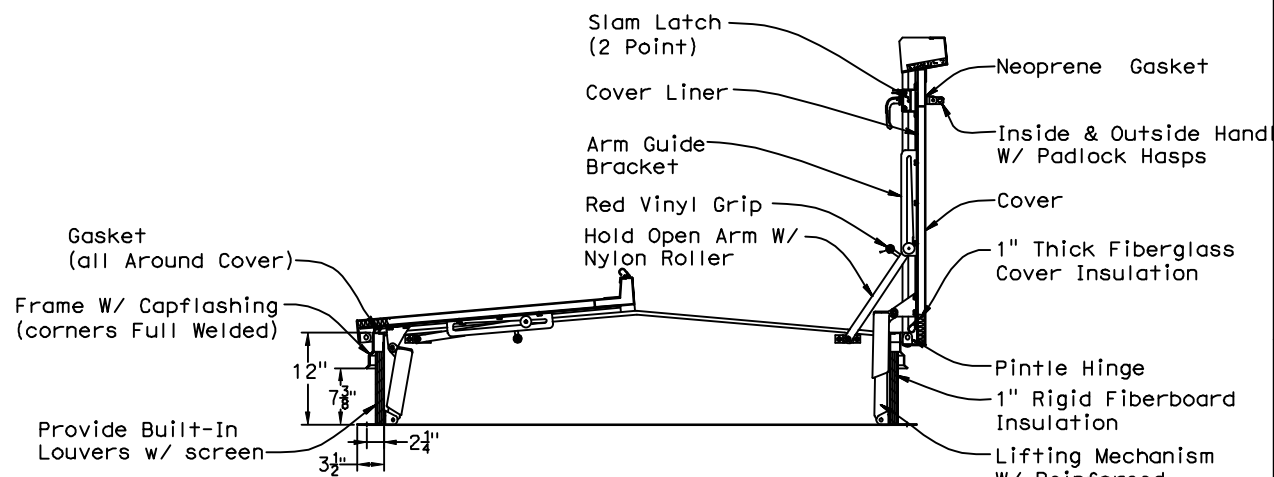
STAIR CASE COVER PLAN
N.T.S.



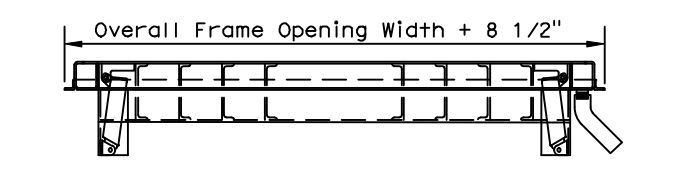
4'X4' & 4'X6' PLAN
N.T.S.



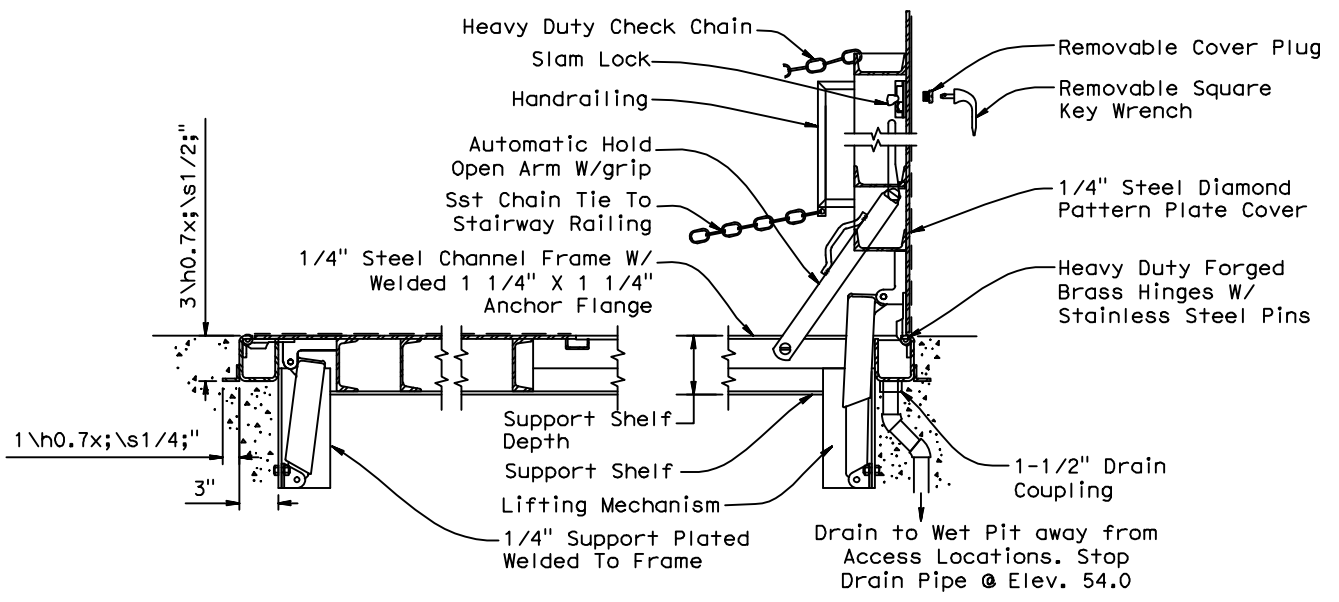
4'X4' & 4'X6' ELEVATION
N.T.S.



4'X4' & 4'X6' SECTION
N.T.S.



STAIR CASE COVER ELEVATION
N.T.S.



STAIR CASE COVER SECTION
N.T.S.

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-17
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	ACCESS OPENING DETAILS No. 2		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 19	OF 47	

TIME PLOTTED => 9-Sep-12 USERNAME => Steven

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	396	423

REGISTERED CIVIL ENGINEER DATE 9/4/12

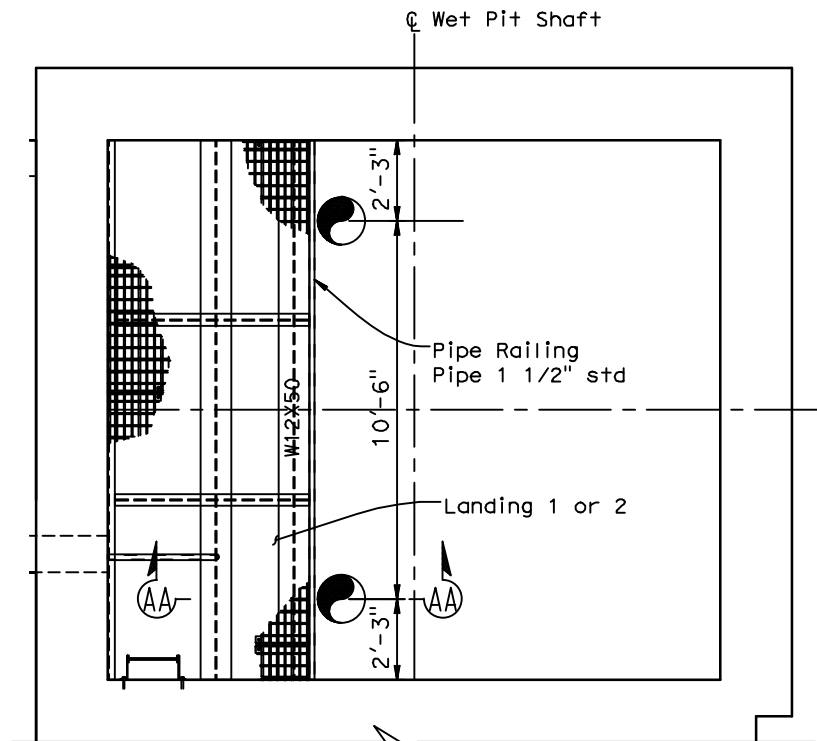
PLANS APPROVAL DATE 9/04/12

REGISTERED PROFESSIONAL ENGINEER
MICHAEL A. PUGH
No. S3967
Exp. 03-31-13
STRUCTURAL
STATE OF CALIFORNIA

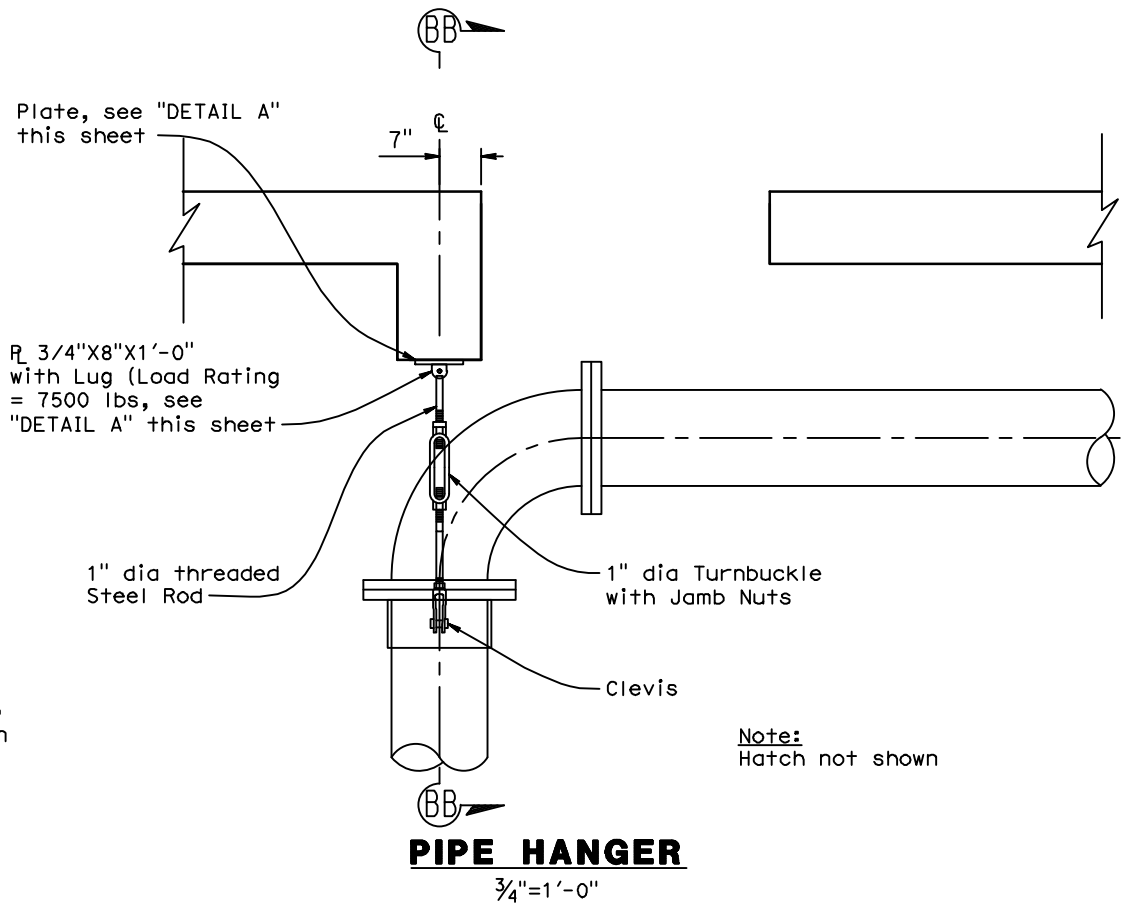
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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

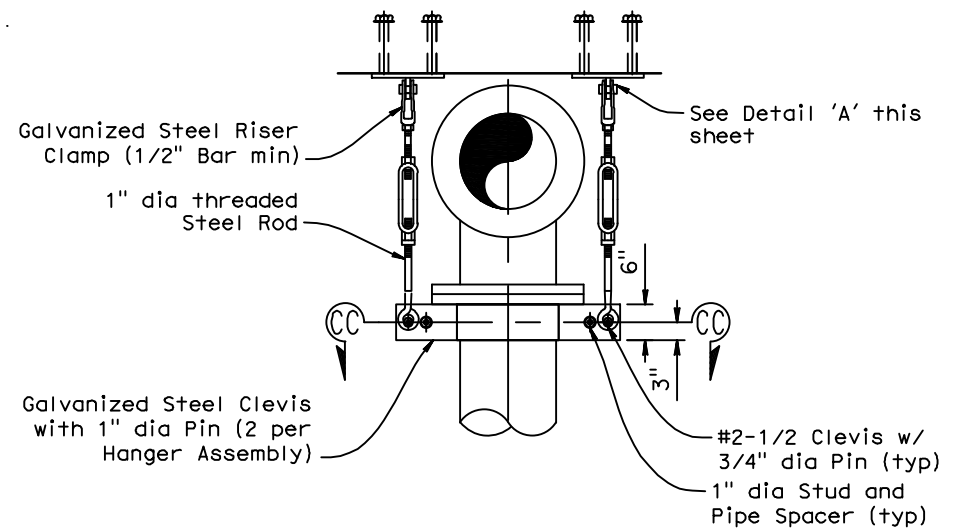
NOLTE ASSOCIATES, INC.
2495 NATOMAS PARK DRIVE, FOURTH FLOOR
SACRAMENTO, CA 95833



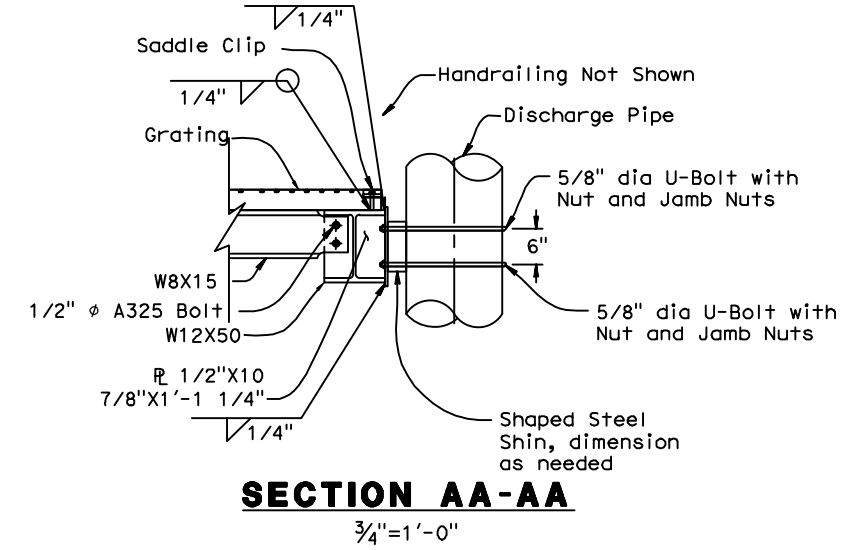
DISCHARGE PIPE SUPPORT PLAN
3/8"=1'-0"



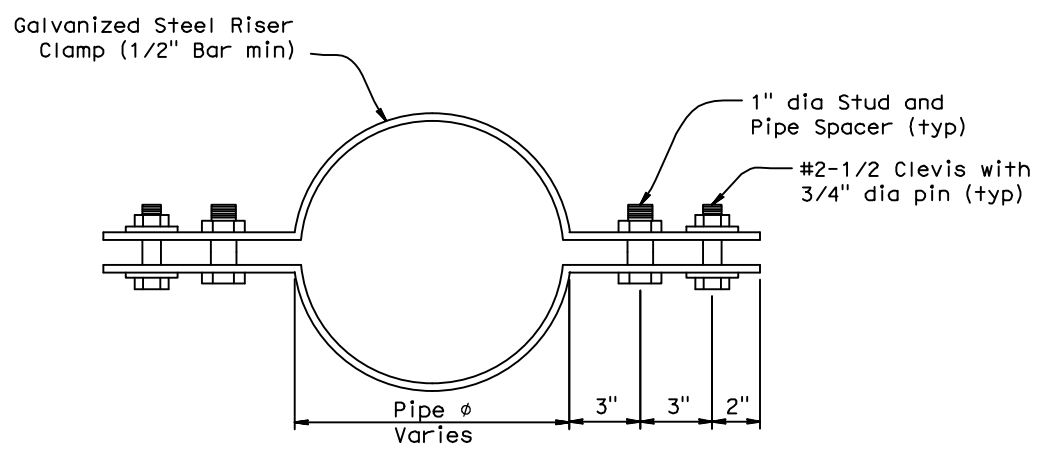
PIPE HANGER
3/4"=1'-0"



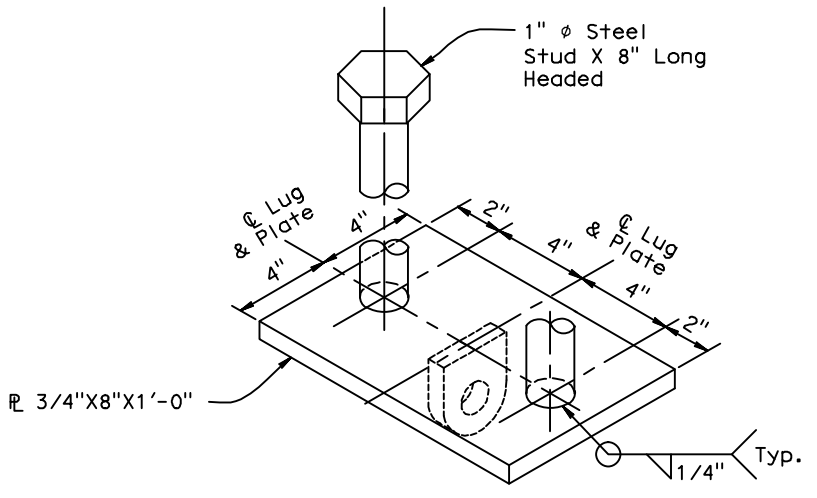
SECTION BB-BB
3"=1'-0"



SECTION AA-AA
3/4"=1'-0"

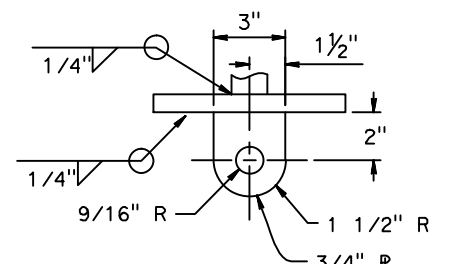


SECTION CC-CC
3"=1'-0"



DETAIL 'A'

NOTE:
THE CONTRACTOR SHALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL



LIFTING LUG DETAIL
No Scale

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-18
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	PIPE SUPPORT AND HANGER DETAILS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 20	OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS

DISREGARD PRINTS BEARING EARLIER REVISION DATES

REVISION DATES: 2/24/12, 3/28/12, 5/1/12, 9/7/12

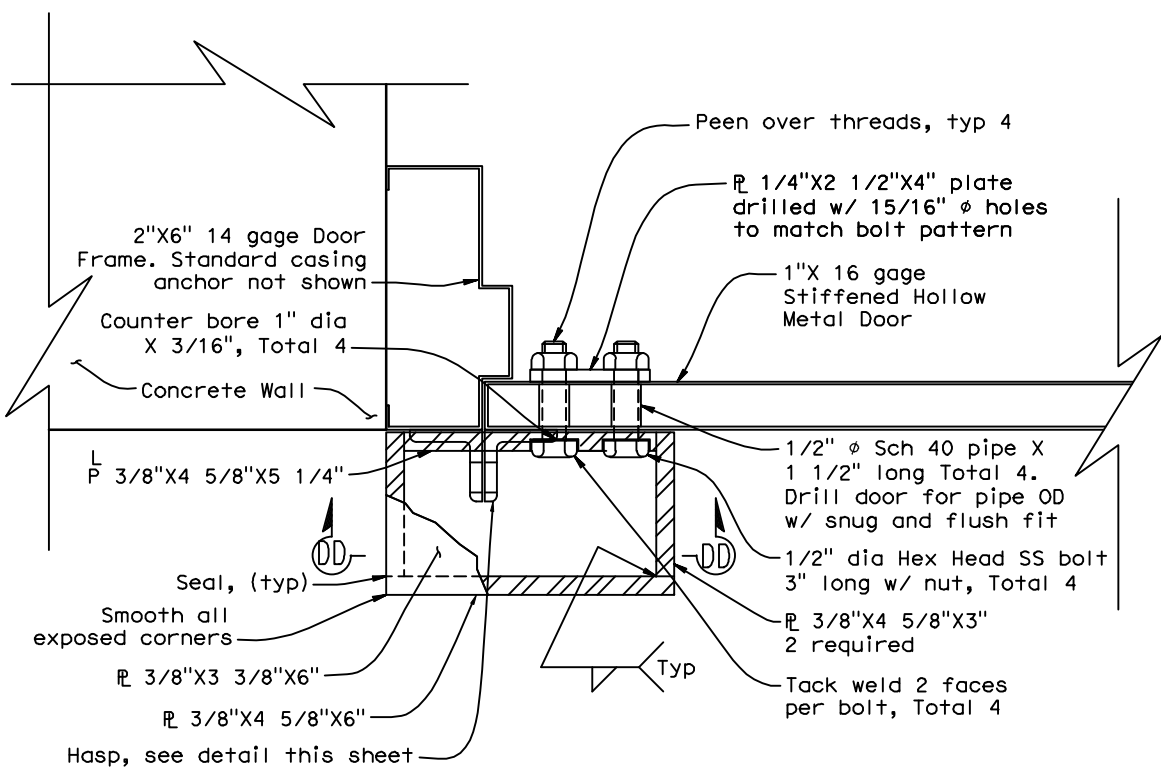
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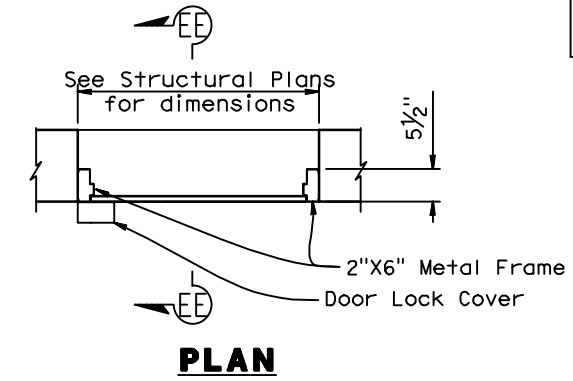
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	397	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 PLANS APPROVAL DATE 9/04/12
 No. 53967 Exp. 03-31-13
 STATE OF CALIFORNIA

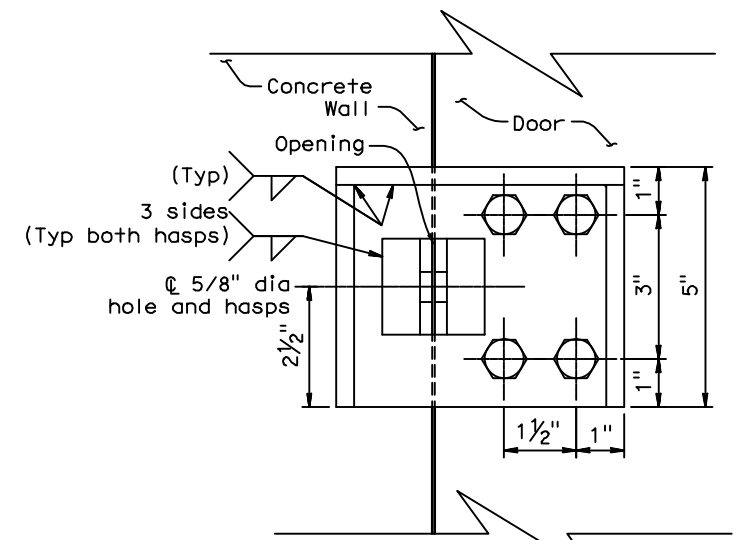
DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



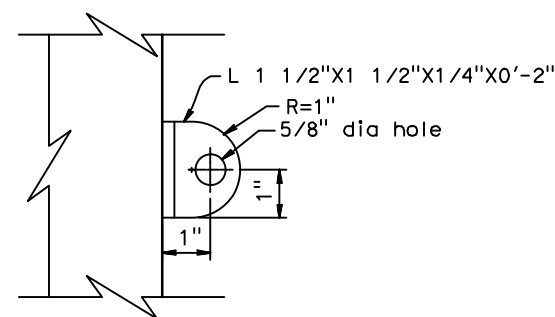
DOOR LOCK COVER DETAIL
 No Scale



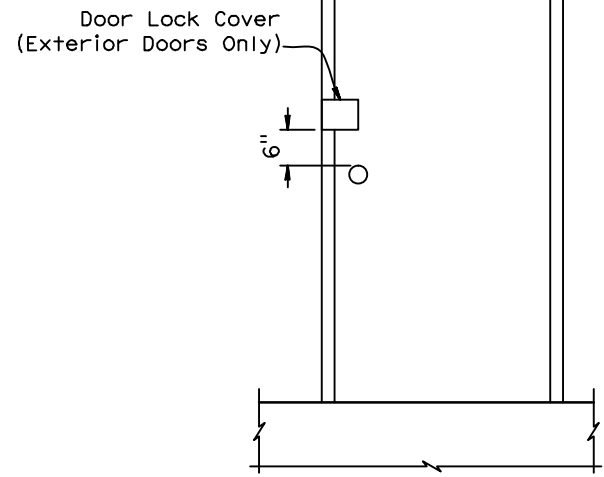
PLAN



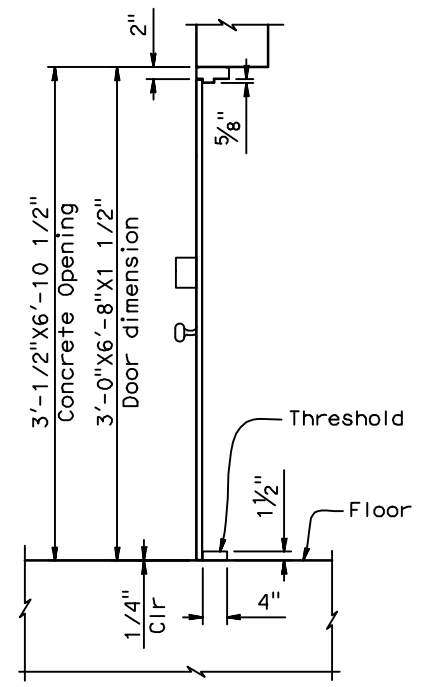
SECTION DD-DD
 No Scale



HASP DETAIL
 No Scale



ELEVATION



SECTION EE-EE

TYPICAL DOOR DETAIL
 No Scale

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-19
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION	PROJECT ENGINEER	POST MILES R22.56	CONTROL ROOM DOOR DETAILS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 21 OF 47

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 USERNAME => Carrick, DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 19:30

1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	398	423

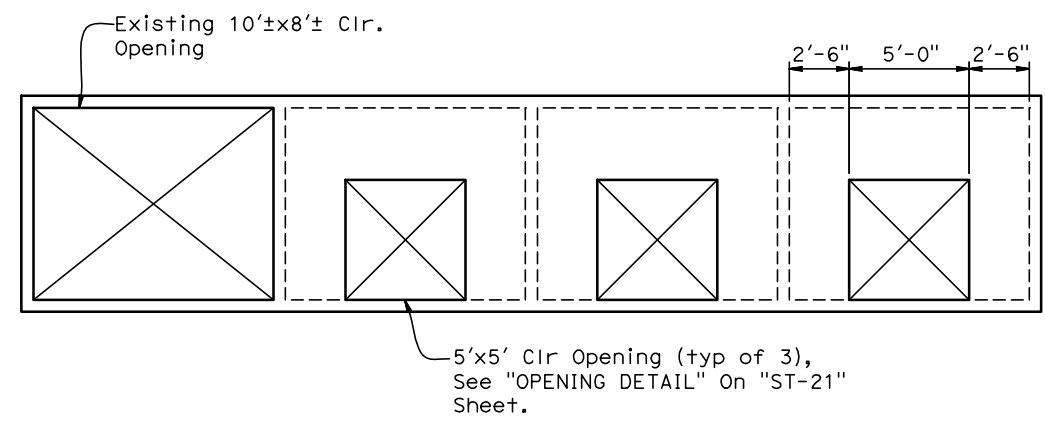
REGISTERED CIVIL ENGINEER 5/1/12 DATE
 MICHAEAL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL STATE OF CALIFORNIA

PLANS APPROVAL DATE

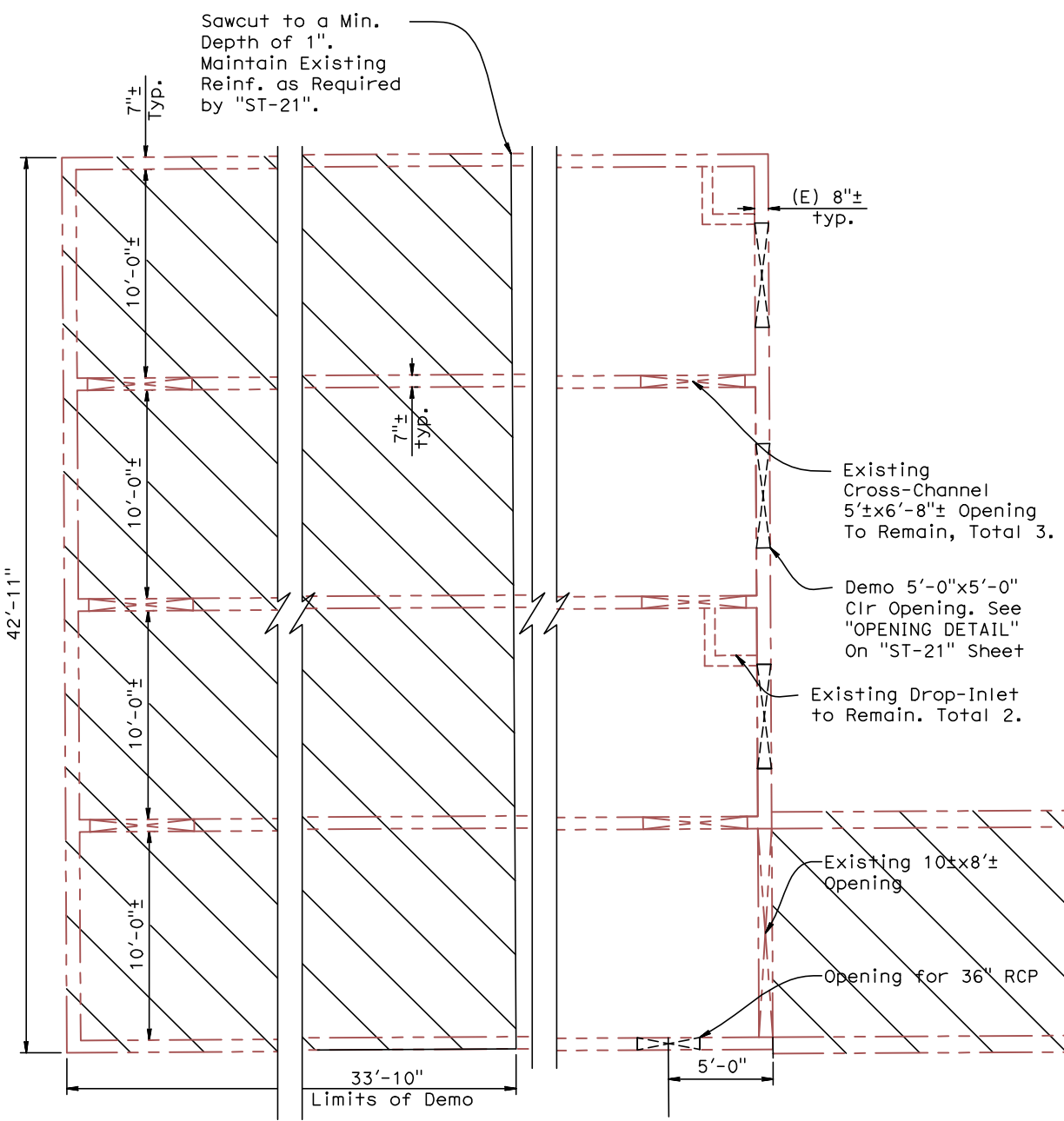
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

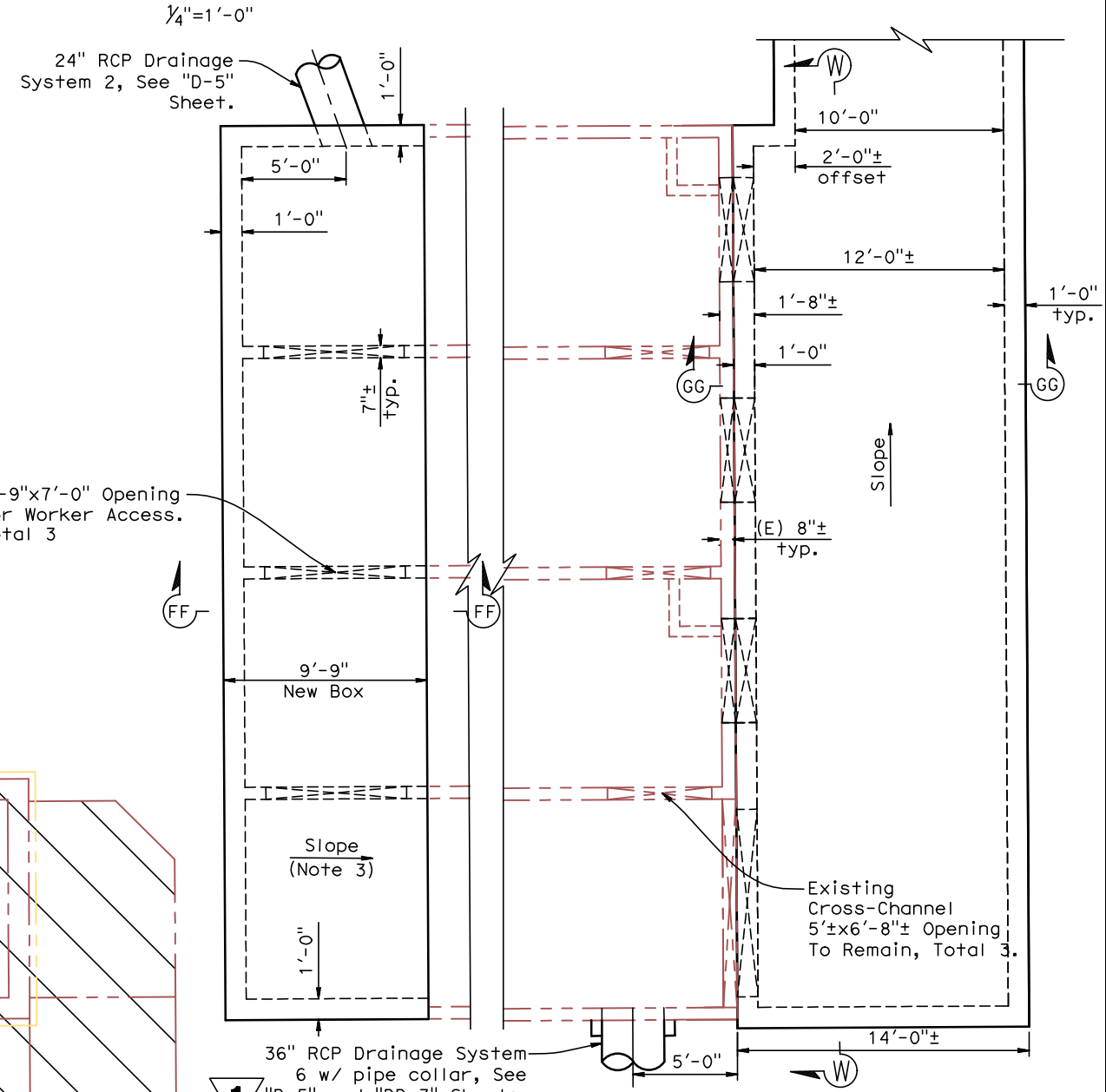


EXISTING WALL DEMO
 $\frac{1}{4}''=1'-0''$



DEMOLITION PLAN
 $\frac{1}{4}''=1'-0''$

- NOTES:**
1. For Location of "W-W", See "ST-8" Sheet
 2. For Location of "FF-FF" and "GG-GG", See "ST-21" Sheet.
 3. Match Existing Storage Box Slope.



IMPROVEMENT PLAN
 $\frac{1}{4}''=1'-0''$

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		ST-20
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION	PROJECT ENGINEER	POST MILES R22.56	STORAGE BOX DEMO & IMPROVEMENTS		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 22	OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS

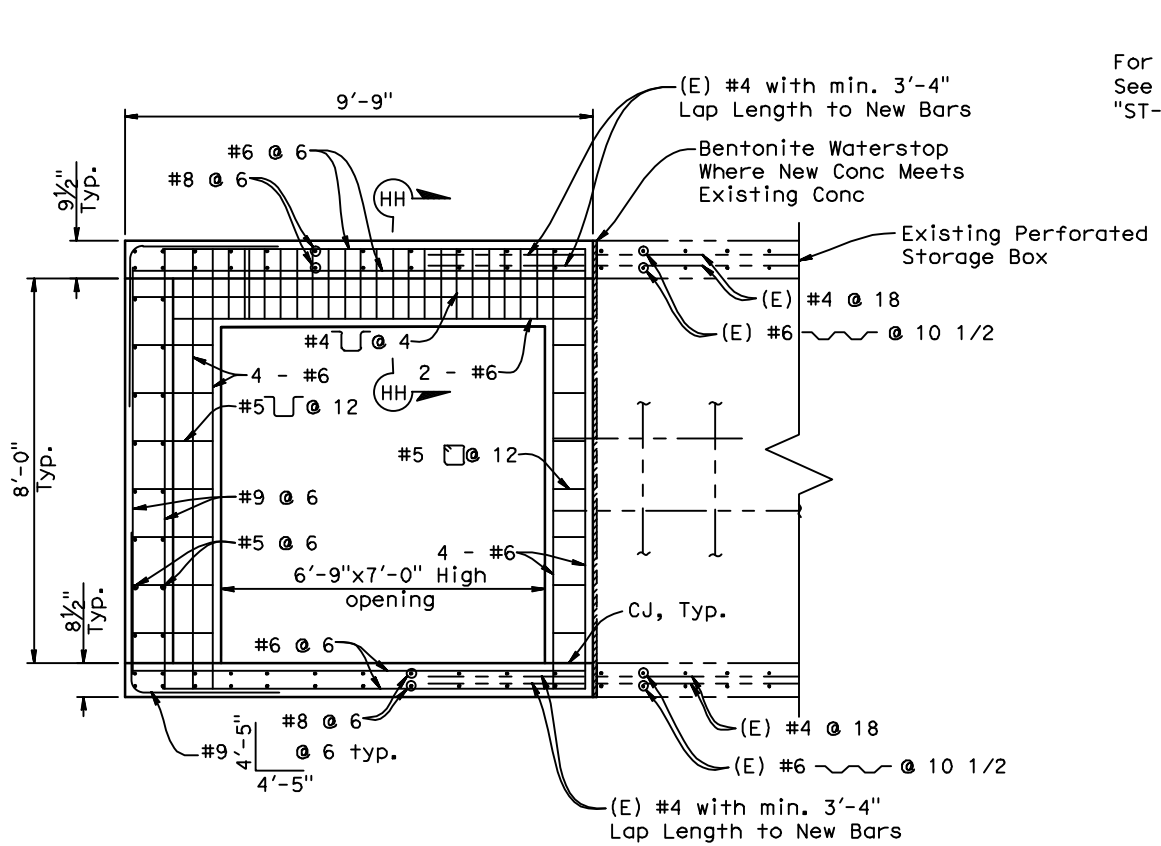
DISREGARD PRINTS BEARING EARLIER REVISION DATES

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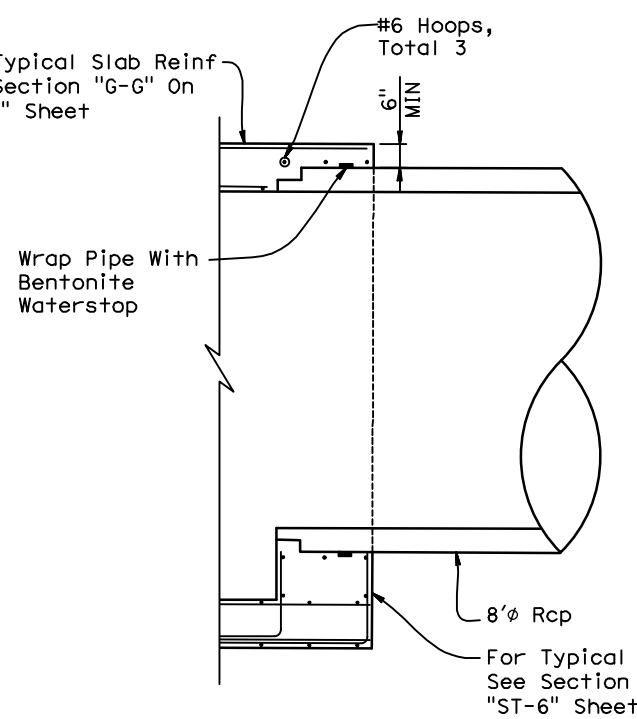
DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 16:14 USERNAME => Gagne, Denise

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	399	423

REGISTERED CIVIL ENGINEER	DATE 9/4/12
PLANS APPROVAL DATE 9/04/12	
The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.	
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358	
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833	

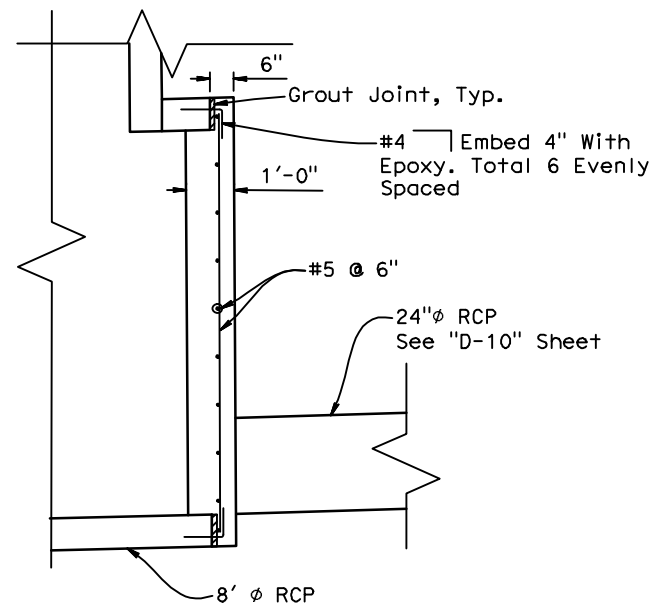


SECTION FF-FF
1/2"=1'-0"

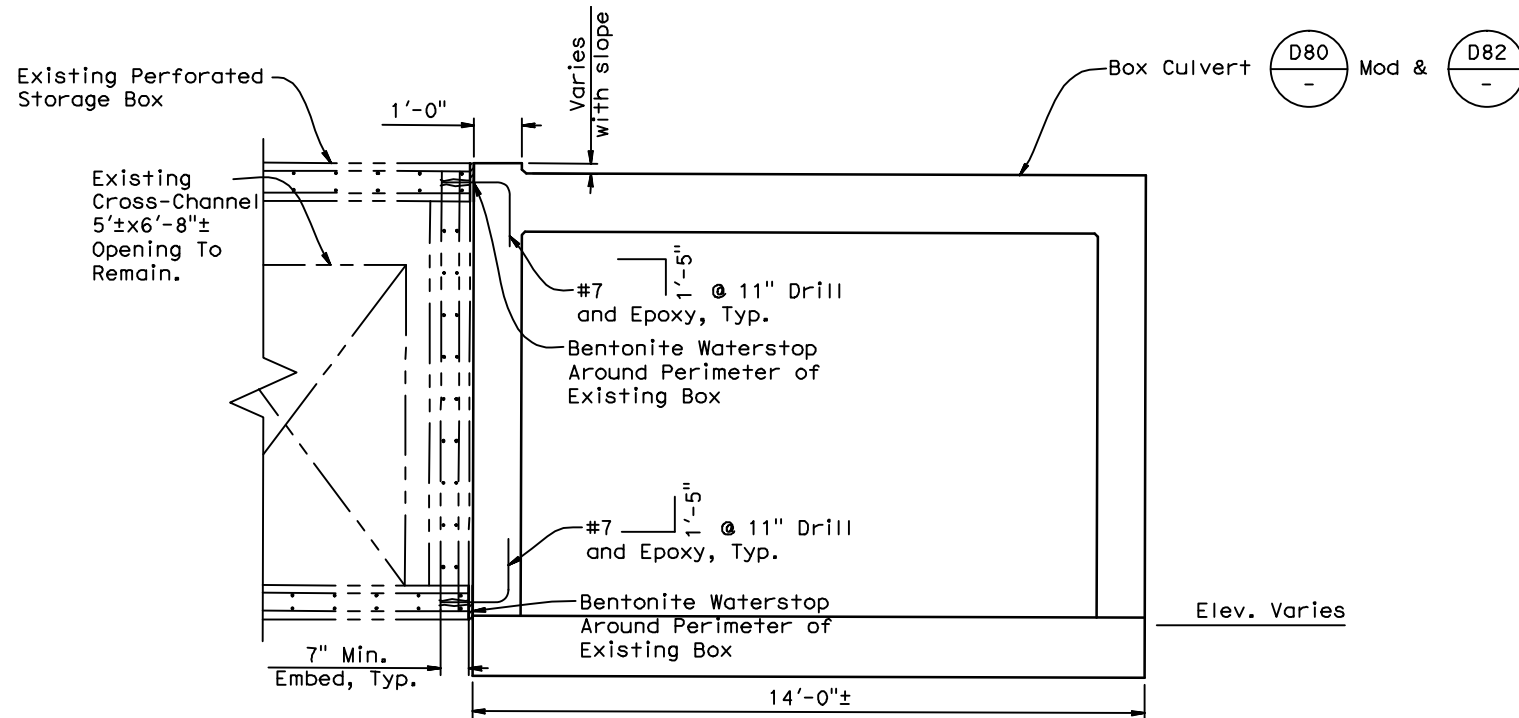


PIPE TO CULVERT CONNECTION DETAIL
1/2"=1'-0"

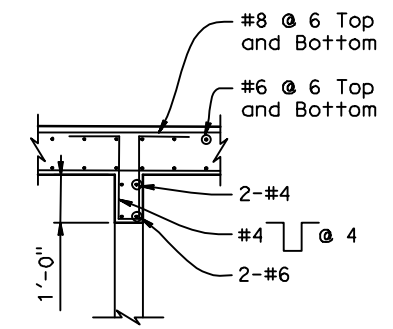
- NOTES:**
1. For Location of Section 'FF-FF', See "ST-20" Sheet.
 2. For Location of section 'GG-GG', See "ST-8" and "ST-20" sheets.
 3. All Joints shall have bentonite waterstops centered in wall. See construction joint detail on "ST-9" Sheet.



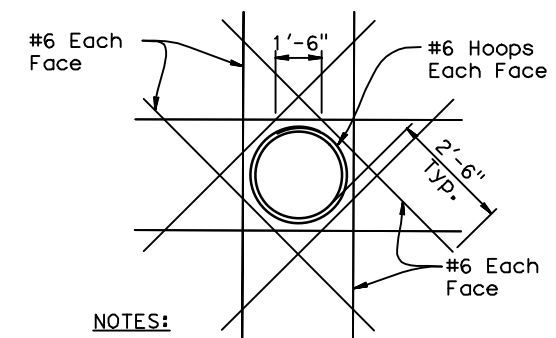
CAP DETAIL
1/2"=1'-0"



SECTION GG-GG
1/4"=1'-0"

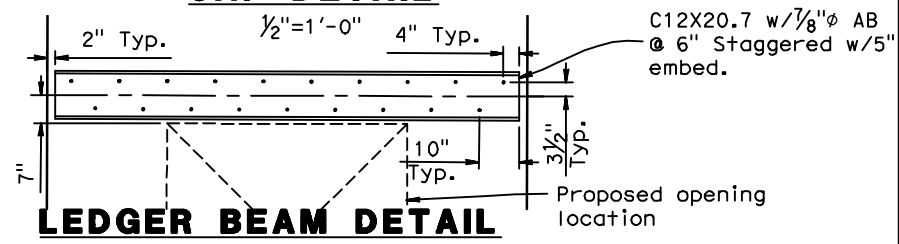


SECTION HH-HH
1/2"=1'-0"

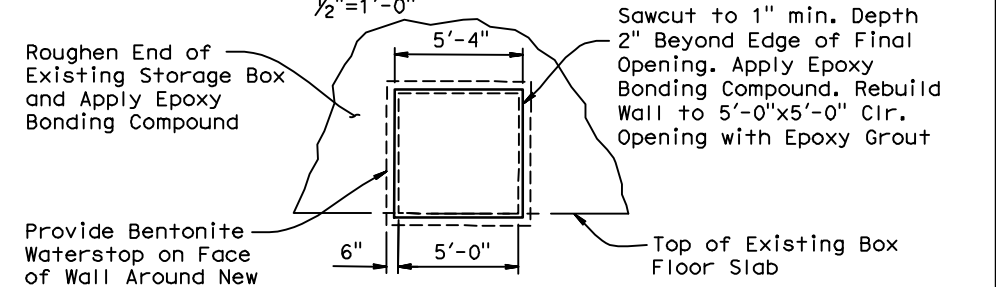


TYPICAL CULVERT/PIPE OPENING DETAIL
N.T.S.

- NOTES:**
1. Hook bars where required to maintain 3" min. cover.



LEDGER BEAM DETAIL
1/2"=1'-0"



OPENING DETAIL
1/4"=1'-0"

- Roughen End of Existing Storage Box and Apply Epoxy Bonding Compound
- Sawcut to 1" min. Depth 2" Beyond Edge of Final Opening. Apply Epoxy Bonding Compound. Rebuild Wall to 5'-0" x 5'-0" Cir. Opening with Epoxy Grout
- Provide Bentonite Waterstop on Face of Wall Around New Wall Opening
- Note: Prior to sawcutting opening contractor shall install C12x on interior face of wall. See "Ledger Beam Detail"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-21
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	DEPARTMENT OF TRANSPORTATION	R22.56	R22.56		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 23 OF 47

TIME PLOTTED => 19:31
USERNAME => Carrick, DATE PLOTTED => 9-Sep-12

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	400	423

REGISTERED CIVIL ENGINEER DATE 5/1/12
 9/04/12
 PLANS APPROVAL DATE

No. S3967
 Exp. 03-31-13
 MICHAEL A. PUGH
 REGISTERED PROFESSIONAL ENGINEER
 STRUCTURAL
 STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

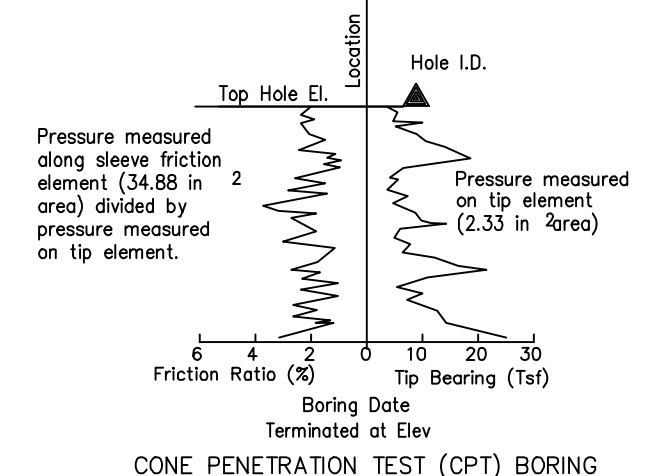
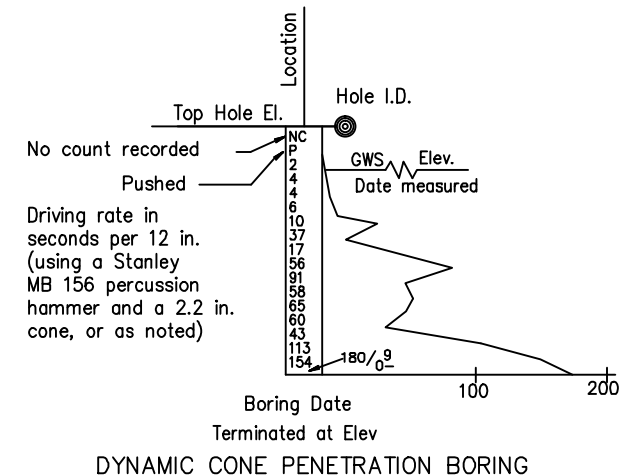
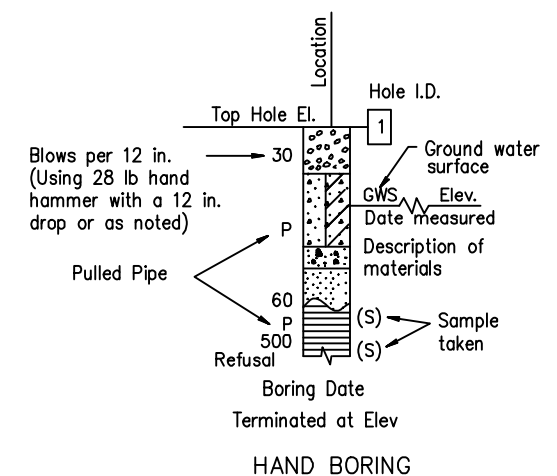
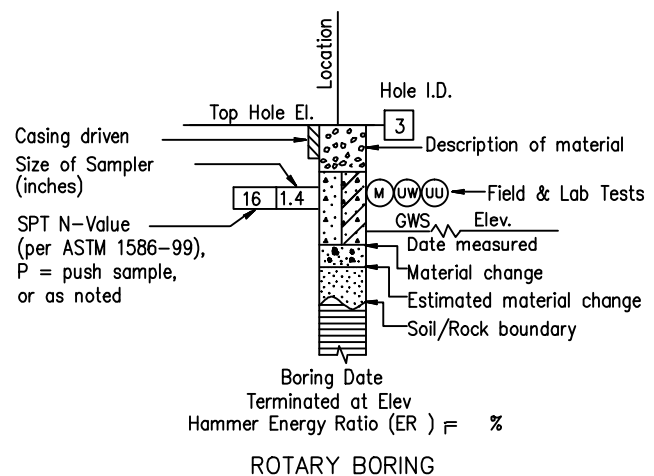
NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

CEMENTATION	
Description	Criteria
Weak	Crumbles or breaks with handling or little finger pressure.
Moderate	Crumbles or breaks with considerable finger pressure.
Strong	Will not crumble or break with finger pressure.

BOREHOLE IDENTIFICATION		
Symbol	Hole Type	Description
	A	Auger Boring (hollow or solid stem bucket)
	R	Rotary drilled boring (conventional)
	RW	Rotary drilled with self-casing wire-line
	RC	Rotary core with continuously-sampled, self-casing wire-line
	P	Rotary percussion boring (air)
	R	Rotary drilled diamond core
	HD	Hand driven (1-inch soil tube)
	HA	Hand Auger
	D	Dynamic Cone Penetration Boring
	CPT	Cone Penetration Test (ASTM D 5778)
	O	Other (note on LOTB)

Note: Size in inches.

CONSISTENCY OF COHESIVE SOILS				
Description	Shear Strength (tsf)	Pocket Penetrometer Measurement, PP, (tsf)	Torvane Measurement, TV, (tsf)	Vane Shear Measurement, VS, (tsf)
Very Soft	Less than 0.12	Less than 0.25	Less than 0.12	Less than 0.12
Soft	0.12 - 0.25	0.25 - 0.5	0.12 - 0.25	0.12 - 0.25
Medium Stiff	0.25 - 0.5	0.5 - 1	0.25 - 0.5	0.25 - 0.5
Stiff	0.5 - 1	1 - 2	0.5 - 1	0.5 - 1
Very Stiff	1 - 2	2 - 4	1 - 2	1 - 2
Hard	Greater than 2	Greater than 4	Greater than 2	Greater than 2



NOTE:
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DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-22	
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	POST MILES R22.56			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	
ORIGINAL SCALE IN INCHES FOR REDUCED PLANS					DISREGARD PRINTS BEARING EARLIER REVISION DATES		REVISION DATES	SHEET 24 OF 47

FILE => N:\SAB041001\Cadd\Structural\10-380163WD+1-ST-22.dwg

TIME PLOTTED => 17:15 USERNAME => Steven

GROUP SYMBOLS AND NAMES					
Graphic/Symbol	Group Names		Graphic/Symbol	Group Names	
	GW	Well-graded GRAVEL Well-graded GRAVEL with SAND		CL	Lean CLAY Lean CLAY with SAND Lean CLAY with GRAVEL SANDY lean CLAY SANDY lean CLAY with GRAVEL GRAVELLY lean CLAY GRAVELLY lean CLAY with SAND
	GP	Poorly-graded GRAVEL Poorly-graded GRAVEL with SAND		CL-ML	SILTY CLAY SILTY CLAY with SAND SILTY CLAY with GRAVEL SANDY SILTY CLAY SANDY SILTY CLAY with GRAVEL GRAVELLY SILTY CLAY GRAVELLY SILTY CLAY with SAND
	GW-GM	Well-graded GRAVEL with SILT Well-graded GRAVEL with SILT and SAND		ML	SILT SILT with SAND SILT with GRAVEL SANDY SILT SANDY SILT with GRAVEL GRAVELLY SILT GRAVELLY SILT with SAND
	GW-GC	Well-graded GRAVEL with CLAY (or SILTY CLAY) Well-graded GRAVEL with CLAY and SAND (or SILTY CLAY and SAND)		OL	ORGANIC lean CLAY ORGANIC lean CLAY with SAND ORGANIC lean CLAY with GRAVEL SANDY ORGANIC lean CLAY SANDY ORGANIC lean CLAY with GRAVEL GRAVELLY ORGANIC lean CLAY GRAVELLY ORGANIC lean CLAY with SAND
	GP-GM	Poorly-graded GRAVEL with SILT Poorly-graded GRAVEL with SILT and SAND		OL	ORGANIC SILT ORGANIC SILT with SAND ORGANIC SILT with GRAVEL SANDY ORGANIC SILT SANDY ORGANIC SILT with GRAVEL GRAVELLY ORGANIC SILT GRAVELLY ORGANIC SILT with SAND
	GP-GC	Poorly-graded GRAVEL with CLAY (or SILTY CLAY) Poorly-graded GRAVEL with CLAY and SAND (or SILTY CLAY and SAND)		OH	ORGANIC fat CLAY ORGANIC fat CLAY with SAND ORGANIC fat CLAY with GRAVEL SANDY ORGANIC fat CLAY SANDY ORGANIC fat CLAY with GRAVEL GRAVELLY ORGANIC fat CLAY GRAVELLY ORGANIC fat CLAY with SAND
	GM	SILTY GRAVEL SILTY GRAVEL with SAND		OH	ORGANIC elastic SILT ORGANIC elastic SILT with SAND ORGANIC elastic SILT with GRAVEL SANDY ORGANIC elastic SILT SANDY ORGANIC elastic SILT with GRAVEL GRAVELLY ORGANIC elastic SILT GRAVELLY ORGANIC elastic SILT with SAND
	GC	CLAYEY GRAVEL CLAYEY GRAVEL with SAND		PT	PEAT
	GC-GM	SILTY, CLAYEY GRAVEL SILTY, CLAYEY GRAVEL with SAND		OL/OH	ORGANIC SOIL ORGANIC SOIL with SAND ORGANIC SOIL with GRAVEL SANDY ORGANIC SOIL SANDY ORGANIC SOIL with GRAVEL GRAVELLY ORGANIC SOIL GRAVELLY ORGANIC SOIL with SAND
	SW	Well-graded SAND Well-graded SAND with GRAVEL		SC	CLAYEY SAND CLAYEY SAND with GRAVEL
	SP	Poorly-graded SAND Poorly-graded SAND with GRAVEL		SC-SM	SILTY, CLAYEY SAND SILTY, CLAYEY SAND with GRAVEL
	SW-SM	Well-graded SAND with SILT Well-graded SAND with SILT and GRAVEL		SM	SILTY SAND SILTY SAND with GRAVEL
	SW-SC	Well-graded SAND with CLAY (or SILTY CLAY) Well-graded SAND with CLAY and GRAVEL (or SILTY CLAY and GRAVEL)		SM	SILTY SAND SILTY SAND with GRAVEL
	SP-SM	Poorly-graded SAND with SILT Poorly-graded SAND with SILT and GRAVEL		SC	CLAYEY SAND CLAYEY SAND with GRAVEL
	SP-SC	Poorly-graded SAND with CLAY (or SILTY CLAY) Poorly-graded SAND with CLAY and GRAVEL (or SILTY CLAY and GRAVEL)		SC-SM	SILTY, CLAYEY SAND SILTY, CLAYEY SAND with GRAVEL
	SM	SILTY SAND SILTY SAND with GRAVEL		PT	PEAT
	PT	PEAT		OL/OH	ORGANIC SOIL ORGANIC SOIL with SAND ORGANIC SOIL with GRAVEL SANDY ORGANIC SOIL SANDY ORGANIC SOIL with GRAVEL GRAVELLY ORGANIC SOIL GRAVELLY ORGANIC SOIL with SAND
	COBBLES and BOULDERS	COBBLES and BOULDERS		PT	PEAT

NOTE: THE CONTRACTOR SHALL CONTROL ALL FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

FIELD AND LABORATORY TESTING	
(C)	Consolidation (ASTM D 2435)
(CL)	Collapse Potential (ASTM D 5333)
(CP)	Compaction Curve (CTM 216)
(CR)	Corrosivity Testing (CTM 643, CTM 422, CTM 417)
(CU)	Consolidated Undrained Triaxial (ASTM D 4767)
(DS)	Direct Shear (ASTM D 3080)
(EI)	Expansion Index (ASTM D 4829)
(M)	Moisture Content (ASTM D 2216)
(OC)	Organic Content-% (ASTM D 2974)
(P)	Permeability (CTM 220)
(PA)	Particle Size Analysis (ASTM D 422)
(PI)	Plasticity Index (AASHTO T 90) Liquid Limit (AASHTO T 89)
(PL)	Point Load Index (ASTM D 5731)
(PM)	Pressure Meter
(R)	R-Value (CTM 301)
(SE)	Sand Equivalent (CTM 217)
(SG)	Specific Gravity (AASHTO T 100)
(SL)	Shrinkage Limit (ASTM D 427)
(SW)	Swell Potential (ASTM D 4546)
(UC)	Unconfined Compression-Soil (ASTM D 2166) Unconfined Compression-Rock (ASTM D 2938)
(UU)	Unconsolidated Undrained Triaxial (ASTM D 2850)
(UW)	Unit Weight (ASTM D 4767)

REGISTERED CIVIL ENGINEER 5/1/12 DATE

9/04/12 PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER
MICHAEL A. PUGH
No. S3967
Exp. 03-31-13
STRUCTURAL
STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
2495 NATOMAS PARK DRIVE, FOURTH FLOOR
SACRAMENTO, CA 95833

APPARENT DENSITY OF COHESIONLESS SOILS	
Description	SPT N ₆₀ (Blows / 12 in.)
Very Loose	0 - 5
Loose	5 - 10
Medium Dense	10 - 30
Dense	30 - 50
Very Dense	Greater than 50

MOISTURE	
Description	Criteria
Dry	No discernable moisture
Moist	Moisture present, but no free water
Wet	Visible free water

PERCENT OR PROPORTION OF SOILS	
Description	Criteria
Trace	Particles are present but estimated to be less than 5%
Few	5% - 10%
Little	15% - 25%
Some	30% - 45%
Mostly	50% - 100%

PARTICLE SIZE		
Description	Size (in.)	
Boulder	Greater than 12	
Cobble	3 - 12	
Gravel	Coarse	3/4 - 3
	Fine	1/5 - 3/4
Sand	Coarse	1/16 - 1/5
	Medium	1/64 - 1/16
	Fine	1/300 - 1/64
Silt and Clay	Less than 1/300	

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BY" AND PERMIT DESIGN VEHICLE
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED

PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION

M. PUGH PROJECT ENGINEER

BRIDGE NO. 38-0163W POST MILES R22.56

219/99 SEPARATION PUMPING PLANT

LOG OF TEST BORINGS 2 OF 7

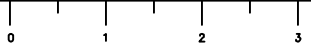
ST-23

UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304

DISREGARD PRINTS BEARING EARLIER REVISION DATES

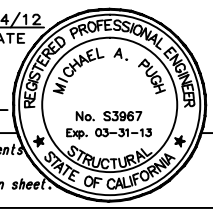
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ORIGINAL SCALE IN INCHES FOR REDUCED PLANS

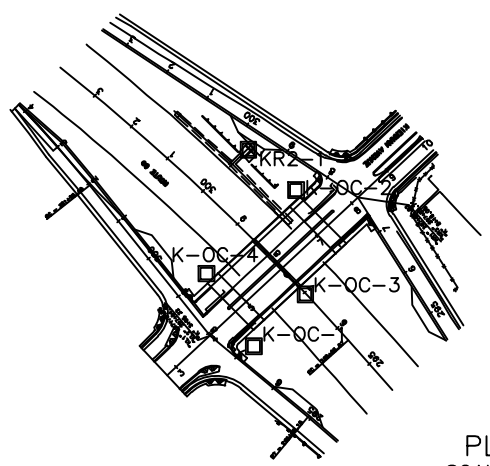


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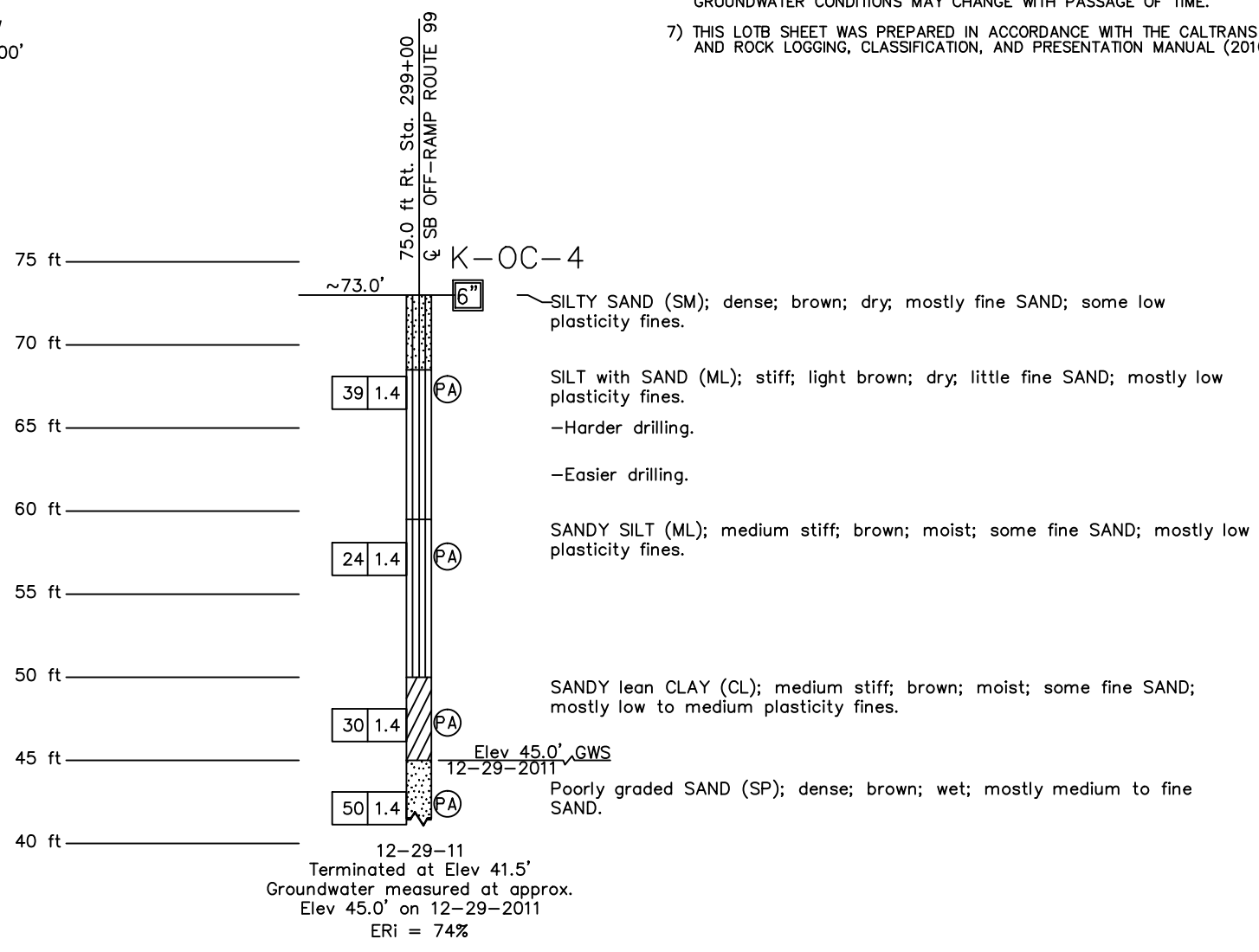
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	402	423
REGISTERED CIVIL ENGINEER			DATE	9/4/12	
PLANS APPROVAL DATE			9/04/12		
<small>The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.</small>					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					



- NOTES:
- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
 - 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
 - 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
 - 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
 - 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
 - 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
 - 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



PLAN VIEW
SCALE : 1" = 200'



PROFILE VIEW
VERT. SCALE : 1" = 5'

NOTE:
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DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-24
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	LOG OF TEST BORINGS 3 OF 7	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 26	OF 47

TIME PLOTTED => 10:07
USERNAME => Carrick, Steven

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	403	423

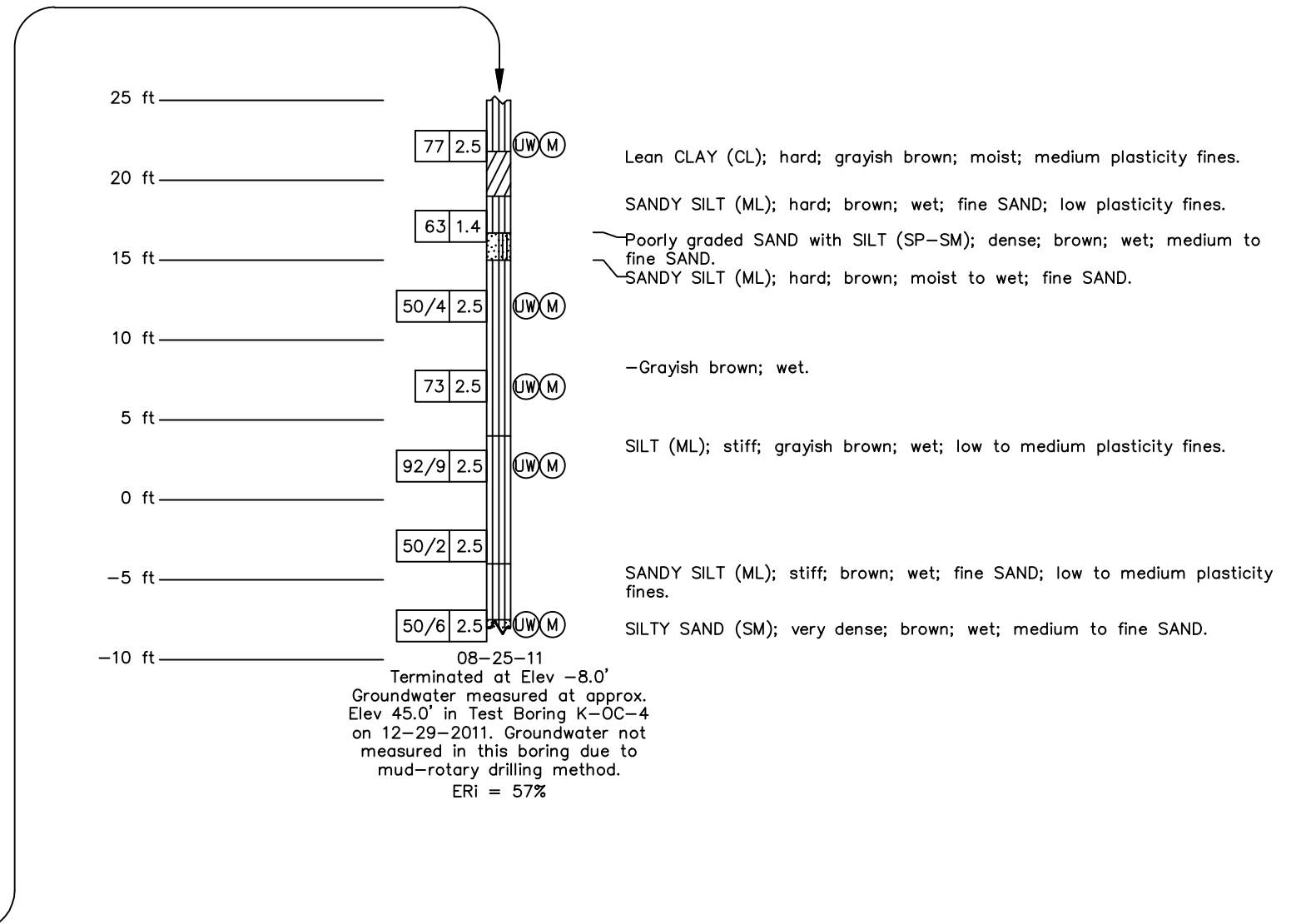
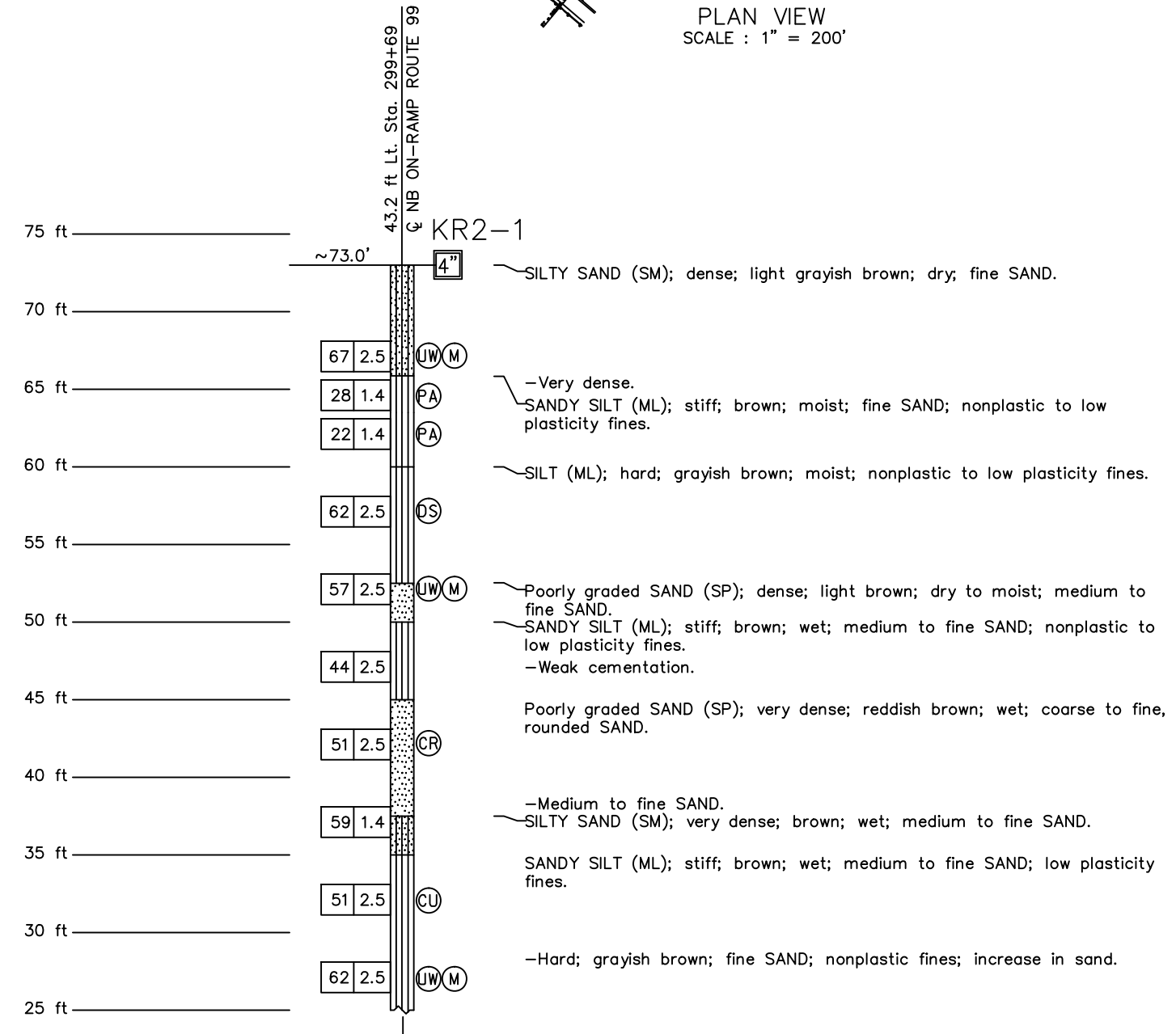
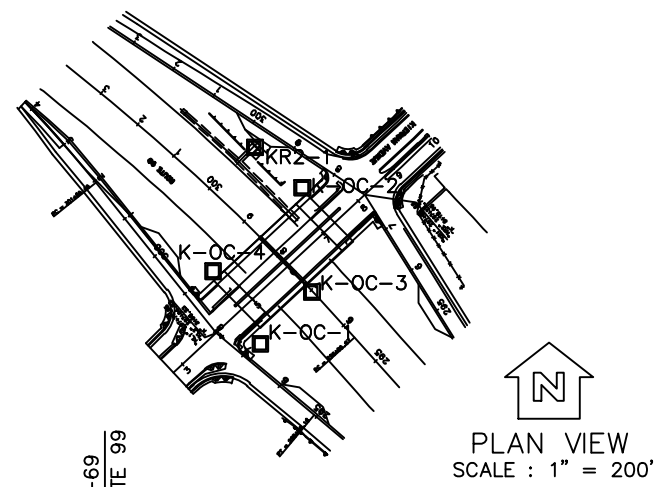
REGISTERED CIVIL ENGINEER	DATE
<i>Michael A. Pugh</i>	9/4/12
PLANS APPROVAL DATE	
9/04/12	

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
2495 NATOMAS PARK DRIVE, FOURTH FLOOR
SACRAMENTO, CA 95833

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PROFILE VIEW
VERT. SCALE : 1" = 5'

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DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-25
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	LOG OF TEST BORINGS 4 OF 7	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 27	OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3

FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163WD1-ST-25.dwg

USERNAME => Steven DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 10:13

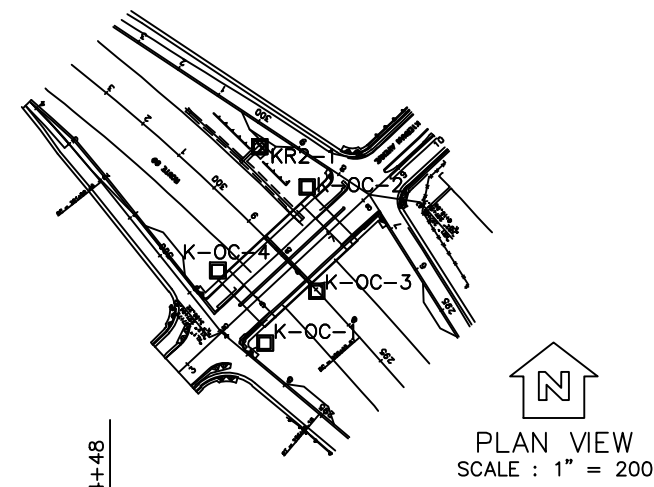
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	404	423

REGISTERED CIVIL ENGINEER	DATE
<i>Michael A. Pugh</i>	5/1/12
PLANS APPROVAL DATE	
9/04/12	

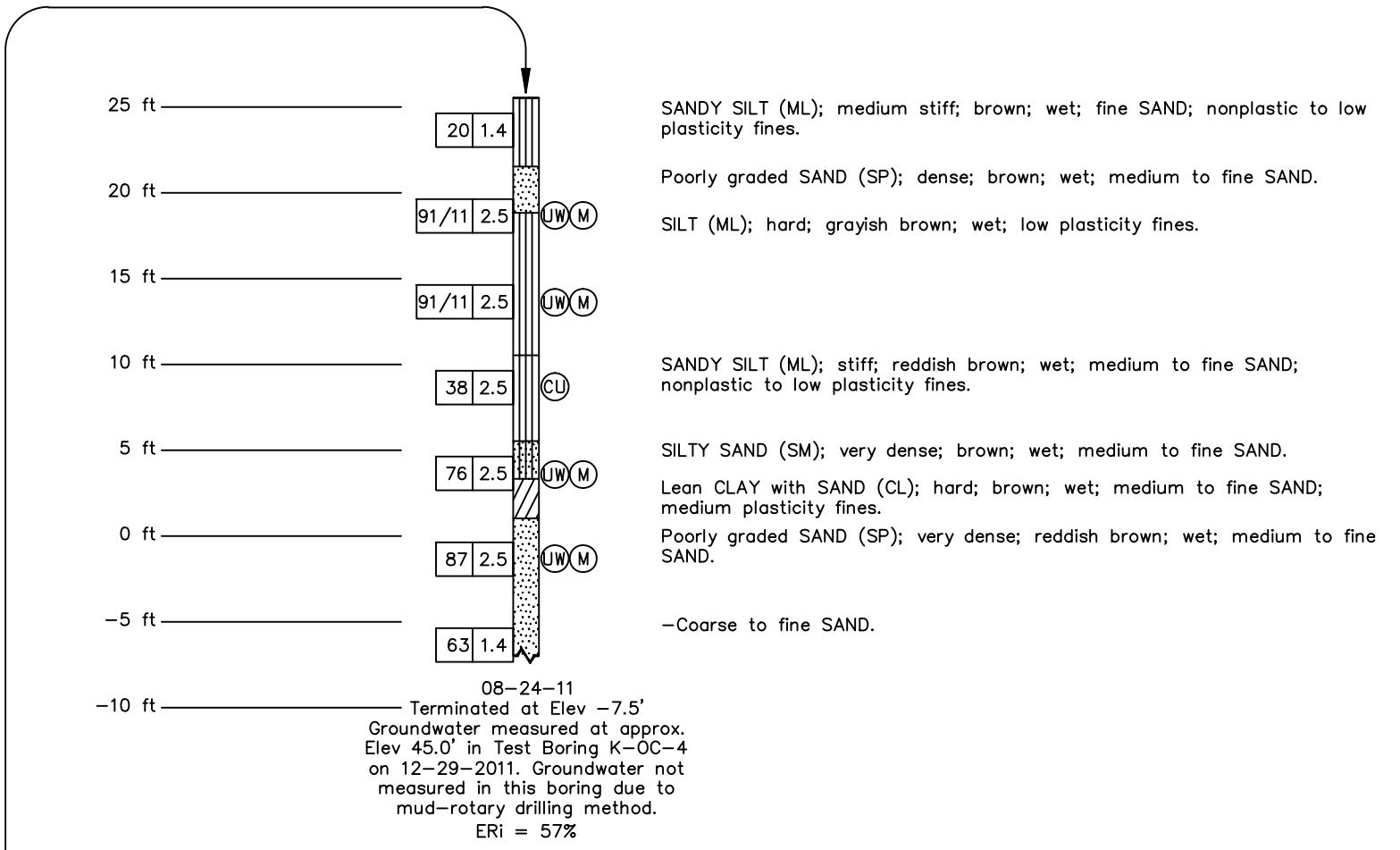
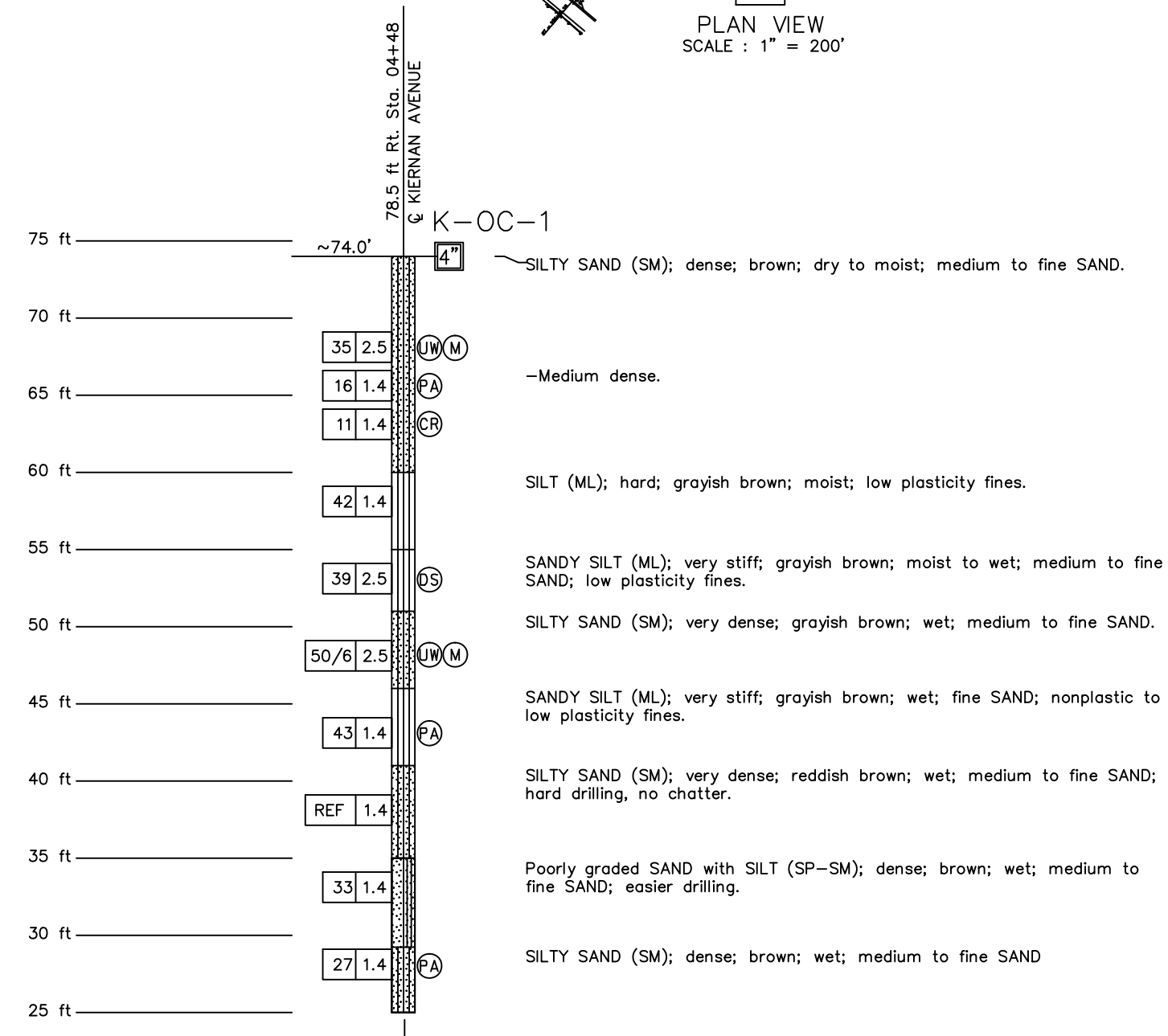
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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
2495 NATOMAS PARK DRIVE, FOURTH FLOOR
SACRAMENTO, CA 95833



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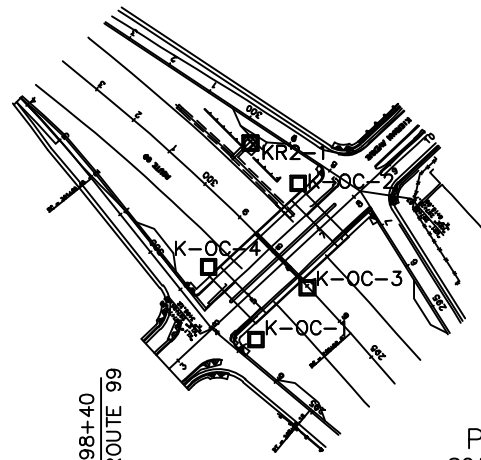


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THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

PROFILE VIEW
VERT. SCALE : 1" = 5'

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W POST MILES R22.56	219/99 SEPARATION PUMPING PLANT LOG OF TEST BORINGS 5 OF 7	ST-26
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304		REVISION DATES	SHEET 28 OF 47	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	FILE => N:\SAB041001\Cadd\Structural\10-380163WD+1-ST-26.dwg		DISREGARD PRINTS BEARING EARLIER REVISION DATES		

TIME PLOTTED => 17:19
USERNAME => Corrick, DATE PLOTTED => 5-May-12 Steven



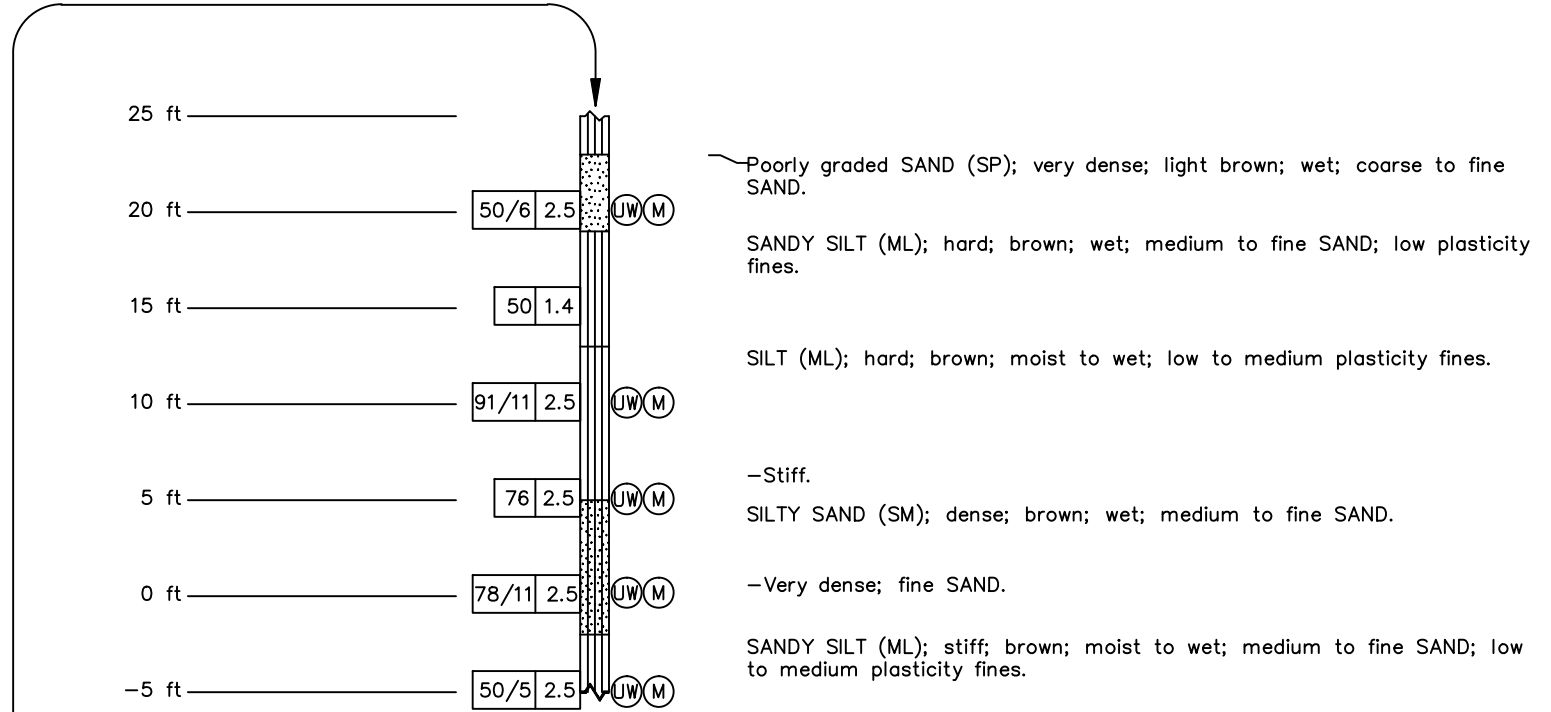
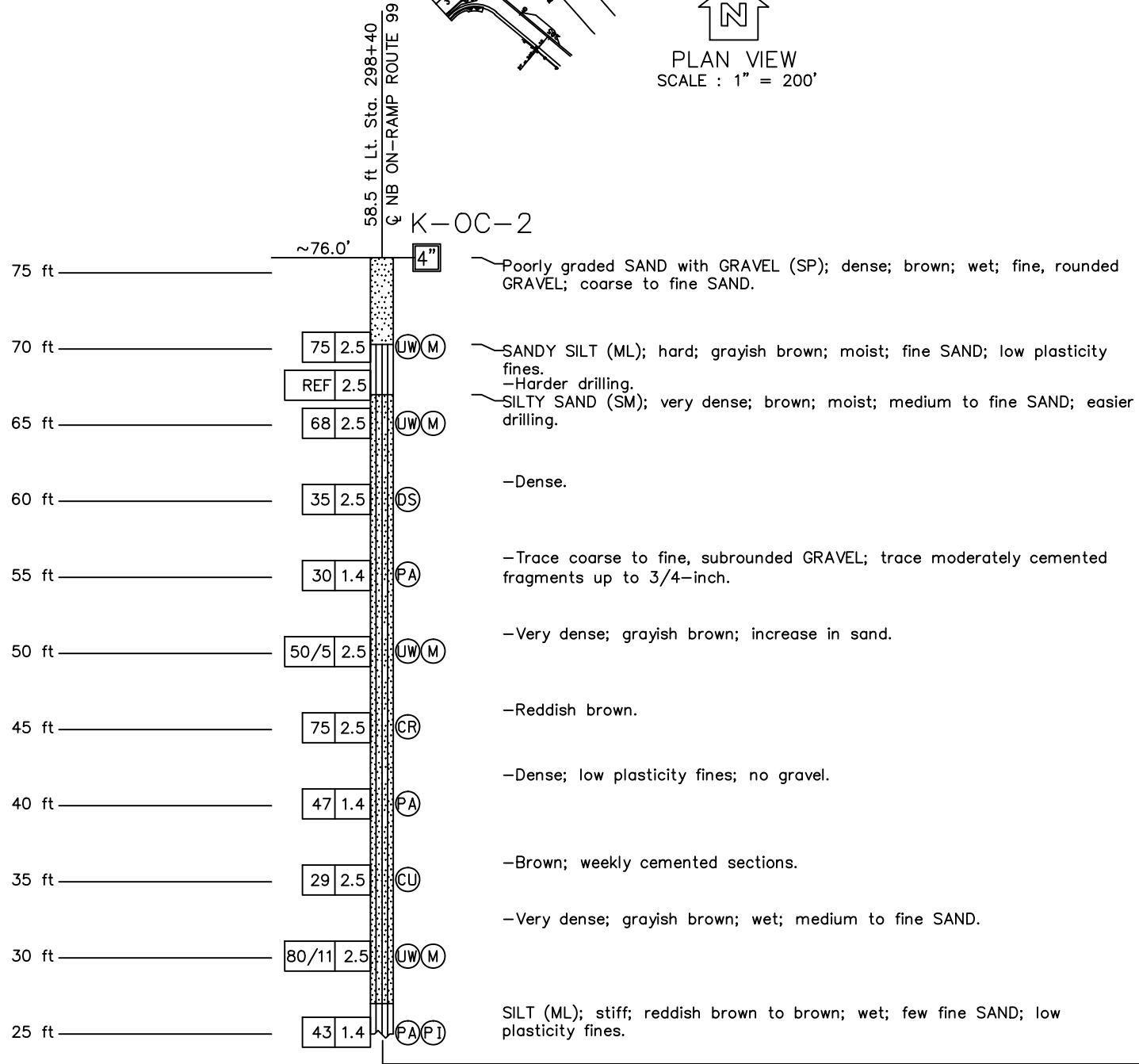
PLAN VIEW
SCALE : 1" = 200'

NOTES:

- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
- 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
- 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
- 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
- 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
- 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
- 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	405	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER MICHAEL A. PUGH No. S3967 Exp. 03-31-13
 PLANS APPROVAL DATE 9/04/12
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 DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358
 NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833



PROFILE VIEW
VERT. SCALE : 1" = 5'

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-27
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	M. PUGH PROJECT ENGINEER	POST MILES R22.56	LOG OF TEST BORINGS 6 OF 7	
	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED				
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)					ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 29 OF 47

FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163WD+1-ST-27.dwg

TIME PLOTTED => 10:15
 USERNAME => Carrick, Steven

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	406	423

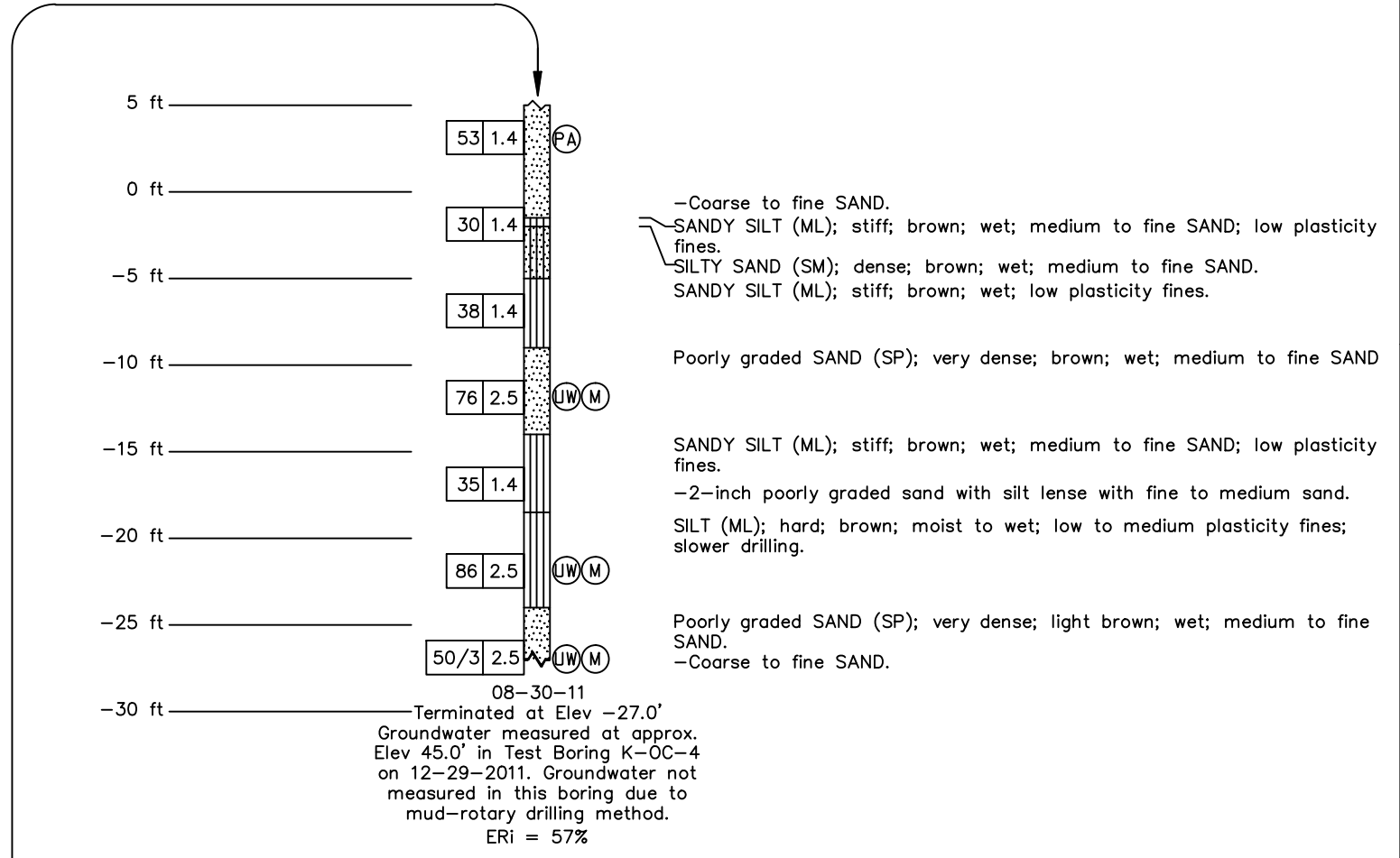
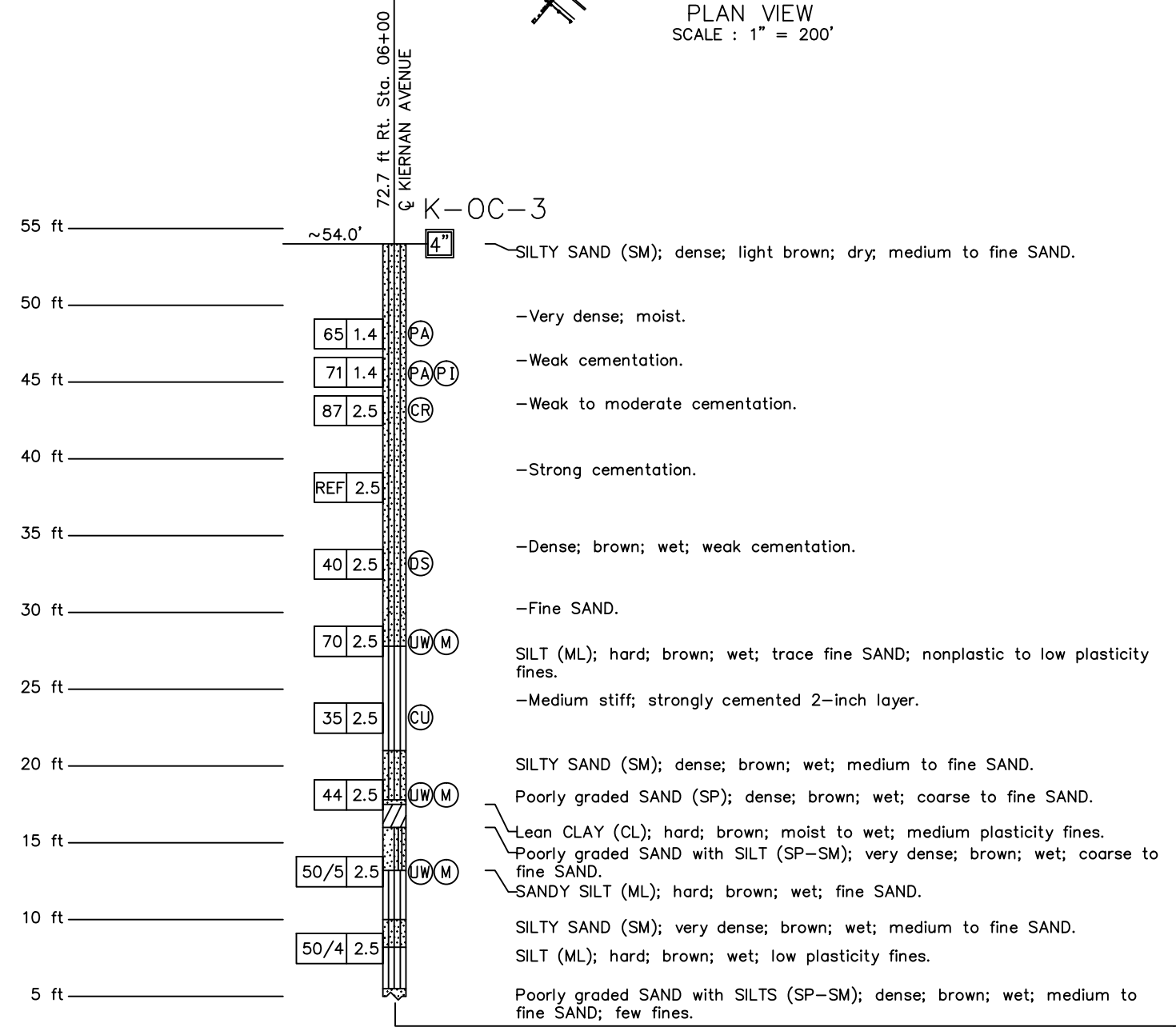
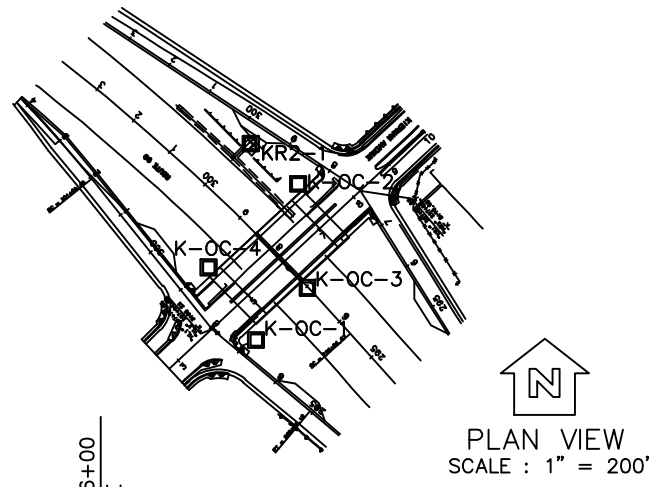
REGISTERED CIVIL ENGINEER	DATE
<i>Michael A. Pugh</i>	9/4/12
PLANS APPROVAL DATE	
9/04/12	

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DEPARTMENT OF PUBLIC WORKS
STANISLAUS COUNTY
1716 MORGAN ROAD
MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
2495 NATOMAS PARK DRIVE, FOURTH FLOOR
SACRAMENTO, CA 95833

- NOTES:
- 1) TEST BORINGS K-OC-1, K-OC-2, K-OC-3 AND KR2-1 WERE DRILLED USING A MOBILE B-53 DRILL RIG EQUIPPED WITH 4-INCH ROTARY WASH. TEST BORING K-OC-4 WAS DRILLED USING A CME 75 DRILL RIG EQUIPPED WITH 6-INCH HOLLOW STEM AUGERS.
 - 2) 2.5-INCH SAMPLES WERE TAKEN USING A MODIFIED CALIFORNIA SOIL-BARREL SAMPLER WITH AN INSIDE DIAMETER OF 2.5-INCH AND OUTSIDE DIAMETER OF 3-INCH.
 - 3) STANDARD PENETRATION TEST (SPT) SAMPLER: I.D.=1.4-INCH; O.D.=2-INCH
 - 4) A 140 LB HAMMER FALLING 30 INCHES WAS USED TO DRIVE SAMPLERS.
 - 5) VISUAL CLASSIFICATION OF EARTH MATERIALS WAS BASED ON FIELD INSPECTION AND WAS CONFIRMED OR REVISED WITH LABORATORY TEST RESULTS.
 - 6) THE BORING LOGS AND RELATED INFORMATION REPRESENT THE OPINION OF THE GEOLOGIST/ENGINEER AS TO THE CHARACTER OF THE MATERIAL AT THE LOCATIONS SHOWN. SOIL AND GROUNDWATER CONDITIONS BETWEEN ADJACENT TEST HOLES AND AT OTHER LOCATIONS MAY DIFFER FROM THOSE SHOWN. GROUNDWATER CONDITIONS MAY CHANGE WITH PASSAGE OF TIME.
 - 7) THIS LOTB SHEET WAS PREPARED IN ACCORDANCE WITH THE CALTRANS SOIL AND ROCK LOGGING, CLASSIFICATION, AND PRESENTATION MANUAL (2010).



NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

PROFILE VIEW
VERT. SCALE : 1" = 5'

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	ST-28
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	POST MILES R22.56	LOG OF TEST BORINGS 7 OF 7	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 30	OF 47

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3

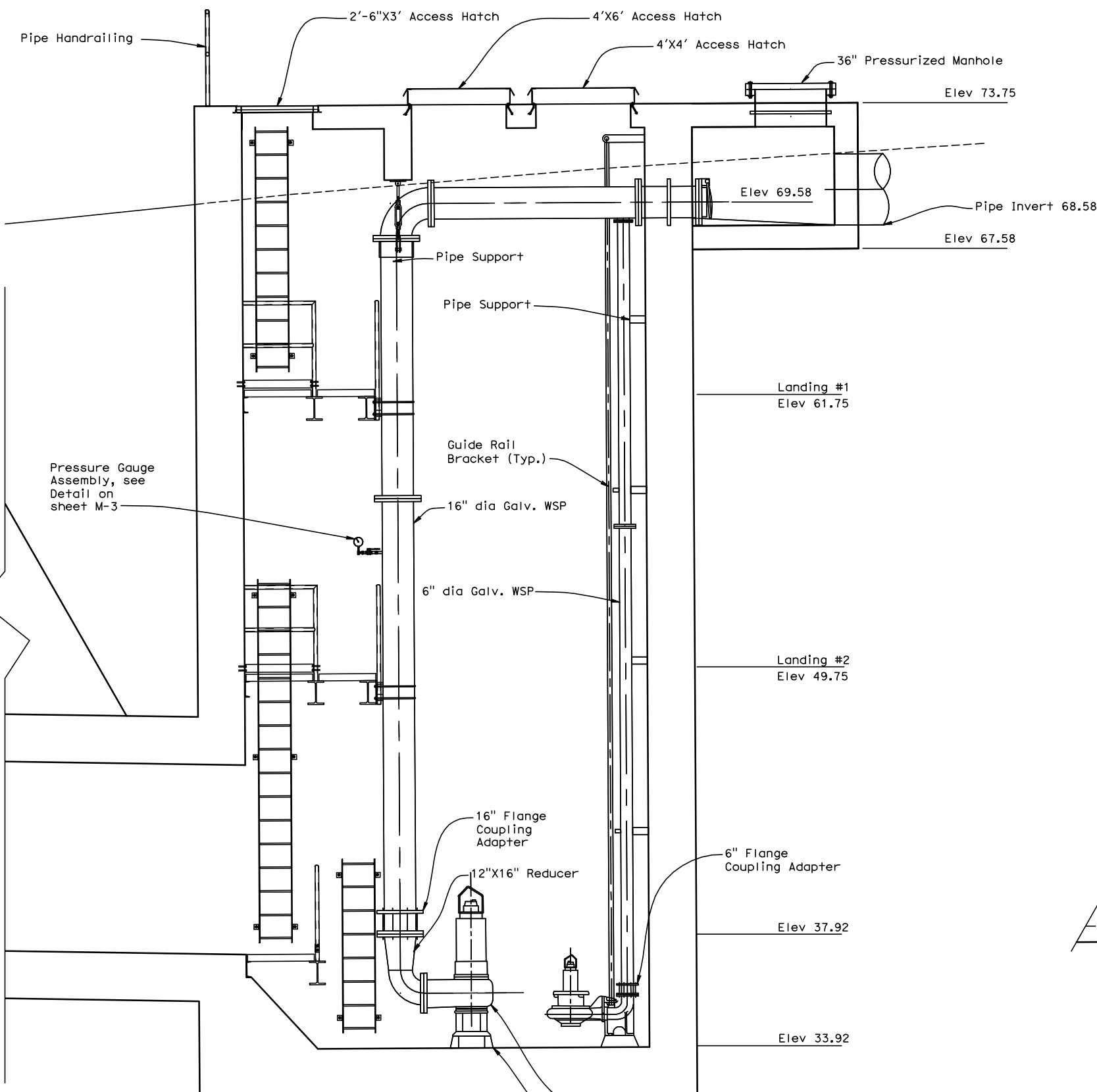
FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163WD1-ST-28.dwg

USERNAME => Steven DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 11:22

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	407	423

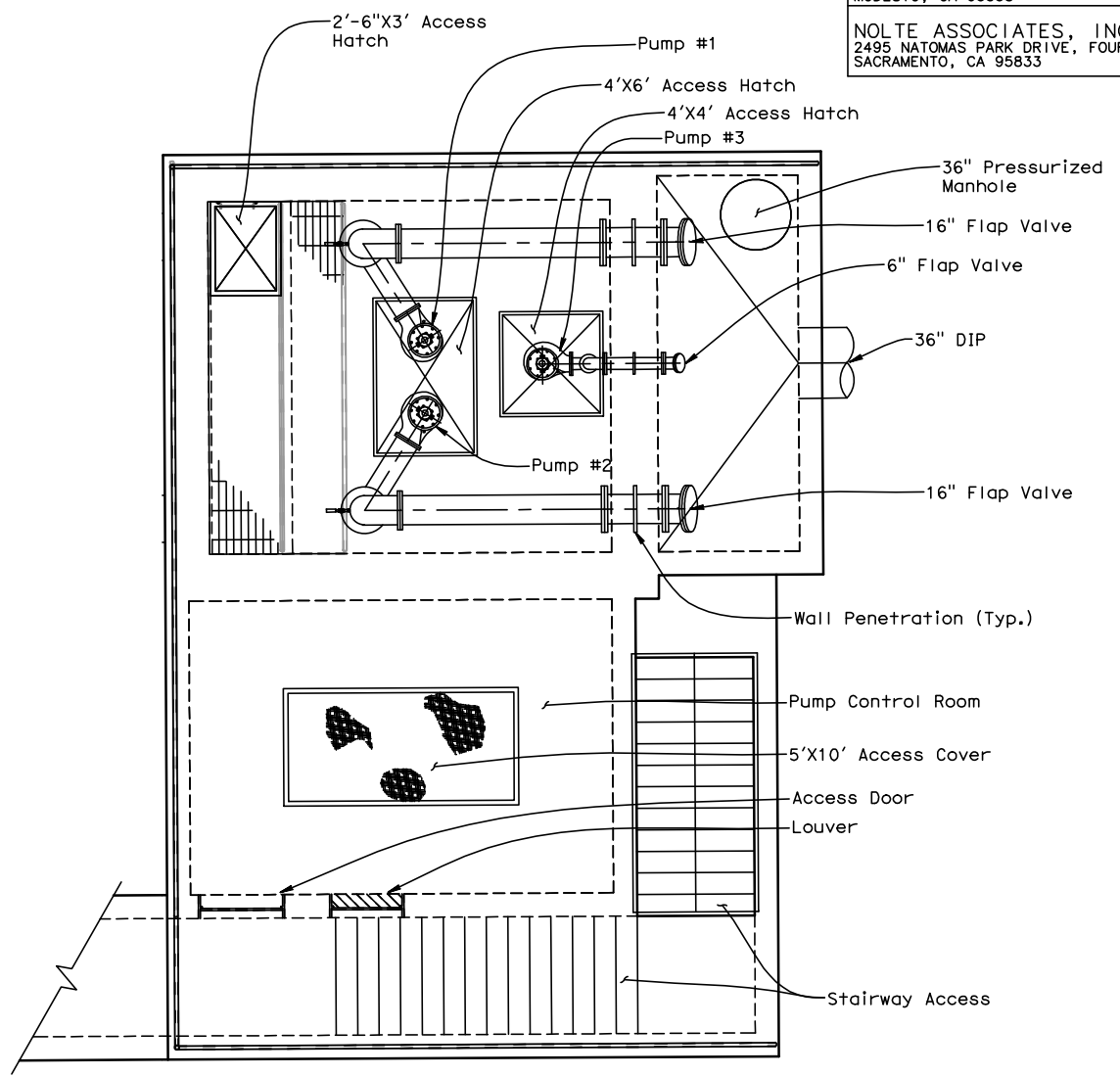
REGISTERED CIVIL ENGINEER **DAVID A. ZENSJUS** DATE 9/7/12
 PLANS APPROVAL DATE 9/04/12
 No. C 59347
 Exp. 6/30/13
 CIVIL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



WET PIT PROFILE
 $\frac{3}{8}''=1'-0''$

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL



WET PIT ROOF PLAN
 $\frac{1}{4}''=1'-0''$

DESIGN OVERSIGHT	DESIGN BY D. ZENSJUS	CHECKED S. GRAY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED S. GRAY	LAYOUT BY S. CARRICK	CHECKED S. GRAY
	QUANTITIES BY S. CARRICK	CHECKED S. GRAY	SPECIFICATIONS BY	PLANS AND SPECS COMPARED

DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0	1	2	3
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PREPARED FOR THE
STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION

PROJECT ENGINEER


BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	M-1
POST MILES R22.56	WET PIT PLAN AND PROFILE	

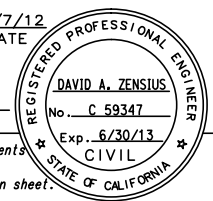
UNIT: 1457
 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304

DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 31	OF 47
	2/21/12 3/28/12 5/07/12 9/5/12		

FILE => N:\SAB041001\Cadd\20120904-DWG_Revisions\10-38100w-m-M01.dwg

DATE PLOTTED => 9-Sep-12 USERNAME => Carrick, Steven TIME PLOTTED => 17:37


 REGISTERED CIVIL ENGINEER DATE 9/7/12
 PLANS APPROVAL DATE 9/04/12
 The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.

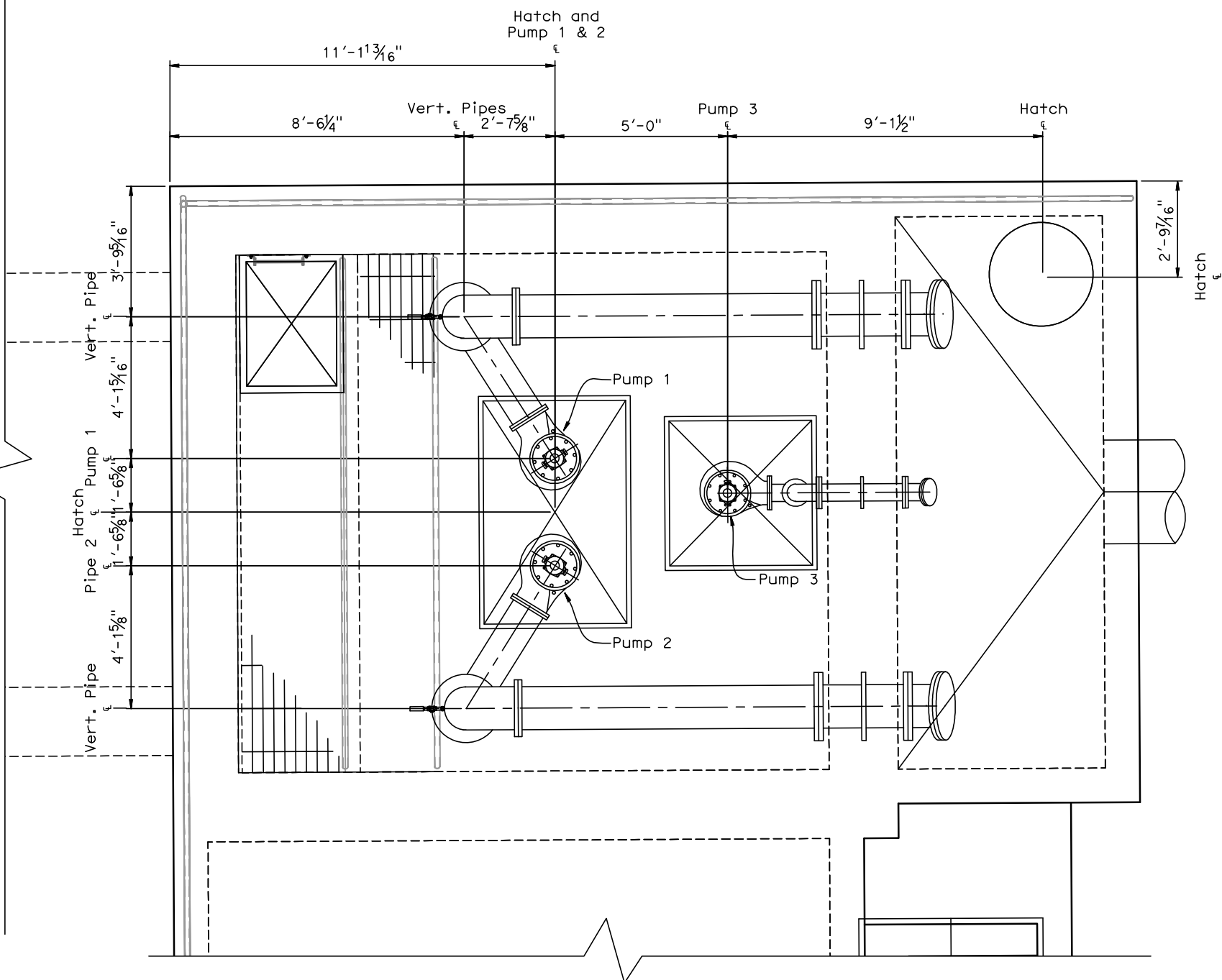


DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
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 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833

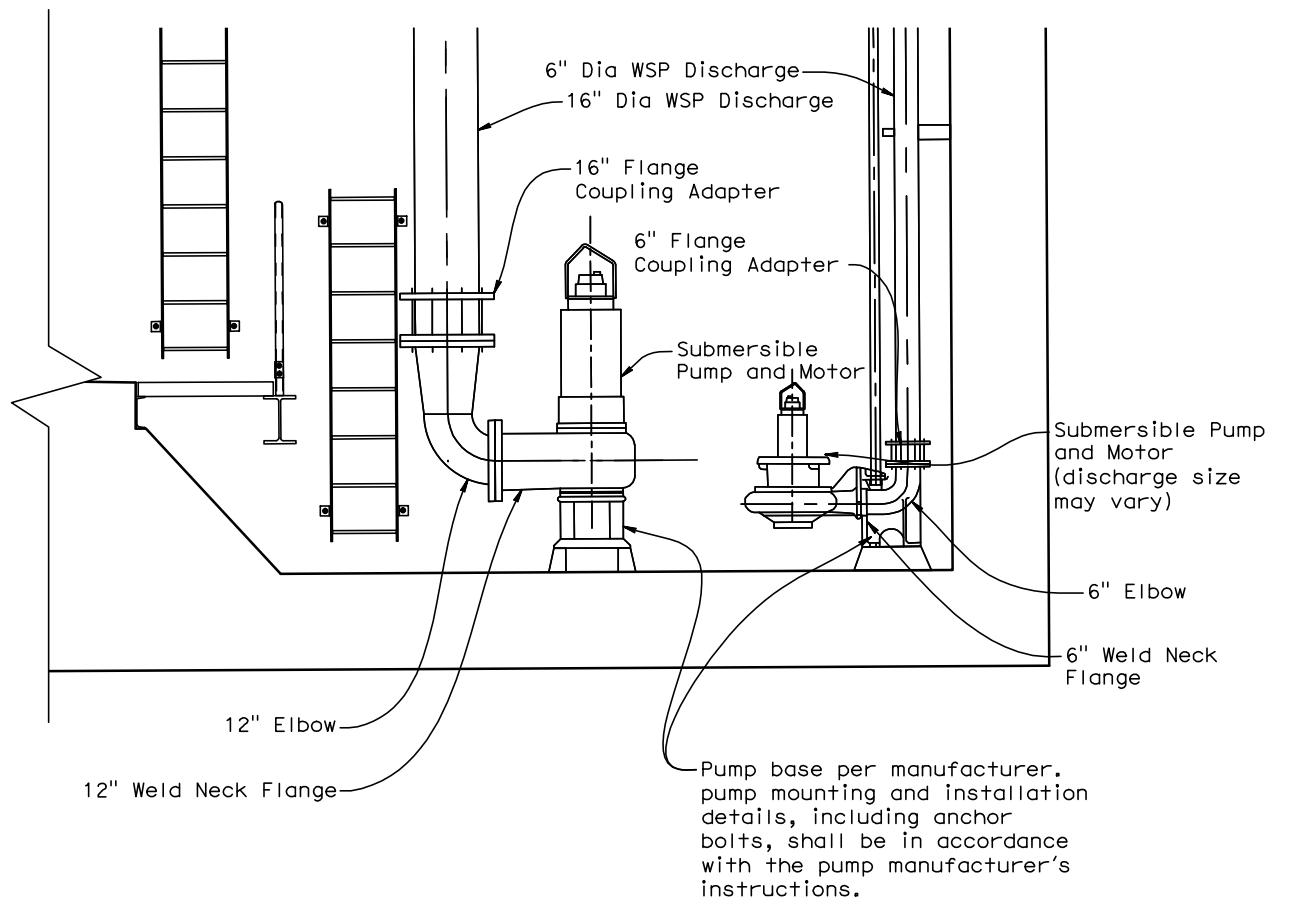
NOTES:

- Pump shall be installed with cast-in-place studs, drilled-in-place epoxy adhesive anchors, or per pump manufacturer's recommendation. The size and number of fasteners shall match manufacturer's recommendations. All fasteners shall be stainless steel.
- Provide elbow and reducer as required to match pump discharge size (may vary by manufacturer selected).
- Pump base anchorage shall be configured so that pump can be installed and removed without the need to remove the discharge elbow or other fixed piping.
- All fasteners within the wet pit shaft shall be stainless steel.

	Pump No. 1 and 2	Pump No. 3
Minimum discharge pipe diameter, inches	12	6
Solids handling capability diameter, mm	76	76
Impeller dynamic balance limit, g-mm/kg rotating mass at 900 rpm	73	73
Pump mounting	Fixed wet pit concrete pier installation	Base elbow-mounted with guide rails



SUMP PLAN
1/2"=1'-0"




WET PIT PROFILE
1/2"=1'-0"

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

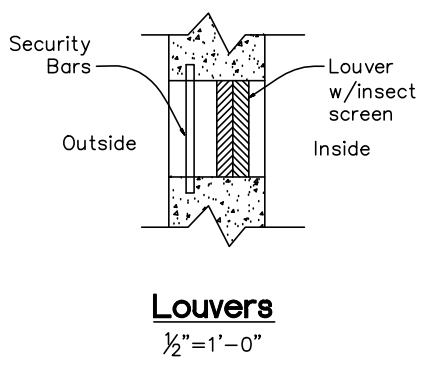
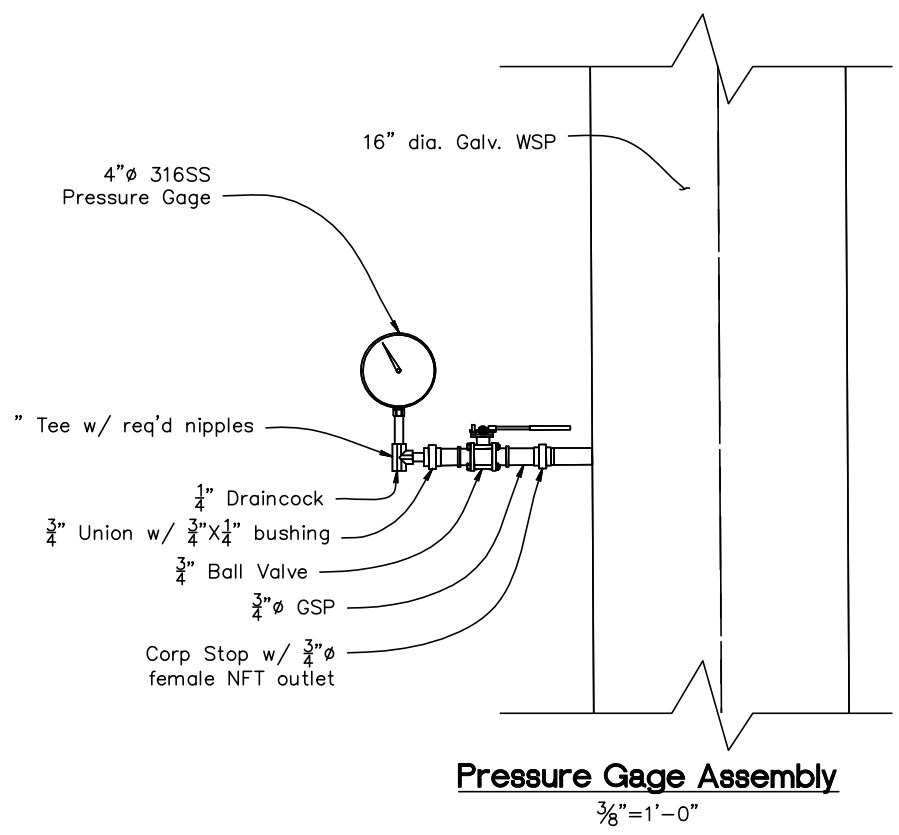
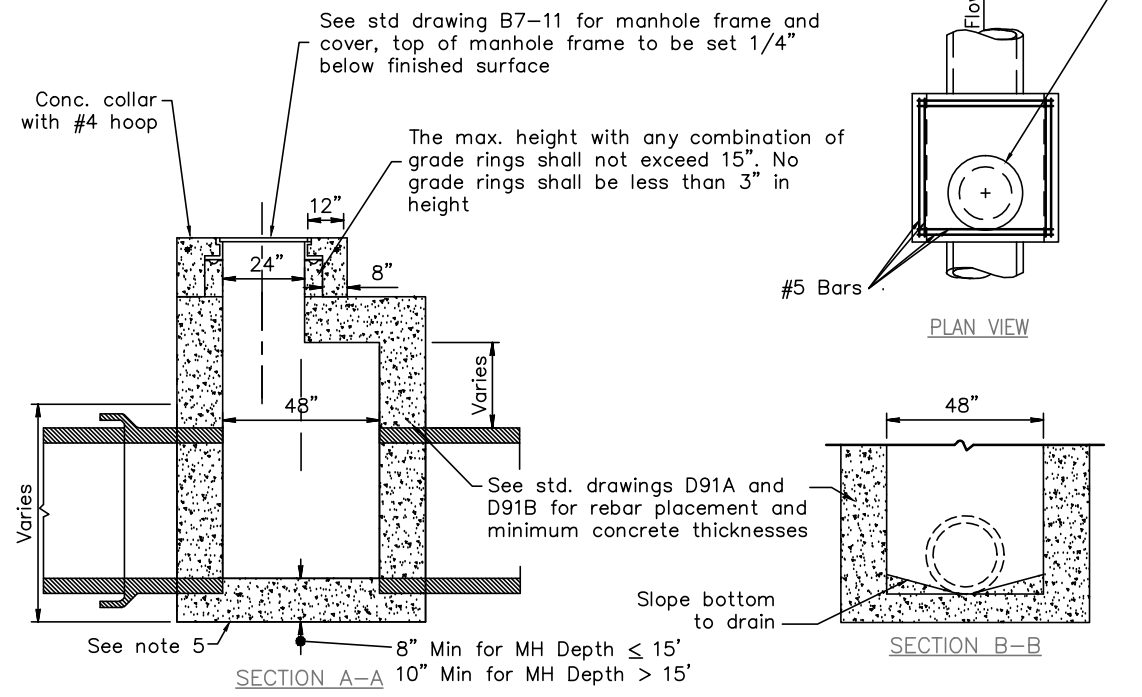
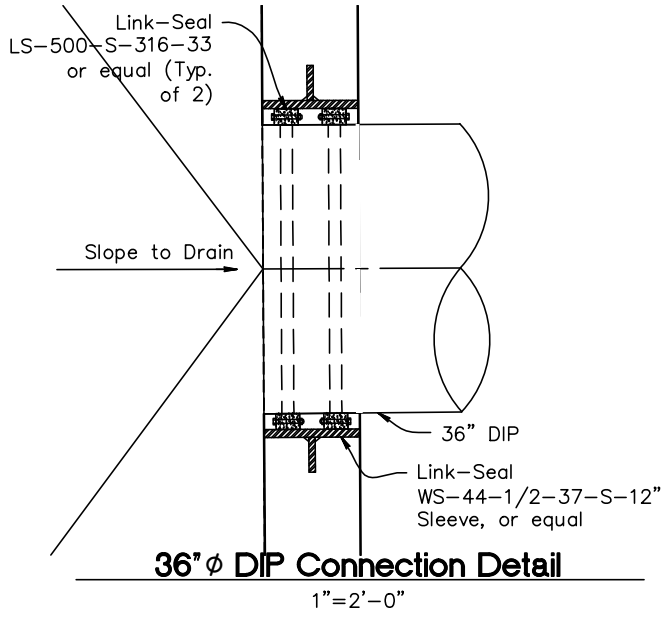
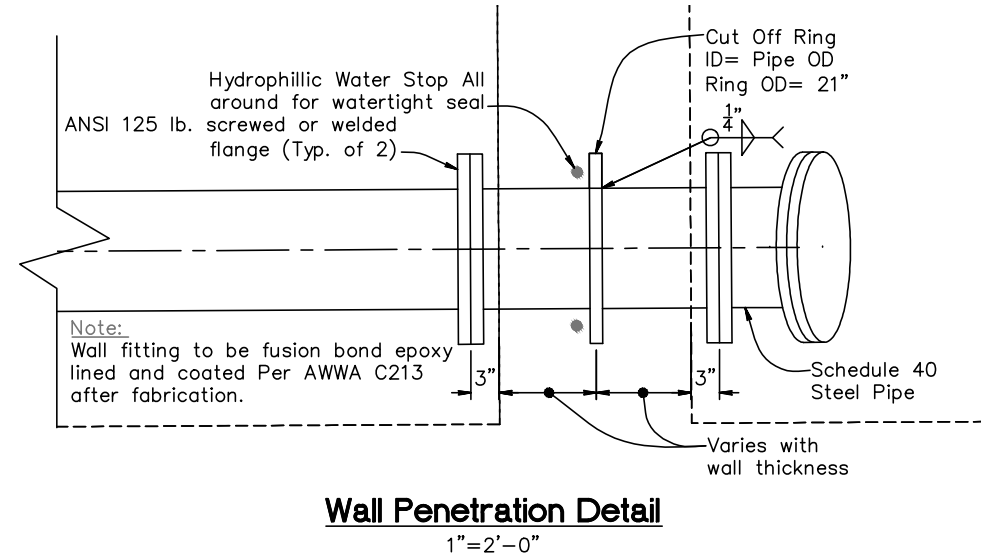
DESIGN OVERSIGHT	DESIGN	BY D. ZENSIUS	CHECKED S. GRAY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO.	38-0163W	219/99 SEPARATION PUMPING PLANT	PROJECT ENGINEER	M-2		
	DETAILS	BY S. CARRICK	CHECKED S. GRAY	LAYOUT	BY S. CARRICK		CHECKED S. GRAY	POST MILES				R22.56	SUMP PLAN AND ELEVATION
	QUANTITIES	BY S. CARRICK	CHECKED S. GRAY	SPECIFICATIONS	BY		PLANS AND SPECS COMPARED	REVISION DATES				2/24/12 3/28/12 5/07/12 9/5/12	
SIGN OFF DATE	DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)						UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	SHEET 32 OF 47		

DATE PLOTTED => 9-Sep-12 USERNAME => Carrick, Steven

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	409	423


 REGISTERED CIVIL ENGINEER DATE 9/7/12
 REGISTERED PROFESSIONAL ENGINEER
 DAVID A. ZENSIUS
 No. C 59347
 Exp. 6/30/13
 CIVIL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE _____
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 DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358
 NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



- Notes:**
- The manhole shall be precast concrete conforming to ASTM specification C478, or cast in place.
 - All cast-in-place concrete shall consist of a minimum of 564 pounds of Portland Cement per cubic yard and have a minimum strength of 3000 psi.
 - Excavations for manholes shall be backfilled as follows:
 - Class II aggregate base rock compacted to a minimum of 95% relative compaction within existing paved areas.
 - Clean native material compacted to a minimum of 90% relative compaction, between 2% to 4% above optimum moisture content, within unpaved and new paved areas.
 - The manhole shall be constructed without steps or ladders.
 - Manhole may be precast or cast in place. Manhole shall be installed over 4" min depth of 3/4" crushed rock. Increase depth if ground water is present.


NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

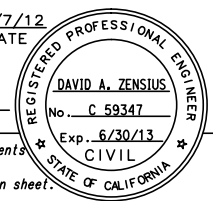
 REVISED PER ADDENDUM No.1 DATED SEPTEMBER 25, 2012

 **Manhole Details**
 N.t.s.

DESIGN OVERSIGHT	DESIGN BY D. ZENSIUS	CHECKED S. GRAY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	M-3
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED S. GRAY	LAYOUT BY S. CARRICK	CHECKED S. GRAY	PROJECT ENGINEER	POST MILES R22.56	DETAILS	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. CARRICK	CHECKED S. GRAY	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	CONTRACT NO.: 10-0L3304	REVISION DATES	SHEET 33 OF 47
				ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	PROJECT NUMBER & PHASE: 1000000100	DISREGARD PRINTS BEARING EARLIER REVISION DATES	2/24/12 3/28/12 5/07/12 9/5/12	
					FILE => N:\SAB041001\Cadd\20120904-DWG_Revisions\10-38100w-m-M03.dwg			

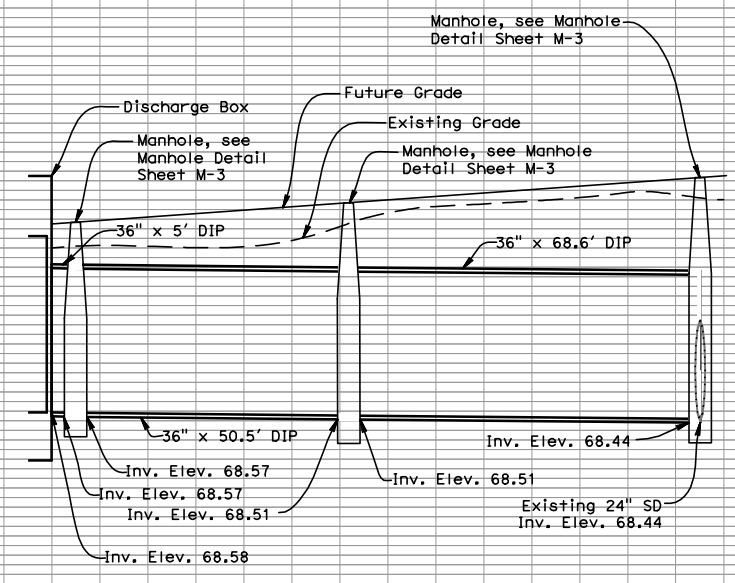
DATE PLOTTED => 9-Sep-12 USERNAME => Michael Powers TIME PLOTTED => 12:02

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	410	423
 REGISTERED CIVIL ENGINEER			9/7/12	DATE	
			9/04/12	PLANS APPROVAL DATE	
<i>The State of California or its officers or agents shall not be responsible for the accuracy or completeness of electronic copies of this plan sheet.</i>					
DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					



80.0
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67.0
66.0
65.0
64.0
63.0
62.0
61.0
60.0



Discharge Pipeline Profile
 Vertical: 1"=2'-0"
 Horizontal: 1"=20'

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY D. ZENSIUS	CHECKED S. GRAY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT	M-4
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED S. GRAY	LAYOUT BY S. CARRICK	CHECKED S. GRAY	PROJECT ENGINEER	POST MILES R22.56	DISCHARGE PIPELINE PROFILE	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. CARRICK	CHECKED S. GRAY	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES: 2/24/12, 3/28/12, 5/07/12, 9/5/12
				ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	DISREGARD PRINTS BEARING EARLIER REVISION DATES		SHEET 34 OF 47

TIME PLOTTED => 17:28
 DATE PLOTTED => 9-Sep-12
 USERNAME => Carrick, Steven

5/1/12
 REGISTERED ELECTRICAL ENGINEER
 DATE
 9/04/12
 PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER
 MICHAEL F. BREGAR
 No. 10246
 Exp. 9/30/12
 ELECTRICAL
 STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

EETS, INC.
 6060 SUNRISE VISTA, #3450
 CITRUS HEIGHTS, CA 95610

ELECTRICAL ABBREVIATIONS

A	AMPERES
AC	ALTERNATING CURRENT
AICS	AMPERES INTERRUPTING CAPACITY, SYMMETRICAL
AT&T	AMERICAN TELEPHONE AND TELEGRAPH
AWG	AMERICAN WIRE GAUGE
/C	MULTI-CONDUCTOR CABLE
C	CONDUIT, CONDUCTOR
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CLG	CEILING
CNTL	CONTROL
CO	CONDUIT ONLY
CPT	CONTROL POWER TRANSFORMER
CT	CURRENT TRANSFORMER
DC	DIRECT CURRENT
DWG	DRAWING
E	EXISTING
ELEV	ELEVATION
G, GND	GROUND
GFCI	GROUND-FAULT CIRCUIT INTERRUPTER
HH	HANDHOLE
HOA	HAND OFF AUTOMATIC
HP	HORSEPOWER
HPS	HIGH PRESSURE SODIUM
HZ	HERTZ
JB	JUNCTION BOX
KCMIL	THOUSAND CIRCULAR MILS
KVA	KILOVOLT-AMPERES
KW	KILOWATT
LA	LIGHTING ARRESTER
LC	LIGHTING CONTACTOR
LTG	LIGHTING
MA	MILLIAMPERE
MAX	MAXIMUM
MCC	MOTOR CONTROL CENTER
MIN	MINIMUM
MTD	MOUNTED
N	NEUTRAL
NC	NORMALLY CLOSED
NO	NORMALLY OPEN
NO.	NUMBER
NTS	NOTE TO SCALE
P	POLE
PB	PUSH BUTTON, PULL BOX
PC	PHOTO CELL
PG&E	PACIFIC GAS & ELECTRIC
PH, Ø	PHASE
PLC	PROGRAMMABLE LOGIC CONTROLLER
PS	PRESSURE SWITCH
PT	POTENTIAL TRANSFORMER
PVC	POLYVINYL CHLORIDE
RECPT	RECEPTACLE
REQ'D	REQUIRED
SW	SWITCH
TSP	TWISTED SHIELDED PAIR
TTB	TELEPHONE TERMINAL BOARD
TYP	TYPICAL
UG	UNDERGROUND
UON	UNLESS OTHERWISE NOTED
V	VOLT
W	WATTS, WIRE
W/	WITH
WP	WEATHERPROOF

ELECTRICAL SYMBOLS ONE LINE DIAGRAM

	KILOWATT HOUR METER
	CURRENT TRANSFORMER, QUANTITY INDICATED
	POWER TRANSFORMER
	MOTOR OVERLOAD HEATER
	CIRCUIT BREAKER, 3 POLE UNLESS NOTED MCP INDICATES MOTOR CIRCUIT PROTECTOR
	MAGNETIC MOTOR STARTER, NEMA SIZE INDICATED FULL-VOLTAGE NON-REVERSING UNLESS NOTED RV=REDUCED VOLTAGE STARTING 2S, 2W=2 SPEED, 2 WINDING
	SWITCH
	MOTOR
	GROUND
	DELTA CONNECTION
	WYE CONNECTION
	POWER FAILURE RELAY
	INCOMING ELECTRIC SERVICE

STANDARD PLANS 2010 REFERENCES

ES-6A	LIGHTING STANDARD, TYPES 15 AND 21
ES-7N	SIGNAL AND LIGHTING STANDARD, DETAIL NO. 2
ES-8A	PULLBOX

ELECTRICAL SYMBOLS COMMON

(AIP)	ABANDON IN PLACE. REMOVE ABOVE GROUND CONDUITS AND CONNECTORS DOWN TO 75mm BELOW GROUND LEVEL. REMOVE CONDUCTORS IN UG CONDUITS. PLUG UNDERGROUND CONDUITS AND ABANDON IN PLACE.
(D)	EXISTING TO BE DEMOLISHED
(E)	EXISTING TO REMAIN
(RL)	DEMO PLANS-EXISTING TO BE RELOCATED. NEW PLANS-FINAL LOCATION OF RELOCATED EQUIPMENT.
(RS)	EXISTING TO BE REMOVED AND SALVAGED
27C-2#12, 1#12G	SINGLE RUN OF RACEWAY AND CIRCUIT CONDUCTORS. FIRST NUMBER IS RACEWAY SIZE. THE FOLLOWING NUMBERS ARE THE CONDUCTOR QUANTITIES AND SIZES.
	INTERCEPTION POINT DEMO PLANS: EXISTING TO REMAIN TO EXISTING TO BE REMOVED. NEW PLANS: EXISTING TO NEW

ELECTRICAL SYMBOLS - PLANS

CLG	WALL	FLOOR	ABANDON IN PLACE. REMOVE ABOVE GROUND CONDUITS
			INCANDESCENT OR HID FIXTURE
			FLUORESCENT FIXTURE - CIRCLE INDICATES J-BOX ABOVE
			AREA LIGHT AND POLE
		*	DUPLEX RECEPTACLE
		*	DUPLEX RECEPTACLE GFCI TYPE
		**	SINGLE POLE SWITCH
		*	JUNCTION BOX * FOR WALL MTD
*			+ 460mm UON
**			+ 1220mm UON
			DETAIL CALL-OUT: X, DETAIL IDENTIFIER; Y, SHEET WHERE DETAIL IS DRAWN
			GROUND ROD, 3/4" X 10'-0" GW NEXT TO SYMBOL INDICATES GROUND ROD IN HANDHOLE (SEE DETAIL 1/EE-8)
			CONDUIT CONCEALED IN WALLS OR CEILING
			CONDUIT UNDER GROUND
			CONDUIT EXPOSED
			CONNECTION TO GROUND BUS.
			GROUNDING CONDUCTOR 30" BELOW GRADE, #4/0 UON
			EXOTHERMIC WELD CONNECTION
			HOMERUN TO PANEL A, CIRCUIT 1 AND 3
			CONDUIT BENDS TOWARD OBSERVER
			CONDUIT BENDS AWAY FROM OBSERVER
			CONDUIT STUB-OUT AND CAPPED
			MOTOR CONNECTION
			SHEET NOTE, SEE NOTE INDICATED

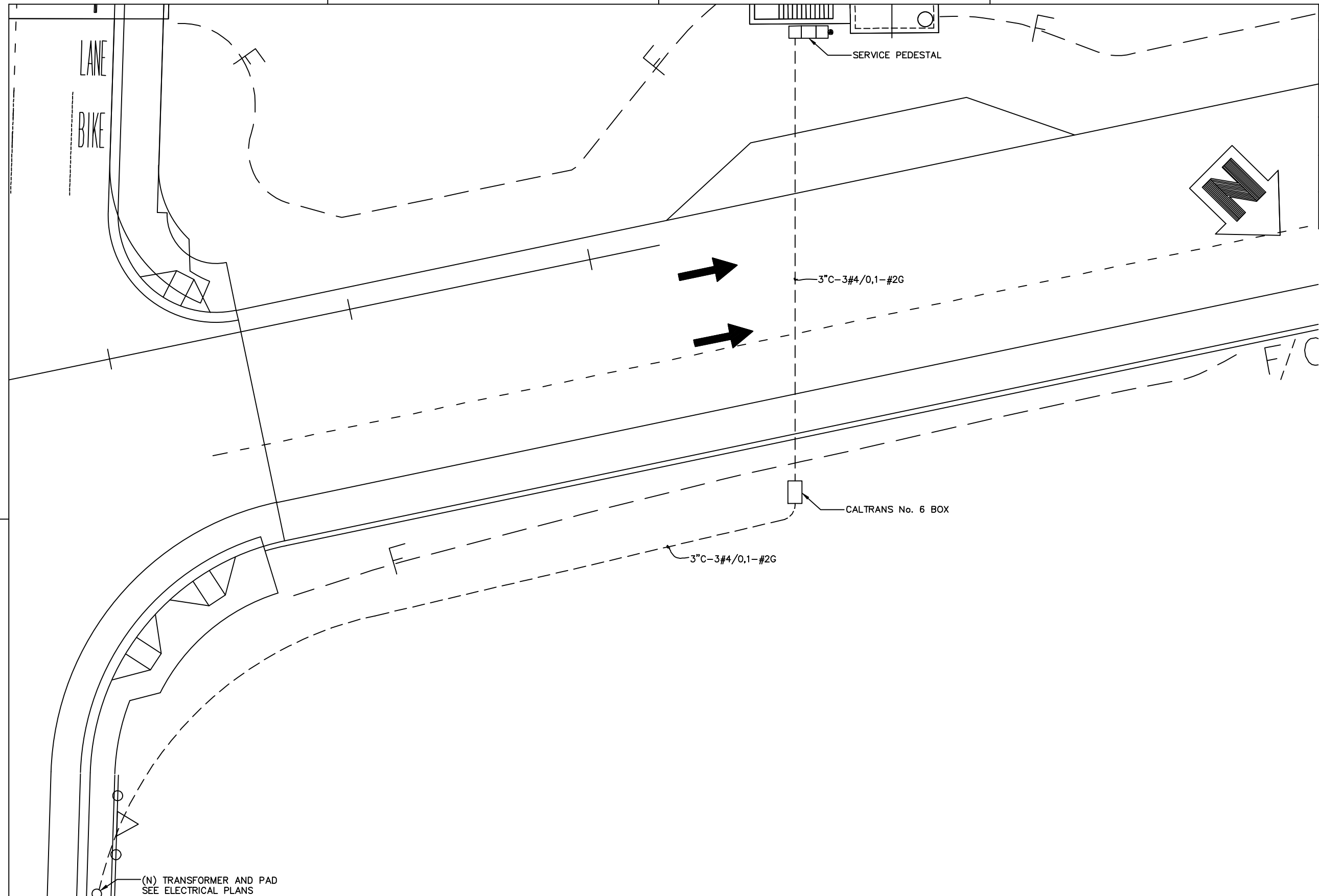
ELECTRICAL SYMBOLS - SCHEMATIC DIAGRAMS

NORMALLY OPEN	NORMALLY CLOSED	DEVICE
		CONTACT
		PUSH BUTTON SINGLE CIRCUIT MOMENTARY CONTACT
		LIQUID LEVEL SWITCH
		TEMPERATURE SWITCH
		TIMED CONTACT CONTACT ACTION RETARDED ON DE-ENERGIZATION
		SELECTOR SWITCH
		PILOT LIGHT R=RED, W=WHITE, G=GREEN, A=AMBER, C=CLEAR
		RELAY
		TIME DELAY RELAY
		STARTER COIL
		SOLENOID OPERATED VALVE
		MOTOR
		FUSE
		CONTROL POWER TRANSFORMER
		GROUND
		WIRING IN MOTOR STARTER OR CONTROL PANEL
		FIELD WIRING
		TERMINAL BLOCK IN MOTOR STARTER OR PANEL
		TERMINAL BLOCK IN PLC
		RESISTOR
		CIRCUIT BREAKER

NOTE:
 THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY M. BREGAR	CHECKED J. GUILLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. BREGAR PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		EE-1	
SIGN OFF DATE	DETAILS BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT BY M. BREGAR	CHECKED J. GUILLORY	PLANS AND SPECS COMPARED BY M. BREGAR		POST MILES R22.56	SYMBOLS & ABBREVIATIONS			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)				ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 35 OF 47
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USERNAME => Tpkcjm DATE PLOTTED => 5-May-12 TIME PLOTTED => 16:01



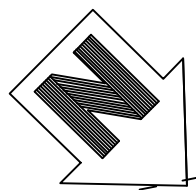
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	412	423

REGISTERED ELECTRICAL ENGINEER
 DATE: 5/1/12
 PLANS APPROVAL DATE: 9/04/12
 MICHAEL F. BREGAR
 No. 10246
 Exp. 9/30/12
 ELECTRICAL
 STATE OF CALIFORNIA

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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

EETS, INC.
 6060 SUNRISE VISTA, #3450
 CITRUS HEIGHTS, CA 95610



SCALE: 1" = 10'

NOTE:
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DESIGN OVERSIGHT	DESIGN BY M. BREGAR	CHECKED J. GULLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE
SIGN OFF DATE	DETAILS BY M. BREGAR	CHECKED J. GULLORY	LAYOUT BY M. BREGAR	CHECKED J. GULLORY
	QUANTITIES BY M. BREGAR	CHECKED J. GULLORY	SPECIFICATIONS BY M. BREGAR	PLANS AND SPECS COMPARED BY M. BREGAR

PREPARED FOR THE
STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

M. BREGAR
 PROJECT ENGINEER

219/99 SEPARATION PUMPING PLANT
ELECTRICAL SITE PLAN

EE-2

DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)

ORIGINAL SCALE IN INCHES FOR REDUCED PLANS

UNIT: 1457
 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304

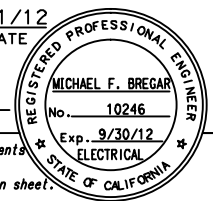
DISREGARD PRINTS BEARING EARLIER REVISION DATES

REVISION DATES	SHEET	OF
10/21/11 12/24/12 3/27/12 5/01/12	36	47

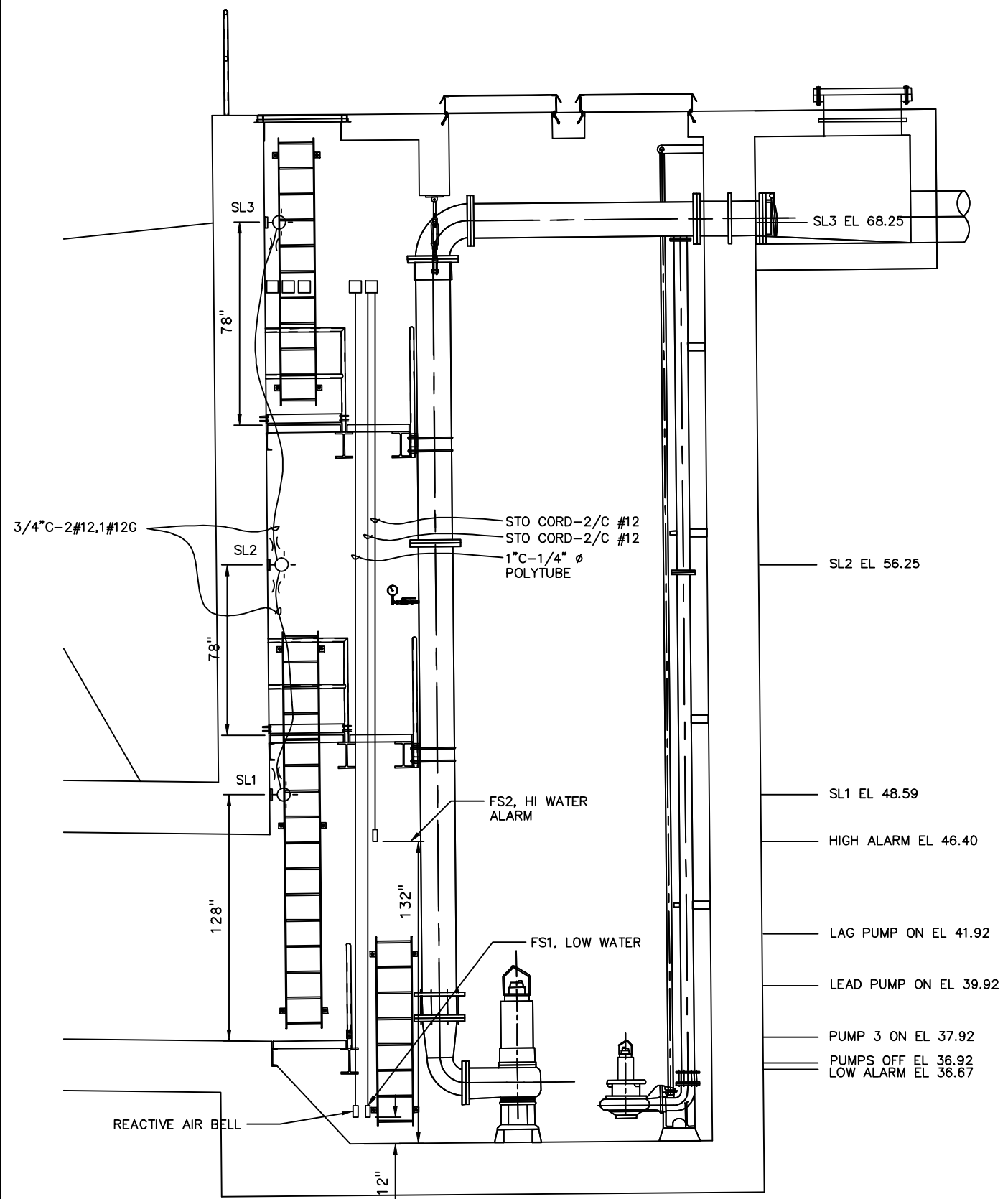
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USERNAME => Tpkcjm DATE PLOTTED => 5-May-12 TIME PLOTTED => 16:01

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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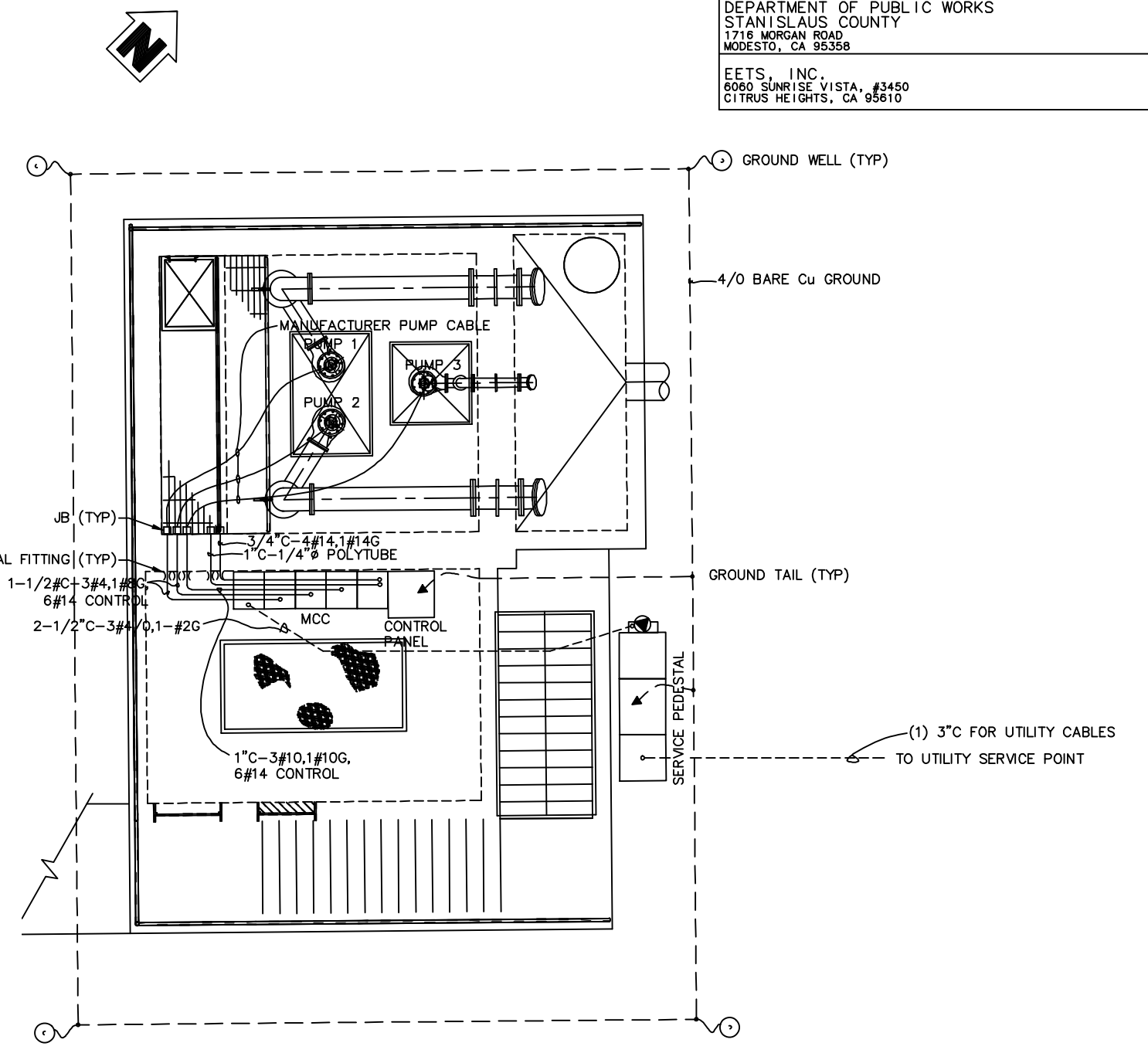

 5/1/12 DATE
 REGISTERED ELECTRICAL ENGINEER
 PLANS APPROVAL DATE 9/04/12
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WET PIT PROFILE
3/8"=1'-0"

NOTE:
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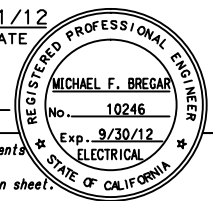
PUMP STATION ELECTRICAL PLAN
1/4"=1'-0"

DESIGN OVERSIGHT	DESIGN BY M. BREGAR	CHECKED J. GUILLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE
SIGN OFF DATE	DETAILS BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT BY M. BREGAR	CHECKED J. GUILLORY
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY M. BREGAR	CHECKED J. GUILLORY	SPECIFICATIONS BY M. BREGAR	PLANS AND SPECS COMPARED BY M. BREGAR

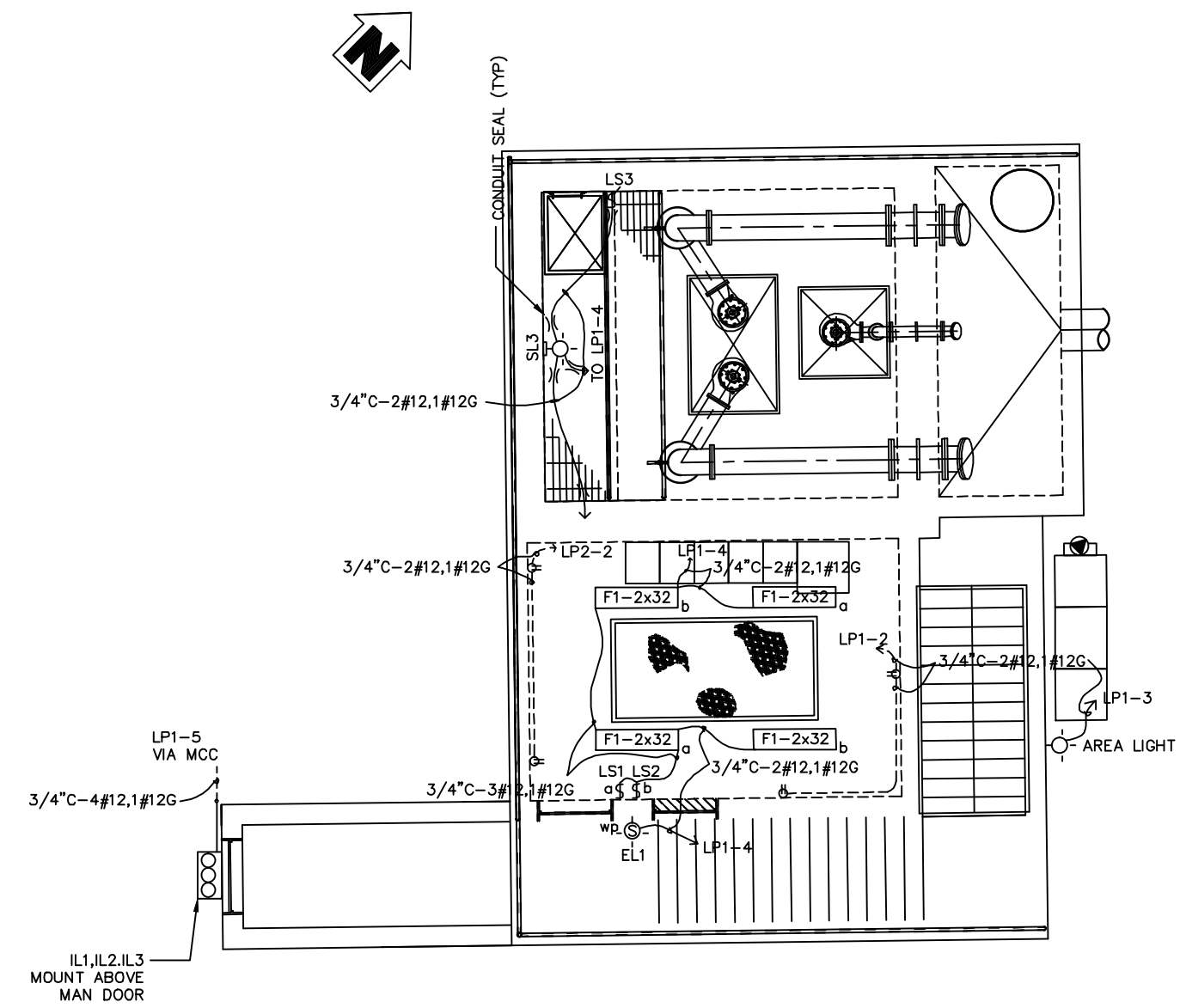
PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. BREGAR PROJECT ENGINEER	BRIDGE NO. 38-0163W POST MILES R22.56	219/99 SEPARATION PUMPING PLANT PUMP STATION PLAN & PROFILE	EE-3
UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES 10/21/11 12/20/12 3/28/12 5/01/12	SHEET 37 OF 47	USERNAME => Tpkim DATE PLOTTED => 5-May-12 TIME PLOTTED => 16:02

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	414	423

 5/1/12
 REGISTERED ELECTRICAL ENGINEER DATE
 9/04/12
 PLANS APPROVAL DATE
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PUMP STATION LIGHTING PLAN
 $\frac{1}{4}''=1'-0''$

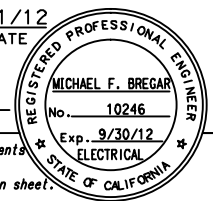
NOTE:
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DESIGN OVERSIGHT	DESIGN	BY M. BREGAR	CHECKED J. GUILLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION M. BREGAR PROJECT ENGINEER	BRIDGE NO.	38-0163W	219/99 SEPARATION PUMPING PLANT PUMP STATION LIGHTING PLAN	EE-4				
	DETAILS	BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT	BY M. BREGAR		CHECKED J. GUILLORY	POST MILES			R22.56			
	QUANTITIES	BY M. BREGAR	CHECKED J. GUILLORY	SPECIFICATIONS	BY M. BREGAR		CHECKED J. GUILLORY	PLANS AND SPECS COMPARED			M. BREGAR			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)						ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 38	OF 47

USERNAME => Tpkhqm DATE PLOTTED => 5-May-12 TIME PLOTTED => 16:02

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	415	423

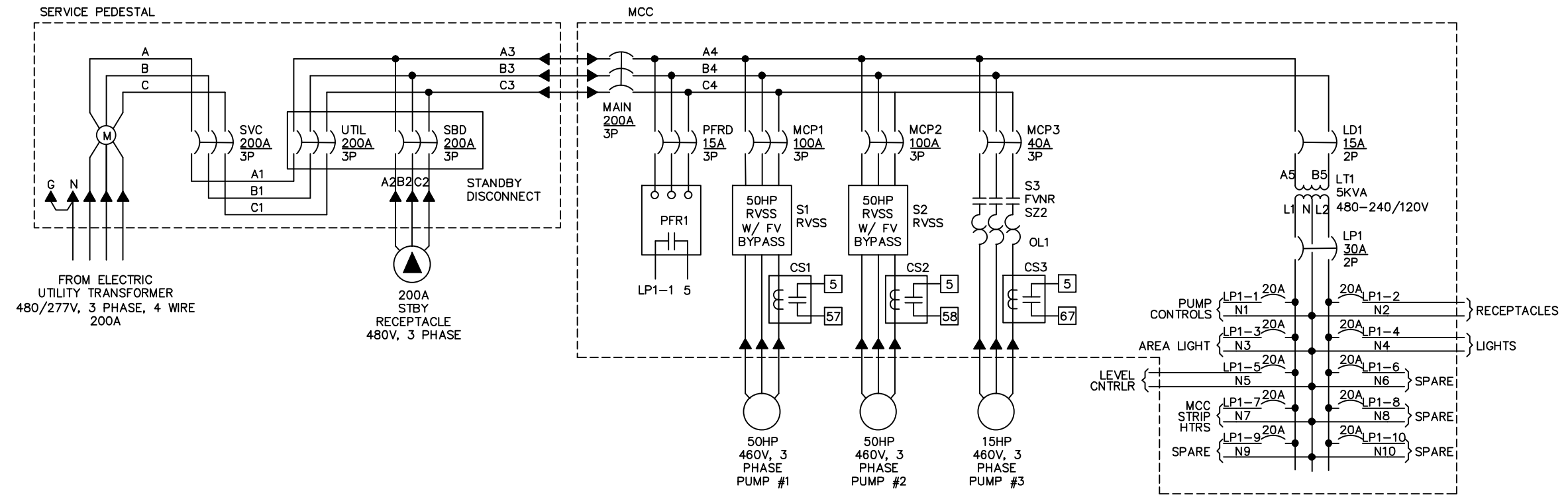
 5/1/12 DATE
 REGISTERED ELECTRICAL ENGINEER
 PLANS APPROVAL DATE 9/04/12
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 EETS, INC.
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LOAD CALCULATIONS FOR STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION SERVICE PEDESTAL	
DESCRIPTION	KVA
PUMP 1, 65 FLA, 460V, 3 PHASE	54
PUMP 2, 65 FLA, 460V, 3 PHASE	54
PUMP 3, 21 FLA, 460V, 3 PHASE	18
MISCELLANEOUS LOADS	5
PLUS 25% OF LARGEST MOTOR = .25 x 54KVA =	13.5
TOTAL KVA	144.5
TOTAL AMPS AT 480V, 3 PHASE	174A

REQUIRE 200A SERVICE



NOTE:
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SIGN OFF DATE	DETAILS BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT BY M. BREGAR	CHECKED J. GUILLORY	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)					ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	5/01/12	

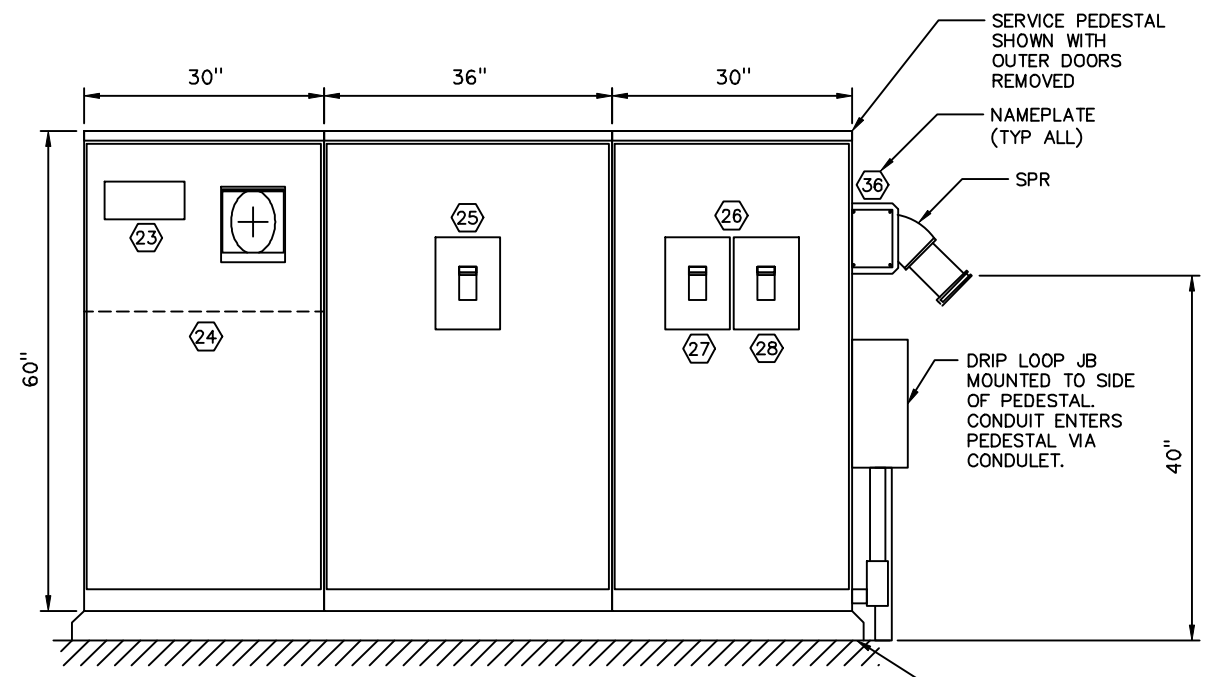
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	416	423

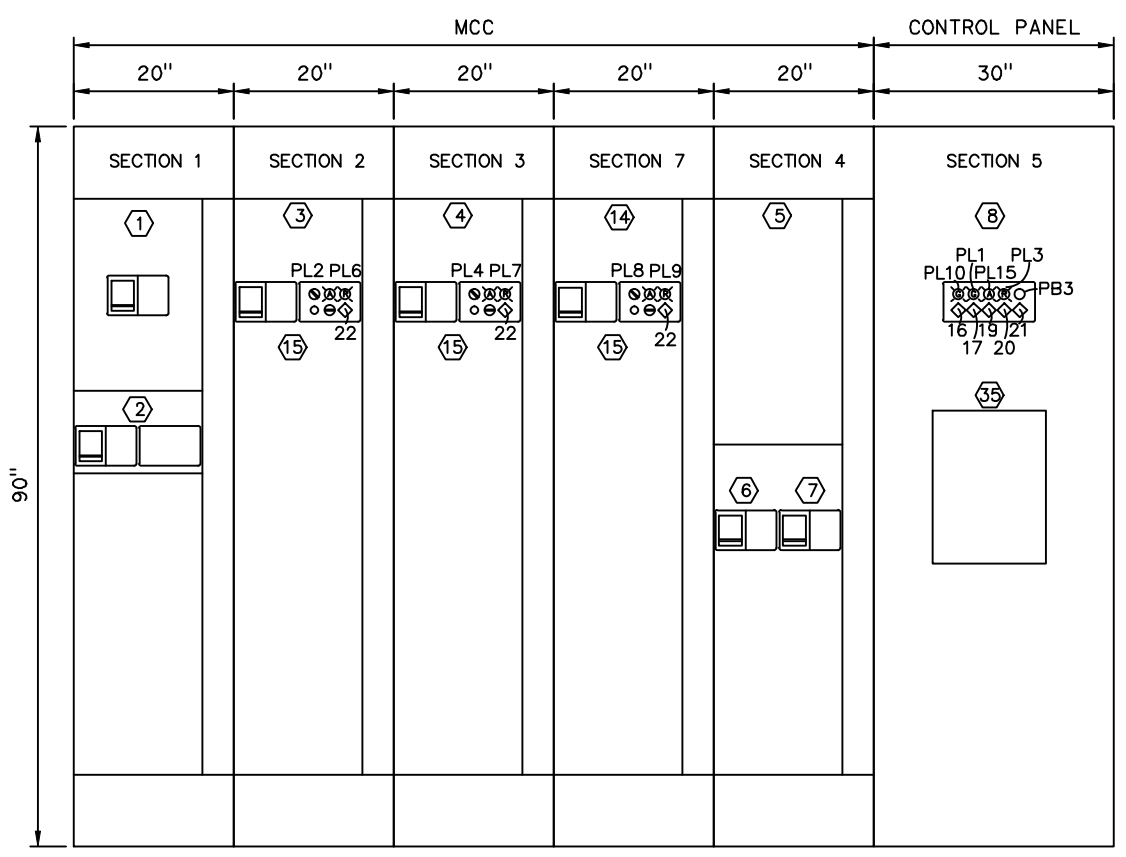
REGISTERED ELECTRICAL ENGINEER
 DATE: 5/1/12
 PLANS APPROVAL DATE: 9/04/12
 MICHAEL F. BREGAR
 No. 10246
 Exp. 9/30/12
 ELECTRICAL
 STATE OF CALIFORNIA

DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

EETS, INC.
 6060 SUNRISE VISTA, #3450
 CITRUS HEIGHTS, CA 95610



SERVICE PEDESTAL
NOT TO SCALE



MCC/CONTROL PANEL ELEVATION
NOT TO SCALE

NAMEPLATE SCHEDULE		
ITEM NO.	DESCRIPTION	LETTER HEIGHT (IN)
1	SERVICE BUILDING DISCONNECT	.25
2	PHASE FAILURE RELAY PFR	.25
3	PUMP #1	.25
4	PUMP #2	.25
5	PANEL LP1	.25
6	TRANSFORMER LD1 DISCONNECT	.25
7	PANEL LP1 DISCONNECT	.25
8	CONTROL PANEL	.25
9	NOT USED	.25
10	NOT USED	.25
11	NOT USED	.25
12	NOT USED	.25
13	NOT USED	.25
14	PUMP #3	.25
15	WARNING: MOTOR DISCONNECT DOES NOT OPEN CONTROL CIRCUIT	.25 *
16	PUMP CONTROL POWER ON	.125
17	PLC CONTROL POWER ON	.125
18	NOT USED	.125
19	LOW WATER	.125
20	HIGH WATER ALARM	.125
21	ALARM RESET	.125
22	SEAL FAILURE	.125
23	UTILITY SERVICE METERING	.25
24	SERVICE PULL SECTION	.25
25	SERVICE DISCONNECT	.25
26	SERVICE POWER TRANSFER SWITCH	.25
27	UTILITY	.25
28	STANBY	.25
29	NOT USED	.25
30	NOT USED	.25
31	NOT USED	.25
32	NOT USED	.25
33	NOT USED	.25
34	NOT USED	.25
35	PC	.25
36	480 VOLTS ONLY	.25 *

* = WHITE ON RED

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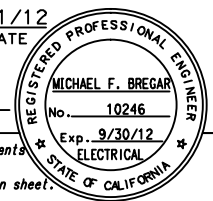
DESIGN OVERSIGHT	DESIGN BY M. BREGAR	CHECKED J. GUILLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT ELECTRICAL ELEVATIONS	EE-6
DETAILS	BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT BY M. BREGAR	CHECKED J. GUILLORY	M. BREGAR PROJECT ENGINEER	POST MILES R22.56		
SIGN OFF DATE	QUANTITIES BY M. BREGAR	CHECKED J. GUILLORY	SPECIFICATIONS BY M. BREGAR	PLANS AND SPECS COMPARED BY M. BREGAR	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	REVISION DATES: 10/21/11, 12/24/11, 3/27/12, 5/01/12
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)					ORIGINAL SCALE IN INCHES FOR REDUCED PLANS: 0 1 2 3	DISREGARD PRINTS BEARING EARLIER REVISION DATES		SHEET 40 OF 47

FILE => N:\SAB041001\Cadd\Elec\EE-6 ELEV.dwg

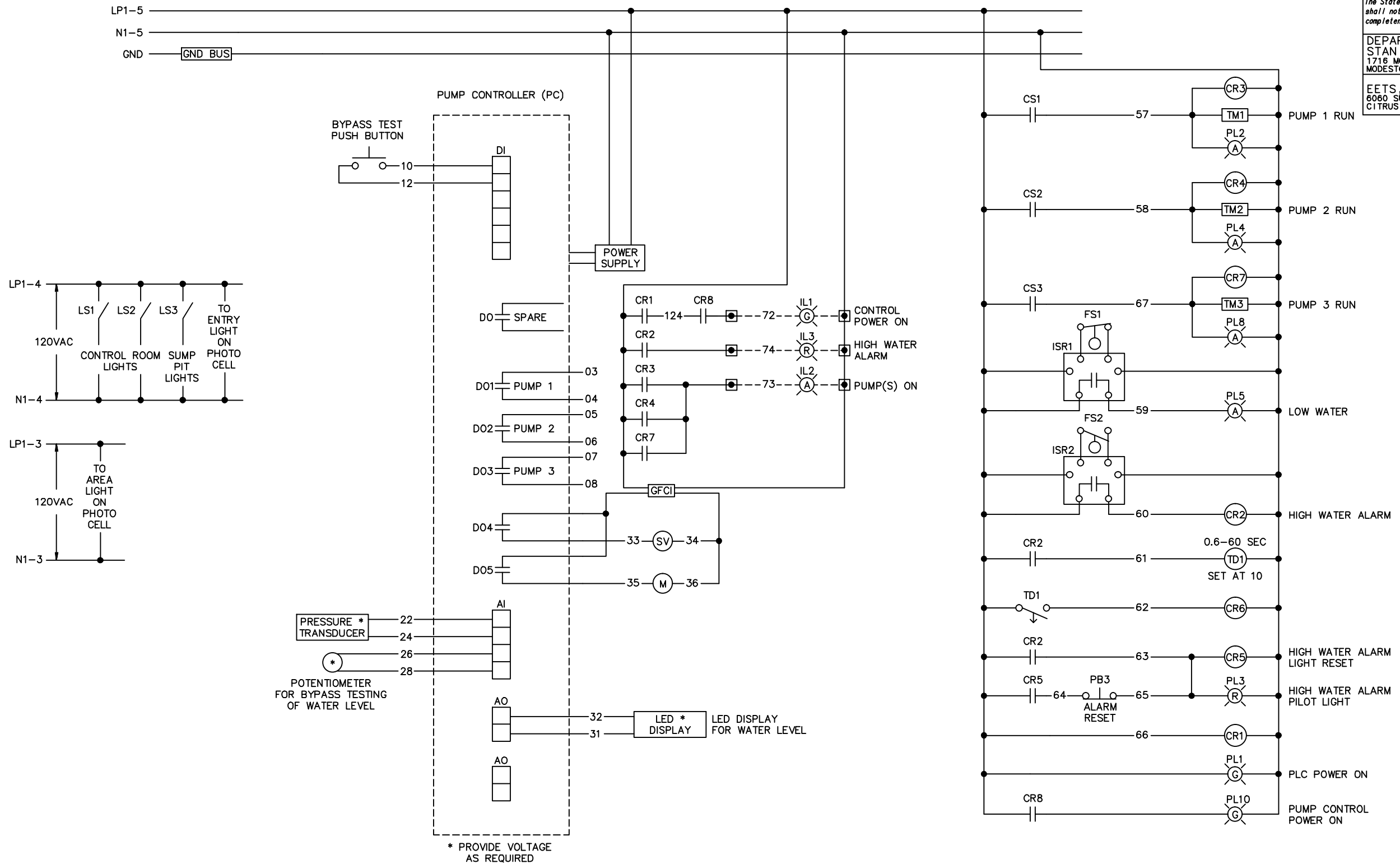
TIME PLOTTED => 16:02 USERNAME => Tpkim DATE PLOTTED => 5-May-12 Kristin

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	417	423

5/1/12
 REGISTERED ELECTRICAL ENGINEER
 DATE
 9/04/12
 PLANS APPROVAL DATE
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 CITRUS HEIGHTS, CA 95610



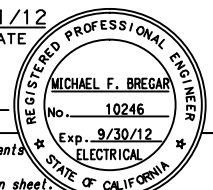
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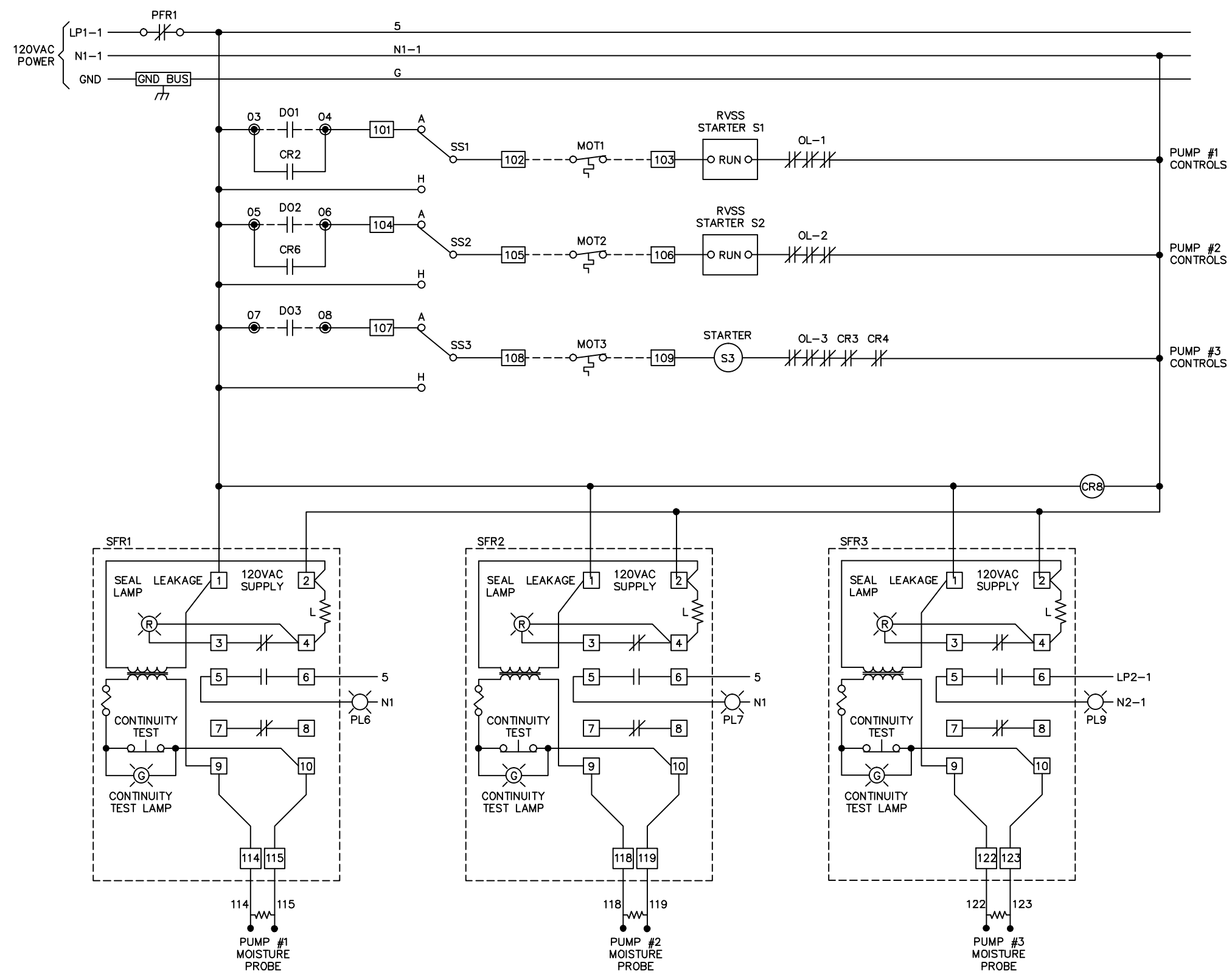
PREPARED FOR THE
STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
 M. BREGAR
 PROJECT ENGINEER

BRIDGE NO.	219/99 SEPARATION PUMPING PLANT	EE-7
POST MILES	38-0163W	
R22.56	LEVEL CONTROL SCHEMATIC	

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	418	423


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SEE FAILURE RELAYS

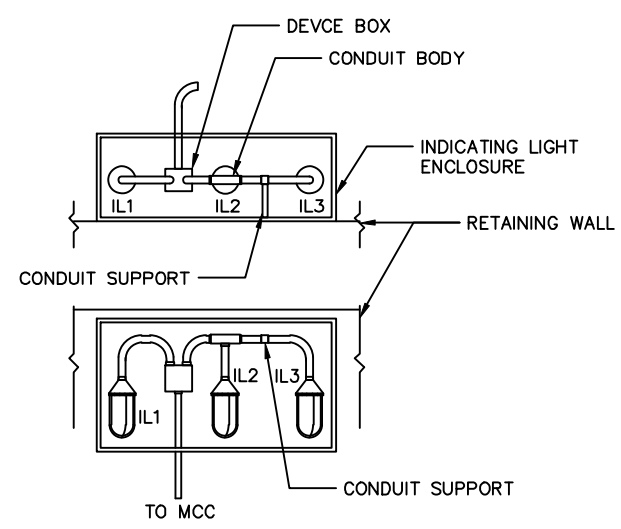
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DESIGN OVERSIGHT	DESIGN	BY M. BREGAR	CHECKED J. GUILLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	BRIDGE NO.	219/99 SEPARATION PUMPING PLANT PUMP SCHEMATICS	EE-8							
	DETAILS	BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT	BY M. BREGAR		CHECKED J. GUILLORY			38-0163W						
	QUANTITIES	BY M. BREGAR	CHECKED J. GUILLORY	SPECIFICATIONS	BY M. BREGAR		PLANS AND SPECS COMPARED			M. BREGAR	R22.56					
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)						UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	<table border="1"> <tr> <th>REVISION DATES</th> <th>SHEET</th> <th>OF</th> </tr> <tr> <td>10/21/11 2/24/12 3/27/12 5/01/12</td> <td>42</td> <td>47</td> </tr> </table>	REVISION DATES	SHEET	OF	10/21/11 2/24/12 3/27/12 5/01/12	42	47
REVISION DATES	SHEET	OF														
10/21/11 2/24/12 3/27/12 5/01/12	42	47														

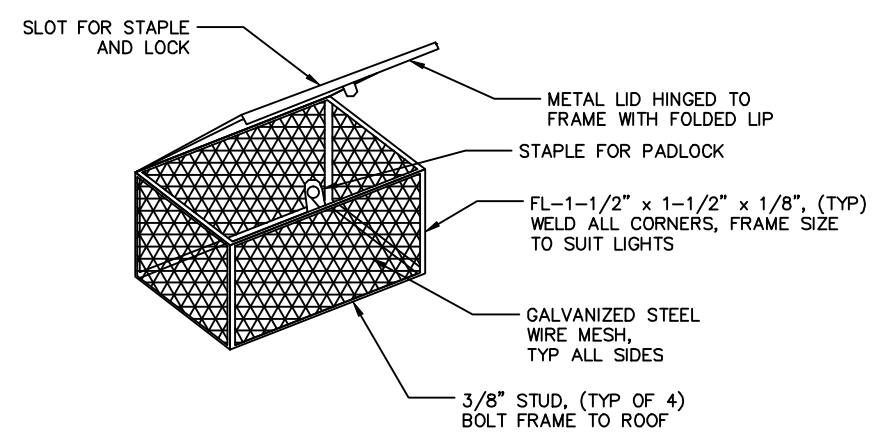
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DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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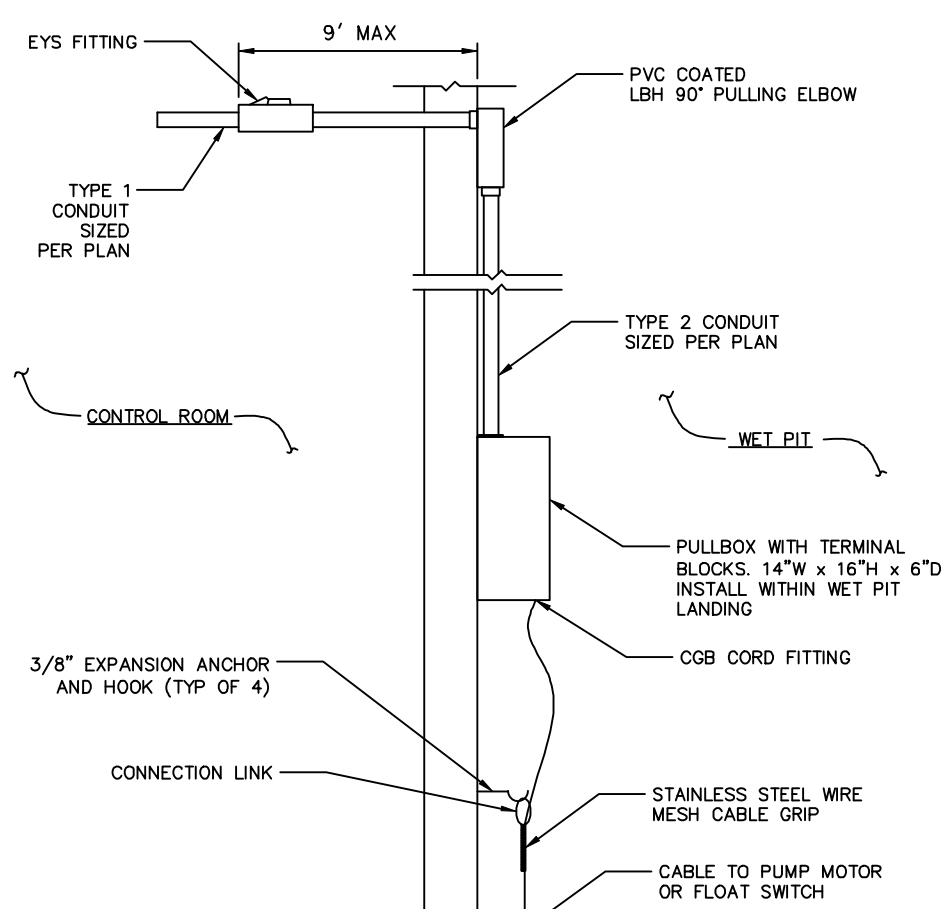
5/1/12 DATE
REGISTERED ELECTRICAL ENGINEER
PLANS APPROVAL DATE 9/04/12
MICHAEL F. BREGAR No. 10246 Exp. 9/30/12 ELECTRICAL STATE OF CALIFORNIA
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CITRUS HEIGHTS, CA 95610



INDICATING LIGHTS MOUNTING DETAIL
NOT TO SCALE



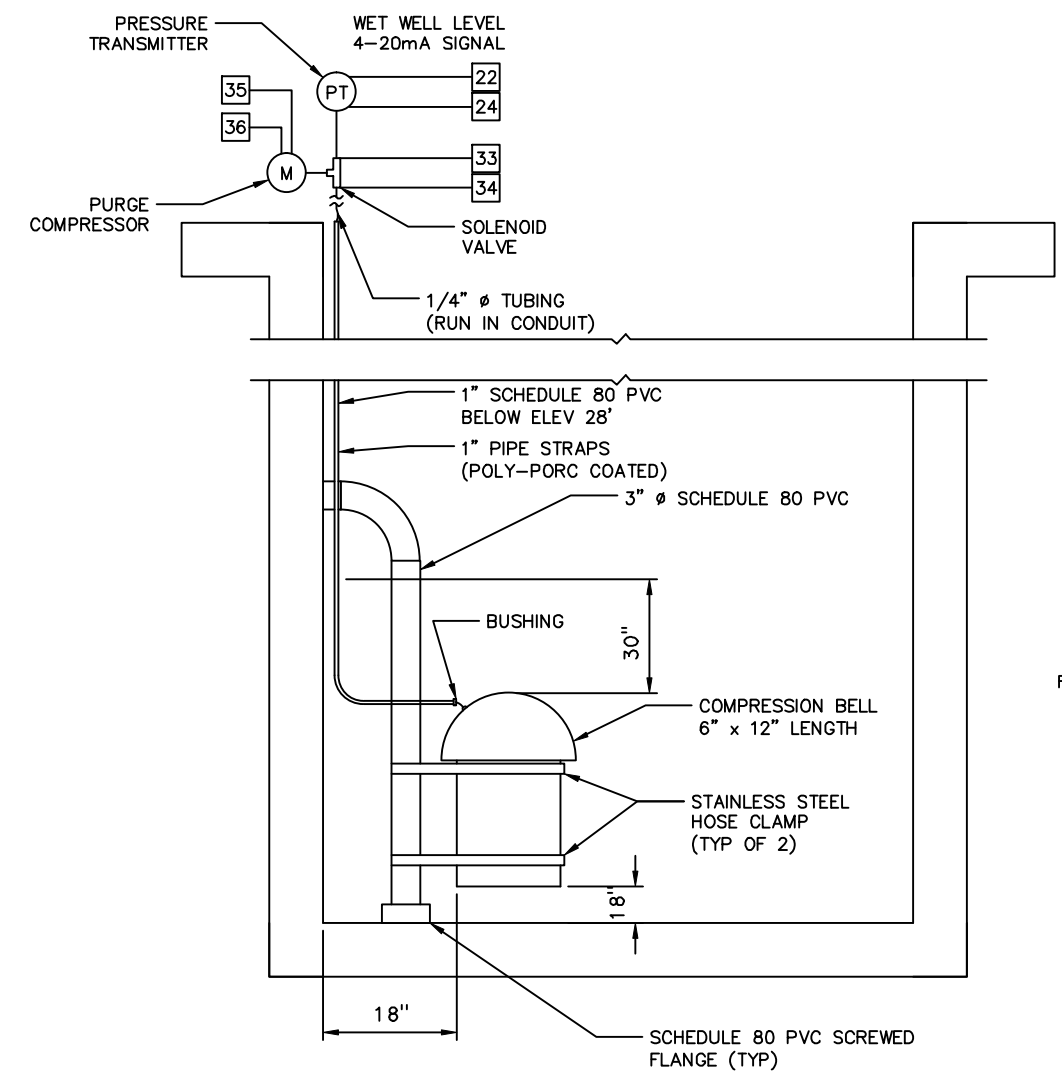
INDICATING LIGHT ENCLOSURE
NOT TO SCALE



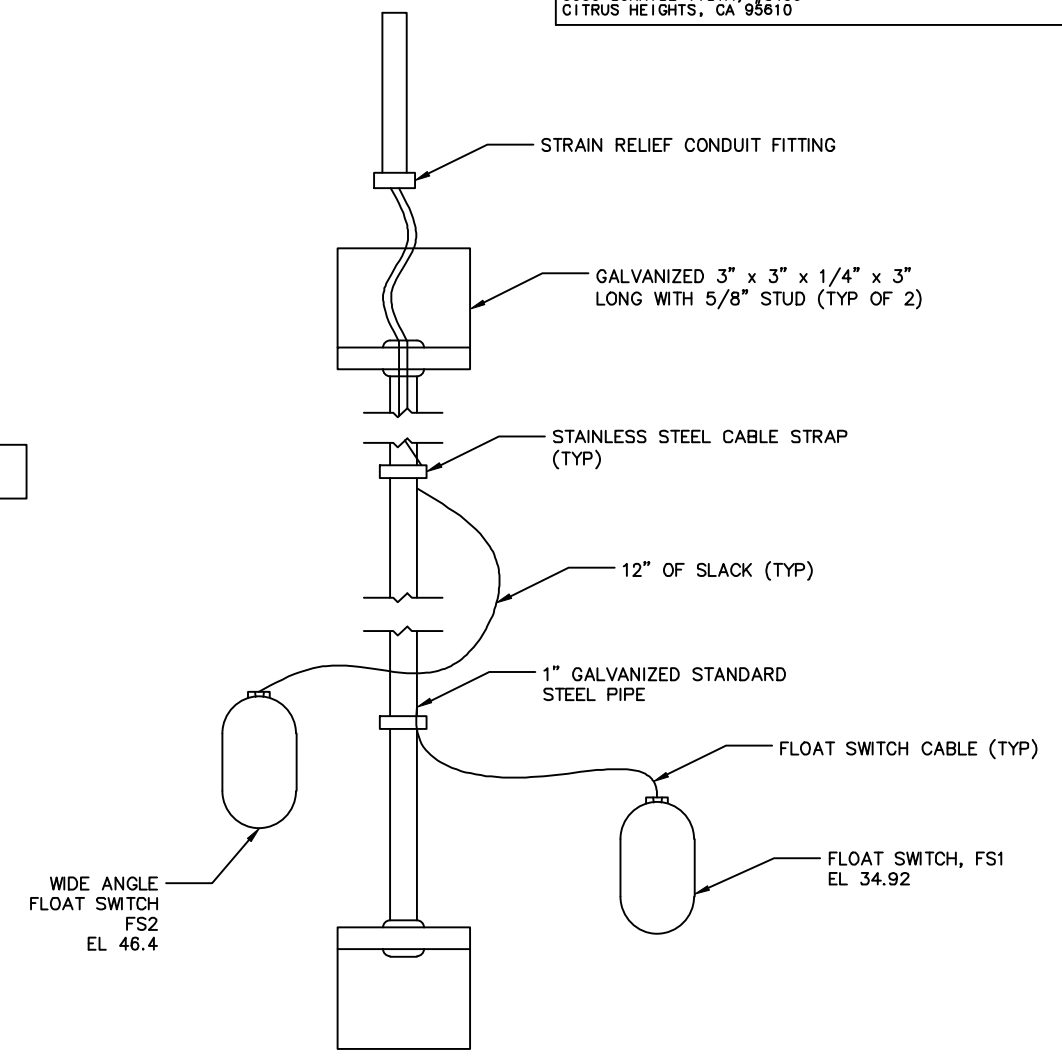
NOTES:

- PURGE COMPRESSOR TUBE CONDUIT INSTALLATION DIFFERS SLIGHTLY
- ALL EQUIPMENT AND DEVICES WITHIN WET PIT SHALL BE INSTALLED TO MEET THE PROVISIONS FOR CLASS 1, DIVISION 2 LOCATION PER NEC

CONDUIT ENTRY/PROFILE DETAIL
NOT TO SCALE



PNEUMATIC DIAGRAM
NOT TO SCALE



FLOAT SWITCH MOUNTING DETAIL
NOT TO SCALE

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY M. BREGAR	CHECKED J. GUILLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. BREGAR PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT ELECTRICAL DETAILS 1	EE-9
SIGN OFF DATE	DETAILS BY M. BREGAR	CHECKED J. GUILLORY	LAYOUT BY M. BREGAR	CHECKED J. GUILLORY	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	R22.56		
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY M. BREGAR	CHECKED J. GUILLORY	SPECIFICATIONS BY M. BREGAR	PLANS AND SPECS COMPARED BY M. BREGAR	UNIT: 1457	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES: 10/21/11, 12/24/12, 3/27/12, 5/01/12	SHEET 43	OF 47

FILE => N:\SAB041001\Cadd\Elec\EE-9 DETAIL1.dwg

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USERNAME => Tpkim DATE PLOTTED => 5-May-12

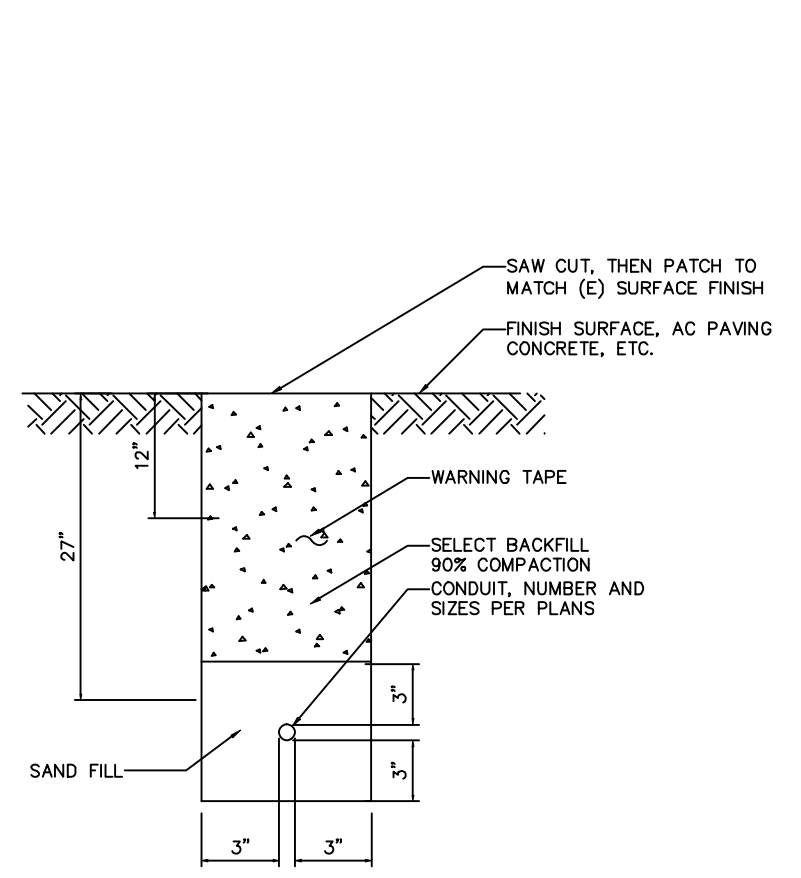
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5/1/12
 REGISTERED ELECTRICAL ENGINEER
 DATE
 9/04/12
 PLANS APPROVAL DATE
 MICHAEL F. BREGAR
 No. 10246
 Exp. 9/30/12
 ELECTRICAL
 STATE OF CALIFORNIA

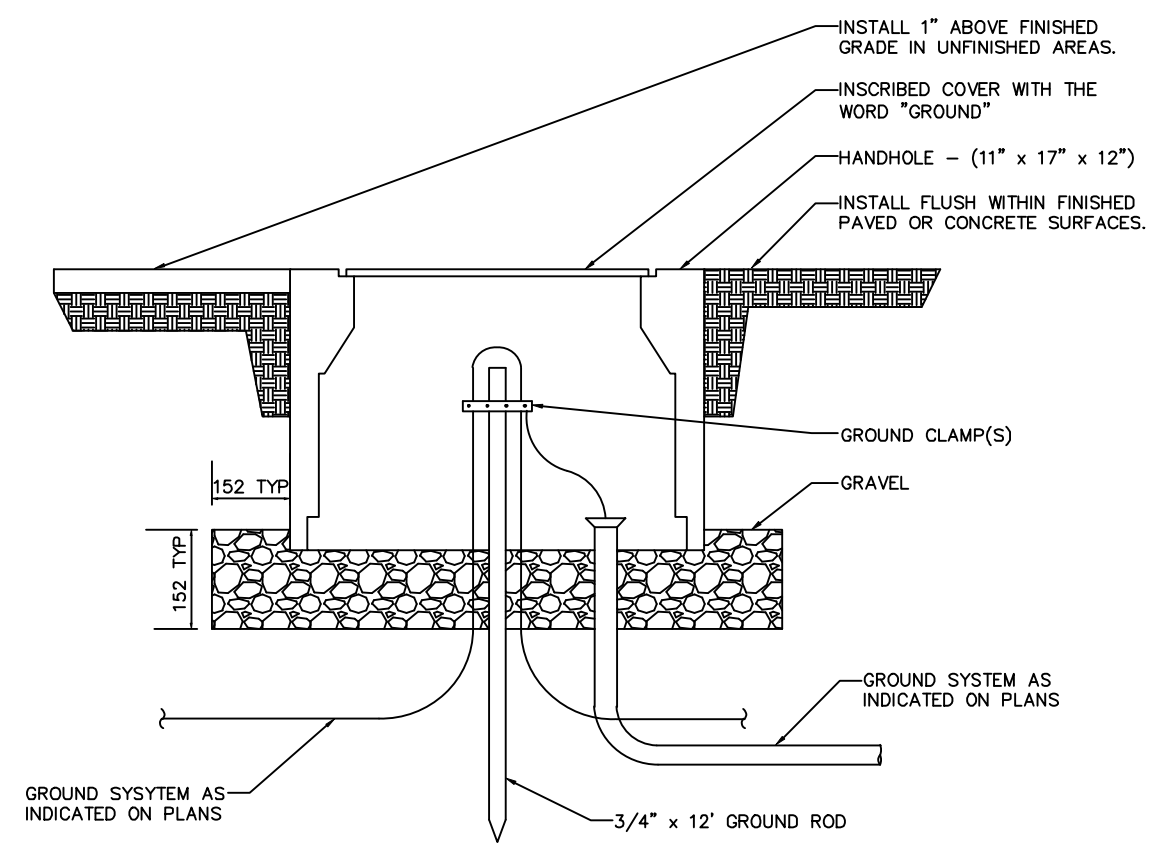
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

EETS, INC.
 6060 SUNRISE VISTA, #3450
 CITRUS HEIGHTS, CA 95610

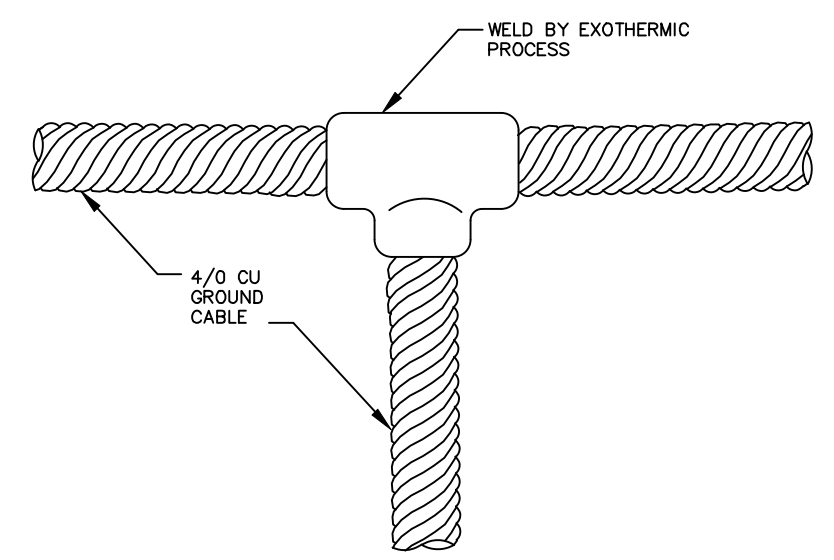


CONDUIT TRENCH
NOT TO SCALE



NOTE:
SEE ONE LINE DIAGRAM AND SITE PLAN FOR CONDUCTOR SIZE AND GROUNDING ARRANGEMENT.

GROUND WELL
NOT TO SCALE



GROUND GRID TEE CONNECTION
NOT TO SCALE

NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

DESIGN OVERSIGHT	DESIGN BY M. BREGAR	CHECKED J. GULLORY	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. BREGAR PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		EE-10	
SIGN OFF DATE	DETAILS BY M. BREGAR	CHECKED J. GULLORY	LAYOUT BY M. BREGAR	CHECKED J. GULLORY	PLANS AND SPECS COMPARED BY M. BREGAR	PROJECT ENGINEER	POST MILES R22.56	ELECTRICAL DETAILS 2			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)					ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 44 OF 47

FILE => N:\SAB041001\Cadd\Elec\EE-10 DETAIL2.dwg

USERNAME => Tpkim DATE PLOTTED => 5-May-12 TIME PLOTTED => 16:02

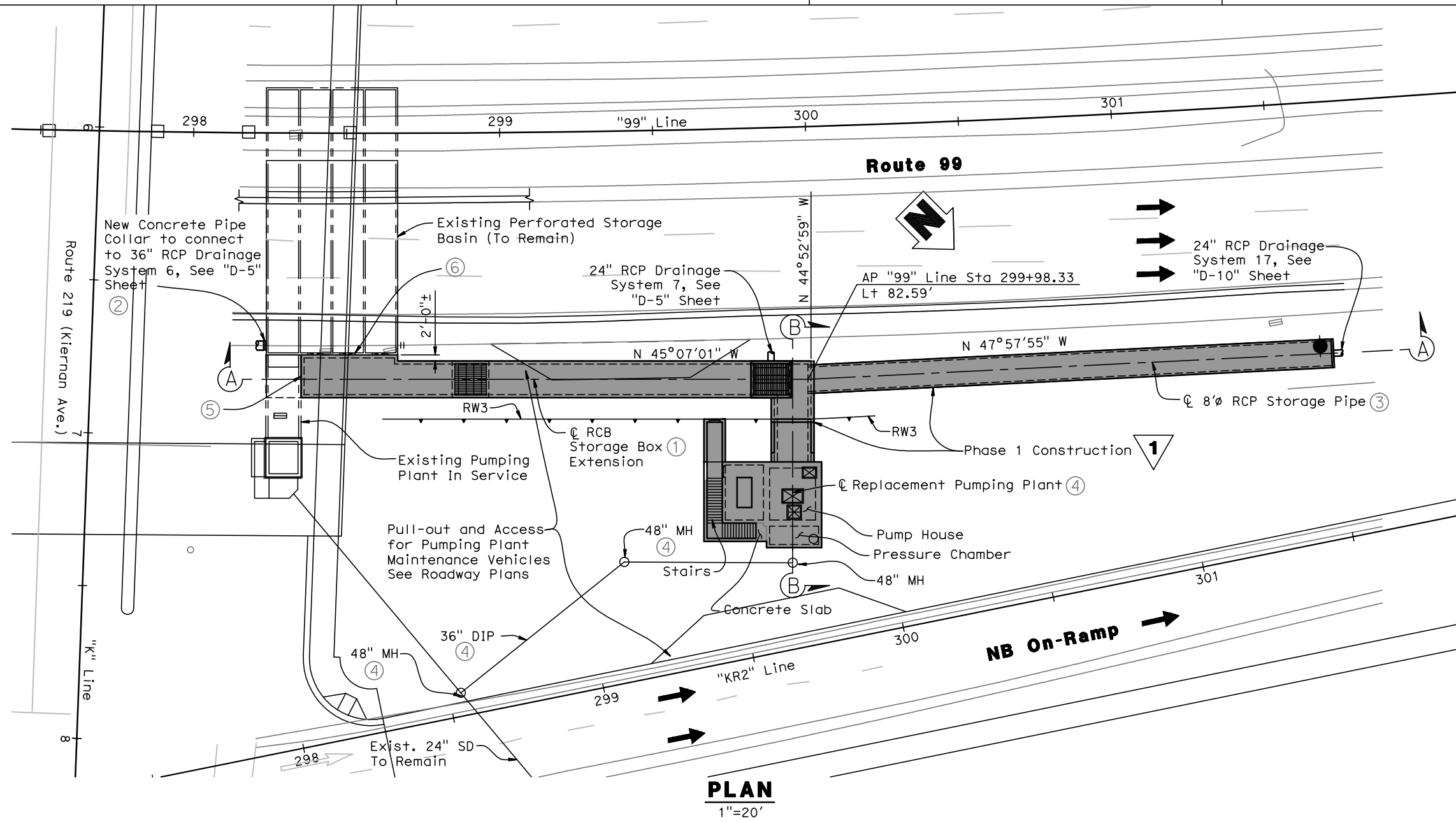
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	421	423

REGISTERED CIVIL ENGINEER DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE
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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



PLAN
1"=20'

Phase 1:

- ① Construct storage box extension,
- ② Construct new concrete pipe collar, See "DD-3" Sheet.
- ③ Construct 8' DIA RCP storage pipe
- ④ Construct Replacement Pumping Plant and Outlet Piping.
- ⑤ Provide temporary bulkhead or concrete knockout wall to seal temporary end of box culvert. Contractor to submit plan to engineer for approval.

Construct RCB storage box up to existing culvert. Provide reinforcement couplers at every longitudinal bar for expansion in Phase 3 construction. Provide Lenton Standard Couplers A2, or equal.

- ⑥ Provide bulkheads and sandbags as required to sawcut new 5'X5' opening in existing basin wall. Provide temporary bulkhead at 5'X5' opening if full capacity of existing storage basin is required prior to putting replacement plant into service in Phase 2.

Notes:

1. Existing pumping plant is to remain in service during Phase 1.
2. Construct all drainage systems necessary to maintain operation of the pumping plant through out construction. At a minimum, construct drainage systems 7, 16, 21, 27, and portions of 2 & 6 to maintain storm flow into the existing box during Phase 2 and Phase 3.

Legend:

- Indicates existing structure
- ➔ Indicates direction of travel

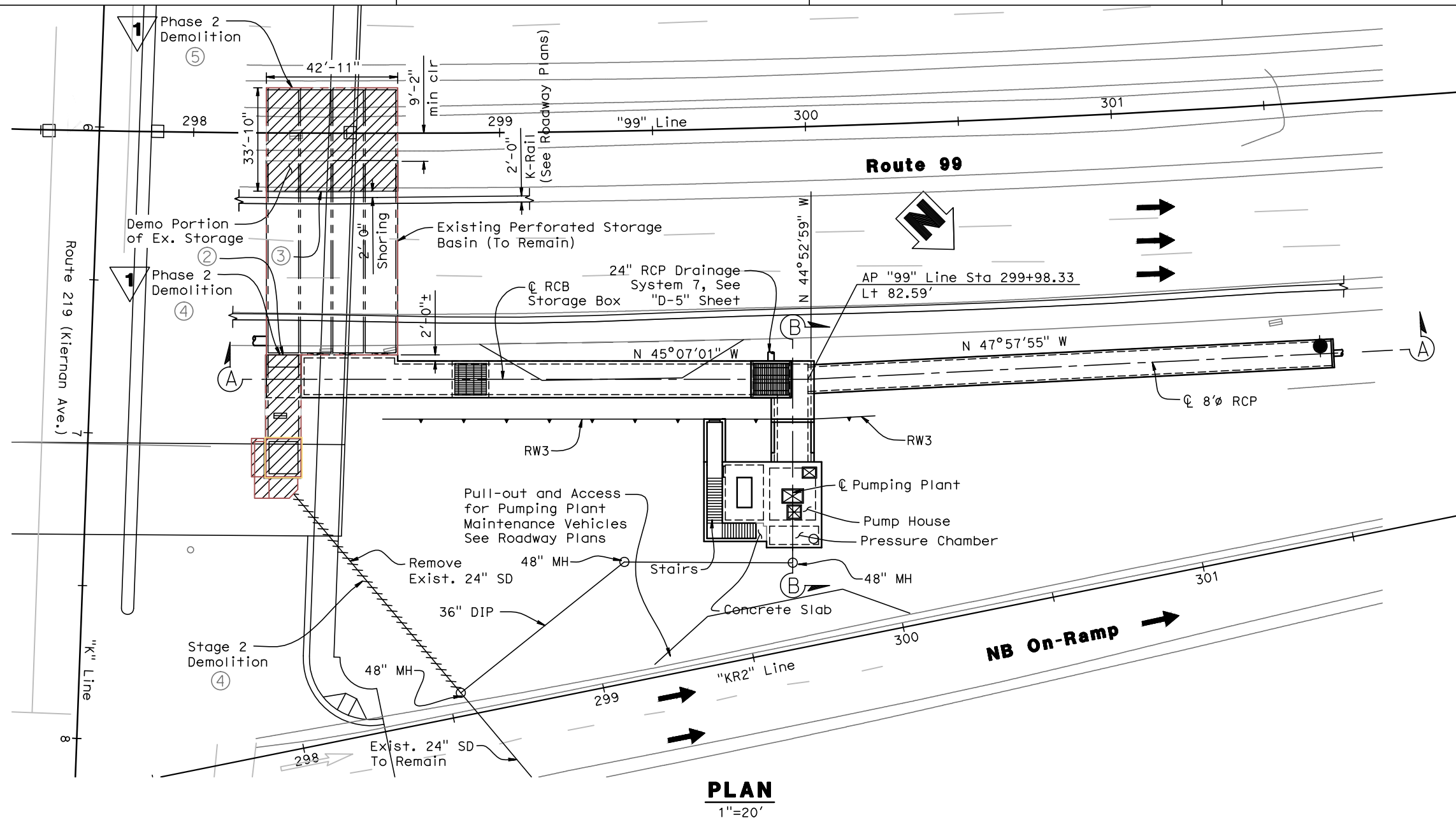
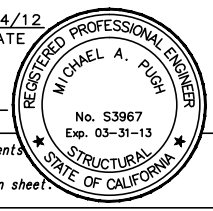
NOTE:
THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL

1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT PHASING PLAN No. 1		PSC-1
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	CONTRACT NO.: 10-0L3304	POST MILES R22.56	REVISION DATES	SHEET 45	OF 47	
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10) ORIGINAL SCALE IN INCHES FOR REDUCED PLANS 0 1 2 3 UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304 DISREGARD PRINTS BEARING EARLIER REVISION DATES 2/24/12 3/28/12 5/1/12 9/7/12 FILE => N:\SAB041001\Cadd\Structural\20120719-DWG_Updates\10-380163W-a-psc01.dwg										

DATE PLOTTED => 9-Sep-12 USERNAME => Dagne, Dantel TIME PLOTTED => 16:22

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
10	Sta	99,219	22.0/23.0, 0.0/0.5	422	423
REGISTERED CIVIL ENGINEER			9/4/12	DATE	
PLANS APPROVAL DATE					
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DEPARTMENT OF PUBLIC WORKS STANISLAUS COUNTY 1716 MORGAN ROAD MODESTO, CA 95358					
NOLTE ASSOCIATES, INC. 2495 NATOMAS PARK DRIVE, FOURTH FLOOR SACRAMENTO, CA 95833					



- Phase 2:**
- ① Put replacement pumping plant in service.
 - ② Provide temporary sandbag dam/bulkhead at opening to existing pumping plant. Contractor to submit proposed plan.
 - ③ Provide temporary sandbag dam/bulkhead at each bay. Contractor to submit proposed plan.
 - ④ Demolish existing pumping plant and piping. See "ST-20" Sheet.
 - ⑤ Demolish 33'-10" section of existing storage box in SR99 median. See "ST-20" Sheet.

Legend:

--- Indicates existing structure

➔ Indicates direction of travel

NOTE:
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1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT PHASING PLAN No. 2		PSC-2
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED			POST MILES R22.56			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	PLANS AND SPECS COMPARED	UNIT: 1457	PROJECT NUMBER & PHASE: 1000000100	CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 46 OF 47

TIME PLOTTED => 15:58 DATE PLOTTED => 9-Sep-12 USERNAME => Gagne, Dantel

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No	TOTAL SHEETS
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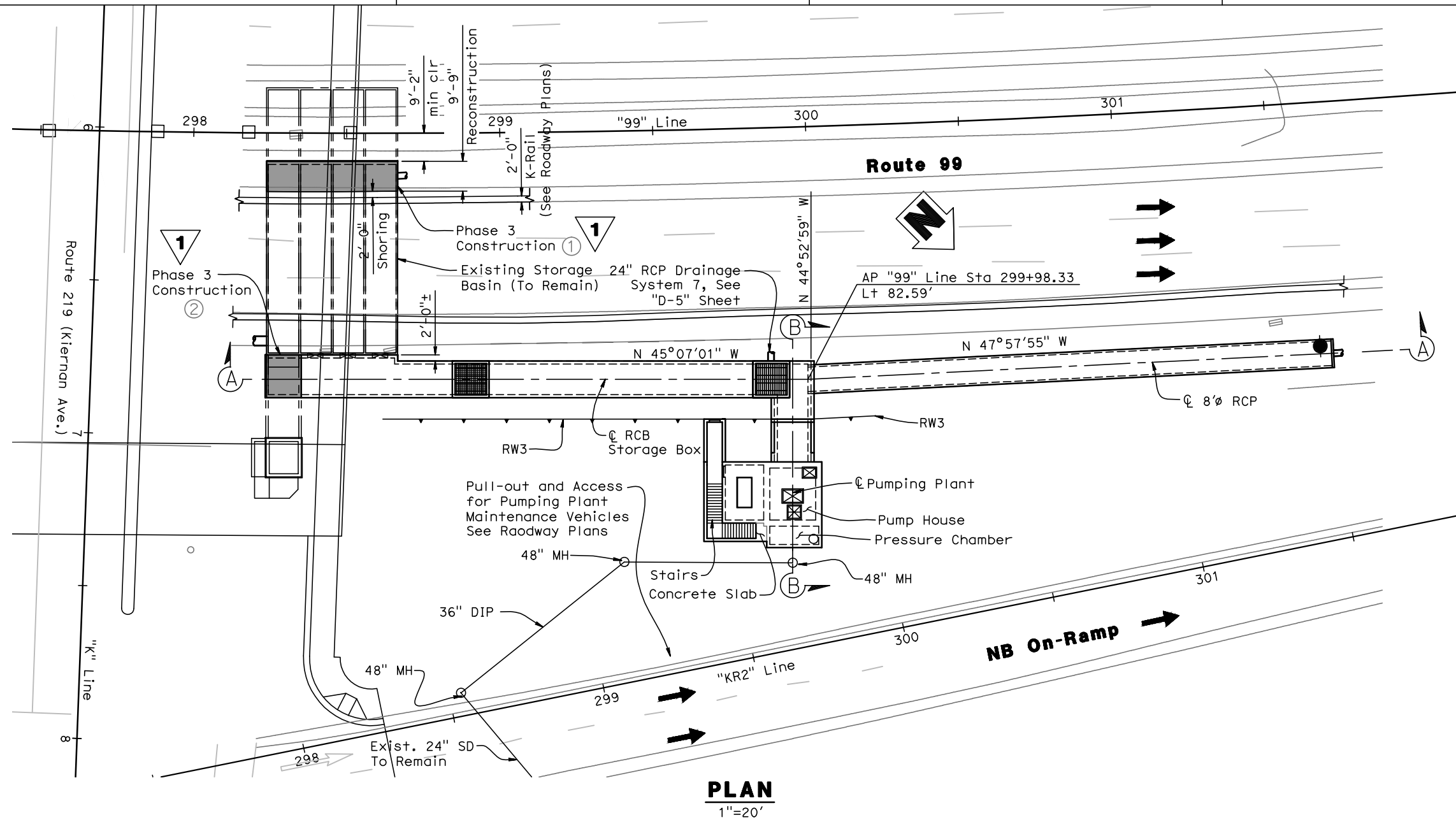
REGISTERED CIVIL ENGINEER *[Signature]* DATE 9/4/12
 REGISTERED PROFESSIONAL ENGINEER
 MICHAEL A. PUGH
 No. S3967
 Exp. 03-31-13
 STRUCTURAL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE _____

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DEPARTMENT OF PUBLIC WORKS
 STANISLAUS COUNTY
 1716 MORGAN ROAD
 MODESTO, CA 95358

NOLTE ASSOCIATES, INC.
 2495 NATOMAS PARK DRIVE, FOURTH FLOOR
 SACRAMENTO, CA 95833



- Phase 3:**
- Reconstruct 9'-9" section of existing storage box. See Improvement Plan on "ST-20" Sheet. Concurrently install 24" RCP Drainage System 2, See "D-5" Sheet.
 - Reconstruct closure portion of box extension.

Legend:

--- Indicates existing structure

➔ Indicates direction of travel

NOTE:
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1 REVISED PER ADDENDUM No. 1 DATED SEPTEMBER 25, 2012

DESIGN OVERSIGHT	DESIGN BY D. GAGNE	CHECKED C. KRIER	LOAD & RESISTANCE FACTOR DESIGN	LIVE LOADING: HL93 W/"LOW-BOY" AND PERMIT DESIGN VEHICLE	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	M. PUGH PROJECT ENGINEER	BRIDGE NO. 38-0163W	219/99 SEPARATION PUMPING PLANT		PSC-3
SIGN OFF DATE	DETAILS BY S. CARRICK	CHECKED D. GAGNE	LAYOUT BY	CHECKED	PLANS AND SPECS COMPARED	R22.56	PHASING PLAN No. 3			
DESIGN GENERAL PLAN SHEET (ENGLISH) (REV.7/16/10)	QUANTITIES BY S. SMITH	CHECKED K. ALACON	SPECIFICATIONS BY	ORIGINAL SCALE IN INCHES FOR REDUCED PLANS	0 1 2 3	UNIT: 1457 PROJECT NUMBER & PHASE: 1000000100 CONTRACT NO.: 10-0L3304	DISREGARD PRINTS BEARING EARLIER REVISION DATES	REVISION DATES	SHEET 47	OF 47

DATE PLOTTED => 9-Sep-12 TIME PLOTTED => 16:29 USERNAME => gagne, Denise